



City of Hamilton

CITY COUNCIL REVISED

22-016

Friday, July 8, 2022, 9:30 A.M.

Council Chambers

Hamilton City Hall

71 Main Street West

Call to Order

1. APPROVAL OF AGENDA

(Added Items, if applicable, will be noted with *)

2. DECLARATIONS OF INTEREST

3. CEREMONIAL ACTIVITIES

3.1. The Lifesaving Society Rescue Award of Merit Presentations

4. APPROVAL OF MINUTES OF PREVIOUS MEETING

4.1. June 22, 2022

5. COMMUNICATIONS

5.1. Notice of the Passing of Education Development Charges Amending By-law, Hamilton-Wentworth Catholic District School Board.

Recommendation: Be received.

- 5.2. Correspondence from the Municipality of Brighton requesting support for their resolution requesting that the Minister of the Solicitor General, the Commissioner of the Ontario Provincial Police and the Premier's Office be requested to make the necessary

changes to the Amber Alert system to create a new alert called the Draven Alert, which will protect vulnerable children who have not yet been abducted but are at high risk of danger, injury or death and alert the public that they are missing.

Recommendation: Be received.

- *5.3. Correspondence from Alma Sarai, Associate Producer / Summer Shakespeare Artistic Director, Tottering Biped Theatre respecting their performance of 'A Midsummer Night's Dream' for 3 weeks in August.

Recommendation: Be received.

- *5.4. Correspondence from the Ministry of the Attorney General in response to the Mayor's letter respecting support for Bill C-229 and a request for provincial legislation regarding hate speech and symbols.

Recommendation: Be received.

- *5.5. Correspondence from the Municipality of Tweed requesting support for their resolution requesting that the Ministry of Agriculture, Food and Rural Affairs review the administration fee provided to municipalities for the administration of the Ontario Wildlife Damage Compensation Program.

Recommendation: Be received.

- *5.6. Correspondence from Sarah Knoll, Associate - Senior Planner/Hamilton Manager and Stuart Hastings, Planner of GSP Group respecting 510 Centennial Parkway North, Hamilton.

Recommendation: Be received and referred to the consideration of Item 6 of Planning Committee Report 22-011.

- *5.7. Correspondence from Arlene Whatmough respecting the conversion of Main and King Street to Two Way.

Recommendation: Be received.

6. COMMITTEE REPORTS

- 6.1. General Issues Committee Report 22-013 - June 23, 2022
- 6.2. General Issues Committee Report 22-014 - July 4, 2022
- 6.3. Planning Committee Report 22-011 - July 5, 2022

- 6.4. Board of Health Report 22-007 - July 6, 2022
- 6.5. Public Works Committee Report 22-011 - July 6, 2022
- 6.6. Audit, Finance and Administration Committee Report 22-013 - July 7, 2022
- 6.7. Emergency and Community Services Committee Report 22-011 - July 7, 2022

7. MOTIONS

8. NOTICES OF MOTIONS

- *8.1. Amendment to Item 11 of the Audit, Finance & Administration Committee Report 22-010, respecting the Grants Sub-Committee Report 22-002, which was approved by Council on May 25, 2022

9. STATEMENT BY MEMBERS (non-debatable)

10. COUNCIL COMMUNICATION UPDATES

- 10.1. June 17, 2022 to June 30, 2022

11. PRIVATE AND CONFIDENTIAL

12. BY-LAWS AND CONFIRMING BY-LAW

- 12.1. 165

To Adopt Official Plan Amendment No. 35 to the Rural Hamilton Official Plan Respecting 5020 Tyneside Road (Glanbrook)

Ward: 11

- 12.2. 166

To Amend Zoning By-law No. 05-200, Respecting Lands Located at 5015 and 5020 Tyneside Road, Glanbrook

Ward: 11

ZAC-21-032/RHOPA-21-015

12.3. 167

To Amend By-law No. 01-218, as amended, Being a By-law to Regulate On-Street Parking

Schedule 6 (Time Limit Parking)

Schedule 8 (No Parking Zones)

Schedule 12 (Permit Parking Zones)

Schedule 13 (No Stopping Zones)

Schedule 14 (Wheelchair Loading Zones)

Schedule 20 (School Bus Loading Zones)

Ward: 1, 2, 3, 4, 10, 12, 13

12.4. 168

To Establish City of Hamilton Land Described as Part of Lot 55, Concession 2 in the Geographic Township of Ancaster, designated as Parts 5 and 6 on Plan 62R-7673 as Part of Scenic Drive

Ward: 12

12.5. 169

To Establish City of Hamilton Land Described as Part of Lot A and Part of the Block Between Guise Street East, James Street North, Hughson Street North, and Burlington Street East, on James Hughson Survey (unregistered), as in Instrument Number AB214414 as Part of Burlington Street East

Ward: 2

12.6. 170

To Establish City of Hamilton Land Described as Part of Lot 55, Concession 1 in the Geographic Township of Ancaster, designated as Part 3 on Plan 62R-4056 as Part of Ewen Road

Ward: 1

12.7. 171

To Establish City of Hamilton Land Described as Part of Lot 40, Concession 2 in the Geographic Township of Ancaster, designated as Parts 12 and 13 on Plan 62R-21873 as Part of Jerseyville Road

Ward: 12

- *12.8. 172
To Amend By-law No. 01-215, Being a By-law to Regulate Traffic
Schedule 2 (Speed Limits)
Schedule 31 (Designated Areas – Reduced Speed Limit – 40km/h Neighbourhoods)
Ward: City Wide
- *12.9. 173
To Amend By-law No. 01-215, Being a By-law to Regulate Traffic
Schedule 2 (Speed Limits)
Schedule 9 (No Right Turn on Red)
Ward: 1, 2, 3, 13
- *12.10. 174
To Establish City of Hamilton Land Described as Part of Block 111 on Plan 62M-1073, designated as Parts 1 and 2 on Plan 62R-21881 as Part of Watershore Drive
Ward: 10
- *12.11. 175
To Amend Zoning By-law No. 6593 (Hamilton) Respecting Lands Located at 1540 Upper Wentworth Street (Hamilton)
ZAC-21-023
Ward: 7
- *12.12. 176
To Amend Zoning By-law No. 05-200 with Respect to Lands Located at 60 Caledon Avenue, Hamilton
ZAC-21-025
Ward: 8
- *12.13. 177
To Repeal By-law No. 87-167 and to Amend Zoning By-law No. 6593 Respecting Lands Located at 221-223 Charlton Avenue East and 200 Forest Avenue, Hamilton
ZAC-21-020
Ward: 2

*12.14. 178

To Adopt Official Plan Amendment No. 169 to the Urban Hamilton Official Plan Respecting 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue (Hamilton)

Ward: 4

*12.15. 179

Respecting Removal of Part Lot Control, Lot 4, Registered Plan No. 62M-1229, Municipally known as 73 Aeropark Boulevard, Hamilton

PLC-22-009

Ward: 11

12.16. 180

To Confirm the Proceedings of City Council

13. ADJOURNMENT



CITY COUNCIL MINUTES 22-015

9:30 a.m.
June 22, 2022
Council Chamber
Hamilton City Hall
71 Main Street West

Present: Mayor F. Eisenberger
Councillors M. Wilson, N. Nann, J.P. Danko, B. Clark, M. Pearson, A. VanderBeek, E. Pauls, L. Ferguson, B. Johnson (Deputy Mayor), J. Farr, J. Partridge, T. Jackson, T. Whitehead and R. Powers.

Absent: Councillor S. Merulla - Personal

Mayor F. Eisenberger called the meeting to order and recognized that Council is meeting on the traditional territories of the Erie, Neutral, HuronWendat, Haudenosaunee and Mississaugas. This land is covered by the Dish with One Spoon Wampum Belt Covenant, which was an agreement between the Haudenosaunee and Anishinaabek to share and care for the resources around the Great Lakes. It was further acknowledged that this land is covered by the Between the Lakes Purchase, 1792, between the Crown and the Mississaugas of the Credit First Nation. The City of Hamilton is home to many Indigenous people from across Turtle Island (North America) and it was recognized that we must do more to learn about the rich history of this land so that we can better understand our roles as residents, neighbours, partners and caretakers.

APPROVAL OF THE AGENDA

The Clerk advised of the following changes to the agenda:

4. COMMUNICATIONS

- 4.6 Correspondence from Alex Bishop on behalf of Harlo Capital and DCR Holdings Inc. requesting a deferral of Item 8 of Audit, Finance and Administration Report 22-012, respecting Development Charge Demolition Credit Extension Request for Hamilton Central Business Park (Studebaker Place and Ferrie Street, formerly 440 Victoria Avenue North) (FCS17067(a)) (City Wide) to the July 8, 2022 Council meeting, so that they can have an opportunity to provide Council with information that they believe is necessary and important to allow Council to properly assess this in order to make its decision.

Recommendation: Be received and referred to the consideration of Item 8 of Audit, Finance and Administration Committee Report 22-012.

- 4.7 Correspondence from David Bronskill, Goodmans LLP respecting Community Benefits Charge Strategy, 41-61 Wilson Street and 97, 99 and 117 John Street North.

Recommendation: Be received and referred to the consideration of Item 6 of Audit, Finance and Administration Committee Report 22-012.

- 4.8 Correspondence from Dawn Danko, Chair of the Board of Trustees, HWDSB respecting the HWDSB Board of Trustees' opposition to the proposed Cultural Heritage Landscape designation of the Ancaster High School lands.

Recommendation: Be received and referred to the Item 3(a) of Planning Committee Report 22-010.

7. NOTICES OF MOTION

- 7.1 Naming of the Great Hall of First Ontario Concert Hall after Boris Brott
- 7.2 Donation from The Patrick J. McNally Charitable Foundation for St Mark's Capital Project

11. BY-LAWS AND CONFIRMING BY-LAW

- 152 To Amend By-law No. 01-215, Being a By-law to Regulate Traffic
 Schedule 2 (Speed Limits)
 Schedule 3 (Flashing School Zones – Reduced Speed Limit)
 Schedule 5 (Stop Sign Locations)
 Schedule 9 (No Right Turn on Red)
 Schedule 20 (Combined Foot & Bicycle Path)
 Schedule 29 (Weight Restrictions on Bridges)
 Schedule 31 (Designated Area – Reduced Speed – 40km/h Neighbourhoods)
 Schedule 34 (Community Safety Zones)
 Ward: 1, 2, 3, 4, 5, 6, 8, 9, 10, 11, 12, 13, 14, 15 – **WITHDRAWN**
- 158 Respecting Removal of Part Lot Control, Lot 4, Registered Plan No. 62M-1229, municipally known as 73 Aeropark Boulevard, Hamilton
 Ward: 11
 PLC-22-009 – **WITHDRAWN**

With the remaining Bills being renumbered accordingly.

(Clark/Pauls)

That the agenda for the June 22, 2022 meeting of Council be approved, as amended.

Result: Motion CARRIED by a vote of 13 to 0, as follows:

- YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr

YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 NOT PRESENT - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

DECLARATIONS OF INTEREST

Councillor B. Clark declared a disqualifying interest with respect to Item 4 of Public Works Committee Report 22-010, Roxborough Park Redevelopment, as his son has a retail business interest with the principal of the planning consultant.

Councillor J.P. Danko declared a non-disqualifying interest with respect to Item 3(a) of Planning Committee Report 22-010, Hamilton Municipal Heritage Committee Report 22-006, Cultural Heritage Assessment for 374 Jerseyville Road West, Ancaster (Ancaster High School) PED22113 (Ward 12), as his spouse is employed by the Hamilton-Wentworth District School Board and a member of the HWDSB Board of Trustees.

APPROVAL OF MINUTES OF PREVIOUS MEETING

3. June 8, 2022 (Item 3.1) and June 16, 2022 (Item 3.2)

(Ferguson/Danko)

That the Minutes of the June 8, 2022 and June 16, 2022 meetings of Council be approved, as presented.

Result: Motion CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson

NOT PRESENT - Ward 10 Councillor Maria Pearson

YES - Ward 9 Councillor Brad Clark

COMMUNICATIONS

(Johnson/Wilson)

That Council Communications 4.1 to 4.8 be approved, as presented, as follows:

- 4.1 Correspondence from the Municipal of Chatham-Kent requesting support for their resolution respecting Retirement Home Funding.

Recommendation: Be received.

- 4.2 Correspondence from the Town of Blue Mountains requesting support for their resolution respecting support for the inclusion of the mailing addresses of voters on the voter's lists provided to candidates.

Recommendation: Be received.

- 4.3 Correspondence from the Town of Aurora requesting support for their resolution calling on the House of Commons to support Member of Parliament Anji Dhillon's Private Member's Bill C-233, that will raise the level of education on domestic violence and coercive control for federally appointed Judges.

Recommendation: Be received.

- 4.4 Correspondence from the Municipal Engineers Association stressing the importance of the role municipal engineers play in the successful operation of cities, counties, towns and townships across Ontario.

Recommendation: Be received.

- 4.5 Notice of Proposed Amendment to Education Development Charges By-law Hamilton-Wentworth Catholic District School Board.

Recommendation: Be received.

- 4.6 Correspondence from Alex Bishop on behalf of Harlo Capital and DCR Holdings Inc. requesting a deferral of Item 8 of Audit, Finance and Administration Report 22-012, respecting Development Charge Demolition Credit Extension Request for Hamilton Central Business Park (Studebaker Place and Ferrie Street, formerly 440 Victoria Avenue North) (FCS17067(a)) (City Wide) to the July 8, 2022 Council meeting, so that they can have an opportunity to provide Council with information that they believe is necessary and important to allow Council to properly assess this in order to make its decision.

Recommendation: Be received and referred to the consideration of Item 8 of Audit, Finance and Administration Committee Report 22-012.

- 4.7 Correspondence from David Bronskill, Goodmans LLP respecting Community Benefits Charge Strategy, 41-61 Wilson Street and 97, 99 and 117 John Street North.

Recommendation: Be received and referred to the consideration of Item 6 of Audit, Finance and Administration Committee Report 22-012.

- 4.8 Correspondence from Dawn Danko, Chair of the Board of Trustees, HWDSB respecting the HWDSB Board of Trustees' opposition to the proposed Cultural Heritage Landscape designation of the Ancaster High School lands.

Recommendation: Be received and referred to the Item 3(a) of Planning Committee Report 22-010.

Result: Motion on the Communication Items, as presented, CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 NOT PRESENT - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

(Johnson/Partridge)

That Council move into Committee of the Whole for consideration of the Committee Reports.

Result: Motion CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek

YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 NOT PRESENT - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

INTEGRITY COMMISSIONER'S REPORT – JUNE 10, 2022
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(Powers/Clark)

- (a) That the City of Hamilton Integrity Commissioner's Report Regarding Complaints Against Councillor Terry Whitehead, June 10, 2022 , be received; and
- (b) That the actions contained within the City of Hamilton Integrity Commissioner's Report Regarding Complaints Against Councillor Terry Whitehead, June 10, 2022, be supported.

(Wilson/Nann)

That the motion respecting the City of Hamilton Integrity Commissioner's Report Regarding Complaints Against Councillor Terry Whitehead, June 10, 2022 be **amended**, by adding the following sub-sections:

- (c) ***That staff be directed to develop a plan of action to the fulfill our obligations under occupational health and safety act of Ontario, as it relates to the work environment of City Hall with respect to the issues raised in this Integrity Commissioners Report; and***
- (d) ***That Councillor Whitehead inform staff and members of council when he plans to attend City Hall, until a report back from staff in recommendation (c) is presented to Council.***

As per Section 2.1, 1. of the *Municipal Conflict of Interest Act*, Councillor T. Whitehead was prohibited from voting on the motions respecting to the City of Hamilton Integrity Commissioner's Report Regarding Complaints Against Councillor Terry Whitehead, June 10, 2022.

Result: Amendment to the Motion respecting the Integrity Commissioner's Report of June 10, 2022, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 PROHIBITED - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson

YES - Ward 10 Councillor Maria Pearson

YES - Ward 9 Councillor Brad Clark

Main Motion, **as Amended** to read as follows:

- (a) That the City of Hamilton Integrity Commissioner's Report Regarding Complaints Against Councillor Terry Whitehead, June 10, 2022 , be received;
- (b) That the actions contained within the City of Hamilton Integrity Commissioner's Report Regarding Complaints Against Councillor Terry Whitehead, June 10, 2022, be supported;
- (c) ***That staff be directed to develop a plan of action to the fulfill our obligations under occupational health and safety act of Ontario, as it relates to the work environment of City Hall with respect to the issues raised in this Integrity Commissioners Report; and***
- (d) ***That Councillor Whitehead inform staff and members of council when he plans to attend City Hall, until a report back from staff in recommendation (c) is presented to Council.***

Result: Main Motion, as Amended respecting the Integrity Commissioner's Report of June 10, 2022, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson

YES - Ward 2 Councillor Jason Farr

YES - Ward 3 Councillor Nrinder Nann

NOT PRESENT - Ward 4 Councillor Sam Merulla

YES - Ward 5 Councillor Russ Powers

YES - Ward 6 Councillor Tom Jackson

YES - Ward 7 Councillor Esther Pauls

YES - Ward 8 Councillor John-Paul Danko

YES - Mayor Fred Eisenberger

YES - Ward 15 Councillor Judi Partridge

PROHIBITED - Ward 14 Councillor Terry Whitehead

YES - Ward 13 Councillor Arlene VanderBeek

YES - Ward 12 Councillor Lloyd Ferguson

YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson

YES - Ward 10 Councillor Maria Pearson

YES - Ward 9 Councillor Brad Clark

BOARD OF HEALTH REPORT 22-006

(Wilson/Partridge)

That Board of Health Report 22-006, being the meeting held on Monday, June 13, 2022, be received and the recommendations contained therein be approved.

Result: Motion on the Board of Health Report 22-006, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson

YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

PUBLIC WORKS COMMITTEE REPORT 22-010

(Nann/Powers)

That Public Works Committee Report 22-010, being the meeting held on Monday, June 13, 2022, be received and the recommendations contained therein be approved.

Due to a declared conflict, Item 4 was voted on separately, as follows:

4. Roxborough Park Redevelopment (Item 9.1)

That staff be directed to continue to work with the Developers, Nick Carnicelli, Sergio Manchia and David Horwood, Roxborough Park Inc., in attempt to resolve their concerns respecting the Roxborough Park Redevelopment and to work with their staff and the Ministry of Environment, Conservation and Parks and report back the appropriate Committee.

Result: Motion on Item 4 of the Public Works Committee Report 22-010, CARRIED by a vote of 12 to 1, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 NO - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 CONFLICT - Ward 9 Councillor Brad Clark

Result: Motion on the balance of the Public Works Committee Report 22-010, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

PLANNING COMMITTEE REPORT 22-010

(Johnson/Ferguson)

That Planning Committee Report 22-010, being the meeting held on Tuesday, June 14, 2022, be received and the recommendations contained therein be approved.

Upon Council's request, Item 3(a) was voted on separately, as follows:

3. Hamilton Municipal Heritage Committee Report 22-006 (Added Item 7.3)

(a) Cultural Heritage Assessment for 374 Jerseyville Road West, Ancaster (Ancaster High School) PED22113 (Ward 12) (Item 8.1)

That 374 Jerseyville Road West Ancaster and the properties in the broader area surrounding it as identified in Archaeological Services Inc.'s (ASI) Cultural Heritage Assessment, be added to the City's Cultural Heritage Landscape Inventory and mapping as part of the Official Plan review exercise.

Result: Motion on Item 3(a) of the Planning Committee Report 22-010, CARRIED by a vote of 11 to 3, as follows:

NO - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 NO - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 NO - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge

NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

(Farr/Eisenberger)

3. Hamilton Municipal Heritage Committee Report 22-006 (Added Item 7.3)

(b) Beasley Neighbourhood Inventory (PED22135) (Ward 2) (Item 8.2)

That Appendix "A" to the Planning Committee Report 22-010, **be further amended**, by adding **240-242 James Street North** to the list of properties to be removed from the Beasley Neighbourhood Inventory, as follows:

- 210 Catharine Street North
- 286 Hughson Street North
- 203-213 James Street North
- 229, 235, 241, 245 and 274 James Street North
- 217 John Street North
- 164 Mary Street
- 198 Mary Street
- 197 Wellington Street North
- **240-242 James Street North**

Result: Amendment to Item 3(b) of the Planning Committee Report 22-010, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

Main Motion, **as Amended**, to read as follows:

3. Hamilton Municipal Heritage Committee Report 22-006 (Added Item 7.3)

(b) Beasley Neighbourhood Inventory (PED22135) (Ward 2) (Item 8.2)

That staff be directed to list the properties identified in Appendix "A" to Report PED22135, as **further** amended **by including 240-242 James Street North**, and attached to Planning Committee Report 22-010 as Appendix "A", on the Municipal Heritage Register as non-designated properties that Council believes to be of cultural heritage value or interest in accordance with Section 27 of the *Ontario Heritage Act*.

Result: Main Motion, as Amended (Item 3(b)) of the Planning Committee Report 22-010, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

Result: Motion on the balance of the Planning Committee Report 22-010, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson

YES - Ward 9 Councillor Brad Clark

(Wilson/Pearson)

That Council recess until 12:40 p.m.

CARRIED

GENERAL ISSUES COMMITTEE REPORT 22-012

(Johnson/Nann)

That General Issues Committee Report 22-012, being the meeting held on Wednesday, June 15, 2022, be received and the recommendations contained therein be approved.

(Johnson/VanderBeek)

WHEREAS, the correspondence to the General Issues Committee, respecting Homeless Encampments, shown in sub-section (g) to the Advisory Committee for Persons with Disabilities Report 22-006, was to still be under review by the Advisory Committee for Persons with Disabilities and not yet ready for the consideration; and,

WHEREAS, that correspondence was inadvertently included in the Advisory Committee for Persons with Disabilities Report 22-006 for the consideration of the General Issues Committee;

THEREFORE, BE IT RESOLVED:

That sub-section (g) to Item 10 of the General Issues Committee Report 22-012, respecting the Advisory Committee for Persons with Disabilities Report 22-006, regarding correspondence to the General Issues Committee, respecting Homeless Encampments, be deleted in its entirety, with the balance of the recommendations to be re-lettered accordingly, to read as follows:

10. Advisory Committee for Persons with Disabilities Report 22-006, May 24, 2022 (Item 10.6)

(a) Resignation of Paula Kilburn from the Outreach Working Group of the Advisory Committee for Persons with Disabilities (Item 7.3)

That the resignation of Paula Kilburn from the Outreach Working Group of the Advisory Committee for Persons with Disabilities, be received.

(b) Resignation of Kim Nolan from the Transportation Working Group of the Advisory Committee for Persons with Disabilities (Item 7.4)

That the resignation of Kim Nolan from the Transportation Working Group of the Advisory Committee for Persons with Disabilities, be received.

(c) Resignation of Paula Kilburn from the Strategic Planning Working Group of the Advisory Committee for Persons with Disabilities (Item 7.5)

That the resignation of Paula Kilburn from the Strategic Planning Working Group of the Advisory Committee for Persons with Disabilities, be received.

(d) Reimbursement for the Purchase of Plants as Get-Well Gifts (Item 11.1)

- (i) That reimbursement to Aznive Mallett, in the amount of \$37.26, including HST, for the purchase of plants from House of Flowers in Ancaster as get-well gifts for two members of the Advisory Committee for Persons with Disabilities, be approved; and,
- (ii) That a plant be purchased for Patty Cameron on behalf of the Advisory Committee for Persons with Disabilities to express the Committee's condolences on her loss.

(e) Policies and Procedures to Rescue and Safely Transport Stranded Pedestrians and their Mobility Devices (Item 11.2)

That the following resolution be referred to staff for a report back to the General Issues Committee:

WHEREAS, there have been an alarming increase in encounters between pedestrians and vehicles in recent months;

WHEREAS, persons with disabilities, especially those who use mobility devices, are particularly vulnerable as pedestrians;

WHEREAS, persons who use mobility devices are susceptible to having their devices malfunction, stranding them in precarious traffic situations; and,

WHEREAS, the Advisory Committee for Persons with Disabilities (ACPD), have advised stakeholders including the Police Service, Fire Department, Paramedic Service, Hamilton Street Railway (HSR), Disabled and Aged Regional Transportation Service (DARTS), Taxicab Companies and Mobility Device Repair Contractors of the need for a coordinated rescue plan in the City of Hamilton for persons who experience an incapacitated mobility device.

THEREFORE, BE IT RESOLVED:

That staff be directed to investigate developing policies and procedures to rescue and safely transport stranded pedestrians and their mobility devices to an appropriate secure location.

(f) Advisory Committee for Persons with Disabilities Guidance to Hamilton BIA Communities on How to Make Outdoor Dining Locations Fully Accessible (Item 11.3)

WHEREAS, Council approved the Permanent Program for Temporary (seasonal) Outdoor dining Patios in the City of Hamilton, effective in 2022;

WHEREAS, the Temporary Outdoor dining Patios Program was made permanent by City Council in spite of the concern of the Advisory Committee for Persons with Disabilities (ACPD) that there were no specific provisions or obligations for outdoor dining facilities to be accessible and no prohibition to occupying pedestrian pathways; and

WHEREAS, there are opportunities to help make outdoor dining facilities accessible through consultation with the ACPD and its Accessible Outdoor Spaces and Parklands Working Group.

THEREFORE, BE IT RESOLVED:

- (a) That the Advisory Committee for Persons with Disabilities (ACPD), in collaboration with the ACPD Outreach Working Group, work with staff to develop print materials, to come back to ACPD for approval, for dissemination to Hamilton BIA communities to provide guidance on how to ensure outdoor dining is fully accessible including space, facilities, amenities and services; and,
- (b) That the Hamilton BIA communities be advised that the ACPD and its Accessible Outdoor Spaces and Parklands Working Group are available to establishments that have outdoor dining facilities should they require advice or guidance on how to make their outdoor dining locations fully accessible including their space, facilities, amenities and services.

~~(g) **Correspondence to the General Issues Committee Respecting Homeless Encampments (Item 11.4)**~~

~~***That the correspondence to the General Issues Committee from the Advisory Committee for Persons with Disabilities respecting Homeless Encampments attached as Appendix "A", be approved.***~~

(g) Invitation to Dr. Lovaye Kajiura, McMaster IMPACT Initiative, to Attend a Future Meeting of the Advisory Committee for Persons with Disabilities (Item 11.5)

WHEREAS, the McMaster IMPACT Initiative is an interdisciplinary collaboration that engages students and volunteer clients in a learning process whereby students come together to understand, appreciate and address challenges experienced by our aging population and people living with disabilities; and,

WHEREAS, Dr. Lovaye Kajiura is one of the co-leaders of the McMaster IMPACT Initiative;

THEREFORE, BE IT RESOLVED:

That Dr. Lovaye Kajiura be invited to attend a future meeting of the Advisory Committee for Persons with Disabilities to present respecting the McMaster IMPACT Initiative.

(h) Invitation to the Director of Transit, City of Hamilton, to Attend a Future Meeting of the Advisory Committee for Persons with Disabilities (Item 11.6)

WHEREAS, the Advisory Committee for Persons with Disabilities will benefit from meeting with the City of Hamilton's Director of Transit to discuss topics of interest to the Advisory Committee for Persons with Disabilities related to Transit Services;

THEREFORE, BE IT RESOLVED:

That the City of Hamilton's Director of Transit be invited to attend a future regular or special meeting of the Advisory Committee for Persons with Disabilities to discuss topics of interest to the Committee related to Transit Services.

Result: Amendment to Item 10 of the General Issues Committee Report 22-012, CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
NOT PRESENT - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

Result: Motion on the General Issues Committee Report 22-012, As Amended, CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
NOT PRESENT - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek

YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

AUDIT, FINANCE AND ADMINISTRATION COMMITTEE REPORT 22-012
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(Pearson/VanderBeek)

That Audit, Finance and Administration Committee Report 22-012, being the meeting held on Thursday, June 16, 2022, be received and the recommendations contained therein be approved.

Upon Council's request, Item 8 was voted on separately, as follows:

8. Development Charge Demolition Credit Extension Request for Hamilton Central Business Park (Studebaker Place and Ferrie Street, formerly 440 Victoria Avenue North) (FCS17067(a)) (City Wide) (Item 10.3)

- (a) That the request for the City Development Charges (DC) demolition credits (623,140 square feet in total) for Hamilton Central Business Park (Studebaker Place and Ferrie Street, formerly 440 Victoria Avenue North) to be extended for three years until July 26, 2025 (310,631.39 square feet) and September 6, 2025 (312,508.61 square feet), be denied; and,
- (b) That the subject matter regarding the "Correspondence from Paul Paradis, Ross & McBride LLP, respecting a request for extension of Development Charge Demolition Credits, Hamilton Central Business Park – formerly 440 Victoria Avenue North now Studebaker Place and Ferris Street – DCR Holdings Inc.", be identified as complete and removed from the Audit, Finance and Administration Committee Outstanding Business List.

Result: Motion on Item 8 of the Audit, Finance and Administration Committee Report 22-012, CARRIED by a vote of 13 to 1, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 NO - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

Upon Council's request, Item 6 (a) was voted on separately, as follows:

6. Community Benefits Charge Strategy (FCS22015(b)) (City Wide) (Item 8.1)

- (a) That the 2022 City of Hamilton Community Benefits Charge Strategy, attached as Appendix "B" to Audit, Finance and Administration Committee Report 22-012 dated June 2, 2022, be approved;

Result: Motion on Item 6 (a) the Audit, Finance and Administration Committee Report 22-012, CARRIED by a vote of 12 to 2, as follows:

- NO - Ward 1 Councillor Maureen Wilson
- YES - Ward 2 Councillor Jason Farr
- NO - Ward 3 Councillor Nrinder Nann
- NOT PRESENT - Ward 4 Councillor Sam Merulla
- YES - Ward 5 Councillor Russ Powers
- YES - Ward 6 Councillor Tom Jackson
- YES - Ward 7 Councillor Esther Pauls
- YES - Ward 8 Councillor John-Paul Danko
- YES - Mayor Fred Eisenberger
- YES - Ward 15 Councillor Judi Partridge
- NOT PRESENT - Ward 14 Councillor Terry Whitehead
- YES - Ward 13 Councillor Arlene VanderBeek
- YES - Ward 12 Councillor Lloyd Ferguson
- YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
- YES - Ward 10 Councillor Maria Pearson
- YES - Ward 9 Councillor Brad Clark

Result: Motion on the balance of the Audit, Finance and Administration Committee Report 22-012, CARRIED by a vote of 14 to 0, as follows:

- YES - Ward 1 Councillor Maureen Wilson
- YES - Ward 2 Councillor Jason Farr
- YES - Ward 3 Councillor Nrinder Nann
- NOT PRESENT - Ward 4 Councillor Sam Merulla
- YES - Ward 5 Councillor Russ Powers
- YES - Ward 6 Councillor Tom Jackson
- YES - Ward 7 Councillor Esther Pauls
- YES - Ward 8 Councillor John-Paul Danko
- YES - Mayor Fred Eisenberger
- YES - Ward 15 Councillor Judi Partridge
- NOT PRESENT - Ward 14 Councillor Terry Whitehead
- YES - Ward 13 Councillor Arlene VanderBeek
- YES - Ward 12 Councillor Lloyd Ferguson
- YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
- YES - Ward 10 Councillor Maria Pearson
- YES - Ward 9 Councillor Brad Clark

EMERGENCY AND COMMUNITY SERVICES COMMITTEE REPORT 22-010**(Clark/Pauls)**

That Emergency and Community Services Committee Report 22-010, being the meeting held on Thursday, June 16, 2022, be received and the recommendations contained therein be approved.

Result: Motion on the Emergency and Community Services Committee Report 22-010, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

**HAMILTON ENTERPRISES HOLDING CORPORATION SHAREHOLDER
ANNUAL GENERAL MEETING REPORT 22-001****(Johnson/Pauls)**

That Hamilton Enterprises Holding Corporation Shareholder Annual General Meeting Report 22-001, being the meeting held on Friday, June 17, 2022, be received and the recommendations contained therein be approved.

Result: Motion on the Hamilton Enterprises Holding Corporation Shareholder Annual General Meeting Report 22-001, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson

YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson

YES - Ward 10 Councillor Maria Pearson

YES - Ward 9 Councillor Brad Clark

HAMILTON UTILITIES CORPORATION SHAREHOLDER ANNUAL GENERAL MEETING REPORT 22-002
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(Johnson/Powers)

That Hamilton Utilities Corporation Shareholder Annual General Meeting Report 22-002, being the meeting held on Friday, June 17, 2022, be received and the recommendations contained therein be approved.

Result: Motion on the Hamilton Utilities Corporation Shareholder Annual General Meeting Report 22-002, CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson

YES - Ward 2 Councillor Jason Farr

YES - Ward 3 Councillor Nrinder Nann

NOT PRESENT - Ward 4 Councillor Sam Merulla

YES - Ward 5 Councillor Russ Powers

YES - Ward 6 Councillor Tom Jackson

YES - Ward 7 Councillor Esther Pauls

YES - Ward 8 Councillor John-Paul Danko

YES - Mayor Fred Eisenberger

YES - Ward 15 Councillor Judi Partridge

NOT PRESENT - Ward 14 Councillor Terry Whitehead

YES - Ward 13 Councillor Arlene VanderBeek

YES - Ward 12 Councillor Lloyd Ferguson

YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson

YES - Ward 10 Councillor Maria Pearson

YES - Ward 9 Councillor Brad Clark

STAFF REPORTS

Mayor Eisenberger relinquished the Chair to Deputy Mayor Johnson, in order to move the following motion:

5.10(a) Sub-Sections (a) and (c) of Report HSC22029(a) - Ukrainian Response Update and Request for Assistance (City Wide)

(Eisenberger/Clark)

- (a) That staff be authorized to respond to supporting the Ukraine Crisis, including but not limited to short-term and temporary accommodations and wrap around supports with an estimated cost **up to** \$670,000 per month **for 2 months**, to be charged to the Corporate Financials – Expenditures/Non-Program Dept Id; and
- (c) That the Mayor and City Clerk be authorized and directed to execute all documentation necessary to support the City's response to the Ukraine Crisis, with content acceptable to the General Manager, City Managers Office, and in a form satisfactory to the City Solicitor.

Result: Motion on Sub-Sections (a) and (c), CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
NOT PRESENT - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

Mayor Eisenberger assumed the Chair.

(Johnson/Danko)

That Council rise from Committee of the Whole to consider the Committee Reports.

Result: Motion CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

MOTIONS

Mayor Eisenberger relinquished the Chair to Deputy Mayor Johnson, in order to move the following motions:

6.1 Amendment to Item 7 of General Issues Committee Report 21-009, respecting Motion - Disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia) (Ward 4) (Item 14.2)

(Eisenberger/Farr)

WHEREAS, Council on April 28, 2021 approved Item 7 of the General Issues Committee Report 21-009, being a motion respecting the Disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia) (Ward 4); and

WHEREAS, staff have been pursuing the disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia) (Ward 4).

THEREFORE, BE IT RESOLVED:

- (i) That Sub-section (a) to Item 7 to the General Issues Committee Report 21-009, respecting the Motion - Disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia) (Ward 4), be **amended** by the amendment in the Confidential Appendix 'A' (attached), to read as follows:
 - (a) That the direction provided to staff in Closed Session, **as amended** in Confidential Appendix 'A' to this motion, respecting the Motion regarding the amendment to the Motion on Disposition of the Biindigen Well Being Center (Former St. Helen Catholic Elementary School Property at 785 Britannia) in Ward 4, be approved; and,
- (ii) That pending the approval of the amendment to sub-section (a) to Item 7 to the General Issues Committee Report 21-009, respecting the Motion - Disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia) (Ward 4), Item 7 (b), be **amended** by deleting (b) in its entirety and replacing (b) with the following:
 - (b) ~~That the Motion respecting the Disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia, Hamilton in Ward 4 remain confidential until completion of the real estate transaction, with the following exception:~~
 - ~~(i) That staff be authorized to share any information that may be necessary to give effect to the sale of the property, located at 785 Britannia Avenue, Hamilton.~~

- (b) ***That the amended Closed Session Motion in Confidential Appendix 'B' (attached), respecting the Disposition of the Biindigen Well Being Centre (Former St. Helen Catholic Elementary School Property at 785 Britannia, Hamilton in Ward 4, be released publicly following Council approval, as follows:***

THEREFORE, BE IT RESOLVED:

- (a) That, subject to De Dwa Da Dehs Nye>s Aboriginal Health Centre obtaining the necessary approvals and funding to create and operate a viable community hub to the satisfaction of the General Manager of Healthy and Safe Communities, the Corporate Real Estate Office be authorized and directed to negotiate the requisite agreements to affect the sale (and secure any requisite conditions) of the Property, located at 785 Britannia Avenue, Hamilton, to the De Dwa Da Dehs Nye>s Aboriginal Health Centre (or organization ultimately responsible for the Biindigen Community Hub) on the following conditions, and such other terms and conditions deemed appropriate to the General Manager of Planning and Economic Development:
- (i) The Property be transferred for nominal consideration, plus applicable real estate and legal fees and related transaction costs;
 - (ii) The City's contribution to the furtherance of the community hub be limited to the land, and the City not have any on-going costs or liabilities (operating or capital) associated with the initiative; and,
 - (iii) Should the Biindigen Community Hub, as contemplated herein, fail to materialize, the contributed lands be returned to the City at nominal consideration, and that a commitment be made by the purchasers of the Property that the contributed lands shall be used to support the social determinants of health of the urban indigenous population of Hamilton by a Not-For-Profit organization with preference given to an Indigenous organization, on such terms deemed appropriate to the General Manager, Planning and Economic Development in consultation with General Manager, Healthy and Safe Communities;
- (b) That the General Manager, Planning and Economic Development Department or designate, acting on behalf of the City as property owner, be authorized to provide any consents, approvals, and notices related to the proposed community hub initiative, prior to the transfer of the Property;

- (c) That the City Solicitor be authorized and directed to complete any agreements for the creation of the Biindigen Community Hub and transfer of property, and amend and waive terms and conditions on such terms as considered reasonable for the purposes of the sale of the property, located at 785 Britannia Avenue, Hamilton, to the De Dwa Da Dehs Nye>s Aboriginal Health Centre (or organization ultimately responsible for the Biindigen Community Hub); and,
- (d) That the Mayor and Clerk be authorized and directed to execute any necessary documents, for the purposes of the sale of the property, located at 785 Britannia Avenue, Hamilton, to the De Dwa Da Dehs Nye>s Aboriginal Health Centre (or organization ultimately responsible for the Biindigen Community Hub), in a form satisfactory to the City Solicitor.

Result: Motion CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
NOT PRESENT - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

6.2 Naming of the Great Hall of First Ontario Concert Hall after Boris Brott

(Eisenberger/Nann)

WHEREAS, Boris Brott was artistic director and conductor of the Hamilton Philharmonic Orchestra from 1969-1990, taking it from an amateur ensemble to a professional one with a popular subscription season and global recognition;

WHEREAS, Boris Brott led the launch of the orchestra's music education programs, paving the way to performing in classrooms and school auditoriums across the region. Many Hamiltonians will cite one of his concerts as their first experience with orchestral music. He taught and conducted many of our current HPO musicians and audience members through HPO education programs as well as through the Brott Music Festival and National Academy Orchestra;

WHEREAS, Boris Brott has a direct connection to the Great Hall as a collaborator on the design of the Hall itself;

WHEREAS, First Ontario Credit Union, current holder of naming rights for the facility are not only very much in support of this initiative, but have been instrumental in working with all parties involved to bring this to fruition;

WHEREAS, Boris Brott was later founder and artistic director of the National Academy Orchestra of Canada and Brott Music Festival, both based in Hamilton, where he lived for many years. He was the former principal conductor for youth and family programs at the National Arts Centre in Ottawa, and the New West Symphony Orchestra in Los Angeles in the mid-1990s;

WHEREAS, Boris Brott brought high quality classical music to as wide a public as possible was Brott's goal, never more so than since he became conductor and artistic director of the Orchestre Classique de Montréal, previously the McGill Chamber Orchestra, which his parents Alexander and Lotte Brott, both acclaimed musicians, founded in 1939;

WHEREAS, Boris Brott was not only a giant of the classical music world, he was also a giant for promoting and building Hamilton and his efforts over the decades helped lift our community to new heights; and

WHEREAS, the City of Hamilton recognizes individuals who have made significant contributions to the public life and well-being of the City of Hamilton through the naming of municipal facilities and properties;

THEREFORE, BE IT RESOLVED:

- (a) That staff be directed to facilitate and execute the naming of the Great Hall of First Ontario Concert Hall in honour and recognition of Boris Brott for his contribution to the Hall itself and the enormous contribution Boris made to the greater Hamilton community arts and culture legacy that has been recognized on a global scale;
- (b) That an interior commemorative plaque be installed and an exterior sign be added to the existing exterior First Ontario Concert Hall sign with the wording "The Great Hall – Boris Brott"; and
- (c) That costs of the installations be funded by First Ontario Credit Union and Oak View Group (OVG) / Hamilton Urban Precinct Entertainment Group LP (HUPEG).

Result: Motion CARRIED by a vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger

NOT PRESENT - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

Mayor Eisenberger assumed the Chair for the remainder of the meeting.

6.3 Donation from The Patrick J. McNally Charitable Foundation for St Mark's Capital Project

(Farr/Pearson)

WHEREAS, the City of Hamilton is the owner and steward of the heritage building known as St Mark's Church located at the corner of Bay St. S. and Hunter St. W. in Hamilton;

WHEREAS the City intends to convert St. Mark's Church into a new downtown cultural space and community venue; and

WHEREAS The Patrick J. McNally Charitable Foundation wishes to make a monetary donation of Two Hundred and Fifty Thousand Dollars (\$250,000.00) towards the cost of the project to enhance the experience of visiting and using the site;

THEREFORE, BE IT RESOLVED:

- (a) That the General Manager, Planning and Economic Development, be authorized to negotiate and execute on behalf of the City all agreements and documents necessary to receive funding in the amount of \$250,000 from The Patrick J. McNally Charitable Foundation, all in a form satisfactory to the City Solicitor;
- (b) That the donation amount of \$250,000 from The Patrick J. McNally Charitable Foundation be placed into capital budget account number 7201841803 to be used for the St Mark's Project; and
- (c) That the Mayor send a letter of thanks, on behalf of the City, to the Patrick J. McNally Charitable Foundation for its generous donation.

Result: Motion CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger

YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

NOTICES OF MOTION

7.1 Naming of the Great Hall of First Ontario Concert Hall after Boris Brott

(Eisenberger/Farr)

That the rules of order be waived to allow for the introduction of a Motion respecting Naming of the Great Hall of First Ontario Concert Hall after Boris Brott.

Result: Motion CARRIED by a 2/3rds vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
NOT PRESENT - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

For further disposition of this matter, refer to Item 6.2

7.2 Donation from The Patrick J. McNally Charitable Foundation for St Mark's Capital Project

(Farr/Pearson)

That the rules of order be waived to allow for the introduction of a Motion respecting a Donation from The Patrick J. McNally Charitable Foundation for St Mark's Capital Project.

Result: Motion CARRIED by a 2/3rds vote of 13 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla

YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
NOT PRESENT - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

For further disposition of this matter, refer to Item 6.3

STATEMENTS BY MEMBERS

Members of Council used this opportunity to discuss matters of general interest.

COUNCIL COMMUNICATION UPDATES

(Johnson/Pearson)

That the listing of Council Communication Updates from June 3, 2022 to June 16, 2022, be received.

Result: Motion CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

PRIVATE AND CONFIDENTIAL

Council determined that discussion of Item 10.1 and 10.2 was not required in Closed Session, therefore, the matters were addressed in Open Session, as follows:

10. Closed Session Minutes – June 8, 2022 (Item 10.1) and June 16, 2022 (Item 10.2)

(Partridge/Powers)

That the Closed Session Minutes dated June 8, 2022 and June 16, 2022 be approved, as presented, and remain confidential.

Result: Motion CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
 YES - Ward 2 Councillor Jason Farr
 YES - Ward 3 Councillor Nrinder Nann
 NOT PRESENT - Ward 4 Councillor Sam Merulla
 YES - Ward 5 Councillor Russ Powers
 YES - Ward 6 Councillor Tom Jackson
 YES - Ward 7 Councillor Esther Pauls
 YES - Ward 8 Councillor John-Paul Danko
 YES - Mayor Fred Eisenberger
 YES - Ward 15 Councillor Judi Partridge
 NOT PRESENT - Ward 14 Councillor Terry Whitehead
 YES - Ward 13 Councillor Arlene VanderBeek
 YES - Ward 12 Councillor Lloyd Ferguson
 YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
 YES - Ward 10 Councillor Maria Pearson
 YES - Ward 9 Councillor Brad Clark

BY-LAWS AND CONFIRMING BY-LAW

(Johnson/Clark)

That Bills No. 22-150 to No. 22-164, be passed and that the Corporate Seal be affixed thereto, and that the By-laws, be numbered, be signed by the Mayor and the City Clerk to read as follows:

150 To Amend By-law No. 01-218, as amended, Being a By-law to Regulate On-Street Parking
 Schedule 8 (No Parking Zones)
 Schedule 12 (Permit Parking Zones)
 Schedule 13 (No Stopping Zones)
 Ward: 2, 3, 11, 13, 14

151 To Establish City of Hamilton Land Described as Part of Lot 14, Concession 8 in the Geographic Township of Barton, designated as Part 4 on Plan 62R-12821, and Part 12 on Plan 62R-9741 Save and Except Part 18 on Plan 62R-15778 as Part of Dicenzo Drive
 Ward: 8

- 152 To Consolidate and Update Delegated Site Plan Control By-laws in the City of Hamilton
Ward: City Wide
- 153 To Amend Zoning By-law No. 3692-92, Respecting Lands Located at 541 and 545 Fifty Road (Stoney Creek)
ZAC-21-045/25CDM-202120
Ward: 10
- 154 To Amend Zoning By-law No. 6593 Respecting Lands Located at 16 Steven Street and Part of 436 King William Street, Hamilton
ZAC-22-019
Ward: 3
- 155 To Amend Zoning By-law No. 6593 (former City of Hamilton), Respecting Housekeeping Amendments to Various Definitions and Certain Day Nursery Regulations
Ward: City Wide
CI-22-F
- 156 To Amend Zoning By-law No. 464 (Glanbrook), Respecting Lands Located at 3140-3150 Binbrook Road
Ward: 11
CI-22-F
- 157 A By-law to Delegate Authority During any Restricted Period following Nomination Day
Ward: City Wide
- 158 Being a By-law Respecting Community Benefits Charges on Lands within the City of Hamilton
Ward: City Wide
- 159 To Repeal and Replace By-law No. 22-065, To Impose a Sanitary Sewer Charge Upon Owners of Land Abutting Springbrook Ave from Approximately 24.5 metres South of Lockman Drive to Approximately 17 metres South of Regan Drive, in the City of Hamilton
Ward: 12
- 160 Respecting Removal of Part Lot Control, Part of Lots 1 to 6, 9 to 51, 55, 56, 59 to 63, 65 to 84, and Part of Blocks 86 and 95 Registered Plan No. 62M-1280, for lands municipally known as 103, 104, 107, 108, 111, 112, 115, 119, 123, 127, 131, 135, 139, 143, 147, 150, 151, 154, 155, 159, 163, 167, 171, 175, 179, 183, 187, 191, 195, 199, 203, 207, 211, 215, 219, 223, 227, 231, 235, 239, 243, 247, 251, 255, 259, 271, 275, 279, 282, 283, 286, 287, 290, 291 Rockledge Drive and 128, 132, 133, 136, 137, 140, 141, 144, 148, 152, 156, 160, 164, 168, 172, 176, 180, 184, 188, 192, 196, 200, 204, Cittadella Boulevard, Glanbrook
PLC-22-002
Ward: 9
- 161 Respecting Removal of Part Lot Control, Part of Lots 385 - 389, 408 – 475, 515 –

529 and Blocks 627 - 630, Registered Plan No. 62M-1266
PLC-22-010
Ward: 15

162 To Adopt Official Plan Amendment No. 168 to the Urban Hamilton Official Plan
Respecting 1107 Main Street West (Hamilton)

Ward: 1

163 To Amend Zoning By-law No. 05-200, Respecting Lands Located at 1107 Main
Street West

Ward: 1

164 To Confirm the Proceedings of City Council

Result: Motion CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson
YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

(Clark/Partridge)

That, there being no further business, City Council be adjourned at 2:21 p.m.

Result: Motion CARRIED by a vote of 14 to 0, as follows:

YES - Ward 1 Councillor Maureen Wilson
YES - Ward 2 Councillor Jason Farr
YES - Ward 3 Councillor Nrinder Nann
NOT PRESENT - Ward 4 Councillor Sam Merulla
YES - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
YES - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Mayor Fred Eisenberger
YES - Ward 15 Councillor Judi Partridge
NOT PRESENT - Ward 14 Councillor Terry Whitehead
YES - Ward 13 Councillor Arlene VanderBeek
YES - Ward 12 Councillor Lloyd Ferguson

YES - Deputy Mayor - Ward 11 Councillor Brenda Johnson
YES - Ward 10 Councillor Maria Pearson
YES - Ward 9 Councillor Brad Clark

Respectfully submitted,

Mayor Fred Eisenberger

Andrea Holland
City Clerk



Notice of the Passing of Education Development Charges Amending By-Law

Hamilton-Wentworth Catholic District School Board

TAKE NOTICE that the Hamilton-Wentworth Catholic District School Board passed an amending Education Development Charges By-law 2019 (the “By-law”) on the 21st day of June, 2022 under section 257.70 of the *Education Act*, R.S.O. 1990, c. E.2, as amended (the “Act”). The amended by-law will continue to have a term of five years with an expiration date of July 5, 2024, unless repealed or rescinded earlier. The Board has amended the 2019 by-law to reflect increased land values within the City of Hamilton.

AND TAKE NOTICE that any person or organization may appeal to the Ontario Land Tribunal (OLT) under Section 257.74 of the Act, in respect of the By-law, by filing with the Secretary of the Board being appealed on or before the **2nd day of August, 2022** a notice of appeal setting out the objection to the By-law and the reasons supporting the objection, along with the required fee payable to the Ministry of Finance in respect of each appeal. An appeal raised in respect of the amending by-law may not be an issue that could have been raised when the by-law was adopted in 2019.

The schedule of Education Development Charges to be imposed in respect of designated categories of residential and non-residential development (effective July 6, 2022) has been calculated in accordance with the legislative rate ‘caps’ imposed by amendments to O. Reg 20/98 and is set out below. For clarity, the rate schedules for both the Hamilton- Wentworth DSB and the Hamilton-Wentworth Catholic DSB, as well as the total education development charges to be paid at building permit issuance, are shown below.

	Year 4 Amended By-law Rates July 6, 2022 to July 5, 2023	Year 5 Amended By-law Rates July 6, 2023 to July 5, 2024	Revised HWCDSB Calculated Rate based on current land values
Hamilton-Wentworth Catholic District School Board			
<i>Residential EDC Rate per Dwelling Unit</i>	\$1,401	\$1,701	\$2,523
<i>Non-residential EDC Rate per Sq. Ft. of GFA</i>	\$0.45	\$0.55	\$0.81
Total Hamilton Boards Residential EDC Rates	\$2,974	\$3,274	\$4,096
Total Hamilton Boards Non-Residential EDC Rates	\$0.92	\$1.05	\$1.31

Education Development Charges are imposed on all development and redevelopment lands in the City of Hamilton, save and except the types of development specifically exempted by the Board’s By-law. Accordingly, a key map showing the location of lands affected by the By-law is not provided as part of this notice.

A copy of the Amending By-law is available for examination in the offices of the Board (address shown above), during regular office hours, being between the hours of 8:30 a.m. and 4:30 p.m. from Monday to Friday, and also on the Board’s website noted above.

Notice of a proposed by-law amending a By-law or the passage of such an amending by-law is not required to be given by a Board to any person or organization, other than to certain clerks of municipalities or secretaries of school boards, unless the person or organization gives the secretary of the Board a written request for notice of any amendments to the By-law and has provided a return address.

Any questions respecting the respective board By-laws or the application thereof may be directed to:

Mrs. Paola Pace-Gubekjian, Associate Director of Corporate Services,
Tel: 905-525-2930, Ext. 2309
Email: pacep@hwcdsb.ca

Dated at the City of Hamilton, Ontario this 8th day of July, 2022

Mr. David Hansen, Director of Education, Hamilton-Wentworth Catholic District School Board
Mr. Patrick J. Daly, Chair of the Board, Hamilton-Wentworth Catholic District School Board



Date: June 20, 2022

Resolution No. ⁰⁰⁰ 2022-256

Moved By: *Debra Pollock*

Seconded By: *M. Bateman*

Whereas the Ontario Amber Alert is a warning system that quickly alerts the public of a suspected abduction of children who are in imminent danger;

And Whereas the goal is to broadcast as much information about the child, the abductor and suspect vehicles as quickly as possible so that the public can respond with any relevant information that might lead to the child's safe return;

And Whereas people are encouraged to share the Amber Alert with as many people as possible. If a child or vulnerable person is abducted, spreading the information quickly is critical to their safe return;

And Whereas an Amber Alert makes the Public aware to keep an eye out for the child, vulnerable person, suspect and the vehicle described, in the alert. If they spot them, try to gather as many details as they can, including the specific location where they saw them, the time, the direction they were travelling in and any other identifying details that will help to locate them;

And Whereas an Amber Alert gives citizens instructions to call 9-1-1 or the phone number included in the alert immediately if they have a trip or a sighting related to an Amber Alert;

An Amber Alert will only be activated if:

- The police have confirmed that an abduction has taken place; and
- There is reason to believe the victim is in danger of serious physical injury, and there is information available that, if broadcast to the public, could assist in the safe recovery of the victim.

And Whereas it is essential to remember that an Amber Alert is not always appropriate in every circumstance and that their continued effectiveness depends on ensuring that they are only used in cases that meet the above criteria;

And Whereas the recent tragic death of 11 year old Draven Graham showed that the Amber Alert system is flawed when it comes to vulnerable children who can go missing but are not abducted;

And Whereas at the time this motion was written, there have been almost 75,000 citizens who had signed a petition on Change.Org requesting that a Draven Alert be created;

And Whereas it is clear that there needs to be an addition to the alert system to allow for law enforcement to send out an alert for vulnerable children who go missing under circumstances that do not involve an abduction but are at serious risk of injury or death;

Therefore be it resolved that the Municipality of Brighton and its Council endorse the following:

1. That the Minister of the Solicitor General and the Commissioner of the Ontario Provincial Police, as well as the Premier's Office, be requested to make the necessary changes to the Amber Alert system and create a new alert called the Draven Alert, which will protect vulnerable children who have not been abducted but are at high risk of danger, injury or death and alert the public that they are missing.
2. That this motion be sent to all municipalities across Ontario and the Association of the Municipalities of Ontario (AMO) for endorsement.



Carried OR Defeated

Mayor

Recorded Vote	<i>For Clerks Use Only</i>				
Recorded vote called by: _____					
	For	Against	Abstain	Absent	COI
Mayor Brian Ostrander					
Councillor Ron Anderson					
Councillor Mark Bateman					
Councillor Doug LeBlanc					
Councillor Emily Rowley					
Councillor Mary Tadman					
Deputy Mayor Laura Knegt					
Total					
Carried <input type="checkbox"/> Defeated <input type="checkbox"/>					Clerk's Initials

Pilon, Janet

Subject: Tottering Biped Theatre Summer Shakespeare Production

From: Alma Sarai <development@totteringbiped.ca>

Sent: July 4, 2022 4:26 PM

To: clerk@hamilton.ca

Cc: Trevor Copp <artisticdirector@totteringbiped.ca>

Subject: Tottering Biped Theatre Summer Shakespeare Production

Hello,

I am writing on behalf of the theatre to request a submission of correspondence to be added to the Council package in order to make the public aware of our upcoming community event.

We at TBT are performing "A Midsummer Night's Dream" for 3 weeks this August at Dundurn Castle and Fieldcote Museum and would be grateful for the opportunity to spread the word.

I am happy to complete and submit any paperwork that is required and thank you for your assistance.

All the best,

Alma Sarai

Associate Producer / Summer Shakespeare Artistic Director (she/her)

905-979-6519



TBT is eternally grateful to all our funders during this turbulent time- we continue to totter on because of you!

JUL 05 2022

Ministry of the Attorney General

Crown Law Office – Criminal

McMurtry-Scott Building
720 Bay Street
10th Floor
Toronto ON M7A 2S9

Tel: 416-326-4600
Fax: 416-326-4656

Ministère du Procureur général

Bureau des avocats de la Couronne – droit criminel

Édifce McMurtry-Scott
720, rue Bay
10^e étage
Toronto ON M7A 2S9

Tél: 416-326-4600
Télec.: 416-326-4656



Our Reference #: M-2022-5821

June 23, 2022

His Worship Fred Eisenberger
Mayor
City of Hamilton
71 Main Street West, 2nd Floor
Hamilton, ON L8P 4Y5

Dear Mayor Eisenberger:

RE: Support for Bill C-229 and request for provincial legislation regarding hate speech and symbols

I am writing to acknowledge receipt of your letter dated May 11, 2022, sent to Attorney General Doug Downey and federal Attorney General David Lametti, advising of a resolution passed by the City of Hamilton supporting federal Bill C-229, and your support for further provincial legislation to address hate speech and symbols.

We are aware of and have been following with interest Hamilton's important work to tackle hate in your community, including through your Hate Prevention & Mitigation Initiative.

Responding to hate is a similarly consistent priority for Ontario. We share your concerns about the widespread and devastating impact that hate can have on communities, and the need to develop additional legal and community strategies to deal with this growing threat. For those reasons, we are committed to finding new ways to prevent and address hate in all its forms, such as Ontario's recent Anti-Racism Anti-Hate Grant Program. We will take your views and suggestions into account in planning future anti-hate initiatives.

Thank you again for taking the time to share council's resolution and request.

Sincerely,

A handwritten signature in cursive script that reads "Michelle Campbell".

Michelle Campbell
Acting Deputy Director - Policy
Crown Law Office – Criminal

255 Metcalf St., Postal Bag 729
Tweed, ON K0K 3J0
Tel.: (613) 478-2535
Fax: (613) 478-6457



Email: info@tweed.ca
Website: www.tweed.ca
facebook.com/tweedontario

July 4, 2022

Ministry of Agriculture, Food and Rural Affairs
1 Stone Road West
Guelph, ON N1G 4Y2

Dear Minister:

Re: Ontario Wildlife Damage Compensation Program

We are writing to you today on behalf of our municipality regarding the administration fees related to wildlife damage claims. This program is necessary to protect our farm producers from the devastating losses incurred when they lose livestock to predators.

At the June 28, 2022 Regular Council Meeting the attached Resolution was passed by Council.

Our Council's concern is the administration fee paid to municipalities to administer the program on the Ministry's behalf which was recently increased from \$30.00 per claim to \$50.00 per claim.

We have recently contracted for a new Livestock Investigator resulting in the following costs directly related to wildlife claims:

Hourly Rate: \$25.00/hour
Mileage Rate: .50/km

On the most recent invoice for this service there were three wildlife claims with costs as follows:

1. April 28, 2022 – 3 hours + mileage = \$95.00
2. May 5, 2022 – 2.5 hours + mileage = \$75.00
3. May 8, 2022 – 3.5 hours + mileage = \$120.00

There is also time spent by municipal staff in preparing the wildlife claims for submission and monitoring the claims for payment to the livestock owner.

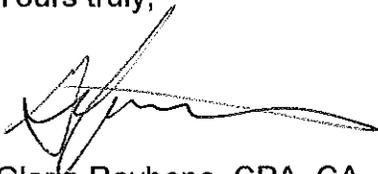
This results in a loss on each wildlife claim for our taxpayers to bear to be able to provide this necessary service to our farm producers.

Ministry of Agriculture, Food and Rural Affairs
Page 2
July 4, 2022

We respectfully request that the Ministry consider increasing the administration fee on wildlife claims to help offset the increasing costs associated with Livestock Investigation.

We look forward to hearing from you about this matter at your earliest convenience.

Yours truly,

A handwritten signature in black ink, appearing to read 'Gloria Raybone', with a long horizontal flourish extending to the right.

Gloria Raybone, CPA, CA
CAO/Treasurer

Encl.

cc. Association of Municipalities of Ontario
Ontario Municipalities

Municipality of Tweed Council Meeting
Council Meeting



Resolution No.

442.

Title:

Ministry of Agriculture, Food and Rural Affairs

Date:

Tuesday, June 28, 2022

Moved by

Brian Treanor

Seconded by

Jacob Palmateer

WHEREAS the Ontario Ministry of Agriculture, Food and Rural Affairs administers the Ontario Wildlife Damage Compensation Program to provide compensation to farm producers for livestock killed by wildlife;

AND WHEREAS Ontario Municipalities administer the Program on behalf of OMAFRA by appointing a Livestock Investigator and staff to work on wildlife damage claims;

AND WHEREAS the costs associated with wildlife damage claims typically exceed the administration fee of \$50.00 per claim as provided to the Municipality by OMAFRA;

NOW THEREFORE BE IT RESOLVED THAT the Council of the Municipality of Tweed request the Ministry of Agriculture, Food and Rural Affairs to review the administration fee provided to

Municipalities for the administration of the Ontario Wildlife Damage Compensation Program;

AND FURTHER, that this Resolution be circulated to the Association of Municipalities of Ontario (AMO) and all Ontario Municipalities for their consideration and support.

Carried

Mayor

J. Albert



July 7, 2022

GSP File No: 21214

Madame Clerk, Mayor Eisenberger and Members of Council
 City of Hamilton
 71 Main Street West
 Hamilton, Ontario
 L8P 4Y5

RE: 510 Centennial Parkway North, Hamilton
Council Agenda 22-016: Friday, July 8, 2022
Item number 6.3: Planning Committee Report 22-011 (July 5, 2022)
PED22129: UHOPA-21-019 / ZAC-21-041

At the City of Hamilton Planning Committee Meeting #22-011 held on July 5, 2022, the applications for Urban Hamilton Official Plan Amendment (UHOPA-21-019) and Zoning By-law Amendment (ZAC-21-041) for the lands located at 510 Centennial Parkway North, were denied.

Staff Report (PED22129) clearly articulated the merit of the application and recommended that the proposal be approved for the following reasons:

- The applications are consistent with the Provincial Policy Statement (2020);
- The applications conform to A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019, as amended;
- The applications comply with the general intent of the UHOP, and complies with the general intent of the Centennial Neighbourhood plan, upon approval of the OPA; and
- The proposed land use is compatible with existing and approved development in the area.

In opposition to these conclusions, Planning Committee Members opined that the approval of the applications would offend Ontario Board Decision PL090764 (from 2010), as well as the policies within the UHOP and the Centennial Neighbourhoods Secondary Plan.

We continue to maintain, with the written support from City of Hamilton Planning Staff, that these applications can be supported on their merit, without concern relating to historical visions for the Site. The City of Hamilton is continually evolving as are trends in retail shopping centres; and the proposal complements the existing uses on the Site while adapting to the changing dynamic of retail trends.

We respectfully request that Council lift the Planning Committee decision for reconsideration and support the approval of the applications, as amended, below. Alternatively, we request that Council defer consideration of the item to allow for additional dialogue to occur, with the item to return to Council on August 12th, 2022.

A summary of the key points to aid in the reconsideration of the decision by Planning committee are provided below:

- The addition of the mini-storage use does not negate the policy that the area caters “primarily” to the weekly and daily shopping needs of residents in the surrounding area. The area continues to primarily cater to those functions. The proposed mini-storage area is only 3.6% of the entire Site area.
- The mini-storage use will complement the existing services and support the existing businesses within the District Commercial designation through increased activity. Furthermore, SmartStop’s operational experience suggests that up to 40% of the end users of the mini-storage units are used by small businesses to support their operations.
- The addition of the mini-storage use does not offend the urban design policies relating to an active public street frontage. This building is located internal to the Site, and the proposed By-law restricts the location of the mini-storage building to be set back a minimum of 50m (164ft) from a public street. All policies relating to design matters adjacent to the public realm will be maintained. In addition, the proposed development will improve the pedestrian experience within the existing shopping centre through the addition of a sidewalk and landscaping.
- The proposed development will contribute to the commercial intensification and build-out of the existing shopping centre while optimizing the use of existing infrastructure. There is an excess of vacant land within the Site (approximately one-third) for which there has been little to no market uptake to develop. SmartCenters acts on the viable interests on its lands across all asset types, and notes that the commercial interest in this site has waned and that it now faces the prospects of future vacancies in light of the impacts associated with the pandemic.
- The addition of the mini-storage use will complement the planned function of the recently approved Centennial Neighbourhoods Secondary Plan. This secondary plan includes Sub-Regional Service Node lands, the limits of which do not include the Site. The Sub-Regional Service Node is one of the priority areas where the City is focusing intensification and density. These planned high density developments include minimal storage space and would stand to benefit from the proposed use.
- It needs to be recognized that the Site is located on former industrial lands and abut a Provincially Significant Employment Zone (PSEZ). While the highest and best use of the lands would likely include a mixed-use development with a high-density residential component, given compatibility factors related to these adjacent lands and environmental factors arising from the Site itself, the potential to add a sensitive land use such as residential on these lands is remote at best. The Risk Assessment tied to the Site would prohibit residential of any kind.

Furthermore, the Risk Assessment would limit most forms of mixed-use development that would require underground components (i.e.: parking garage). As well, residential uses are not currently permitted nor contemplated on the Site per the policy and regulatory direction provided by the UHOP, Centennial Neighbourhoods Secondary Plan, and Zoning By-law.

We continue to highlight the synergy between SmartCenters and SmartStop Self Storage and the successful developments that have thrived in numerous municipalities across Ontario. Users of these facilities, including women and seniors, appreciate and prefer to use self-storage facilities that are located in highly visible, well-lit, accessible locations where there are other people around, such as a commercial plaza; as opposed to secluded industrial parks.

In order to be explicit in the use, and restriction thereof, we request the following revision to Appendix C of Report PED22129 (the draft Zoning By-law), to restrict the mini-storage building to only one, 4-storey building.

Specifically, Appendix C, page 2 of 5, regulation 2 c) should state (bolded words added):

*That in addition to Section **2.1 (a) and 2.3 (a)** of By-law No. 10-081 and Section 2 b) of this By-law, a Warehouse (Mini Storage Facility) shall be restricted a maximum of one Mini Storage Facility, **at a maximum height of four storeys (19.5m to uppermost point)** and to a maximum total gross floor area of 12,900 square metres and maximum ground floor area of 3,500 square metres.*

The addition of the mini-storage use will contribute to the vision of the Centennial Neighbourhoods Secondary Plan and provides a complementary use to the existing and planned function of the area.

Therefore, we would appreciate the reconsideration of the applications for Urban Hamilton Official Plan Amendment (UHOPA-21-019) and Zoning By-law Amendment (ZAC-21-041) for the lands located at 510 Centennial Parkway North (PED22129), as amended in this letter, and welcome further discussion.

Respectfully,



Sarah Knoll, MCIP, RPP
Associate - Senior Planner/Hamilton Manager



Stuart Hastings, MCIP, RPP
Planner

cc. Steve Robichaud, Chief Planner and Director of Planning, City of Hamilton
Rino Dal Bello, Senior Project Manager – Urban Area, City of Hamilton
Daniel Barnett, Planner 2, Urban Team, City of Hamilton
Allan Scully, Executive Vice President, Development, SmartCentres
Adam Hawkswell, Manager, Development, SmartCentres

July 6, 2022

5.7

Arlene Whatmough
[REDACTED]

Mayor Fred Eisenberger and members of Council

Re: Conversion of Main and King Street To Two Way

This is the worst thing you can do. We moved here in 1955 and it was an absolute nightmare to cross Main Street. I was eleven or twelve years old. We crossed to the middle and waited there with cars going behind and in front of us until the traffic cleared in order to cross the rest of the way. It was a very treacherous ordeal and was not an option to walk two blocks to lock street to the stop light and back just to cross the street.

If you make space for bike lanes, parked cars, and those stupid bump outs; that leaves one lane each way to drive, we will be in total gridlock just like Toronto. No driver will be able to make a left hand turn since they will have to cross the lane of traffic coming from the other direction. Drivers trying to make that left hand turn will be blocking the cars behind them from driving straight ahead. Also, drivers in the cross streets will not be able to go straight ahead since they will be fighting two lanes of traffic going in both directions.

Now, with one lane of traffic going each way how will the emergency vehicles get through? Drivers can not pull over a lane of parked cars or bike lanes to let them pass.

Every twenty minutes, seven busses go by (three different Delaware busses a King 1 and King 5, a Bee-Line and a Locke Street Bus); which will also now be lined up behind all the cars that are waiting to go. Delivery trucks are also in the line-up.

I have heard City Hall people say drivers will find other streets. There are no other streets a driver can go to that go East or West in order to drive across the City.

Bicycle riders do not follow the rules. They will ride across Main Street at a stop light and ride against the light thereby blocking cars that have the right of way. It happens all the time at Queen and Main.

While I am on the subject, what about whenever there is a concert at Hamilton Place or First Ontario Place? Are you aware, that whenever these happen, the left hand side of Main Street is a parking lot? Drivers are trying to make left hand turns to get into the underground parking areas and only two or three cars at a time get to turn.

Two way conversion of Main Street will not stop a pedestrian from crossing in the middle of the street. A case in point—almost every time I drive downtown a pedestrian crosses Main Street in the middle of the block, between Hess and Caroline Streets. There are stop lights at both Hess and Caroline Streets and there is nothing on the North side of Main, and the person has to walk one way or the other, so why don't they just walk to the light nearest to their destination?

Riding a bus to get to an appointment is not a good option; it takes a full hour or more to get up the mountain and the same length of time or more to get back home which leaves me no time to do anything else for whatever is left of the day.

Sincerely
[REDACTED]



GENERAL ISSUES COMMITTEE REPORT 22-013

1:00 p.m.

June 23, 2022

Council Chambers, City Hall, 2nd Floor
71 Main Street West, Hamilton, Ontario

Present: Deputy Mayor B. Johnson (Chair)
Councillors M. Wilson, J. Farr, N. Nann, E. Pauls, J.P. Danko, B Clark,
M. Pearson, A. VanderBeek

Absent: Mayor F. Eisenberger, Councillors L. Ferguson, J. Partridge – Other City Business
Councillors S. Merulla, R. Powers, T. Jackson, T. Whitehead – Personal

THE GENERAL ISSUES COMMITTEE PRESENTS REPORT 22-013 FOR INFORMATION:

(a) APPROVAL OF AGENDA (Item 2)

The Committee Clerk advised that there were no changes the agenda.

The agenda for the June 23, 2022 General Issues Committee (Special) meeting was approved, as presented.

(b) TRAINING PRESENTATIONS (Item 2)

(i) Introduction: Cultivating a Culture of Public Engagement at the City of Hamilton Summary Report (Item 2.1)

Morgan Stahl, Director of Government Relations & Community Engagement, provided a verbal overview of the Introduction: Cultivating a Culture of Public Engagement at the City of Hamilton Summary Report.

(ii) Introduction to Foundations in Public Participation - City of Calgary Case Study: Public Engagement Policy and Administrative Framework (Item 2.2)

Lara Tierney, of the Canadian Trainers Collective (CTC), provided a PowerPoint presentation respecting Introduction to Foundations in Public Participation - City of Calgary Case Study: Public Engagement Policy and Administrative Framework.

(iii) Overview: Evolution of Public Engagement Practices at the City of Hamilton (Item 2.3)

Cindy Mutch, Manager of Community Initiatives, provided a PowerPoint presentation respecting Overview: Evolution of Public Engagement Practices at the City of Hamilton.

(iv) Reimaging Public Participation Together: City-wide Public Engagement Campaign and Survey Results (Item 2.4)

Cindy Mutch, Manager of Community Initiatives, provided a PowerPoint presentation respecting Reimaging Public Participation Together: City-wide Public Engagement Campaign and Survey Results.

(v) Reimaging Public Participation Together: Community Partner and Key Stakeholder Engagement Results (Item 2.5)

Jesse Shea, Senior Consultant of Public Affairs and Communications; and, Kalvin Reid, Director of Public Relations, both of Enterprise Canada, provided a PowerPoint presentation respecting Reimaging Public Participation Together: Community Partner and Key Stakeholder Engagement Results.

(vi) Reimaging Public Participation Together: Specialized Engagement Results with Racialized, Equity-Seeking and Traditional Under-Represented Groups (Item 2.6)

Deirdre Pike, Senior Social Planner, Social Planning & Research Council, provided a PowerPoint presentation respecting Reimaging Public Participation Together: Specialized Engagement Results with Racialized, Equity-Seeking and Traditional Under-Represented Groups.

The presentations respecting public participation and engagement (Items 2.1 through 2.6) were received.

(c) ADJOURNMENT (Item 15)

There being no further business, the General Issues Committee adjourned at 3:48 p.m.

Respectfully submitted,

Brenda Johnson, Deputy Mayor
Chair, General Issues Committee

Stephanie Paparella
Legislative Coordinator,
Office of the City Clerk



GENERAL ISSUES COMMITTEE REPORT 22-014

July 4, 2022

Council Chambers, City Hall, 2nd Floor
71 Main Street West, Hamilton, Ontario

Present: Mayor F. Eisenberger, Deputy Mayor E. Pauls (Chair)
Councillors J. Farr, N. Nann, S. Merulla, R. Powers, T. Jackson,
J.P. Danko, B Clark, M. Pearson, B. Johnson, L. Ferguson,
A. VanderBeek, J. Partridge

Absent: Councillors M. Wilson and T. Whitehead – Personal

THE GENERAL ISSUES COMMITTEE PRESENTS REPORT 22-014, AND RESPECTFULLY RECOMMENDS:

1. **Waterdown Business Improvement Area (BIA) Revised Board of Management (PED22152) (Ward 15) (Item 7.1)**
 - (a) That the following individuals be appointed to the Waterdown Business Improvement Area (BIA) Board of Management, effective July 8, 2022;
 - (i) Shari Reaume; and,
 - (ii) Buket Necip; and,
 - (b) That the following individual be appointed to the Waterdown Business Improvement Area (BIA) Board of Management, effective July 19, 2022.
 - (i) Christina Birmingham.
2. **Emergency Operations Centre COVID-19 Recovery Phase and After-Action Reporting (CM22010) (City Wide) (Item 7.2)**

That Report CM22010, respecting the Emergency Operations Centre COVID-19 Recovery Phase and After-Action Reporting, be received.

3. Annual Update on Economic Development Action Plan (PED22104) (City Wide) (Item 8.1)

That Report PED22104, respecting the Annual Update on Economic Development Action Plan, be received.

4. City Manager 2021 – 2022 Review (CM22011) (City Wide) (Item 8.2)

That Report CM22011, respecting the City Manager 2021-2022 Review, be received.

5. Term of Council Priorities 2018 to 2022 Summary Report (CM16003(e)) (City Wide) (Item 8.3)

That Report CM16003(e), respecting the Term of Council Priorities 2018 to 2022 Summary Report, be received.

6. Commonwealth Games 2030 - Revised Memorandum of Understanding (PED19108(i)) (City Wide) (Item 10.1)

That the General Manager of Finance and Corporate Services, or their designate, be authorized and directed to execute, on behalf of the City of Hamilton, a Memorandum of Understanding (MOU) with Hamilton100 Commonwealth Games Bid Corporation (Hamilton100), attached as Appendix "A" to Report 22-014, and any other required ancillary documents or agreements, all in a form satisfactory to the City Solicitor.

7. Environmental Remediation and Site Enhancement (ERASE) Redevelopment Grant Application, 415 Main Street West, Hamilton ERG-21-05 (PED22136) (Ward 1) (Item 10.2)

- (a) That Environmental Remediation and Site Enhancement (ERASE) Redevelopment Grant Application ERG-21-05, submitted by Main 415 Inc. (Morteza Vossough and Shahram Heidari), owner of the property at 415 Main Street West, Hamilton for an ERASE Redevelopment Grant not to exceed \$239,325, for estimated eligible remediation costs provided over a maximum of ten years, be authorized and approved in accordance with the terms and conditions of the ERASE Redevelopment Agreement;
- (b) That the General Manager of the Planning and Economic Development Department be authorized and directed to execute the Environmental Remediation and Site Enhancement (ERASE) Redevelopment Agreement

together with any ancillary documentation required, to give effect to the ERASE Redevelopment Grant for Main 415 Inc. (Morteza Vossough and Shahram Heidari), owner of the property at 415 Main Street West, Hamilton in a form satisfactory to the City Solicitor; and,

- (c) That the General Manager of the Planning and Economic Development Department be authorized and directed to administer the Grant and Grant Agreement including but not limited to: deciding on actions to take in respect of events of default and executing any Grant Amending Agreements, together with any ancillary amending documentation, if required, provided that the terms and conditions of the Environmental Remediation and Site Enhancement (ERASE) Redevelopment Grant, as approved by City Council, are maintained and that any applicable Grant Amending Agreements are undertaken in a form satisfactory to the City Solicitor.

8. Environmental Remediation and Site Enhancement (ERASE) Redevelopment Grant Application, 4 Vickers Road, Hamilton ERG-20-03 (PED22148) (Ward 7) (Item 10.3)

- (a) That Environmental Remediation and Site Enhancement (ERASE) Redevelopment Grant Application ERG-20-03, submitted by DiCenzo (Linden Park) Holdings Inc., owner of the property at 4 Vickers Road, Hamilton for an ERASE Redevelopment Grant not to exceed \$68,527 in actual eligible costs for the abatement and removal of designated substances and hazardous materials to be provided over a maximum of ten years, be authorized and approved in accordance with the terms and conditions of the ERASE Redevelopment Agreement;
- (b) That the General Manager of the Planning and Economic Development Department be authorized and directed to execute the Environmental Remediation and Site Enhancement (ERASE) Redevelopment Agreement together with any ancillary documentation required, to give effect to the ERASE Redevelopment Grant for DiCenzo (Linden Park) Holdings Inc., owner of the property at 4 Vickers Road, Hamilton in a form satisfactory to the City Solicitor; and,
- (c) That the General Manager of the Planning and Economic Development Department be authorized and directed to administer the Grant and Grant Agreement including but not limited to: deciding on actions to take in respect of events of default and executing any Grant Amending Agreements, together with any ancillary amending documentation, if required, provided that the terms and conditions of the Environmental Remediation and Site Enhancement (ERASE) Redevelopment Grant, as approved by City Council, are maintained and that any applicable Grant

Amending Agreements are undertaken in a form satisfactory to the City Solicitor.

**9. Revitalizing Hamilton Tax Increment Grant - 9 Robert Street (PED22147)
(Ward 2) (Item 10.4)**

- (a) That a Revitalizing Hamilton Tax Increment Grant Program (RHTIG) Application submitted by 2691597Ontario Inc. (Golden Spruce Homes Inc., 1943255 Ontario Inc., Damian Guiducci, Joshua Abraham, Terrence Chung), for the property at 9 Robert Street, Hamilton, estimated at \$155,738.85 over a maximum of a four year period, and based upon the incremental tax increase attributable to the redevelopment of 9 Robert Street, Hamilton, be authorized and approved in accordance with the terms and conditions of the RHTIG;
- (b) That the General Manager of the Planning and Economic Development Department be authorized and directed to execute a Grant Agreement together with any ancillary documentation required, to give effect to the RHTIG for 2691597Ontario Inc. (Golden Spruce Homes Inc., 1943255 Ontario Inc., Damian Guiducci, Joshua Abraham, Terrence Chung) for the property known as 9 Robert Street, Hamilton, in a form satisfactory to the City Solicitor; and,
- (c) That the General Manager of the Planning and Economic Development Department be authorized and directed to administer the Grant, and Grant Agreement including, but not limited to, deciding on actions to take in respect of events of default and executing any Grant Amending Agreements, together with any ancillary amending documentation, if required, provided that the terms and conditions of the RHTIG Program, as approved by City Council, are maintained and that any applicable Grant Amending Agreements are undertaken in a form satisfactory to the City Solicitor.

**10. Public Engagement Policy and Administrative Framework (CM21011(a))
(City Wide) (Item 10.5)**

- (a) That staff be directed to develop a corporate-wide Public Engagement Policy and Administrative Framework that is informed by public feedback and consultation efforts with relevant stakeholder groups including residents, community partners, the City's Public Engagement Community of Practice cross-departmental staff members, City's Senior Leadership Team, special workshop with City Council, and priority City-led equity, diversity and inclusion initiatives;

- (b) That temporary resources for 9 months for an amount up to, but not exceeding \$225,000, to be funded from the Tax Stabilization Reserve #110046, be approved for the estimated operating costs to support the development of a corporate-wide public engagement policy and framework; corporate-wide public engagement programming and training; and, Our Future Hamilton Public Engagement Summit, in order to continue the momentum of existing work;
- (c) That the equivalent of 2 FTEs and the associated program budget of \$300,000, be referred to the 2023 operating budget for the Government and Community Relations team, to facilitate corporate-wide public engagement programming, training, and annual costs associated with the Our Future Annual Public Engagement Summit, in order to provide the necessary resources and structure to the support the Government and Community Relations team;
- (d) That staff be directed to identify and source options to support corporate-wide public engagement efforts that are accessible and meet the City's Equity, Diversity and Inclusion goals;
- (e) That staff be directed to update the City's Public Engagement Charter, in consultation with key stakeholders, to ensure alignment with the City's Public Engagement Policy, Administrative Framework and Implementation Plan; and,
- (f) That staff be directed to report back to the General Issues Committee by December 31, 2023 with a status update respecting the Public Engagement Policy and Administrative Framework.

11. Business Improvement Area Advisory Committee Report 22-006, June 14, 2022 (Item 10.6)

- (a) **International Village Business Improvement Area Expenditure Request (Item 11.1)**
 - (i) That the expenditure request from the International Village Business Improvement Area, in the amount of \$7,316.97 for the purchase of banners, programming and promotions, to be funded from the Community Improvement Plan (CIP) Contribution Program (BIA Payments Account 815010-56905), be approved; and,
 - (ii) That the expenditure request from the International Village Business Improvement Area, in the amount of \$12,362.71 for the cost of graffiti removal and banners, to be funded from the Shared

Parking Revenue Program (Parking Revenue Account 815010-45559), be approved.

12. Hamilton-Wentworth District School Board Liaison Committee Report 22-001, May 9, 2022 (Item 10.7)

(a) Public Access to Modernized Hamilton Wentworth District School Board Facilities (Artificial Turf Fields) (Item 10.1)

- (i) That the Public Access to Modernized Hamilton-Wentworth District School Board (HWDSB) Facilities (Artificial Turf Fields) report, be referred to HWDSB staff for a formal detailed response to be brought forward at the next Committee meeting; and
- (ii) That Hamilton-Wentworth District School Board (HWDSB) staff report back to the Hamilton-Wentworth District School Board Liaison Committee with a formal response respecting Public Access to Modernized HWDSB Facilities (Artificial Turf Fields), with all details including costs.

(b) Bernie Custis Secondary Access to Tim Horton's Field (Item 10.2)

That staff from the City and Hamilton-Wentworth District School Board report back to the Hamilton-Wentworth District School Board Liaison Committee, with respect to the details, including costs, regarding the removal of the berms at Bernie Custis Secondary School.

(c) Status of City of Hamilton and Hamilton-Wentworth District School Board Property/Facility Agreements (Item 10.3)

That the report respecting the Status of City of Hamilton and Hamilton-Wentworth District School Board Property/Facility Agreements, be received.

(d) Active and Sustainable School Transportation Charter Agreement - Opportunities coming out of the pandemic (Added Item 10.5)

That staff be directed to report back to the Hamilton-Wentworth District School Board Liaison Committee on the status of and availability of Metrolinx funding for the Active and Sustainable School Transportation Charter Agreement.

(e) Hamilton-Wentworth District School Board Liaison Committee Terms of Reference Review (Added Item 10.6)

That the Membership and Mandate sections of the Hamilton-Wentworth District School Board Liaison Committee Terms of Reference, be amended to read as follows:

(a) MEMBERSHIP

Total of three Trustees (Chair of the Board and two Trustees)

Total of two Members of Council (Mayor and one member of Council)

Total of three HWDSB Secondary Students (non-voting)

City and Board Staff will be invited to attend meetings, as required.

The term of membership on each committee shall be consistent with the practices of each of the respective bodies.

(b) MANDATE

- (1) To strengthen the relationship between the City and HWDSB;
- (2) To assist in addressing issues affecting the governing bodies;
- (3) To promote increased co-operation, synergies and efficiencies between City Council and HWDSB;
- (4) To explore common interests;
- (5) To better understand and co-ordinate services;
- (6) To increase and maintain regular communication;
- (7) To work in a spirit of co-operation to further the mission of the City and HWDSB;
- (8) Joint initiatives and outcomes will align;

- (9) To develop ongoing collaborative and timely public communication strategies reflective of common purpose, work in progress and outcomes;
- (10) To monitor the work developed by staff on the Joint Property Asset Committee (JPAC) as outlined in the Terms of Reference of JPAC and attached as Appendix "A"; and,
- (11) To promote the health and wellbeing of children and their families.

**13. Non-Union Compensation Sub-Committee Report 22-001, June 21, 2022
(Item 10.8)**

**(a) Senior Leadership Team Preventative Health Assessment
(HUR22009) (City Wide) (Item 9.2)**

- (i) That the direction provided to staff in Closed Session, respecting Report HUR22009, Senior Leadership Team Preventative Health Assessment, be approved; and,
- (ii) That Report HUR22009, Senior Leadership Team Preventative Health Assessment, remain confidential until approved by Council.

(b) Non-Union Benefit Policy (HUR22008) (City Wide) (Item 9.3)

- (i) That the Non-Union Benefit Policy, attached as Private & Confidential Appendix "A" to Report HUR22008, be approved; and,
- (ii) That Report HUR22008, respecting the Non-Union Benefit Policy and its Private & Confidential Appendix "A" remain confidential until approved by Council, at which time both the report and the appendix may be publicly released.

14. Advisory Committee for Persons with Disabilities Report 22-007, June 14, 2022 (Item 10.9)

(a) Accessible Open Spaces and Parklands Working Group Draft Terms of Reference (Item 7.6 (a))

That the Accessible Open Spaces and Parklands Working Group Draft Work Plan, attached as Appendix "B" to Report 22-014, as amended, be approved.

(b) Appointment of Paula Kilburn to the Accessible Open Spaces and Parklands Working Group (Added Item 7.6 (c))

That Paula Kilburn be appointed to the Accessible Open Spaces and Parklands Working Group of the Advisory Committee for Persons with Disabilities.

(c) Invitation to Staff Respecting the Differences Between By-laws, Regulations and Guidelines (Added Item 7.6 (d))

That the appropriate staff be invited to a future meeting of the Advisory Committee for Persons with Disabilities (ACPD) respecting the differences between by-laws, regulations and guidelines, what the City can and cannot enact and what ACPD could recommend be enacted with respect to by-laws, regulations and guidelines.

15. Airport Sub-Committee Report 22-002, June 28, 2022 (Item 10.10)

(a) Airport Sub-Committee – Terms of Reference (Item 7.1)

That the updated Terms of Reference for the Airport Sub-Committee, that were approved by City Council on June 8, 2022, be received.

(b) John C. Munro Hamilton International Airport - Conventional Transit Service Levels and Ridership (PW22057) (City Wide) (Item 7.2)

That Report PW2205,7 respecting the John C. Munro Hamilton International Airport - Conventional Transit Service Levels and Ridership, be received.

- (c) Potential Development and Expansion Proposal at John C. Munro Hamilton International Airport (CYHM, YHM) (PED22159) (Ward 11) (Item 14.2)**

 - (i) That the Closed Session Recommendations respecting Report PED22159, Potential Development and Expansion Proposal at John C. Munro Hamilton International Airport (CYHM, YHM), be approved and remain confidential; and,
 - (ii) That the complete Report PED22159 respecting the Potential Development and Expansion Proposal at John C. Munro Hamilton International Airport (CYHM, YHM), remain confidential.

- 16. Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands (PED19063(d)) (Ward 2) (Item 10.11)**

 - (a) That the materials presented by Aeon Studio Group (Aeon) in Appendix “C” to Report PED19063(d) entitled “Aeon Studio Group Submission Package” and Confidential Appendix “D” entitled “Aeon Studio Group Confidential Submission Package – Preliminary Project Construction Costing”, be received;
 - (b) That staff be authorized and directed to negotiate the necessary contractual agreements with Aeon, including a Master Development Agreement and/or proposed Agreement(s) of Purchase and Sale, based on the materials submitted by Aeon Studio Group, and the key City parameters, outlined in Appendix “C” to Report 22-014, and report to back to the General Issues Committee for approval, and on such terms and conditions deemed appropriate by the General Manager of Planning and Economic Development or designate, and in a form acceptable to the City Solicitor;
 - (c) That staff be authorized and directed to undertake an appraisal, and to retain a development consultant to confirm the developability of the concept for the Barton-Tiffany lands, with a cost not to exceed \$150 K, to be funded from West Harbour Capital Project Account 4411706201, and report back to the General Issues Committee at the same time as the recommended agreements identified in Recommendation (b) of Report PED19063(d);
 - (d) That Appendix “D” to Report PED19063(d), respecting the Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands, remain confidential;

- (e) That staff be directed to review opportunities for affordable housing through proceeds of the sale of the Barton-Tiffany lands and report back to the General Issues Committee; and,
- (f) That staff be directed to determine a definition of "affordable housing", from the City's perspective, in the forthcoming report, respecting review opportunities for affordable housing through proceeds of the sale of the Barton-Tiffany lands.

17. Advisory Committee for Persons with Disabilities Report 22-008, June 28, 2022 (Item 10.12)

(a) Advisory Committee for Persons with Disabilities - Accessibility Fair Budget (Item 7.3 (a))

That the proposed budget for the "Ability First" Advisory Committee for Persons with Disabilities Accessibility Fair, to be held October 5, 2022, attached as Appendix "D" to Report 22-014, be approved, with the purchase, cost and the reservation fee for the banner being approved subject to the following condition:

- (i) the City's ability to store the banner being purchased for this event and for future events by the Advisory Committee for Persons with Disabilities.

18. Property Acquisition and Exchange to Facilitate Parkland and New School (PED22045) (Ward 6 and Ward 12) (Item 14.1)

- (a) That the direction provided to staff in Closed Session respecting Report PED22045, Property Acquisition and Exchange to Facilitate Parkland and New School, be approved;
- (b) That the budget for the Property Acquisition and Exchange to Facilitate Parkland and New School, as outlined in Appendix "E" to Report PED22045, be approved;
- (c) That all costs for the Property Acquisition and Exchange to Facilitate Parkland and New School be charged to Project ID Account No. 4401356300, and real estate and legal costs of \$382 K be debited from 59806-4401356300 and credited to Dept. ID Account No. 59806-812036 (Real Estate – Admin Recovery);
- (d) That the City Solicitor be authorized and directed to complete the transactions respecting the Property Acquisition and Exchange to

Facilitate Parkland and New School on behalf of the City, including paying any necessary expenses, amending the closings, due diligence and other dates, and amending and waiving such other terms and conditions as deemed reasonable;

- (e) That the Mayor and Clerk be authorized and directed to execute the requisite agreements and all associated and necessary documents respecting the Property Acquisition and Exchange to Facilitate Parkland and New School, with all such documents to be in a form satisfactory to the City Solicitor; and,
- (f) That the complete Report PED22045, respecting the Property Acquisition and Exchange to Facilitate Parkland and New School, remain confidential until completion of the real estate transactions, except for Appendix “E” to Report PED22045, which is to remain confidential.

19. Partial Acquisition of Land in Ward 15 (PED22142) (Ward 15) (Item 14.2)

- (a) That an Option to Purchase, scheduled to close on July 28, 2022, for the Partial Acquisition of Land in Ward 15, as shown in Appendix “A” to Report PED22142, based substantially on the Terms and Conditions, attached as Appendix “B” to Report PED22142, and such other terms and conditions deemed appropriate by the General Manager of the Planning and Economic Development Department, be approved;
- (b) That all costs related to the Partial Acquisition of Land in Ward 15, be charged to Project ID Account No. 59806-4031280288;
- (c) That the sum of \$20,202 be funded from Project ID Account No. 59806-4031280288 and be credited to Dept ID Account No. 59806-812036 (Real Estate – Admin Recovery) for recovery of expenses including real estate and legal fees and costs, with respect to the Partial Acquisition of Land in Ward 15;
- (d) That the City Solicitor be authorized and directed to complete the Partial Acquisition of Land in Ward 15 on behalf of the City, including paying any necessary expenses, amending the closing, due diligence and other dates, and amending and waiving terms and conditions on such terms, as considered reasonable;
- (e) That the Mayor and Clerk be authorized and directed to execute the necessary documents for the Partial Acquisition of Land in Ward 15, in a form satisfactory to the City Solicitor; and,

- (f) That the complete Report PED22142, respecting the Partial Acquisition of Land in Ward 15, remain confidential until completion of the real estate transaction.

20. Red Hill Valley Parkway Class Action Update (LS20014(b)) (City Wide) Red Hill Valley Parkway Class Action Update (LS20014(b)) (City Wide) (Item 14.4)

- (a) That the direction provided to staff in Closed Session, respecting Report LS20014(b) - Red Hill Valley Parkway Class Action Update, be approved; and,
- (b) That Report LS20014(b) respecting the Red Hill Valley Parkway Class Action Update and its appendix remain confidential.

FOR INFORMATION:

(a) APPROVAL OF AGENDA (Item 2)

The Committee Clerk advised of the following changes the agenda:

6. DELEGATION REQUESTS

- 6.1. Dr. Bob Maton, respecting the City's Vaccine Mandate

This delegation has withdrawn their request.

- 6.8 Val Sarjeant, Stoney Creek Athletics, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding

This delegation has withdrawn their request.

- 6.16. Kevin Duffy, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (For the July 4, 2022 GIC)

- 6.17 Dean Hustwick, President and Board Chair, Athletics Ontario, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (For the July 4, 2022 GIC)

10. DISCUSSION ITEMS

10.12. Advisory Committee for Persons with Disabilities Report 22-008,
June 28, 2022

The agenda for the July 4, 2022 General Issues Committee meeting was approved, as presented.

(b) DECLARATIONS OF INTEREST (Item 3)

- (i) Councillor J. P. Danko, declared a non-disqualifying interest to Item 10.7, respecting the Hamilton-Wentworth District School Board Liaison Committee Report 22-001, May 9, 2022, as his wife is the Chair of the Hamilton-Wentworth District School Board.
- (ii) Councillor S. Merulla declared a non-disqualifying interest to Item 14.1, respecting Report PED22045 - Property Acquisition and Exchange to Facilitate Parkland and New School, as his daughter works for the Conseil Scolaire Catholique Monavenir.
- (iii) Mayor F. Eisenberger declared a non-disqualifying interest to Item 14.1, respecting Report PED22045 - Property Acquisition and Exchange to Facilitate Parkland and New School, as his son works for the Conseil Scolaire Catholique Monavenir.

(c) APPROVAL OF MINUTES OF PREVIOUS MEETINGS (Item 4)

(i) June 15, 2022 and June 23, 2022 (Items 4.1 and 14.2)

The Minutes of the June 15, 2022 and June 23, 2022 General Issues Committee meetings were approved, as presented.

(d) DELEGATION REQUESTS (Item 6)

The following delegation requests were approved for the July 4, 2022 General Issues Committee:

- 6.2 Cesare Di Donato, Industry Education Council, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding

- 6.3 Richard Gelder, Hamilton Olympic Club, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.4 Carmella Trombetta respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.5 Jeff Anders, Aeon Studio Group, respecting Item 10.11 - Report PED19063(d)), Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands
- 6.6 Mike Strange, Two-time Commonwealth Games Gold Medalist, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.7 Mike Moore, Hamilton Challenger Baseball Association, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.9 Louis Frapporti and P.J. Mercanti, Hamilton100, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.10 Paul Paletta, Alinea Group Holdings Inc., respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.11 Shendal Yalchin, The Hamilton Club, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.12 Ed Dunn, International Children's Games, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.13 Helen Downey, SportHamilton, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.14 Michael Norris, ACFO, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding

- 6.15 Anthony Frisina, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (video)
- 6.16. Kevin Duffy, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding
- 6.17 Dean Hustwick, President and Board Chair, Athletics Ontario, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding

The delegation request submitted by Louis Frapporti and P.J. Mercanti, Hamilton100, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding, was moved up on the agenda to be heard as the first delegate.

(e) STAFF PRESENTATIONS (Item 8)

(i) Annual Update on Economic Development Action Plan (PED22104) (City Wide) (Item 8.1)

Norm Schleeahn, Director of Economic Development; and, Carrie Brooks-Joiner, Director, Tourism & Culture, provided the presentation respecting Report PED22104 - Annual Update on Economic Development Action Plan.

The presentation, respecting Report PED22104, Annual Update on Economic Development Action Plan, was received.

For disposition of this matter, please refer to Item 3.

(ii) City Manager 2021 – 2022 Review (CM22011) (City Wide) (Item 8.2)

Janette Smith, City Manager, provided a presentation respecting Report CM22011, City Manager 2021 – 2022 Review.

The presentation respecting Report CM22011, City Manager 2021 – 2022 Review, was received.

For disposition of this matter, please refer to Item 4.

The presentation and consideration of Item 8.3 - Report CM16003(e), respecting the Term of Council Priorities 2018 to 2022 Summary Report, was DEFERRED until after the consideration of Item 10.1.

(iii) Term of Council Priorities 2018 to 2022 Summary Report (CM16003(e)) (City Wide) (Item 8.3)

Janette Smith, City Manager, provided a presentation respecting Report CM16003(e), Term of Council Priorities 2018 to 2022 Summary Report.

The presentation respecting Report CM16003(e), Term of Council Priorities 2018 to 2022 Summary Report, was received.

For disposition of this matter, please refer to Item 6.

(f) PUBLIC HEARINGS / DELEGATIONS (Item 9)

(i) Louis Frapporti and P.J. Mercanti, Hamilton100, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.8)

The 5-minute speaking limit, outlined in sub-section (6), Section 5.12 – Delegations, of the Procedural by law 21-021, as amended, was waived and the delegation of Louis Frapporti and P. J. Mercanti, Hamilton100 was permitted to take the appropriate time required to provide their presentation to Committee

Louis Frapporti and P. J. Mercanti, Hamilton100, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

(ii) Cesare Di Donato, Industry Education Council, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.1)

Cesare Di Donato, Industry Education Council, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (iii) Richard Gelder, Hamilton Olympic Club, respecting Item 10.1 – Report PED19108(i), respecting the Commonwealth Games 2030 – Revised Memorandum of Understanding (Item 9.2)**

Richard Gelder, Hamilton Olympic Club, addressed the Committee respecting Item 10.1 – Report PED19108(i), respecting the Commonwealth Games 2030 – Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (iv) Carmella Trombetta respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.3)**

Carmella Trombetta addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (v) Jeff Anders and Mazyar Mortazavi, Aeon Studio Group, respecting Item 10.11 - Report PED19063(d)), Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands (Item 9.4)**

The 5-minute speaking limit, outlined in sub-section (6), Section 5.12 – Delegations, of the Procedural by law 21-021, as amended, was waived and the delegation of Jeff Anders and Mazyar Mortazavi, Aeon Studio Group, respecting Item 10.11 - Report PED19063(d)), Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands, was permitted to take an additional 5 minutes to provide their presentation to Committee.

Jeff Anders and Mazyar Mortazavi, Aeon Studio Group, addressed the Committee respecting Item 10.11 - Report PED19063(d)), Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands.

For disposition of this matter, please refer to Item 16.

- (vi) **Mike Strange, Two-time Commonwealth Games Gold Medalist, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.5)**

Mike Strange, two-time Commonwealth Games gold medalist, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (vii) **Mike Moore, Hamilton Challenger Baseball Association, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.6)**

Mike Moore, Hamilton Challenger Baseball Association, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (viii) **Paul Paletta, President, Alinea Group Holdings Inc., respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.9)**

Paul Paletta, President, Alinea Group Holdings Inc., addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (ix) **Shendal Yalchin, General Manager and Chief Operating Officer, The Hamilton Club, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.10)**

Shendal Yalchin, General Manager and Chief Operating Officer, The Hamilton Club, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (x) **Ed Dunn, President, International Children's Games Hamilton Inc., respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.11)**

Ed Dunn, President, International Children's Games Hamilton Inc., addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (xi) **Helen Downey, SportHamilton, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.12)**

Helen Downey, SportHamilton, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (xii) **Michael Norris, ACFO Hamilton, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.13)**

Michael Norris, ACFO Hamilton, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (xiii) **Anthony Frisina, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (video) (Item 9.14)**

The video from Anthony Frisina, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding, was shown to the Committee.

For disposition of this matter, please refer to Item 6.

- (xiv) **Kevin Duffy, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.15)**

Kevin Duffy addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

- (xv) **Dean Hustwick, President and Board Chair, Athletics Ontario, respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding (Item 9.16)**

Dean Hustwick, President and Board Chair, Athletics Ontario, addressed the Committee respecting Item 10.1 - Report PED19108(i), respecting the Commonwealth Games 2030 - Revised Memorandum of Understanding.

For disposition of this matter, please refer to Item 6.

All of the delegations respecting Item 10.1 - Report PED19108(i), Commonwealth Games 2030 - Revised Memorandum of Understanding; and, Item 10.11 - Report PED19063(d)), Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands, were received.

For disposition of these matters, please refer to Items 6 and 16.

The General Issues Committee recessed for one half hour until 3:05 p.m.

(g) DISCUSSION ITEMS (Item 10)

- (i) **Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands (PED19063(d)) (Ward 2) (Item 10.11)**

The following Amendment was DEFEATED:

That sub-section (k)(3) to Appendix "E" to Report PED19063(d), respecting the Memorandum of Understanding with the City and Aeon Studios on Barton-Tiffany Lands, ***be amended*** by adding the words "***options including geared to income housing***", to read as follows:

- (k) Any agreements shall have specific requirements with respect to:
 - (1) the provision of affordable space for local artists;
 - (2) residential unit mix, inclusive of family-friendly residential units;
- (3) affordable housing ***options including geared to income housing***; and,
- (4) sustainability, climate resilience, and environmental performance.

For disposition of this matter, please refer to Item 16.

Report PED19063(d), respecting the Memorandum of Understanding with the City and Aeon Studios on Barton-Tiffany Lands, ***was amended*** by adding new sub-sections (e) and (f), to read as follows:

- (e) ***That staff be directed to review opportunities for affordable housing through proceeds of the sale of the Barton-Tiffany lands and report back to the General Issues Committee; and,***
- (f) ***That staff be directed to determine a definition of "affordable housing", from the City's perspective, in the forthcoming report, respecting review opportunities for affordable housing through proceeds of the sale of the Barton-Tiffany lands.***

For disposition of this matter, please refer to Item 16.

(h) PRIVATE & CONFIDENTIAL (Item 14)

Committee moved into Closed Session to discuss Items 14.1, 14.4 and 14.5, pursuant to Section 9.1, Sub-sections (b), (c), (e), (f), and (k) of the City's Procedural By-law 21-021, as amended, and Section 239(2), Sub-sections (b), (c), (e), (f), and (k) of the *Ontario Municipal Act*, 2001, as amended, as the subject matters pertain to personal matters about an identifiable individual, including municipal or local board employees; a proposed or pending acquisition or disposition of land by the municipality or local board; litigation or potential litigation, including matters before administrative tribunals, affecting the municipality or local board; advice that is subject to solicitor-client privilege, including communications necessary for that purpose; and, a position, plan,

procedure, criteria or instruction to be applied to any negotiations carried on or to be carried on by or on behalf of the municipality or local board.

(i) Appendix "D" to Report PED19063(d), respecting the Memorandum of Understanding with City and Aeon Studios on Barton-Tiffany Lands (Item 14.3)

For disposition of this matter, please refer to Item 16.

(ii) City Manager 2021-2022 Review (Item 14.5)

There was no information to report in Open Session respecting the City Manager's 2021-2022 review.

(h) ADJOURNMENT (Item 15)

There being no further business, the General Issues Committee adjourned at 7:14 p.m.

Respectfully submitted,

Esther Pauls, Deputy Mayor
Chair, General Issues Committee

Stephanie Paparella
Legislative Coordinator,
Office of the City Clerk

MEMORANDUM OF UNDERSTANDING
(the "MOU")

BETWEEN:

CITY OF HAMILTON
(the "City")

-AND-

HAMILTON100 COMMONWEALTH GAMES BID CORPORATION
(**"Hamilton100"**)

(Each a "**Party**" and collectively the "**Parties**")

WHEREAS the City is a municipality created by the *City of Hamilton Act*, 1999, S.O. 1999, c.14, Sched. C;

AND WHEREAS Hamilton100 is a corporation incorporated under the *Canada Not-for-profit Corporations Act*, S.C. 2009, c.23 and was created for the purpose of organizing, facilitating, preparing, financing and submitting a hosting proposal to secure the 2030 Commonwealth Games in Ontario (the "**Proposal**");

AND WHEREAS the Parties previously entered into a memorandum of understanding dated October 3, 2019, as amended, which established areas of collaboration between the Parties in relation to the initial development of the Proposal;

AND WHEREAS Hamilton100 has subsequently submitted the Proposal to key Commonwealth Games 2030 stakeholders, including the Provincial Government of Ontario and the Federal Government of Canada, in order to commence the Multi-Party Agreement negotiation process;

AND WHEREAS Hamilton100 wishes for the City to become a key stakeholder in the further development and promotion of the Proposal;

AND WHEREAS on May 25, 2022, the Council of the City of Hamilton ("**Council**") authorized and directed City staff to negotiate a revised draft memorandum of understanding with Hamilton100 for Council's review and consideration.

AND WHEREAS the Parties acknowledge and agree that the City shall execute this MOU subject to the following conditions: (i) that the City shall not assume any liability of any kind, be it legal, financial or otherwise, in relation to the Proposal and (ii) that this MOU shall not be legally binding upon the City;

NOW THEREFORE, in consideration of the foregoing, the covenants contained herein, and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged by both Parties, the City and Hamilton100 agree as follows:

1. PRIMARY OBJECTIVES AND TIMELINES

1.1. The Primary objectives of this MOU are as follows:

- (a) to confirm Hamilton100's commitment to lead the development of the Proposal for a potential Canadian bid in relation to the 2030 Commonwealth Games;
- (b) to establish the scope of work Hamilton100 are assuming in relation to the development of the Proposal; and
- (c) to establish the role and resources required from the City to support the evolution of the Proposal.

1.2. The Parties further acknowledge and agree that the Proposal shall:

- (a) celebrate Hamilton's legacy with the Commonwealth Games and, particularly, the 100th anniversary of the initial British Empire Games held in Hamilton in 1930;
- (b) align with the City's vision ("To be the best place to raise a child and age successfully"), plans, values and priorities including, in particular, its:
 - (i) commitment to equality, diversity, accessibility, tolerance, Green/climate change initiatives, social procurement and social housing; and
 - (ii) Strategic Plan and Recreational Indoor/Outdoor Master Plan;
- (c) enhance Hamilton's reputation, identity and civic pride;
- (d) maximize investment in Hamilton wherever possible;
- (e) utilize existing facilities in Hamilton where appropriate;
- (f) engage in public/private partnerships where appropriate; and
- (g) identify and create long-term legacy projects, and supportive public and private stakeholders, that will facilitate serving the needs of the Hamilton community.

2. CITY OBLIGATIONS

The City shall:

- (a) advocate for provincial and federal financial support for the Proposal as part of the City's government relations activities; and

- (b) provide Hamilton100 with an updated letter of endorsement, signed by the Mayor, to host the 2030 Commonwealth Games which acknowledges that the City may be a financial contributor towards the planning, delivery and legacies of the 2030 Commonwealth Games. The City acknowledges that such letter shall be added to the Proposal.

3. HAMILTON100 OBLIGATIONS

Hamilton100 Shall:

- (a) continue to engage with Commonwealth Sport Canada and other public and private stakeholders to advance the primary objectives and timelines under this MOU;
- (b) advocate for provincial and federal financial support for the Proposal, and
- (c) promptly communicate with the City on matters relevant to the advancement of the primary objectives and timelines under this MOU.

4. COMMUNICATIONS

Hamilton100 and the City shall jointly develop and enter into a communications protocol satisfactory to both Parties. The communications protocol will identify Hamilton100 as the lead agency for the development and delivery of bid communications and outline how and when the City will promote Hamilton100 communications on its channels for major announcements and milestones, which shall include the use of the City's social media channels, the City's website and digital/physical assets used by the City for promotional purposes.

5. PRINCIPAL CONTACTS

The principal contacts for each Party are:

City of Hamilton:

Mike Zegarac
General Manager Finance and Corporate Services
71 Main St W 1st Floor
Hamilton, ON L8P 4Y5
(905) 546-2424 x6150

Hamilton100 Commonwealth Games Bid Corporation:

PJ Mercanti
President
Suite 300 - 77 James Street North
Hamilton, ON L8K 2K3
(905) 383-4100

Each Party shall immediately advise the other Party in writing of any change in the contact information for its principal contacts, from time to time.

6. EFFECTIVE DATES

This MOU shall take effect on July 8, 2022 (the "**Effective Date**") and shall remain in effect until the earlier of:

- (a) the execution of a Multi-Party Agreement relating to the hosting of the Commonwealth 2030 Games between the City, Hamilton100, Commonwealth Sport Canada, the Provincial Government of Ontario and the Federal Government of Canada;
- (b) either the Provincial Government of Ontario or the Federal Government of Canada declines to participate in the negotiation of a Multi-Party Agreement relating to the hosting of the Commonwealth 2030 Games; or
- (c) either Party provides written notice of termination of this MOU to the other Party.

7. NO JOINT VENTURE/NON-ENFORCEABILITY/ENTIRE AGREEMENT

The Parties agree that, notwithstanding this MOU, they are not entering into a partnership, joint venture, agency relationship or any other business arrangement, nor is the intent of the Parties to enter into a commercial undertaking for monetary gain. Neither Party shall be granted the authority under this MOU to bind the other Party.

For greater certainty, and consistent with the advice provided by Commonwealth Sport Canada, the Parties acknowledge and agree that the City shall execute this MOU subject to the following conditions: (i) that the MOU is not legally binding upon the City and that if the 2030 Commonwealth Games are awarded to Canada then the respective obligations of all parties shall be codified in a subsequent agreement, subject to the mutual agreement of all parties, and (ii) that the City shall not assume any liability of any kind, be it legal, financial or otherwise, in relation to or arising from the Proposal and/or this MOU.

The Parties acknowledge and agree that this MOU sets forth the entire understanding of the Parties with respect to the subject matter contained herein and supersedes all previous memorandums of understanding, agreements or understandings between the Parties with respect to the subject matter herein, whether written, oral, expressed or implied.

[SIGNATURE PAGE TO FOLLOW]

IN WITNESS WHEREOF the Parties have executed this MOU as of the Effective Date.

HAMILTON100 COMMONWEALTH GAMES BID CORPORATION

Per: _____
Name: PJ Mercanti
Title: President
I have the authority to bind Hamilton100.

CITY OF HAMILTON

Per: _____
Name:
Title:
I/We have the authority to bind the Corporation.

Accessible Open Spaces and Parklands Working Group
Work Plan (Working Group Guidelines)
Draft 2
March 23, 2022

Purpose

The Accessible Open Spaces and Parklands Working Group (OSPWG) supports the work of the City of Hamilton’s Advisory Committee for Persons with Disabilities (ACPD) by providing guidance to the Committee as it advises City Council on matters of accessibility and compliance with the Accessibility for Ontarians with Disabilities Act. Specifically, the working group focuses on open spaces, parklands and all components or facilities contained therein not regulated by statute or addressed under other ACPD working groups, except where specified.

Scope

The OSPWG scope shall include, but not necessarily limited to:

- City owned or leased open spaces, parklands and facilities contained therein;
- Parks, sports parks and outdoor recreational areas including play areas and play area structures;
- Outdoor recreational pools, splash pads and other related water facilities;
- Trails;
- Golf Courses;

- Outdoor dining sites including dining which encroaches sidewalks;
- Any AODA regulation insufficient for which upgraded accessibility is deemed necessary by ACPD or City staff;
- Any other outdoor spaces not specifically cited herein or at the pleasure of the ACPD or City Council and staff.

Outdoor spaces NOT typically in scope include, and not necessarily limited to:

- Sidewalks;
- Parking lots;
- Roads and highways;
- New building construction and related site plans;
- Any AODA regulations deemed sufficient by ACPD or City staff.

Meetings

Meetings shall be held the fourth Wednesday of each month from 1:30 p.m. – 3:30 p.m.

Meetings to be held at City Hall, when in person, or virtually via the City’s supported virtual meeting platform.

City staff, those who support the work of the ACPD, shall make arrangements for any virtual meetings or arrange meeting space at City Hall whatever is appropriate month to month.

Activities

Advise staff with the design of any open space and parkland facilities which comprise any part of City owned or leased open spaces and parklands as may be necessary.

Advise on the development of a range of accessible standard features regularly incorporated as part of many, or most parks and open spaces for staff reference when designing new projects.

Respond to any questions or concerns regarding open spaces and parklands which may be raised by the ACPD

Respond to any questions or concerns raised by City Council or staff as they relate to open spaces and parklands.

Review, as requested by staff, open space design standards arising from the AODA to determine suitability or need for upgrade.

Review any current open spaces under design as requested by staff or as may be permissible under the AODA.

Make any recommendations to staff or City Council respecting open spaces and parklands as may be necessary from time to time, even where such recommendations may be controversial.

Communicate with ACPD for referral to City Council any concerns regarding perceived contravention of the AODA in the design of any open spaces or parklands.

Communicate with any other ACPD working group where clear overlap exists, or in response to inquiries for information.

Framework of City Conditions for Disposition of Barton-Tiffany Lands

Location:	Lands as identified in the Barton-Tiffany Area with Appendix “A” to Report PED19063(d).
Parties:	City of Hamilton (the “City”); AEON Studio Group Inc. and its partner companies (collectively the “Studios”).
Objectives:	Creation of Studio District (Film and Studio TV Production and related supports, and local artistic and creative industry centre/hub).
Value Considerations:	All financial transactions to be based on defined “Highest and Best Use”, or “Fair Market Value” financial considerations
Key Terms and Conditions:	<ul style="list-style-type: none"> (a) Lands to be developed in a manner that is substantially consistent with the development concept as provided to the City on February 28, 2022 and the Memorandum of Understanding; (b) Disposition transaction of parcels owned, fee-simple, by the City in the areas as identified on the map attached as Appendix “A” to Report PED19063(d).; (c) There will be no representations and/or warranties by the City whatsoever with respect to any of the lands being sold and that the lands are being sold and purchased on an “as is”, “where is” basis subject to and inclusive of easements and rights-of-way (if any) held by the City and as may be necessary towards an approved development by the Studios; (d) The City will not make any representations or warranties with respect to the suitability of any of the lands for the contemplated development as outlined in Report PED19063(d) or representation, warranty, condition, as to soil or other environmental conditions, utilities, fitness for purpose, zoning and building by-laws, parkland, road widenings, or other possible dedications, or as to charges, levies, and regulations of the City, Utilities or other Regulatory Authorities; (e) Responsibility for ascertaining the suitability of any of the lands to rest solely with the purchaser; (f) The purchaser is to assume any and all risks relating to the physical condition of any of the lands including any and all geotechnical and

	<p>environmental liabilities relating to any of the lands that may exist, including but not limited to any liability for clean-up of any hazardous substances on or under the lands;</p> <p>(g) Agreements to include the City’s standard registered conditions that follow on closing towards a purchaser’s obligation to develop, including:</p> <ul style="list-style-type: none"> (1) To immediately after closing, seek the formal approval of the City as a municipal authority for the development and use which shall be in the nature of and in keeping with the contemplated development outlined in Report PED19063(d) and use set out in an agreed upon Master Plan; (2) To commence any construction and landscaping required by virtue of the formal approvals within a set time (i.e. 30 months) of the closing of a transaction, subject only to force majeure and delays attributable to the City as a municipal authority in providing approvals; (3) Completion of the construction and landscaping within a set time (i.e. 54 months) of the closing of a transaction subject only to force majeure and delays attributable to the City as a municipal authority in providing approvals; <p>(h) In the event of termination or default on development and/or disposition agreements, the City shall have the right of first opportunity to re-purchase the respective lands (as identified in the Barton-Tiffany Area with Appendix “A” to Report PED19063(d));</p> <p>(i) The Studios shall substantially develop and construct the non-residential elements of the Studio District (Film and TV Studio District) before any residential development; and,</p> <ul style="list-style-type: none"> (1) No transfer of a property may be made by the purchaser until the City confirms that the registered covenants to commence and complete the contemplated non-residential development have been complied with;
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	<p>(2) That any contemplated development outlined in Report PED19063(d) pursuant to the approved Master Plan will not require any amendments to the City’s applicable zoning by-law or Official Plan for the development outlined in Report PED19063(d), without the unfettered consent, in its sole discretion, of the City; and,</p> <p>(3) That, should the purchaser, prior to completing construction, and having received the consent of the City, successfully obtains incremental improvements to land use permissions, then the City shall be granted additional reasonable consideration, either financially or equivalent social or other benefit, in accordance to the value uplift.</p> <p>(j) Any agreements contemplated herein are:</p> <ul style="list-style-type: none"> (1) subject to Council approval, and (2) shall not fetter the City’s Municipal discretion and decision-making authorities. <p>(k) Any agreements shall have specific requirements with respect to:</p> <ul style="list-style-type: none"> (1) the provision of affordable space for local artists; (2) residential unit mix, inclusive of family-friendly residential units; (3) affordable housing;and, (4) sustainability, climate resilience, and environmental performance.
<p>Development Framework:</p>	<p>As contemplated in this Report PED19063(d) and proposed by the Studios towards any agreed upon Master Plan.</p>

ACPD Accessibility Fair Budget As Of 27-06-2022

Operational Costs and Equipment

50 x 8' Tables	\$12.00 Per Unit	\$600.00
70 Basic Folding Chairs	\$2.00 Per Unit	\$140.00
30 Padded Chairs	\$4.88 Per Unit	\$146.40
12 Retractable stanchions	\$19.69 Per Unit	\$236.28
Delivery and Pick Up Fee		\$69.99
1 Roll of Caution Tape	\$12.99 Per Unit	\$12.99
Parking Permits for Buses		\$62.09
Food/Drink for 50 volunteers	\$11.00 Per Unit	\$550.00
Equipment to Set Up Activities *		\$250.00
Liability Insurance up to \$5 Million		\$500.00
Taxes		\$333.08
Total		\$2900.83

Advertising

Website Address and Support	\$106.22
Banner Reservation Fee	\$386.00
Banner Cost	\$1200.00
Badges for Volunteers and Members	\$114.58
Additional Signage*	\$400.00

Taxes	\$286.88
Total	\$2493.68

Food Service

Inspection Costs Per Vendor	\$41.80
Incidental Costs for Food Service Upset Limit*	\$500.00
Total	\$541.80

Grand Total	\$5936.31
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The figures are representing a maximum requirement and are set with an upset limit. I doubt it will cost as much to run the event, but I don't want to shortchange us when we need it.

The asterisked items are contingency funds in an effort to cover as yet unforeseen expenses like additional signage.

The Liability Insurance will cover the entire event and protect us in case of an issue. It seems reasonably priced for \$5 Million in Liability Protection. As it was only an estimate, I added 20% to the number in case of overages.

The incidental cost for food service is there in case we need to provide extension cords, hoses or anything else not yet quoted.



**PLANNING COMMITTEE
REPORT
22-011**

July 5, 2022

9:30 a.m.

**Council Chambers, Hamilton City Hall
71 Main Street West**

Present: Councillor B. Johnson (Chair)
Councillor L. Ferguson (1st Vice Chair),
Councillors J.P. Danko, J. Partridge, J. Farr, and M. Pearson

Absent with Regrets: Councillor M. Wilson - Personal

THE PLANNING COMMITTEE PRESENTS REPORT 22-011 AND RESPECTFULLY RECOMMENDS:

1. Hamilton-Oshawa Port Authority - City of Hamilton Liaison Committee Report 22-002 (Item 7.1)

1. Committee Terms of Reference Review

That the Hamilton-Oshawa Port Authority Liaison Committee Terms of Reference be amended as follows:

- (a) That the Mandate be deleted in its entirety and replaced with new wording, as follows:

~~To liaise on real estate and planning matters of mutual interest within the West Harbour, Bayfront, and waterfront areas by:~~

- ~~• Ensuring that development on Port lands is compatible with both the economic development of the Port and the interest of the City;~~
- ~~• Ensuring effective public consultation on significant decisions relating to the Port and adjacent City lands;~~
- ~~• Promotion of the business in the Port;~~
- ~~• Enhancement of the marine environment;~~
- ~~• Provision of access to the waterfront and activities of the Port for citizens where this is consistent with the work of the Port;~~

- ~~Effective management of the use of the harbour, including recreational use where this is consistent with the work of the Port;~~
- ~~Provision of effective road and rail access and utility services for the efficient operations of the Port;~~
- ~~Provision of access to water and necessary City utility services; and,~~
- ~~Provision of effective policing and emergency services in the Port and adjacent area of the City, including emergency planning and disaster response.~~

To share information and discuss matters of mutual interest within the West Harbour, Bayfront, and waterfront areas including:

- Development and investment on Port lands that supports the economic development of the Port and the interests of the City;
- Public consultation on significant decisions relating to the Port and adjacent City lands;
- Promotion of the Port of Hamilton as part of the economic development of the City;
- Enhancement of the marine environment;
- Provision of access to the waterfront and activities of the Port for citizens where this is compatible with the work of the Port;
- Effective management of the use of the harbour, including recreational use where this is compatible with the work of the Port;
- Effective road and rail access and utility services for the efficient operations of the Port;
- Access to water and necessary City utility services;
- Security and emergency services as it relates to the Port and adjacent area of the City, including emergency planning and disaster response;
- Initiatives to mitigate and adapt to the impacts of climate change;
- Joint government relations strategies; and
- Establish principles to guide any new agreements or MOUs between the two parties.

(b) That the Composition be amended as follows:

The Mayor and three Members of City Council (4)
Hamilton-Oshawa Port Authority Board Chair and ~~two~~ three members from the Board or senior staff ~~(3)~~ (4)

(c) That the Support Staff be amended as follows:

City Manager and Hamilton-Oshawa Port Authority ~~Director~~ President and CEO

2. Ontario Building Code (OBC) Review of Microbrewery Occupancies (PED22094) (City Wide) (Item 7.2)

That Report PED22094 respecting Ontario Building Code (OBC) Review of Microbrewery Occupancies, be received.

3. Active Official Plan Amendment, Zoning By-law Amendment, and Plan of Subdivision Applications (PED22151) (City Wide) (Item 7.3)

That Report PED22151 respecting Active Official Plan Amendment, Zoning By-law Amendment, and Plan of Subdivision Applications, be received.

4. Planning and Economic Development Department (PED) Power Platform Portal (PED22163) (City Wide) (Item 7.4)

That the Chief Building Official be authorized to use up to \$700,000 from the Building Permit Fee Reserve (104050) to support the development of online services for the Building Division on a Microsoft Power Platform portal for the Planning and Economic Development Department (PED).

5. Application for a Zoning By-law Amendment for Lands Located at 1540 Upper Wentworth Street, Hamilton (PED22149) (Ward 7) (Item 9.1)

(a) That Revised Zoning By-law Amendment Application ZAC-21-023 by T. Johns Consulting Group Ltd. c/o Katelyn Gillis on behalf of Hamilton East Kiwanis Non-Profit Homes Inc. c/o New Commons Development, owner, for a change in zoning from "RT-20" (Townhouse - Maisonette) District to "RT-20/S-1823" (Townhouse - Maisonette) District, Modified (Block 1) and "E-2/S-1823-H" (Multiple Dwellings) District, Modified, Holding (Block 2) in order to permit the demolition of two blocks of existing townhouses for the development of an eight storey, 126 unit multiple dwelling on a portion of the lands located at 1540 Upper Wentworth Street, as shown on Appendix "A" attached to Report PED22149, be APPROVED on the following basis.

- (i) That the draft By-law, attached as Appendix "B" to Report PED22149, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;
- (ii) That the amending By-law apply the Holding Provisions of Section 36(1) of the Planning Act, R.S.O. 1990 to the subject lands by introducing the Holding 'H' as a suffix to the proposed zoning for 1540 Upper Wentworth Street as shown on Schedule "A" of Appendix "B" attached to Report PED22149.

The Holding Provision "E-2/S-1823-H" (Multiple Dwellings) District, Modified, Holding, be removed conditional upon:

- 1) An acceptable tenant relocation and assistance plan addressing the right to return to occupy the replacement housing at similar rents, the provision of alternative accommodation at similar rents, and other assistance to lessen the hardship, is provided to the satisfaction of the Director of Housing Services and the Director of Planning and Chief Planner;
 - (iii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2019, as amended) and complies with the Urban Hamilton Official Plan (UHOP);
 - (b) That upon finalization of the amending By-law, the subject lands be re-designated from “Attached Housing” to “High Density Apartments” in the Barnstown Neighbourhood Plan.
 - (c) That the public submissions regarding this matter were received and considered by the Committee.
- 6. Urban Hamilton Official Plan Amendment (UHOPA-21-019) and Zoning By-law Amendment (ZAC-21-041) Applications for the Lands Located at 510 Centennial Parkway North (PED22129) (Ward 5) (Item 9.3)**
- (c) That the public submissions regarding this matter were received and considered by the Committee.
- 7. Application for a Zoning By-law Amendment for Lands Located at 60 Caledon Avenue, Hamilton (PED22143) (Ward 8) (Item 9.4)**
- (a) That Revised Zoning By-law Amendment Application ZAC-21-025 by GSP Group Inc. c/o Brenda Khes on behalf of Victoria Park Community Homes Inc. and Hamilton East Kiwanis Non-Profit Homes Inc., owners, for a change in zoning from the Community Institutional (I2) Zone to the Community Institutional (I2, 810, H127) Zone, to permit the lands to be developed for two, six storey, multiple dwellings and five, three storey, maisonette (back-to-back townhouse) buildings on lands located at 60 Caledon Avenue, as shown as on Appendix “A” attached to Report PED22143, be APPROVED on the following basis:
 - (i) That the draft By-law, attached as Appendix “B” attached to Report PED22143, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;
 - (ii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow:

Growth Plan for the Greater Golden Horseshoe (2019, as amended) and complies with the Urban Hamilton Official Plan (UHOP);

- (b) That the amending By-law apply the Holding Provision of Section 36(1) of the Planning Act, R.S.O. 1990 to the subject lands by adding the Holding symbol H127 to Schedule D of City of Hamilton Zoning By-law No. 05-200; and, that it shall be lifted upon completion of the following:
 - (i) That the Owner submits and receives approval of an updated Functional Servicing Report and Stormwater Management Report and related drawings to demonstrate that adequate capacity exists in the existing municipal sanitary sewer downstream of the site to support the proposed development, to the satisfaction of the Director of Growth Management;
 - (c) That upon finalization of the amending By-law, the subject lands be re-designated from “Civic and Institutional” to “Medium Density Apartments” in the Yeoville Neighbourhood Plan.
 - (d) That the public submissions regarding this matter were received and considered by the Committee.

8. Application for Zoning By-law Amendment for Lands Located at 221 and 223 Charlton Avenue East and 200 Forest Avenue, Hamilton (PED22144) (Ward 2) (Item 9.5)

- (a) That Revised Zoning By-law Amendment Application ZAC-21-020, by T. Johns Consulting Group on Behalf of Corktown Co-operative Homes Inc. and Charlton Housing Co-operative Homes, Owner, to change the zoning from the “DE-3/S-970” (Multiple Dwellings) District, Modified (Block 1) and “D” (Urban Protected Residential – One and Two Family Dwellings, Etc.) District (Block 2) to the “DE-3/S-1819” (Multiple Dwellings) District, Modified, to permit a three storey, 17 unit multiple dwelling to be constructed on the same lot as an existing four storey, 51 unit multiple dwelling, on lands located at 221 and 223 Charlton Avenue East and 200 Forest Avenue, Hamilton, as shown on Appendix “A” attached to Report PED22144, be APPROVED on the following basis:
 - (i) That the draft By-law, attached as Appendix “B”, as amended, to Report PED22144, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;

That Appendix “B” to Report PED22144 be amended by deleting sub-section 3 (c) and adding sub-sections 3 (c) and (d) as follows:

- ~~(c) In addition to Section 10C(2), the four storey building height of the multiple dwelling existing on the date of the passing of this By-law shall be permitted.~~
 - (c) Notwithstanding Section 10C(2), no building shall exceed three storeys or 11.5 metres.
 - (d) In addition to Section 3.c) above, the four storey building height of the multiple dwelling existing on the date of the passing of this By-law shall be permitted.
 - (ii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019, as amended, and complies with the Urban Hamilton Official Plan;
 - (b) That upon finalization of the amending By-law, that the lands located at 221 and 223 Charlton Avenue East be re-designated from “Single and Double” to “Medium Density Apartments” in the Corktown Neighbourhood Plan.
 - (c) That the public submissions regarding this matter were received and considered by the Committee.
- 9. Application for Official Plan Amendment for lands located at 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue, Hamilton (PED22153) (Ward 4) (Item 9.6) (Item 9.6)**
- (a) That amended Urban Hamilton Official Plan Amendment Application UHOPA-22-011, by Roxborough Park Inc., Owner, to amend Site Specific Policy UHN-25 to delete the maximum net residential density of 165 units per hectare and to require that increases in height and density be implemented through a Zoning By-law Amendment in accordance with applicable servicing and design criteria, for the lands located at 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue, as shown on Appendix “A” attached to Report PED22153, be APPROVED on the following basis:

 - (i) That the draft Official Plan Amendment (OPA), attached as Appendix “B”, as amended, to Report PED22153, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;

- (a) That the draft Official Plan Amendment (OPA), attached as Appendix “B” to Report PED22153 be amended to add an additional policy to Site Specific Policy UHN-25 as follows:
 - Notwithstanding Policy e) of Site Specific Policy UHN-25, a modification to the zoning shall be considered minor and implementable through a minor variance for any development with a building height greater than 12 storeys, but not more than 18 storeys, and / or that results in a net residential density greater than 165 units per hectare, but not more than 254 units per hectare.”
- (ii) That the proposed Official Plan Amendment (OPA) is consistent with the Provincial Policy Statement (2020) and conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe 2019, as amended.

(b) That there were no public submissions received regarding this matter.

10. Draft Amendments to the Urban Hamilton Official Plan and Rural Hamilton Official Plan to Implement Bill 13 and Bill 109 (PED22112(b)) (City Wide) (Item 10.1)

- (a) That staff be directed and authorized to undertake public and stakeholder consultation on the proposed amendments to the Urban Hamilton Official Plan and Rural Hamilton Official Plan to implement Bill 13 and Bill 109;
- (b) That staff report back to Planning Committee summarizing input received from the public and stakeholder consultation with recommended amendments to the Urban Hamilton Official Plan and Rural Hamilton Official Plan in August 2022.

11. Licensing Short-Term Rental (STR) Accommodations (PED17203(b)) (City Wide) (Item 10.2)

- (a) That Report PED17203(b), Licensing Short-Term Rentals (STR), respecting the results of the on-line public survey, public consultation and Staff preliminary findings for a recommended approach for licensing and regulating Short-Term Rentals in Hamilton, be received;
- (b) That the amendment to the Licensing By-law 07-170 with respect to Short-Term Rental (STR) Accommodations described in Report PED17203(b) and detailed in the proposed amending By-law, attached as Appendix “A” to Report PED17203(b), be received and be brought back to a future Planning Committee meeting;

- (c) That Planning Staff be directed to report back to Planning Committee with the necessary zoning changes relevant to short-term rentals to address historical zoning by-law definitions that are no longer applicable or that are redundant, and that appropriate public notice be provided.

12. Applications for Amendments to the Urban Hamilton Official Plan and Zoning By-law No. 05-200 for Lands Located at 442, 450, 454 and 462 Wilson Street East (Ancaster) (PED22037) (Ward 12) (Deferred from the February 15, 2022 meeting) (Item 10.3)

- (a) That Urban Hamilton Official Plan Amendment Application UHOPA-21-023, by GSP Group Inc. (c/o Brenda Khes, Applicant) on behalf of 2691893 Ontario Inc. (c/o IronPoint Capital Management Inc., Owner) to establish a Site Specific Policy Area in the Ancaster Wilson Street Secondary Plan to permit a seven storey retirement home with 211 beds and four commercial units, or permit a six storey, 161 unit mixed use building with seven commercial units, on lands located at 442, 450, 454 and 462 Wilson Street East, Ancaster, as shown on Appendix "A" attached to Report PED22037, be DENIED on the following basis:
 - (i) That the proposed amendment does not meet the general intent of the Urban Hamilton Official Plan and the Ancaster Wilson Street Secondary Plan with respect to building height, scale, massing, privacy, overlook, compatibility, and enhancing the character of the existing neighbourhood;
 - (ii) That the proposal is not considered to be good planning and is considered an over development of the site;
- (b) That Zoning By-law Amendment Application ZAC-21-049, by GSP Group Inc. (c/o Brenda Khes, Applicant) on behalf of 2691893 Ontario Inc. (c/o IronPoint Capital Management Inc., Owner) to further modify the Mixed Use Medium Density - Pedestrian Focus (C5a, 570) Zone in order to permit a seven storey retirement home with 211 beds and four commercial units, or permit a six storey, 161 mixed use building with seven commercial units, on lands located at 442, 450, 454 and 462 Wilson Street East, Ancaster, as shown on Appendix "A" attached to Report PED22037, be DENIED on the following basis:
 - (i) That the proposed change in zoning does not meet the general intent of the Urban Hamilton Official Plan and the Ancaster Wilson Street Secondary Plan with respect to setbacks, building height, and massing;
 - (ii) That the proposal is not considered to be good planning and is considered an over development of the site.

13. Demolition Permit for 15 Church Street, Ancaster (Item 11.1)

WHEREAS, the subject property on Church Street in Ancaster has a home that has been vacant and boarded up for years;

WHEREAS, demolition permits are being requested to alleviate ongoing trespass and vandalism issues associated with these vacant structures while planning issues are being resolved;

THEREFORE, BE IT RESOLVED:

That the Chief Building Official be authorized to issue a demotion permit for 15 Church Street, Ancaster, in accordance with By-law 09-208, as amended by By-law 13-185, pursuant to Section 33 of the Planning Act as amended, without having to comply with the conditions 6(a), (b), and (c) of the Demolition Control By-law 09-208.

14. Demolition Permits for 179, 183 and 187 Wilson Street West, Ancaster (Item 11.2)

WHEREAS, the subject properties at 179, 183 & 187 Wilson Street West in Ancaster are owned by Wilson West Development Corporation;

WHEREAS, these three properties are vacant and awaiting final site plan approval once the lot is registered as 1 parcel;

WHEREAS, demolition permits are being requested to allow for servicing;

WHEREAS, demolition will alleviate ongoing trespass and vandalism issues associated with these vacant structures while planning issues are being resolved.

THEREFORE BE IT RESOLVED:

That the Chief Building Official be authorized to issue a demotion permit for 179, 183 & 187 Wilson Street West in Ancaster, in accordance with By-law 09-208, as amended by By-law 13-185, pursuant to Section 33 of the Planning Act as amended, without having to comply with the conditions 6(a), (b), and (c) of the Demolition Control By-law 09-208.

15. Minor Variance Request for 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue (Added Item 11.3)

That the Council of the City of Hamilton provide authorization to the Roxborough Group to apply for minor variances to the zoning by-law for lands located at 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2,

4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue, as shown on Appendix "A" attached to Report PED22153, on the following basis:

- (i) That the minor variances provide for a development of not more than 18 storeys and that this increase in height apply only to the lands identified as Block 6 as shown on Appendix "C" attached to Report PED22153.
- (ii) That the minor variances provide for a development having a density of no more than 254 units per hectare across the entire Roxborough site.
- (iii) That the minor variances amend the existing Holding Provision, H74 as required, to provide for a development of no more than 18 storeys on Block 6 and a density of no more than 254 units per hectare across the entire Roxborough site.
- (iv) That the minor variance include a condition of approval applicable to Block 6 restricting development exceeding 12.0 metres in height until such time as an Urban Design Brief, Visual Impact Assessment, Shadow Impact Study and Noise Study have been submitted and implemented to the satisfaction of the Chief Planner.
- (v) That the minor variance include a condition of approval applicable to Block 6 restricting development exceeding 12.0 metres in height, and restricting the overall density of the Roxborough site to not more than 165 units per hectare, until such time as the applicant has submitted the necessary studies to demonstrate the availability of water, wastewater and storm water capacity to the satisfaction of the Director of Growth Management.

16. Appeal to the Ontario Land Tribunal (OLT) for Lack of Decision on Zoning By-law Amendment (ZAC-18-049) and for Lack of Decision on Draft Plan of Subdivision Application (25T-202109) for Lands located at 860 and 884 Barton Street, (Stoney Creek) (OLT-22-003075/OLT-22-003076) (LS22022/PED22138) (Ward 10) (Item 14.1)

- (a) That the directions to staff respecting Report LS22022/ PED22138 be released to the public, following approval by Council;
- (b) That the balance of Report LS22022/PED22138 remain confidential.

16. Appeal to the Ontario Land Tribunal (OLT) for Lack of Decision on Zoning By-law Amendment Application ZAC-20-043 and for Lack of Decision on Draft Plan of Subdivision Application (25T-202009) for Lands located at 262 McNeilly Road and 1036, 1038, 1054 and 1090 Barton Street, Stoney Creek (OLT-21-001725) (LS22023/PED22022(a)) (Ward 10) (Added Item 14.2)

- (a) That the directions to staff respecting Report LS22023/ PED22022(a) be released to the public, following approval by Council; and

- (b) That the balance of Report LS22023/PED22022(a) remain confidential.

FOR INFORMATION:

(a) APPROVAL OF AGENDA (Item 2)

The Committee Clerk advised of the following changes to the agenda:

1. PUBLIC HEARINGS/ DELEGATIONS (Item 9)

- 9.1 Application for a Zoning By-law Amendment for Lands Located at 1540 Upper Wentworth Street, Hamilton (PED22149) (Ward 7)
- a. Added Written Submissions
 - (i) Peter Jensen
 - (ii) David Hood
 - (iii) Richard and Janet Keating
- 9.2 Applications for Amendments to the Urban Hamilton Official Plan, Stoney Creek Zoning By-law No. 3692-92, and Hamilton Zoning By-law No. 05-200, and Draft Plan of Subdivision for Lands Located at 11, 19, 20, 21, 23, 27 and 30 Lakeside Drive and 81 Waterford Crescent, Stoney Creek (PED22150) (Ward 10)
- a. Added Written Submissions
 - (ii) Premalal Galagoda
 - b. Added Registered Delegations
 - (ii) Premalal Galagoda (in person)
 - (iv) Alaa Yousif (virtual)
 - (v) David Neligan (virtual)
- 9.3 Urban Hamilton Official Plan Amendment (UHOPA-21-019) and Zoning By-law Amendment (ZAC-21-041) Applications for the Lands Located at 510 Centennial Parkway North (PED22129) (Ward 5)
- b. Added Registered Delegations
 - (i) Lynda Lukasik, Environment Hamilton (virtual)

9.4 Application for a Zoning By-law Amendment for Lands Located at 60 Caledon Avenue, Hamilton (PED22143) (Ward 8)

a. Added Written Submissions

(i) Rosalind Minaji

2. DISCUSSION ITEMS (Item 10)

10.2 Licensing Short-Term Rental (STR) Accommodations (PED17203(b)) (City Wide)

a. Added Written Submissions

(i) Lynda Lukasik, Environment Hamilton

3. PRIVATE AND CONFIDENTIAL (Item 14)

14.2 Appeal to the Ontario Land Tribunal (OLT) for Lack of Decision on Zoning By-law Amendment ZAC-20-043 and for Lack of Decision on Draft Plan of Subdivision Application (25T-202009) for Lands located at 262 McNeilly Road and 1036, 1038, 1054 and 1090 Barton Street, Stoney Creek (OLT-21-001725) (LS22023/PED22022(a)) (Ward 10)

The agenda for the July 5, 2022 Planning Committee meeting was approved, as amended.

(b) DECLARATIONS OF INTEREST (Item 3)

Councillor Pearson declared a disqualifying interest with Item 9.1, Application for a Zoning By-law Amendment for Lands Located at 1540 Upper Wentworth Street, Hamilton (PED22149) (Ward 7) as she is a landlord of rental properties.

(c) APPROVAL OF MINUTES OF PREVIOUS MEETING (Item 4)

(i) June 14, 2022 (Item 4.1)

The Minutes of the June 14, 2022 meeting were approved, as presented.

(d) PUBLIC HEARINGS / DELEGATIONS (Item 9)

In accordance with the *Planning Act*, Chair Johnson advised those viewing the meeting that the public had been advised of how to pre-register to be a delegate at the Public Meetings on today's agenda.

In accordance with the provisions of the *Planning Act*, Chair Johnson advised that if a person or public body does not make oral submissions at a public meeting or make written submissions to the Council of the City of Hamilton before Council makes a decision regarding the Development applications before the Committee today, the person or public body is not entitled to appeal the decision of the Council of the City of Hamilton to the Ontario Land Tribunal, and the person or public body may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

(i) Application for a Zoning By-law Amendment for Lands Located at 1540 Upper Wentworth Street, Hamilton (PED22149) (Ward 7) (Item 9.1)

Michael Fiorino, Planner II, with the Planning and Economic Development Department, addressed the Committee with the aid of a PowerPoint presentation.

The staff presentation was received.

Terri Johns and Katelyn Gillis from T. Johns Consulting, were in attendance and indicated support for the staff report.

The delegation from Terri Johns and Katelyn Gillis with T. Johns Consulting, was received.

The Chair asked whether there were any individuals in attendance who wanted to speak to this matter.

The following individuals apprised the Chair of their intention to speak to this matter and therefore, addressed the Committee:

- (i) Richard Keating, in opposition of the staff report.
- (ii) Stan Kawnik, in opposition of the staff report.

The following written and oral submissions were received and considered by the Committee:

- (a) Written Submissions (Added Item 9.1 (a))
 - (i) Peter Jensen, in opposition of the staff report
 - (ii) David Hood, in opposition of the staff report
 - (iii) Richard and Janet Keating, in opposition of the staff report
- (b) Oral Submissions (Added Item 9.1 (b))
 - (i) Richard Keating
 - (ii) Stan Kawnik

The public meeting was closed.

- (a) That Revised Zoning By-law Amendment Application ZAC-21-023 by T. Johns Consulting Group Ltd. c/o Katelyn Gillis on behalf of Hamilton East Kiwanis Non-Profit Homes Inc. c/o New Commons Development, owner, for a change in zoning from "RT-20" (Townhouse - Maisonette) District to "RT-20/S-1823" (Townhouse - Maisonette) District, Modified (Block 1) and "E-2/S-1823-H" (Multiple Dwellings) District, Modified, Holding (Block 2) in order to permit the demolition of two blocks of existing townhouses for the development of an eight storey, 126 unit multiple dwelling on a portion of the lands located at 1540 Upper Wentworth Street, as shown on Appendix "A" attached to Report PED22149, be APPROVED on the following basis.
 - (i) That the draft By-law, attached as Appendix "B" to Report PED22149, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;
 - (ii) That the amending By-law apply the Holding Provisions of Section 36(1) of the Planning Act, R.S.O. 1990 to the subject lands by introducing the Holding 'H' as a suffix to the proposed zoning for 1540 Upper Wentworth Street as shown on Schedule "A" of Appendix "B" attached to Report PED22149.
The Holding Provision "E-2/S-1823-H" (Multiple Dwellings) District, Modified, Holding, be removed conditional upon:
 - 1) An acceptable tenant relocation and assistance plan addressing the right to return to occupy the replacement housing at similar rents, the provision of alternative accommodation at similar rents, and other assistance to lessen the hardship, is provided to the satisfaction of the Director of Housing Services and the Director of Planning and Chief Planner;
 - (iii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2019, as amended) and complies with the Urban Hamilton Official Plan (UHOP);
- (b) That upon finalization of the amending By-law, the subject lands be re-designated from "Attached Housing" to "High Density Apartments" in the Barnstown Neighbourhood Plan

Tricia Lewis, Director, Kiwanis Homes, was permitted to speak to address the concerns of the Delegates.

The Delegation from Tricia Lewis, Director Kiwanis Homes, was received.

The recommendations in Report PED22149 were **amended** by adding the following sub-section (c):

- (c) That the public submissions regarding this matter were received and considered by the Committee.**

For disposition of this matter, refer to Item 5.

- (ii) Applications for Amendments to the Urban Hamilton Official Plan, Stoney Creek Zoning By-law No. 3692-92, and Hamilton Zoning By-law No. 05-200, and Draft Plan of Subdivision for Lands Located at 11, 19, 20, 21, 23, 27 and 30 Lakeside Drive and 81 Waterford Crescent, Stoney Creek (PED22150) (Ward 10) (Item 9.2)**

Tim Vrooman, Senior Planner, with the Planning and Economic Development Department, addressed the Committee with the aid of a PowerPoint presentation.

The staff presentation was received.

John Ariens with IBI Group, was in attendance and indicated support for the staff report.

The delegation from John Ariens with IBI Group, was received.

The Chair asked whether there were any individuals in attendance who wanted to speak to this matter.

The following individuals apprised the Chair of their intention to speak to this matter:

- (i) Lou Franco and Mario Vespa (Added Item 9.2 (b)(vi))

The following registered Delegations (Item 9.2 (b)) were not in attendance:

- (i) Ira Idzkowski
(iv) Alaa Yousif

The following Delegations addressed the Committee (Item 9.2 (b)):

- (ii) Terrance Glover, in opposition to the proposed development

- (iii) Premalal Galagoda, Owners of Lakeside Drive, concerned with the proposed development
- (v) David Neligan, in opposition to the proposed development
- (vi) Lou Franco and Mario Vespa, in opposition to the proposed development

The following written and oral submissions were received and considered by the Committee:

- (a) Written Submissions (Item 9.2 (a))
 - (i) Glenn and Ulrike Cleland, in opposition of the staff report
 - (ii) David Neligan, Agent for Romesh Galagoda, in opposition of the staff report
 - (iii) Premalal Galagoda, Owners of Lakeside Drive, concerned with the staff report
- (b) Oral Submissions (Item 9.2 (b))
 - (ii) Terrance Glover
 - (iii) Premalal Galagoda
 - (v) David Neligan
 - (vi) Lou Franco and Mario Vespa

The public meeting was closed.

Report PED22150 respecting Applications for Amendments to the Urban Hamilton Official Plan, Stoney Creek Zoning By-law No. 3692-92, and Hamilton Zoning By-law No. 05-200, and Draft Plan of Subdivision for Lands Located at 11, 19, 20, 21, 23, 27 and 30 Lakeside Drive and 81 Waterford Crescent, Stoney Creek (PED22150) (Ward 10), was DEFERRED to the August 9, 2022 Planning Committee meeting to allow staff to address concerns raised by the Delegations.

The Committee recessed from 1:10 p.m. to 1:20 p.m.

- (iii) Urban Hamilton Official Plan Amendment (UHOPA-21-019) and Zoning By-law Amendment (ZAC-21-041) Applications for the Lands Located at 510 Centennial Parkway North (PED22129) (Ward 5) (Item 9.3)**

Rino Dal Bello, Senior Planner, with the Planning and Economic Development Department addressed the Committee with the aid of a PowerPoint presentation.

The staff presentation was received.

Stuart Hastings with GSP Group, was in attendance and indicated support for the staff report.

The delegation from Stuart Hastings with GSP Group, was received.

The Chair asked whether there were any individuals in attendance who wanted to speak to this matter.

There were no individuals in attendance who wished to speak on this matter.

The following Registered Delegations addressed the Committee:

- (i) Lynda Lukasik, Environment Hamilton, in opposition of the staff report.

The following written and oral submissions were received and considered by the Committee:

- (a) Written Submissions (Item 9.3 (a))
 - (i) Bill Dermody, concerned with the staff report
- (b) Oral Submissions (Item 9.3 (b))
 - (i) Lynda Lukasik, Environment Hamilton

The public meeting was closed.

- (a) That Official Plan Amendment Application UHOPA-21-019 by Confederation Park Shopping Centres Limited and Confederation Park Shopping Centres II Limited, Owner (c/o Adam Anthony Hawkswell) for a further modification to Area Specific Policy - Area "D" in the Centennial Neighbourhoods Secondary Plan to permit the establishment of a four storey warehouse (mini storage facility), to restrict the maximum gross floor area of the mini storage use to 12,900 square metres and to permit a mini storage facility in addition to the existing maximum gross floor area cap of 45,058 square metres for all District Commercial uses, for lands located at 510 Centennial Parkway North, as shown on Appendix "A" attached to Report PED22129, be APPROVED on the following basis:
 - (i) That the draft Official Plan Amendment, attached as Appendix "B" to Report PED22129, be adopted by City Council;

- (ii) That the proposed Official Plan Amendment is consistent with the Provincial Policy Statement (2020) and conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019, as amended;

- (b) That Zoning By-law Amendment Application ZAC-21-041 by Confederation Park Shopping Centres Limited and Confederation Park Shopping Centres II Limited (c/o Adam Anthony Hawkswell) for a further modification to the “G-1/S-1613” (Designated Shopping Centre) District, Modified, to permit a four storey, 12,482 square metre warehouse (mini storage facility) on the lands located at 510 Centennial Parkway North, as shown on Appendix “A” attached to Report PED22129, be APPROVED on the following basis:
 - (i) That the draft By-law, attached as Appendix “C” to Report PED22129, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;

 - (ii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019, as amended, and complies with the Urban Hamilton Official Plan upon the approval of Official Plan Amendment No. _____.

The above Motion was DEFEATED.

The recommendations in Report PED22129 were **amended** by adding the following sub-section (c):

- (c) ***That the public submissions regarding this matter were received and considered by the Committee.***

For disposition of this matter, refer to Item 6.

- (iv) **Application for a Zoning By-law Amendment for Lands Located at 60 Caledon Avenue, Hamilton (PED22143) (Ward 8) (Item 9.4)**

James Van Rooi, Planner I, from the Planning and Economic Development Department, addressed the Committee with the aid of a PowerPoint presentation.

The staff presentation was received.

Brenda Khes with GSP Group, was in attendance and indicated support for the staff report.

The delegation from Brenda Khes with GSP Group, was received.

The Chair asked whether there were any individuals in attendance who wanted to speak to this matter.

There were no individuals in attendance who wished to speak on this matter.

The following public submissions were received and considered by the Committee:

- (a) Written Submissions (Item 9.3 (a))
 - (i) Rosalind Minaji, in support of the staff report

The public meeting was closed.

- (a) That Revised Zoning By-law Amendment Application ZAC-21-025 by GSP Group Inc. c/o Brenda Khes on behalf of Victoria Park Community Homes Inc. and Hamilton East Kiwanis Non-Profit Homes Inc., owners, for a change in zoning from the Community Institutional (I2) Zone to the Community Institutional (I2, 810, H127) Zone, to permit the lands to be developed for two, six storey, multiple dwellings and five, three storey, maisonette (back-to-back townhouse) buildings on lands located at 60 Caledon Avenue, as shown as on Appendix "A" attached to Report PED22143, be APPROVED on the following basis:
 - (i) That the draft By-law, attached as Appendix "B" attached to Report PED22143, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;
 - (ii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2019, as amended) and complies with the Urban Hamilton Official Plan (UHOP);
- (b) That the amending By-law apply the Holding Provision of Section 36(1) of the Planning Act, R.S.O. 1990 to the subject lands by adding the Holding symbol H127 to Schedule D of City of Hamilton Zoning By-law No. 05-200; and, that it shall be lifted upon completion of the following:
 - (i) That the Owner submits and receives approval of an updated Functional Servicing Report and Stormwater Management Report and related drawings to demonstrate

that adequate capacity exists in the existing municipal sanitary sewer downstream of the site to support the proposed development, to the satisfaction of the Director of Growth Management;

- (c) That upon finalization of the amending By-law, the subject lands be re-designated from “Civic and Institutional” to “Medium Density Apartments” in the Yeoville Neighbourhood Plan.

The recommendations in Report PED22143 were **amended** by adding the following sub-section (d):

- (d) *That the public submissions regarding this matter were received and considered by the Committee.***

For disposition of this matter, refer to Item 7.

- (v) Application for Zoning By-law Amendment for Lands Located at 221 and 223 Charlton Avenue East and 200 Forest Avenue, Hamilton (PED22144) (Ward 2) (Item 9.5)**

The staff presentation was waived.

Katelyn Gillis with T. Johns Consulting, was in attendance and indicated support for the staff report.

The delegation from Katelyn Gillis with T. Johns Consulting, was received.

The Chair asked whether there were any individuals in attendance who wanted to speak to this matter.

There were no individuals in attendance who wished to speak on this matter.

The public meeting was closed.

- (a) That Revised Zoning By-law Amendment Application ZAC-21-020, by T. Johns Consulting Group on Behalf of Corktown Co-operative Homes Inc. and Charlton Housing Co-operative Homes, Owner, to change the zoning from the “DE-3/S-970” (Multiple Dwellings) District, Modified (Block 1) and “D” (Urban Protected Residential – One and Two Family Dwellings, Etc.) District (Block 2) to the “DE-3/S-1819” (Multiple Dwellings) District, Modified, to permit a three storey, 17 unit multiple dwelling to be constructed on the same lot as an existing four storey, 51 unit multiple dwelling, on lands located at 221 and 223 Charlton Avenue East and 200 Forest

Avenue, Hamilton, as shown on Appendix “A” attached to Report PED22144, be APPROVED on the following basis:

- (i) That the draft By-law, attached as Appendix “B” to Report PED22144, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;
 - (ii) That the proposed change in zoning is consistent with the Provincial Policy Statement (2020), conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019, as amended, and complies with the Urban Hamilton Official Plan;
- (b) That upon finalization of the amending By-law, that the lands located at 221 and 223 Charlton Avenue East be re-designated from “Single and Double” to “Medium Density Apartments” in the Corktown Neighbourhood Plan.

The recommendations in Report PED22144 were **amended** by adding the following sub-section (c):

- (c) *That the public submissions regarding this matter were received and considered by the Committee.***

Appendix “B” to Report PED22144 was **amended** by deleting sub-section 3 (c) and adding sub-sections 3 (c) and (d) as follows:

- ~~(c) In addition to Section 10C(2), the four storey building height of the multiple dwelling existing on the date of the passing of this By-law shall be permitted.~~
- (c) *Notwithstanding Section 10C(2), no building shall exceed three storeys or 11.5 metres.***
- (d) *In addition to Section 3.c) above, the four storey building height of the multiple dwelling existing on the date of the passing of this By-law shall be permitted.***

For disposition of this matter, refer to Item 8.

- (vi) Application for Official Plan Amendment for lands located at 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue, Hamilton (PED22153) (Ward 4) (Item 9.6)**

Mark Kehler, Senior Planner, with the Planning and Economic Development Department addressed the Committee with the aid of a PowerPoint presentation.

The staff presentation was received.

Matt Johnston with Urban Solutions, was in attendance and indicated support for the staff report.

The delegation from Matt Johnston with Urban Solutions, was received.

The Chair asked whether there were any individuals in attendance who wanted to speak to this matter.

There were no individuals in attendance who wished to speak on this matter.

The public meeting was closed.

That amended Urban Hamilton Official Plan Amendment Application UHOPA-22-011, by Roxborough Park Inc., Owner, to amend Site Specific Policy UHN-25 to delete the maximum net residential density of 165 units per hectare and to require that increases in height and density be implemented through a Zoning By-law Amendment in accordance with applicable servicing and design criteria, for the lands located at 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue, as shown on Appendix "A" attached to Report PED22153, be APPROVED on the following basis:

- (a) That the draft Official Plan Amendment (OPA), attached as Appendix "B" to Report PED22153, which has been prepared in a form satisfactory to the City Solicitor, be enacted by City Council;
- (b) That the proposed Official Plan Amendment (OPA) is consistent with the Provincial Policy Statement (2020) and conforms to A Place to Grow: Growth Plan for the Greater Golden Horseshoe 2019, as amended. _.

The recommendations in Report PED22153 were **amended** by adding the following sub-section (c):

- (c) ***That there were no public submissions received regarding this matter.***

- (a) ***That the draft Official Plan Amendment (OPA), attached as Appendix “B” to Report PED22153 be amended to add an additional Policy to Site Specific Policy UHN-25 as follows:***
- ***Notwithstanding Policy e) of Site Specific Policy UHN-25, a modification to the zoning shall be considered minor and implementable through a minor variance for any development with a building height greater than 12 storeys, but not more than 18 storeys, and / or that results in a net residential density greater than 165 units per hectare, but not more than 254 units per hectare.”***

For disposition of this matter, refer to Item 9.

- (vi) **Minor Variance Request for 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue (Added Item 11.3)**

Councillor Farr introduced a Motion respecting a Minor Variance exemption for 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue, which had been requested by the agent. (Added Item 11.3)

For disposition of this matter, refer to Item 15.

(e) DISCUSSION ITEMS (Item 10)

- (i) **Licensing Short-Term Rental (STR) Accommodations (PED17203(b)) (City Wide) (Item 10.2)**

Robert Ustrzycki, Manager of Licensing and By-law, addressed Committee with the aid of a PowerPoint presentation.

The staff presentation was received.

The following written submission, was received:

- (i) Lynda Lukasik, Environment Hamilton

For disposition of this matter, refer to Item 11.

- (ii) **Applications for Amendments to the Urban Hamilton Official Plan and Zoning By-law No. 05-200 for Lands Located at 442, 450, 454 and 462 Wilson Street East (Ancaster) (PED22037) (Ward 12) (Deferred from the February 15, 2022 meeting) (Item 10.3)**

The following written submission, was received:

- (i) Aaron Waxman, Iron Point Capital Management

For disposition of this matter, refer to Item 12.

(f) GENERAL INFORMATION / OTHER BUSINESS (Item 13)

(i) Outstanding Business List (Item 13.1)

The following change to the Outstanding Business List, was approved:

- (a) Items to Be Removed:

18H - Condominium Conversions, Planning Report 18-009,
addressed at the May 31, 2022, Planning Committee, Item 10.1.

(ii) General Manager's Update (Added Item 13.2)

General Manager of Planning and Economic Development, Jason Thorne, addressed the Committee to advise there would two Planning Committee meetings being held in August on the 3rd and 9th.

The General Manager's Update, was received.

(g) PRIVATE AND CONFIDENTIAL (Item 14)

The Committee determined they did not need to move into Closed Session for Items 14.1 and 14.2

- (i) **Appeal to the Ontario Land Tribunal (OLT) for Lack of Decision on Zoning By-law Amendment (ZAC-18-049) and for Lack of Decision on Draft Plan of Subdivision Application (25T-202109) for Lands located at 860 and 884 Barton Street, (Stoney Creek) (OLT-22-003075/OLT-22-003076) (LS22022/PED22138) (Ward 10) (Item 14.1)**

For disposition of this matter refer to Item 16.

- (ii) **Appeal to the Ontario Land Tribunal (OLT) for Lack of Decision on Zoning By-law Amendment Application ZAC-20-043 and for Lack of Decision on Draft Plan of Subdivision Application (25T-202009) for Lands located at 262 McNeilly Road and 1036, 1038, 1054 and 1090 Barton Street, Stoney Creek (OLT-21-001725) (LS22023/PED22022(a)) (Ward 10) (Added Item 14.2)**

For disposition of this matter refer to Item 17.

(h) ADJOURNMENT (Item 15)

There being no further business, the Planning Committee adjourned at 5:12 p.m.

Councillor B. Johnson
Chair, Planning Committee

Lisa Kelsey
Legislative Coordinator



BOARD OF HEALTH REPORT 22-007

9:30 a.m.

Wednesday, July 6, 2022

Council Chambers, City Hall, 2nd Floor
71 Main Street West, Hamilton, Ontario

Present: Mayor F. Eisenberger
Councillors J. Farr, N. Nann, S. Merulla, R. Powers, T. Jackson, E. Pauls, J.P. Danko, B. Clark, M. Pearson, B. Johnson, A. VanderBeek and J. Partridge

**Absent with
Regrets:** Councillors M. Wilson, L. Ferguson and T. Whitehead – Personal

THE BOARD OF HEALTH PRESENTS REPORT 22-007 AND RESPECTFULLY RECOMMENDS:

1. Communications (Items 5.1 to 5.5)

That the following Communications be approved, as presented:

- (a) Correspondence from the Toronto Board of Health, respecting a COVID-19 Response (Item 5.1)

Recommendation: That Item 9, respecting the Expansion of the Collection of Sociodemographic Data be endorsed, and the remainder of the correspondence be received as presented.

- (b) Correspondence from the Sudbury District Health Unit, respecting the Healthy Babies Healthy Children Program Funding (Item 5.2)

Recommendation: Be endorsed.

- (c) Correspondence from the Grey Bruce Health Unit, respecting Support for the South West Tobacco Control Area Network (Item 5.3)

Recommendation: Be received.

- (d) Correspondence from Anita Dubeau, Board Chair for the Simcoe Muskoka District Health Unit respecting their 2020/2021 Annual Report (Item 5.4)

Recommendation: Be received.

- (e) Correspondence from R. Cooper, respecting Natural Science (referred from the General Issues Committee, June 15, 2022) (Item 5.5)

Recommendation: Be received.

2. Board of Health Governance Follow-Up (BOH21006(b)) (City Wide) (Item 7.1)

- (a) That Report BOH21006(b), respecting a Board of Health Governance Follow-up, be received; and
- (b) That the Board of Health communicate with the province on the feasibility of including members of the public on the Board of Health, with a report back to the Board.

3. Scarsin COVID-19 Forecasting Technology Procurement (BOH22013) (City Wide) (Item 10.1)

- (a) That the Board of Health approve the single source procurement, pursuant to Procurement Policy #11 – Non-competitive Procurements, for:
 - (i) Scarsin Decision Support Software platform services;
 - (ii) Decision Support Software;
 - (iii) Software maintenance and support until February 16, 2023, with the option to extend for up to 24 months which option may be exercised incrementally or otherwise; and,
 - (iv) That the Medical Officer of Health be authorized to negotiate, enter into and execute a contract or amendment(s) to the City's existing agreement and any ancillary documents required to give effect thereto with Scarsin Corporation, in a form satisfactory to the City Solicitor;
- (b) That the Board of Health authorize the Medical Officer of Health to execute an amendment(s) to our Collaboration Agreement with St. Joseph's Healthcare Hamilton, Hamilton Health Sciences Corporation and the Greater Hamilton Health Network to outline the continued cost sharing, use and sharing of the Scarsin Decision Support Software COVID-19 forecasting intelligence; and
- (c) That Appendix "A" to Report BOH22013 respecting Scarsin COVID-19 Forecasting Technology Procurement, remain confidential.

4. 2022 Public Health Services Organizational Risk Management Plan (BOH22014) (City Wide) (Item 10.2)

That the Board of Health approve Appendix "A" to Report BOH22014, the 2022 Public Health Services Organizational Risk Management Action Plan.

FOR INFORMATION:

(a) CEREMONIAL ACTIVITIES (Item 1)

There were no ceremonial activities.

(b) CHANGES TO THE AGENDA (Item 2)

The Committee Clerk advised the Board of the following changes to the agenda:

13. GENERAL INFORMATION/OTHER BUSINESS

13.1(b) Items Requiring New Due Date:

2016-A : Hamilton Airshed Modelling System (BOH18016)
(City Wide) (April 16, 2018, Item 7.1) (moved from 13.1a)
Due Date: TBD

The agenda for the July 6, 2022 Board of Health was approved, as amended.

(c) DECLARATIONS OF INTEREST (Item 3)

None

(d) APPROVAL OF MINUTES OF PREVIOUS MEETING (Item 4)

(i) June 13, 2022 (Item 4.1)

The Minutes of June 13, 2022 were approved, as presented.

(e) DELEGATION REQUEST (Item 6)

(i) Robert Cooper, respecting Support for Correspondence referred from the General Issues Committee (June 15, 2022) regarding Natural Science (for today's meeting) (Item 6.1)

The delegation request from Robert Cooper, respecting Support for Correspondence referred from the General Issues Committee (June 15, 2022) regarding Natural Science was approved, for today's meeting.

(g) CONSENT ITEMS (Item 7)

**(i) Board of Health Governance Follow-Up (BOH21006(b)) (City Wide)
(Item 7.1)**

That Report BOH21006(b), respecting a Board of Health Governance Follow-up, be received.

Report BOH21006(b), respecting the Board of Health Governance Follow-Up, **was amended** by adding a sub-section (b), to read as follows:

(b) *That the Board of Health communicate with the province on the feasibility of including members of the public on the Board of Health, with a report back to the Board.*

For further disposition of this matter, refer to Item 2.

(h) DELEGATION (Item 9)

(i) Robert Cooper, respecting Support for Correspondence referred from the General Issues Committee (June 15, 2022) regarding Natural Science (Added Item 9.1)

Robert Cooper addressed the Board respecting Natural Science.

The Delegation from Robert Cooper, respecting Support for Correspondence referred from the General Issues Committee (June 15, 2022) regarding Natural Science, was received.

For further disposition of this matter, refer to Item 1(e).

(i) GENERAL INFORMATION / OTHER BUSINESS (Item 13)

(i) Outstanding Business List for the Board of Health (Item 13.1)

The following amendments to the Outstanding Business List were approved:

(a) Items to be Removed (Item 13.1(a))

2015-A: Review of the City of Hamilton's Pest Control By-law (November 16, 2015, Item 9.1)

Note: Item is now on the Planning Committee Outstanding Business List

2019-H: Hamilton Millennial Survey Study – Employment Precarity (April 15, 2019, Item 8.1)

Addressed at Emergency & Community Services Committee, 22-004, City of Hamilton Youth Strategy (CES15056(d)) (City Wide) (Item 8.2)

2021-F: Ottawa and Toronto Board of Health Governance Models (September 20, 2021, Item 11.1)

Addressed in Item 7.1 of this Agenda

2022-A: Public Beach Signage (BOH22004) (City Wide) (March 21, 2022, Item 7.1)

Addressed at Board of Health, June 13, 2022, in Item 7.1, Green Millen Trail Waterfront Assessment (BOH22004(a)) (City Wide)

(b) New Due Date Required (Item 13.1(b)):

2016-A : Hamilton Airshed Modelling System (BOH18016) (City Wide) (April 16, 2018, Item 7.1)

(j) ADJOURNMENT (Item 15)

There being no further business, the Board of Health adjourned at 11:21 a.m.

Respectfully submitted,

Mayor Eisenberger,
Chair, Board of Health

Loren Kolar
Legislative Coordinator
Office of the City Clerk



**PUBLIC WORKS COMMITTEE
REPORT 22-011**

1:30 p.m.

Wednesday, July 6, 2022

Council Chambers

Hamilton City Hall

71 Main Street West

Present: Councillors N. Nann (Chair), R. Powers (Vice-Chair), J.P. Danko, J. Farr, L. Ferguson, T. Jackson, S. Merulla, E. Pauls, M. Pearson and A. VanderBeek

**Absent with
Regrets:** Councillor T. Whitehead – Personal

**THE PUBLIC WORKS COMMITTEE PRESENTS REPORT 22-011 AND
RESPECTFULLY RECOMMENDS:**

1. 2021 Annual Energy Report (PW21049(a)) (City Wide) (Item 7.1)

That Report PW21049(a), respecting the 2021 Annual Energy Report (City Wide), be received.

2. Sewer Use By-law Program 2021 Annual Update (PW22051) (City Wide) (Item 7.2)

That Report PW22051, respecting the Sewer Use By-law Program 2021 Annual Update (City Wide), be received.

3. 2021 Wastewater Quality Management System (WWQMS) Annual Summary Report (PW22052) (City Wide) (Item 7.3)

That Report PW22052, respecting the 2021 Wastewater Quality Management System (WWQMS) Annual Summary Report (City Wide), be received.

4. In-Service Road Safety Review of the Intersection of Barton Street and Wellington Street (PW22054) (Wards 2 and 3) (Item 7.4)

That Report PW22054, respecting the In-Service Road Safety Review of the Intersection of Barton Street and Wellington Street (Wards 2 and 3), be received.

5. Surface Water Quality Program 2021 Annual Update (PW22058) (City Wide) (Item 7.5)

That Report PW22058, respecting the Surface Water Quality Program 2021 Annual Update (City Wide), be received.

6. Addressing the Need for the Certificate of Recognition (COR™) Program (PW22060) (City Wide)-(Item 7.6)

That Report PW22060, respecting Addressing the Need for the Certificate of Recognition (COR™) Program (City Wide), be received.

7. City of Hamilton Annual Collision Report – 2021 Statistics and Trends (PW22061) (City Wide) (Item 7.7)

That Report PW22061, respecting City of Hamilton Annual Collision Report – 2021 Statistics and Trends (City Wide), be received.

8. Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide) (Item 8.1)

- (a) That the Complete Streets Design Manual attached as Appendix "A" Public Works Report 22-011 be approved as the basis for planning and designing City streets;
- (b) That staff be directed to update roadway design manuals and guidelines to reflect Complete Streets Design Manual, including, but not limited to, the Construction and Materials Specifications Manual, Traffic Signal and Pavement Marking Designs Drawings, and other documents identified by staff;
- (c) That staff be directed to update the relevant sections of the Comprehensive Development Guidelines and Financial Policies to incorporate complete street elements, in consultation with the Development Industry Liaison Group; and
- (d) That staff be directed to undertake a mapping exercise as part of the Council approved Road Classification Harmonization Study and Right-of-Way Review (Project ID 4031955987) to categorize all existing and planned roadways based on the Complete Streets Typologies and compare existing and future Right-of-Way opportunities and constraints, and to inform future Official Plan updates and on-going planning studies.

9. Proposed Permanent Closure of a Portion of 390 Melvin Avenue, Hamilton (PW22055) (Ward 4) (Item 9.2)

- (a) That the application of the City of Hamilton's Corporate Real Estate Office, to permanently close a portion of 390 Melvin Avenue, Hamilton ("Subject Lands"), as shown on Appendix "B" & "C", attached to Public Works Report 22-011 be approved, subject to the following conditions:
 - (i) That the City Solicitor be authorized and directed to prepare all necessary by-laws to permanently close the highway, for enactment by Council;
 - (ii) The Corporate Real Estate Office of the Planning and Economic Development Department be authorized and directed to enter into any requisite easement agreements, right of way agreements, and/or other agreements deemed necessary to affect the orderly disposition of the Subject Lands and proceed to sell the Subject Lands as they deem fit, in accordance with the City of Hamilton Sale of Land Policy By-law 14-204;
 - (iii) That the City Solicitor be authorized and directed to register a certified copy of the by-law(s) permanently closing the highway in the proper land registry office;
 - (iv) The City Solicitor be authorized to complete the transfer of the Subject Lands pursuant to an Agreement of Purchase and Sale or Offer to Purchase as negotiated by the Corporate Real Estate Office of the Planning and Economic Development Department;
 - (v) That the City Solicitor be authorized to amend and waive such terms as they consider reasonable to give effect to this authorization and direction;
 - (vi) That the Public Works Department publish any required notice of the City's intention to pass the by-laws and/or permanently sell the closed highway pursuant to the City of Hamilton Sale of Land Policy By-law 14-204; and
 - (vii) That the applicant be fully responsible for the deposit of a reference plan in the proper land registry office, and that said plan be prepared by an Ontario Land Surveyor to the satisfaction of the Manager, Geomatics and Corridor Management Section, and that the applicant also deposit a reproducible copy of said plan with the Manager, Geomatics and Corridor Management Section.

10. York and Valley (HD016) Booster Pumping Station Municipal Class Environmental Assessment Addendum Report (PW06121(b)) (Ward 13) (Item 10.1)

(Ferguson/Pearson)

- (a) That the General Manager, Public Works, be authorized and directed to file the Notice of Completion and issue the York and Valley (HD016) Booster Pumping Station Upgrades Schedule B Municipal Class Environmental Assessment Addendum for the mandatory 30-day review period; and,
- (b) That upon completion of the 30-day agency and public review period, the General Manager, Public Works, be authorized and directed to proceed with the implementation of the preferred solution within the York and Valley (HD016) Booster Pumping Station Upgrades Schedule B Municipal Class Environmental Assessment Addendum Report funded from Project ID. No. 5141695883.

11. Standardization of Hamilton Water Equipment and Services (PW22053) (City Wide) (Item 10.2)

- (a) That the standardization of the products, services, manufacturers and distributors identified in the attached Appendices “D”, “E”, and “F” to Public Works Report 22-011 pursuant to Procurement Policy #14 - Standardization and as the single source of supply for the listed equipment, parts, supplies and services for the Hamilton Water Division be approved;
- (b) That the General Manager of Public Works, or their designate, be authorized to negotiate, enter into and execute any required Contract and any ancillary documents required to give effect thereto with those suppliers identified in the attached Appendices “D”, “E”, and “F” to Public Works Report 22-011 with content acceptable to the General Manager of Public Works, and in a form satisfactory to the City Solicitor; and
- (c) That the General Manager of Public Works, or their designate, be authorized to amend any Contracts executed and any ancillary documents as required in the event that a service provider, manufacturer, or distributor identified in the attached Appendices “D”, “E”, or “F” to Public Works Report 22-011 undergoes a name change or a new distributor relationship in a form satisfactory to the City Solicitor.

12. Highway 52 at Powerline Road West - Roundabout Implementation Plan (PW22056) (Ward 12) (Item 10.3)

That a Municipal Class Environmental Assessment (MCEA) be undertaken to review the implementation of a roundabout at the intersection of Highway 52 and

Powerline Road West (Ward 12) at an estimated cost of \$400,000 and be submitted and considered as part of the 2023 capital budget process.

13. Municipal Hazardous and Special Waste Program Update and Contract Extension (PW22059) (City Wide) (Item 10.4)

- (a) That Council approve the continuation of the City of Hamilton's Household Hazardous Waste program at its current level of service including operation of the City of Hamilton's three Household Hazardous Waste depots and disposal of collected Household Hazardous Waste material;
- (b) That staff be directed to continue to budget for Household Hazardous Waste materials that are not covered by Regulation 449/21: Hazardous and Special Products and therefore are not eligible for reimbursement under Extended Producer Responsibility, in the 2023 Tax and Rate Budgets; and
- (c) That Council approve the single source procurement, pursuant to Procurement Policy #11 – Non-competitive Procurements, and extend Contract T-025-15 for the Operations, Maintenance, Removal and Disposal Services for the three City of Hamilton Household Hazardous Waste depots until December 31, 2022, with the option to further extend for up to 12 months, which option may be exercised incrementally or otherwise, and that the General Manager, Public Works be authorized to negotiate and execute an amendment to the City of Hamilton's existing contract and any ancillary documents required to give effect thereto with GFL Environmental Inc., in a form satisfactory to the City Solicitor.

14. Installation of a Speed Cushion as a Traffic Calming Measure on East 26th Street (Ward 7) (Item 11.1)

WHEREAS, residents on East 26th Street in Ward 7 have advocated for the installation of a speed cushion to address roadway safety concerns as a result of speeding; and

WHEREAS, signatures were collected from residents resulting in support by 21 of 30 homes on East 26th Street for the installation of a speed cushion as a traffic calming measure.

THEREFORE, BE IT RESOLVED:

- (a) That Transportation and Operations Maintenance staff be authorized and directed to install one speed cushion as a traffic calming measure on East 26th Street between Concession Street and Crockett Street as part of the 2022 Traffic Calming Program's fall application;

- (b) That all costs associated with the installation of one speed cushion on East 26th Street between Concession Street and Crockett Street be funded from the Ward 7 Capital Re-Investment Reserve (#108057), to be completed under contract # C15-12-22, Project ID 4242209704, at an upset limit, including contingency, not to exceed \$7,000; and
- (c) That the Mayor and City Clerk be authorized and directed to execute any required agreement(s) and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

15. Accessible Portable Toilets at Inch Park, 400 Queensdale Avenue East, Hamilton (Ward 7) (Item 11.2)

WHEREAS, Inch Park, 400 Queensdale Avenue East, Hamilton, is home to Hamilton Challenger Baseball, an organization that provides sports for players with special needs;

WHEREAS, Hamilton Challenger Baseball consists of more than 190 players and 60 dedicated volunteers and offers the opportunity for those with developmental and physical disabilities to be empowered and assert their independence through sport;

WHEREAS, Hamilton Challenger Baseball has funded improvements to Inch Park over the years; and

WHEREAS, additional funding for the rental and cleaning of accessible portable toilet(s) for the 2022 season is required.

THEREFORE, BE IT RESOLVED:

- (a) That a maximum of \$1,250 of funding be allocated from the Ward 7 Special Capital Re-Investment Discretionary Fund (#3302009700), for the provision of accessible portable toilet(s) at Inch Park, 400 Queensdale Avenue East, Hamilton, to support Hamilton Challenger Baseball 2022 season, be approved; and
- (b) That the Mayor and City Clerk be authorized and directed to approve and execute all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

16. Father Sean O’Sullivan Memorial Park Improvements, 1139 Greenhill Avenue, Hamilton (Ward 5) (Item 11.3)

WHEREAS, the play structure located at Father Sean O’Sullivan Memorial Park, 1139 Greenhill Avenue, Hamilton was originally installed in the early 1990s;

WHEREAS, the play structure has surpassed its useful life span and needs to be replaced;

WHEREAS, Council previously approved \$175,000 funded from the Ward 5 – Special Capital Re-Investment Reserve Fund (#108055) for the new play structure and safety surfacing; and

WHEREAS, additional funding may be required to deliver the proposed conceptual design for the play equipment, given cost escalations associated with the tendering process.

THEREFORE, BE IT RESOLVED:

- (a) That additional funds for the replacement of the play structure at Father Sean O’Sullivan Park, 1139 Greenhill Avenue, Hamilton, to be funded from the Ward 5 Special Capital Re-Investment Reserve Fund (#108055) at an upset limit, including contingency, not to exceed \$35,000 and added to Project ID #4242109503, be approved; and
- (b) That the Mayor and City Clerk be authorized and directed to approve and execute all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

17. Park Improvements to Macassa Park, 701 Upper Sherman Avenue, Elmar Park, 140 Brigade Drive, Burkholder Park, 478 East 25th Street, and Peace Memorial Park, 85 East 36th Street (Ward 7) (Item 11.4)

WHEREAS, the play structures at Macassa Park, 701 Upper Sherman Avenue, Elmar Park, 140 Brigade Drive, Burkholder Park, 478 East 25th Street, and Peace Memorial Park, 85 East 36th Street, have reached the end of their usable life;

WHEREAS, the asphalt pathways at Elmar Park would benefit from replacement; and

WHEREAS, these community amenities are valuable recreation opportunities for children, youth and families within the Ward 7 neighbourhoods.

THEREFORE, BE IT RESOLVED:

- (a) That replacement of the existing play structures at Macassa Park, 701 Upper Sherman Avenue, Elmar Park, 140 Brigade Drive, Burkholder Park, 478 East 25th Street, and Peace Memorial Park, 85 East 36th Street, at a cost of \$875,000 including contingency, to be funded from the Ward 7 Special Capital Re-Investment Reserve Fund (#108057), be approved;
- (b) That existing park pathways in need of replacement at Elmar Park, 140 Brigade Drive, be replaced at a cost of \$125,000 including contingency, to be funded from the Ward 7 Minor Maintenance Fund #4031911607, be approved; and
- (c) That the General Manager of Public Works be authorized and directed to approve and execute any and all required agreements and ancillary documents, in a form satisfactory to the City Solicitor.

18. Park Improvements to Dr. William Bethune Park, 60 DiCenzo Drive, Allison Park, 51 Piano Drive, T. Melville Bailey Park, 45 Cloverhill Road, and Kennedy East Park, 130 Malton Drive (Ward 8) (Item 11.5)

WHEREAS, the play structures at Dr. William Bethune Park, 60 DiCenzo Drive, Allison Park, 51 Piano Drive, T. Melville Bailey Park, 45 Cloverhill Road, and Kennedy East Park, 130 Malton Drive, have reached the end of their usable life;

WHEREAS, the asphalt pathways at Kennedy East Park, 130 Malton Drive, would benefit from replacement;

WHEREAS, these community amenities are valuable recreation opportunities for children, youth and families within the Ward 8 neighbourhoods.

THEREFORE, BE IT RESOLVED:

- (a) That replacement of the existing play structures at Dr. William Bethune Park, 60 DiCenzo Drive, Allison Park, 51 Piano Drive, T. Melville Bailey Park, 45 Cloverhill Road, at a cost of \$675,000 including contingency, to be funded from the Ward 8 Special Capital Re-Investment Reserve Fund (#108058), be approved;
- (b) That existing park pathways in need of replacement at Kennedy East Park, 130 Malton Drive, be replaced at a cost of \$60,000 including contingency, to be funded from the Ward 8 Minor Maintenance Fund (#4031911608), be approved; and
- (c) That the General Manager of Public Works be authorized and directed to approve and execute any and all required agreements and ancillary documents, in a form satisfactory to the City Solicitor.

19. Installation of Speed Cushions as a Traffic Calming Measure on Fletcher Road and Mistywood Drive (Ward 9) (Item 11.6)

WHEREAS, Vision Zero and Complete Streets principles are used in the City of Hamilton to provide a safer environment for all road users;

WHEREAS, residents have repeatedly advocated for installation of traffic calming measures on Fletcher Road between Rymal Road East to Blue Mountain Drive and the Ward 9 office has requested Transportation and Operations Maintenance staff to conduct a review to address roadway safety concerns;

WHEREAS, a motion was approved in August 2021 to implement a corner radius reduction on Fletcher Road at Pinehill Drive on the north west corner;

WHEREAS, residents of Mistywood Drive have advocated for traffic calming measures and signatures were collected from 23 of 26 households who are in support of the installation of speed cushions;

THEREFORE, BE IT RESOLVED:

- (a) That Transportation and Operations Maintenance staff be authorized and directed to install two speed cushions as a traffic calming measure on Fletcher Road from Rymal Road East to Blue Mountain Drive as part of the 2022 Traffic Calming Program's fall application;
- (b) That Transportation Operations and Maintenance staff be authorized and directed to implement the following roadway improvements on Fletcher Road from Rymal Road East to Blue Mountain Road:
 - (i) widened centerline; and
 - (ii) ladder crosswalk implementations.
- (c) That Transportation Operations and Maintenance staff be authorized and directed to install two speed cushions as a traffic calming measure on Mistywood Drive as part of the 2022 Traffic Calming Program's fall application;
- (d) That all costs associated with the installation of traffic calming measures on Fletcher Road and on Mistywood Drive be funded from the Stoney Creek Compensation Royalties (Terrapure Landfill) Reserve (No. 117036) at an upset limit, including contingency, not to exceed \$36,000; and
- (e) That the Mayor and City Clerk be authorized and directed to execute any required agreement(s) and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

20. Mohawk Sports Park - Ray Lewis Track Improvements, 1100 Mohawk Road East, Hamilton (Ward 6) (Item 11.7)

WHEREAS, Ray Lewis Track and Field Centre is located at Mohawk Sports Park, 1100 Mohawk Road East, Hamilton;

WHEREAS, a shade structure was installed in Spring 2022 to provide overhead protection from the rain and sun and be an integral part of the Community Fitness Program for seniors proposed by the Golden Horseshoe Track & Field Council to enhance the outdoor fitness opportunities for the community;

WHEREAS, Council previously approved the Hamilton Olympic Club and the Golden Horseshoe Track & Field Council \$70,000 of funding from the Ward 6 Special Capital Re-Investment Reserve Fund (#108056) to contribute to this project;

WHEREAS, the Hamilton Olympic Club has since received additional grants to provide for the full funding of the shade structure project; and

WHEREAS, there is an opportunity to reallocate this \$70,000 to support other improvements at the Mohawk Sports Park, Ray Lewis Track and Field amenity.

THEREFORE, BE IT RESOLVED:

- (a) That the reallocation of previously approved funding in Project ID 4402249900, Mohawk Sports Park – Bleachers-Shade, that was funded from the Ward 6 Special Capital Re-Investment Reserve Fund (#108056) of \$70,000, to support amenity improvements, such as but not limited to fencing and drinking water fountain at Mohawk Sports Park, 1100 Mohawk Road East Hamilton, be approved;
- (b) That the renaming of Project ID #4402249900 Mohawk Sports Park – Bleachers-Shade to Mohawk Sports Park - Ray Lewis Track Improvements, be approved; and
- (c) That the General Manager of Public Works be authorized and directed to approve and execute any and all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

21. Replacement of Play Structures at Dundurn Park, 800 York Boulevard, Hamilton (Ward 1) (Item 11.8)

WHEREAS, the play structures at Dundurn Park, 800 York Boulevard, Hamilton, have reached the end of their usable life; and

WHEREAS, these community amenities are valuable recreation opportunities for children, youth and families within the Strathcona Neighbourhood, and the greater Ward 1 community.

THEREFORE, BE IT RESOLVED:

- (a) That replacement of the existing play structures at Dundurn Park, 800 York Boulevard, Hamilton, at an upset limit of \$250,000 including contingency, to be funded from the Ward 1 Special Capital Re-Investment Reserve Fund (#108051), be approved; and
- (b) That the General Manager of Public Works be authorized and directed to approve and execute any and all required agreements and ancillary documents, in a form satisfactory to the City Solicitor.

22. Eleanor Park Playground Improvements, 80 Presidio Drive, Hamilton (Ward 6) (Added Item 11.9)

WHEREAS, the play structure, park pathways, and multi-use court located at Eleanor Park, 80 Presidio Drive, Hamilton were originally installed in the early 1990s;

WHEREAS, all the park recreational amenities have now surpassed useful life span and need to be replaced;

WHEREAS, Council previously approved \$120,300 be funded from the Ward 6 – Special Capital Re-Investment Reserve Fund (#108056) for the new Jude Strickland Memorial Play Structure and safety surfacing; and

WHEREAS, additional funding is required to deliver the construction project for the park amenities due to the cost escalations associated with the tendering process.

THEREFORE, BE IT RESOLVED:

- (a) That additional funds required due to escalated costs during the tendering process to support the replacement of the play structure at Eleanor Park, 80 Presidio Drive, Hamilton, to be funded from the Ward 6 Special Capital Re-Investment Reserve Fund (#108056) at an upset limit, including contingency, not to exceed \$29,000 be approved and added to Project ID #4402152600 Playground Lifecycle Replacement Program, be approved;
- (b) That \$27,000 to support the park pathway replacement at Eleanor Park, 80 Presidio Drive, Hamilton, be approved and added to Project ID #4402149101 Park Pathway Resurfacing Program; and

- (c) That the Mayor and City Clerk be authorized and directed to approve and execute all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

23. Addition of Clear Acrylic Backboards for the Multi-Use Courts at Montgomery Park, 1570 Main Street East, Hamilton (Ward 4) (Added Item 11.10)

WHEREAS, Montgomery Park has a two multi-use courts with basketball nets to serve the community's recreation needs; and

WHEREAS, improving the basketball play experience with the introduction of a set of clear acrylic backboards is desired by the community.

THEREFORE, BE IT RESOLVED:

- (a) That one set of clear acrylic backboards for the multi-use courts at Montgomery Park, 1570 Main Street East, Hamilton, be purchased and installed, through funding from the Ward 4 Discretionary Account (#1 3302109400) at an upset limit, including contingency, not to exceed \$6,000; and
- (b) That the Mayor and City Clerk be authorized and directed to approve and execute all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

24. First Road West and Isaac Brock Drive Roadway Safety Enhancements and Traffic Calming Measures (Ward 9) (Added Item 11.11)

WHEREAS, Vision Zero and Complete Streets principles are used in the City of Hamilton to provide a safer environment for all road users; and

WHEREAS, residents have repeatedly advocated for installation of traffic calming measures and roadway safety enhancements on Isaac Brock Drive and on First Road West between Mud Street West and Highland Road West and the Ward 9 office has requested Transportation Operations staff to conduct a review to address roadway safety concerns.

THEREFORE, BE IT RESOLVED:

- (a) That Transportation Operations & Maintenance staff be directed to review Isaac Brock Drive and First Road West between Mud Street West and Highland Road West in order to identify roadway safety enhancements and traffic calming measures which are applicable on minor collector roadways;

- (b) That Transportation Operations & Maintenance staff be authorized and directed to implement the identified roadway safety enhancements and traffic calming measures for Isaac Brock Drive and First Road West between Mud Street West and Highland Road West in 2023, as reviewed and accepted by the Ward Councillor;
- (c) That all costs associated with the installation of the roadway safety enhancements and traffic calming measures for Isaac Brock Drive and First Road West be funded from the Stoney Creek Compensation Royalties (Terrapure Landfill) Reserve (No. 117036) at an upset limit, including contingency, not to exceed \$200,000; and
- (d) That the Mayor and City Clerk be authorized and directed to execute any required agreement(s) and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

25. Dewatering of Groundwater from the Roxborough Mixed Income/Tenure Demonstration Project to the City's Wastewater Collection System (Added Item 11.12)

WHEREAS, the Roxborough Development Group has secured participation in the Canada Mortgage and Housing Corporation (CMHC) Rental Construction Financing Initiative (RCFI) program designed to facilitate the construction of new rental housing that is attainable and affordable, which is supporting the Roxborough Demonstration Project to construct 352 affordable rental units;

WHEREAS, there is a significant time sensitivity to beginning the construction of the apartment towers that incorporate these affordable rental units;

WHEREAS, the Roxborough Demonstration Project has approval from the Ministry of the Environment, Conservation and Parks to construct an underground stormwater detention tank with a maximum storage volume of 2,000 cubic metres;

WHEREAS, the groundwater levels in the area will impact the future operation, serviceability and maintenance of the underground stormwater detention tank;

WHEREAS, the Roxborough Development Group plans to remove groundwater from the property in order to sustain the future operation of the stormwater detention tank;

WHEREAS, the City's Sewer Use Bylaw 14-090 permits the discharge of groundwater to the City's sewer works, provided that the discharge complies with a valid Sewer Discharge Permit;

WHEREAS, the Roxborough Demonstration Project will result in a net benefit to wet weather flow discharged to the City's combined sewer system;

THEREFORE, BE IT RESOLVED

- (a) That the General Manager, Public Works or designate, be directed to approve a Sewer Discharge Permit for the discharge of groundwater from the Roxborough Demonstration Project to the City sewer works on Queenston Road, subject to the following conditions:
 - (i) The Roxborough Development Group will procure, construct, install, and always operate a treatment system for the groundwater that will reduce the concentrations of any contaminants in the groundwater below the parameter limits listed in Schedule B to City Bylaw 14-090;
 - (ii) The Roxborough Development Group will install and maintain measurement and monitoring devices as required by the City for the ongoing measurement of the groundwater quantities that are discharged to the City sewer works;
 - (iii) The Roxborough Development Group will collect treated groundwater samples at a frequency specified by the City, to be analysed by an accredited laboratory, for the chemical constituents required by the City, in order to demonstrate compliance with Schedule B to City Bylaw 14-090;
 - (iv) The Roxborough Development Group will provide records of groundwater volumes discharged to the City sewer works, at a frequency stipulated by the City;
 - (v) The Roxborough Development Group will provide records of analysis of the treated groundwater to the City, at a frequency stipulated by the City;
 - (vi) The Roxborough Development group will pay all permit and treatment costs to the City, related to the groundwater discharge, at the frequency specified by the City;
 - (vii) The Roxborough Development Group will implement measures to reduce or eliminate the quantity of groundwater that is discharged to the City's sewer works during periods where there is snow melt, or when a wet weather is expected with anticipated rainfall accumulation of 10mm or greater;

- (viii) The Roxborough Development Group will establish a Surcharge Contingency Plan should the discharge of groundwater volumes exceeding 0.4 liters per second be required, or should the City advise that the discharge of groundwater must be reduced or eliminated;
- (ix) At any point should the volume of groundwater to be discharged meet or exceed a threshold of 50,000 litres per day, the Roxborough Development Group will apply to the Ministry of the Environment, Conservation and Parks for a Permit to Take Water in accordance with the requirements of the Ontario Water Resources Act;
- (b) That the Roxborough Development Group prepare and implement the recommendation of slope stability analysis, settlement analysis, monitoring plan contingency plans and remedial action plans due to permanent dewatering, to the satisfaction of the Director of Growth Management in accordance with any peer review of the study complete by the City. Costs for a City peer review will be recovered from The Roxborough Development Group if required.
- (c) That the Roxborough Development Group enter into and register a site plan agreement with the City to address all related issues to the satisfaction of the Manager of Development Approvals.

26. Standardization of Enterprise Asset Management Systems - Phase 3 - Amendment ((PW19035(c) (City Wide) (Item 14.1)

- (a) That the General Manager, Public Works, or their designate be authorized and directed to negotiate, enter into and execute any required agreements and ancillary documents required to give effect thereto with Conectado to standardize enterprise processes and consolidate the existing 11 Enterprise Asset Management Systems into a single Enterprise Asset Management System for the Public Works Department, with contents acceptable to the General Manager of Finance and Corporate Services and in a form satisfactory to the City Solicitor; and
- (b) That recommendations (a) and (b) and the contents of Report PW19035(c), respecting the Standardization & Rationalization of Enterprise Asset Management Systems - Phase 3, remain confidential.

FOR INFORMATION:

(a) APPROVAL OF AGENDA (Item 2)

The Committee Clerk advised of the following changes to the agenda:

5. COMMUNICATIONS

- 5.1 Correspondence from Environment Hamilton respecting the Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide) (Outstanding Business List Item)

Recommendation: To be received and referred to the consideration of Item 8.1

- 5.2 Correspondence from Gabriel Nicholson respecting the Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide) (Outstanding Business List Item)

Recommendation: To be received and referred to the consideration of Item 8.1

6. DELEGATION REQUESTS

- 6.1 Susan Creer respecting Item 8.1 Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide) (Outstanding Business List Item) (for today's meeting)

- 6.2 Delegation Requests respecting Item 10.3 - Highway 52 at Powerline Road West - Roundabout Implementation Plan (PW22056) (Ward 12) (for today's meeting)

- (a) David Jones, Turn on Powerline
- (b) Ben Loewith
- (c) Penny Santa-Barbara (WITHDRAWN)

- 6.3 Delegation Requests respecting Item 12.6 Dewatering of Groundwater from the Roxborough Mixed Income/Tenure Demonstration Project to the City's Wastewater Collection System (for today's meeting) (WITHDRAWN)

- (a) Sergio Manchia, Roxborough Park Inc.
- (b) Ralph DiCenzo, Roxborough Park Inc.
- (c) Scott Llewellyn, Roxborough Park Inc.
- (d) David Horwood, Roxborough Park Inc.

(e) Nick Carnicelli, Roxborough Park Inc.

9. PUBLIC HEARINGS / DELGATIONS

9.2 Proposed Permanent Closure of a Portion of 390 Melvin Avenue, Hamilton (PW22055) (Ward 4) (for today's meeting)

(a) Registered Speakers:

(a) Laura Taylor

12. NOTICES OF MOTION

12.1 Eleanor Park Playground Improvements, 80 Presidio Drive, Hamilton (Ward 6)

12.2 Contribution to the Commemorative Park Bench & Tree Program in Honour of Eristo Grandoni (Ward 3)

12.3 Safety Enhancements on Wentworth Street North in Proximity to Cathy Wever Elementary School (Ward 3)

12.4 Addition of Clear Acrylic Backboards for the Multi-Use Courts at Montgomery Park, 1570 Main Street East, Hamilton (Ward 4)

12.5 First Road West and Isaac Brock Drive Roadway Safety Enhancements and Traffic Calming Measures (Ward 9)

12.6 Dewatering of Groundwater from the Roxborough Mixed Income/Tenure Demonstration Project to the City's Wastewater Collection System

The agenda for the July 6, 2022 Public Works Committee meeting was approved, as amended.

(b) DECLARATIONS OF INTEREST (Item 3)

There were no declarations of interest.

(c) APPROVAL OF MINUTES OF PREVIOUS MEETING (Item 4)

(i) June 13, 2022 (Item 4.1)

The Minutes of the June 13, 2022 meeting of the Public Works Committee were approved, as presented.

(d) COMMUNICATIONS (Item 5)

(i) Correspondence respecting the Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide)

The following correspondence items respecting Report PED21020(a)/PW21002(a), Complete Streets Design Manual were received and referred to the consideration of Item 8.1:

- (1) Environment Hamilton (Item 5.1)
- (2) Gabriel Nicholson (Item 5.2)

(e) DELEGATION REQUESTS (Item 6)

The following Delegation Requests were approved for today's meeting:

- (i) Susan Creer respecting Report PED21020(a)/PW21002(a) Complete Streets Design Manual (City Wide) (for today's meeting) (Item 6.1)
- (ii) Delegation Requests respecting Report PW22056 Highway 52 at Powerline Road West - Roundabout Implementation Plan (Ward 12) (for today's meeting) (Item 6.2)
 - (1) David Jones, Turn on Powerline
 - (2) Ben Loewith

(f) CONSENT ITEMS (Item 7)

(i) Various Advisory Committee Minutes (Item 7.8)

The following Consent Items were received:

- (i) Keep Hamilton Clean and Green Advisory Committee - No Quorum Notes - June 7, 2022 (Item 7.8(a))
- (ii) Hamilton Cycling Committee Minutes - May 4, 2022 (Item 7.8(b))

(g) STAFF PRESENTATIONS (Item 8)

(i) Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide) (Item 8.1)

The presentation respecting Complete Streets Design Manual (PED21020(a)/PW21002(a)) (City Wide), was received.

For further disposition of this matter, refer to Item 8.

(h) PUBLIC HEARINGS / DELEGATIONS (Item 9)

(i) John Kozlowski respecting Monuments at East Lawn Cemetery (approved on May 30, 2022) (Item 9.1)

John Kozlowski was not present when called upon.

(i) Proposed Permanent Closure of a Portion of 390 Melvin Avenue, Hamilton (PW22055) (Ward 4) (Item 9.2)

Councillor Nann advised that the notice of the Proposed Permanent Closure of a Portion of 390 Melvin Avenue, Hamilton (PW22055) (Ward 4) was provided as required under the City's By-law No. 14-204 – the Sale of Land Policy By-law.

The Committee Clerk advised that there was one registered speaker.

Registered Speaker:

1. Laura Taylor

Laura Taylor addressed the Committee with concerns respecting the Proposed Permanent Closure of a Portion of 390 Melvin Avenue, Hamilton (PW22055) (Ward 4).

The registered delegation was received.

The public meeting was closed.

For further disposition of this matter, refer to Item 9.

(iii) Susan Creer respecting Report PED21020(a)/PW21002(a), Complete Streets Design Manual (City Wide) (Item 9.3)

Susan Creer, addressed the Committee respecting Report PED21020(a)/PW21002(a) Complete Streets Design Manual (City Wide).

(iv) Delegation Requests respecting Report PW22056, Highway 52 at Powerline Road West - Roundabout Implementation Plan (Ward 12) (Item 9.4)

David Jones, Turn On Powerline, addressed the Committee respecting Report PW22056 Highway 52 at Powerline Road West - Roundabout Implementation Plan.

Ben Loewith addressed the Committee respecting Report PW22056 Highway 52 at Powerline Road West - Roundabout Implementation Plan.

The following delegations, were received:

- (i) Susan Creer, respecting Report PED21020(a)/PW21002(a), Complete Streets Design Manual (City Wide).
- (ii) David Jones, Turn On Powerline, respecting Report PW22056 Highway 52 at Powerline Road West - Roundabout Implementation Plan (Ward 12).
- (iii) Ben Loewith respecting Report PW22056 Highway 52 at Powerline Road West - Roundabout Implementation Plan (Ward 12).

(i) NOTICES OF MOTION (Item 12)

(i) Eleanor Park Playground Improvements, 80 Presidio Drive, Hamilton (Ward 6) (Item 12.1)

The Rules of Order were waived to allow for the introduction of a Motion respecting Eleanor Park Playground Improvements, 80 Presidio Drive, Hamilton (Ward 6).

For disposition of this matter, refer to Item 22.

Councillor Nann relinquished the Chair to Councillor Powers in order to introduce the following Notices of Motion.

(ii) Contribution to the Commemorative Park Bench & Tree Program in Honour of Eristo Grandoni (Ward 3) (Item 12.2)

Councillor Nann introduced the following Notice of Motion:

WHEREAS, the Environmental Services Division of Public Works offers a Commemorative Park Bench & Tree Program;

WHEREAS, the Commemorative Park Bench & Tree Program provides options to honour an individual or group by donating a bench or tree to a City park which allows for improvements to our parks and inspires community pride and a sense of place;

WHEREAS, the Commemorative Park Bench & Tree Program is operated at full cost recovery through the donations provided;

WHEREAS, Eristo Grandoni lived on Rosslyn Avenue South for 50 years in the same house where he was known for tending to his garden and helping out whenever needed in the local community; and

WHEREAS, a donation to the Commemorative Park Bench & Tree Program in Ward 3 requires funding approval.

THEREFORE, BE IT RESOLVED:

- (a) That a contribution to the Commemorative Park Bench & Tree Program be funded from the Ward 3 Special Capital Re-Investment Discretionary Fund (# 3302209300) for the purchase of a Bench and Plaque at Gage Park in honour of Eristo Grandoni, a long-time resident, with an upset limit of \$1,600, be approved; and
- (b) That the Mayor and City Clerk be authorized and directed to approve and execute any and all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

(iii) Safety Enhancements on Wentworth Street North in Proximity to Cathy Wever Elementary School (Ward 3) (Item 12.3)

Councillor Nann introduced the following Notice of Motion:

WHEREAS, Montgomery Park has a two multi-use courts with basketball nets to serve the community's recreation needs; and

WHEREAS, improving the basketball play experience with the introduction of a set of clear acrylic backboards is desired by the community.

THEREFORE, BE IT RESOLVED:

- (a) That one set of clear acrylic backboards for the multi-use courts at Montgomery Park, 1570 Main Street East, Hamilton, be purchased and installed, through funding from the Ward 4 Reserve Fund (#108054) at an upset limit, including contingency, not to exceed \$6,000; and
- (b) That the Mayor and City Clerk be authorized and directed to approve and execute all required agreements and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

Councillor Nann assumed the Chair for the remainder of the meeting.

(iv) Addition of Clear Acrylic Backboards for the Multi-Use Courts at Montgomery Park, 1570 Main Street East, Hamilton (Ward 4) (Item 12.4)

The Rules of Order were waived to allow for the introduction of a Motion respecting Addition of Clear Acrylic Backboards for the Multi-Use Courts at Montgomery Park, 1570 Main Street East, Hamilton (Ward 4).

For disposition of this matter, refer to Item 23.

(v) First Road West and Isaac Brock Drive Roadway Safety Enhancements and Traffic Calming Measures (Ward 9) (Item 12.5)

The Rules of Order were waived to allow for the introduction of a Motion respecting First Road West and Isaac Brock Drive Roadway Safety Enhancements and Traffic Calming Measures (Ward 9).

For disposition of this matter, refer to Item 24.

(vi) Dewatering of Groundwater from the Roxborough Mixed Income/Tenure Demonstration Project to the City's Wastewater Collection System (Item 12.6)

The Rules of Order were waived to allow for the introduction of a Motion respecting Dewatering of Groundwater from the Roxborough Mixed Income/Tenure Demonstration Project to the City's Wastewater Collection System.

For disposition of this matter, refer to Item 25.

(j) GENERAL INFORMATION / OTHER BUSINESS (Item 13)

(i) Amendments to the Outstanding Business List (Item 13.1)

The following amendments to the Public Works Committee's Outstanding Business List, were approved.

(a) Items Considered Complete and Needing to be Removed: (Item 13.1(a)):

- 13.1(a)(a) Addressing Traffic Speed and Volume on Harbourfront Drive
Addressed as Item 2, PW Report 22-009
(PW22027(a))
Item on OBL: ACF

- 13.1(a)(b) Road Safety Audit of the Intersection of Barton Street East and Wellington Street North
Addressed as Item 7.4 on today's agenda - Report PW22054 (Wards 2 and 3)
Item on OBL: ACH
- 13.1(a)(c) Certificate of Recognition (COR™) Program
Addressed as Item 7.6 on today's agenda - Report PW22060 (City Wide)
Item on OBL: AQ
- 13.1(a)(d) Complete Liveable Better Streets Design Manual
Addressed as Item 8.1 on today's agenda - Report PW22055 (City Wide)
Item on OBL: ABO

(b) Items Requiring a New Due Date: (Item 13.1(b)):

- 13.1(b)(a) Municipal Class Environmental Assessment and Conceptual Design of Ancaster Elevated Water Reservoir
Item on OBL: AAP
Current Due Date: June 13, 2022
Proposed New Due Date: June 2023
- 13.1(b)(b) Free-Floating Carshare Pilot Program
Item on OBL: ABW
Current Due Date: Late Q2 2023
Proposed New Due Date: June 12, 2023
- 13.1(b)(c) HDR01 Kenilworth Reservoir Soil Removal and East Cell Upgrades
Item on OBL: ACL
Current Due Date: June 13, 2022
Proposed New Due Date: August 10, 2022

(c) Items to be Referred to another Standing Committee:

- 13.1(c)(a) Increase in Capital Project Expenses
Referred to: General Issues Committee
Item on OBL: ACY

(k) ADJOURNMENT (Item 15)

There being no further business, the meeting adjourned at 5:43 p.m.

Result: Motion CARRIED by a vote of 7 to 0, as follows:

YES - Ward 2 Councillor Jason Farr
YES - Chair - Ward 3 Councillor Nrinder Nann
YES - Ward 4 Councillor Sam Merulla
YES - Vice Chair - Ward 5 Councillor Russ Powers
YES - Ward 6 Councillor Tom Jackson
NOT PRESENT - Ward 7 Councillor Esther Pauls
YES - Ward 8 Councillor John-Paul Danko
YES - Ward 10 Councillor Maria Pearson
NOT PRESENT - Ward 12 Councillor Lloyd Ferguson
NOT PRESENT - Ward 13 Councillor Arlene VanderBeek
NOT PRESENT - Ward 14 Councillor Terry Whitehead

Respectfully submitted,

Councillor N. Nann, Chair,
Public Works Committee

Carrie McIntosh
Legislative Coordinator
Office of the City Clerk



HAMILTON

COMPLETE
STREETS
DESIGN GUIDELINES

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DISCLAIMER

The material presented in this Design Manual has been carefully researched and presented, and is based on latest available standards, guidelines and best practices. However, street design is highly context-sensitive. The guidance presented herein is intended to be flexible so it can be adapted to the context of local conditions. Practitioners should confirm that design treatments are appropriate to a given location, with consideration of relevant site-specific features. Street design is also an evolving practice. Practitioners should confirm that designs are consistent with current legislation, regulations and by-laws before implementation.

PHOTO CREDITS

Unless otherwise specified, all photos were provided by the City of Hamilton or by members of the WSP project team.

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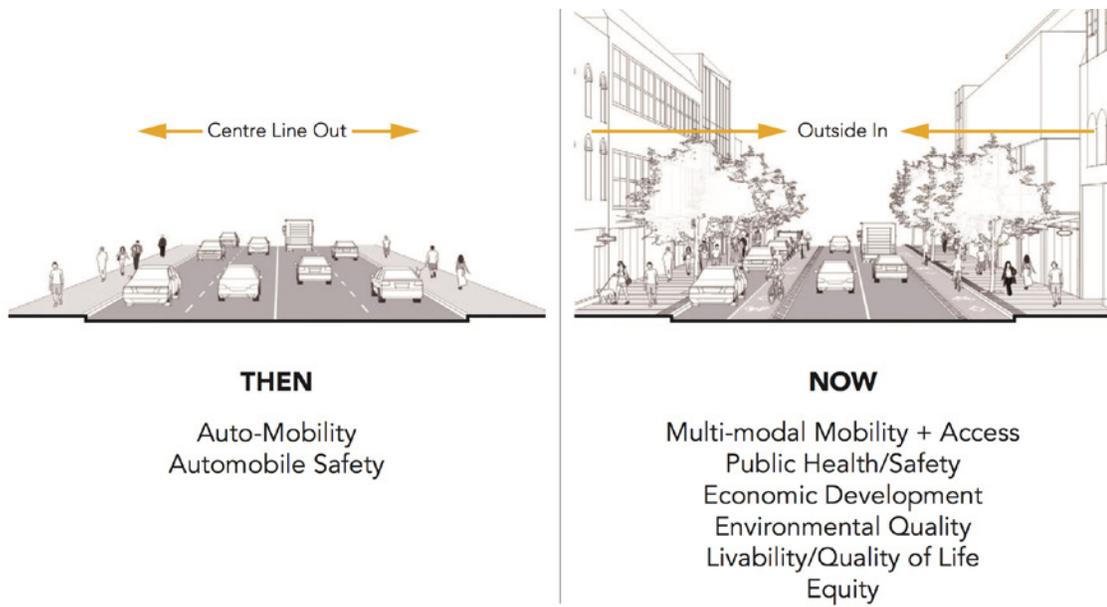
CHAPTER ONE

INTRODUCTION

1.1 WHAT ARE COMPLETE STREETS

Complete Streets are defined as roadways that are planned and designed to balance the needs of all road users. The goal of Complete Streets is to allow people to get around safely no matter their age, ability or how they choose to move. The Complete Streets concept is closely tied to the Safe Systems and Vision Zero approaches to road safety, which aim to design the transportation system to anticipate human error and accommodate human injury tolerance with the ultimate goal of eliminating death or serious injury on roadways.

The Complete Streets approach recognizes that there is no one-size-fits-all solution to street design, as different streets have different priorities, depending on the street's location, context, and role within the transportation system. In contrast to the traditional "centreline out" approach to road design, which is primarily focused on motor vehicle capacity, Complete Streets takes an "outside in" approach that equitably considers the needs of all road users and recognizes the importance of streets not only as conduits to move from one place to another, but also as public spaces and an integral component of the public realm.



Visualization of the "centreline out" and "outside in" approach to road design. Source: City of Toronto

1.1.1 EQUITABLE COMPLETE STREETS

A Complete Streets approach, when applied with a focus on equity, can help to improve the mobility, access and safety of communities who have been marginalized by previous urban infrastructure planning and design practices. Traditional automobile-centric approaches to street design perpetuate systemic inequities, prioritizing the mobility needs of those who have the means to own and operate a private automobile. A Complete Streets approach recognizes these modal inequities and aims to provide equal access for all road users.

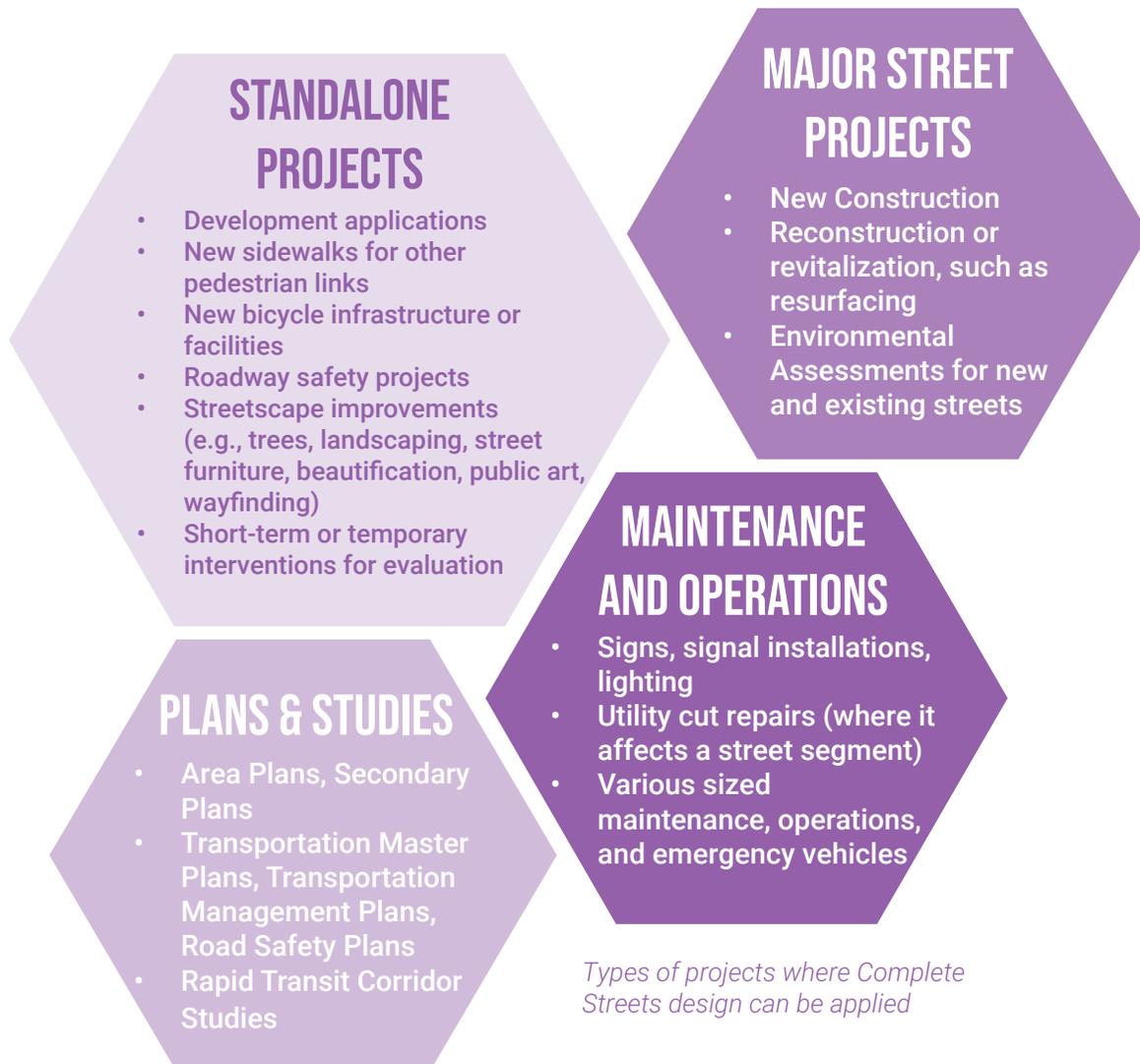
However, an equitable Complete Streets approach requires more than simply applying Complete Streets design principles to every project in the same way. An equity-centric approach explicitly recognizes the historic ways that the benefits and harms associated with previous transportation decisions have accrued across space and time and takes defined action to rebalance how communities benefit from transportation investment. This approach recognizes that mobility has been unequally distributed depending on a person's race, gender, sexual orientation, age, income or ability, and creates policies and practices that seek to eliminate those structural barriers to mobility.



Automobile-centric street designs result in inequitable outcomes, placing those who do not have the means or the capacity to operate a private automobile at a significant disadvantage.

1.2 WHO IS THE MANUAL FOR?

This manual is a resource for everyone involved in the planning, design or maintenance of Hamilton's roadways, including local residents who support safer streets. Guidance for street design, intersection design, implementation, and general design components is covered in this manual to ensure users and practitioners are well equipped to understand and apply the principles of Complete Streets.



1.3 OTHER GUIDELINES

These design guidelines should be used in conjunction with other design guidelines and standards including (but not limited to) the Ontario Traffic Manual, Forestry Guidelines, Urban Design Guidelines, Vision Zero Action Plan, Transit Stop Design Guidelines, and the City of Hamilton Comprehensive Development Guidelines.

The Complete Streets Design Manual is expected to be the go-to resource embedded in other documents. When there are conflicting policies or guidelines, they should be reviewed in tandem to find the most appropriate resolution.

1.4 SUPPORTIVE STRATEGIES AND POLICIES

An effective Complete Streets program requires policies that hold decision makers and practitioners accountable to investing and implementing these streets. While manuals and guidelines may outline processes, designs, and best practices for implementation, policies are what dictate when, how and where guidelines are applied. Policy is a planning tool which provides statutory and regulatory direction on where and how community elements are guided and implemented. Policies serve as mechanisms to enact planning direction and hold municipal staff accountable to regulatory promises established by their governing body. All municipalities are required to plan, adopt, and uphold policies ranging from topic-specific standards and guidelines to higher-order long-term visions.

Policies related to Complete Streets may be incorporated into high-level planning documents to help reinforce the importance of advancing the Complete Streets program in support of other planning objectives. The City's Transportation Master Plan, Cycling Master Plan, Truck Route Master Plan, Recreational Trails Master Plan, Pedestrian Mobility Plan, and several Street Master Plans are all examples of documents that may incorporate policies supportive of Complete Streets.

The Complete Streets Design Manual can play an important role in supporting the City's environmental plans and policies. These include Hamilton's Climate Change goal of net-zero greenhouse gas emissions by 2050 and climate change plans such as Hamilton's Community Climate Change Action Plan, Hamilton's Climate Emergency Declaration, the Draft Community Energy and Emissions Plan, and

the Corporate Climate Change Task Force Corporate Goals and Areas of Focus for Climate Mitigation and Adaptation. Complete Streets can play a very important role of facilitating the design of streets that encourage people to make fewer trips by personal automobiles and that make active and sustainable modes of travel much more attractive.

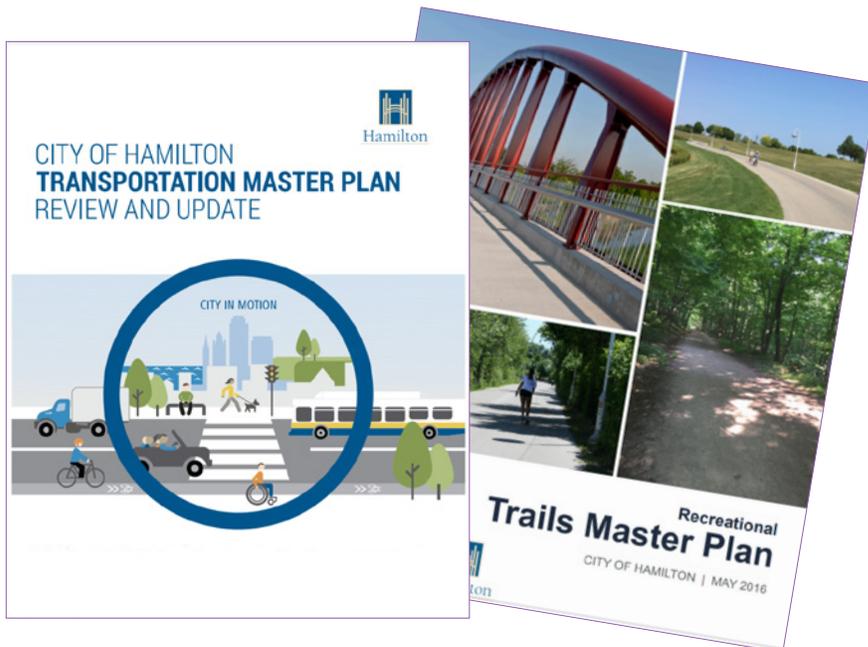
When developing a design manual, it is important to understand how it must comply with existing policy and identify gaps that must be addressed by the design manual itself or by a new policy. Policy is an essential component of an effective Complete Streets program.

A street design manual does not typically serve as a standalone policy document but as a set of guidelines and best practices to operationalize the policy, related to design, implementation, and maintenance. It is therefore imperative that other planning documents and policies reference the Complete Streets Design Manual to necessitate its use in future roadway construction and reconstruction projects.

1.4.1 THE POLICY HIERARCHY

Complete Streets or Complete Streets policies have been referenced in the City of Hamilton's Urban and Rural Official Plans (OPs), and the Transportation Master Plan (TMP) Review and Update. The policies in the urban and rural OPs support the development of guidelines as implementation tools to meet the City's objectives. The

TMP identifies the need for the development of a Complete Streets Design Manual and recommends policy changes to support the implementation of Complete Streets. The table below provides an overview of the relationship of the Complete Streets Design Manual with higher level policy guidance.



Examples of Hamilton's guiding planning documents

Complete Streets Design in the Policy Hierarchy

PROVINCIAL STATUTES	Provincial legislation such as the Highway Traffic Act that must be strictly upheld without deviation.
PROVINCIAL POLICIES	Provincial statutory documents such as the Provincial Policy Statement or the Growth Plan for the Greater Golden Horseshoe that outline implementable processes and actions that may be interpreted differently depending on context.
OFFICIAL PLANS	Municipal statutory documents that are required by the Provincial Planning Act and Policy Statement that outline how the City will use land, how it will allocate resources to its departments and services, and how it is planning for future growth.
TRANSPORTATION MASTER PLANS	Municipal statutory documents that reflect the objectives of the Official Plan and outline actions to implement the City's vision for transportation infrastructure and services. This includes the City-Wide Transportation Master Plan (TMP) and specific master plans such as the Ancaster TMP, Downtown TMP, Cycling Master Plan, or Truck Route Master Plan.
COMPLETE STREETS DESIGN MANUAL	A municipal document that reflects the City's street design, implementation, and maintenance objectives for Complete, Livable, Better Streets. The guidance included within this document will be flexible and may be interpreted differently depending on context.
OTHER DESIGN MANUALS AND GUIDELINES	Municipal documents that provide design guidance such as the Construction and Materials Specifications Manual, Traffic Signal and Pavement Marking Designs Drawings, Comprehensive Development Guidelines and Financial Policies, and other design guidelines and standards as identified by staff.

1.5 COMPLETE STREET DESIGN APPROACH

The following design approach is intended to establish consistent decision-making parameters that may be used across all elements of the streetscape. This approach will help guide the design process of Hamilton's future and current streets so that they may meet the Complete Streets objectives outlined in this manual.

Consider the street context. The streetscape should align with the context of the street: Where is it located? Who are the main users of this street? Is this street designed for access or movement? Design parameters for a residential local road should differ from those needed for a rural collector. As such, it is important to consider how features such as right-sizing the roadway for speed and volume, pedestrian amenities, cycling infrastructure types, and the connection between surrounding buildings and the street relate to a street's intended function and environment. Context sensitive street design ensures that a street is designed to maximize its potential as a part of Hamilton's overall transportation network.



A street in Hamilton with various multi-modal options.

Create attractive, vibrant places. Streets that attract and support pedestrians contribute to its sense of vibrancy, further encouraging future visits from residents and visitors alike. This directly relates to streets' function as points of access: streets facilitate a sense of place through people's interaction with the streetscape, appealing to people through an attractive environment built at the pedestrian scale. As such, for streets with a focus on pedestrian accommodation and movement, it is just as important to consider placemaking strategies as it is mobility and safety strategies. Doing so contributes to a healthy pedestrian realm while also supporting local economic activity.

Prioritize transit and active transportation. Crucial to sustainable mobility is the provision of transit and active transportation infrastructure. Not only does this encourage healthy and active living for Hamilton's residents, but this also aligns with Hamilton's goal to achieve net zero greenhouse gas emissions by 2050. Enhancing pedestrian and cyclist comfort along key routes helps demonstrate that these are accessible and safe options, contributing overall to fewer trips made by private vehicle. Ensuring a connective network of cycling and transit options enhances the convenience of these travel modes, further contributing to increased trips made with active modes of transportation.



Bike share options can provide opportunities for tourists or casual bike riders.

Provide safe and accessible options. To create Complete Streets that people want to use, it is important to design streets with the goal of improving safety and accessibility. This will differ depending on the specific road user in mind. Pedestrians, cyclists, transit users, and motorists must be fairly considered in the planning and design of each street classification and the associated services and utilities. While roads have historically been planned with motorists at the top of the hierarchy, Complete Streets aims to provide more consideration for pedestrians, cyclists, and transit users so that they may feel equally part of the roadway design, a vital component of Vision Zero goals. This contributes to the idea that streets do not just facilitate movement but also a sense of place. A sense of safety and ease of access contributes to a desire to casually walk and linger along certain streets, thereby contributing to a place's sense of vibrancy and attractiveness.



It is important to consider a variety of users and their levels of accessibility when designing for Complete Streets.

Account for equity. There are many effects of inequitable transportation investments, with the burden landing hardest on vulnerable members of the community including seniors, children, people with disabilities, communities of colour and low-income residents. In the past, transportation investments have prioritized the mobility of automobile owners, frequently through the construction of high-volume, high-speed roadways that travel through or adjacent to communities with a higher proportion of communities that have been marginalized. In theory, these investments provide equity of access. For example, the low-income community can use the road the same as the commuters driving through their neighbourhoods. Where they clearly fail, however, is in equity of outcome. Because low-income communities have a greater reliance on transit, walking and cycling due to the high costs to own, operate and maintain a vehicle, the high-speed road serves as a barrier to their mobility. The road, which prioritizes a mode of transportation that is less accessible to the residents of the community it intersects, brings more risk in the form of air pollution, noise pollution and elevated crash risk than it generates in benefits in the form of improved mobility.

The City of Hamilton should strive for Equity of Outcome when it is implementing this Manual, aiming to provide improved mobility for all users while also reducing the level of risk placed on communities, particularly those who have historically been at highest risk of the negative consequences of the current transportation system.

Prioritize connectivity. New Complete Streets should not be designed as standalone segments. Instead, keep in mind the role each street plays in the overall road network. By enhancing the City's overall network connectivity, this can increase utilization and encourage new active transportation users. To do so, consider emphasizing active transportation infrastructure along streets with many connections to neighbourhood amenities (retail, community spaces, green space). The effectiveness of Complete Streets infrastructure grows when coupled with the presence of destinations that are accessible within a fifteen minute walk or ride from residents.

Consider cost effectiveness. The design of Complete Streets should consider the environmental, social, and economic benefits and costs associated with their construction, operation and maintenance. Consider both the direct and indirect costs of such infrastructure. Designing streets for continued long-term use can reduce the number of costly retrofit projects needed in the future. Complete Streets design should be future-focused with resiliency built into the networks, materials, and travel modes they include.

1.6 FORMAT OF THIS MANUAL

The remainder of this manual is structured into the following chapters:

- **Chapter 2** outlines the process for planning, designing and implementing Complete Streets
- **Chapter 3** describes the major elements of complete streets, and provides design guidance on each of them
- **Chapter 4** outlines the eight street typologies that make-up Hamilton's Complete Streets Policy
- **Chapter 5** provides guidance on intersection treatment

2

CHAPTER TWO

UNDERTAKING COMPLETE STREETS DESIGN

2.1 PROCESS OVERVIEW

Rethinking the City’s streets from a Complete Streets lens requires a multi-stage process that spans from early planning and conceptualization stages to implementation and monitoring. The following five stage process outlines the key steps City staff should follow to incorporate Complete Streets elements into capital projects for new construction, reconstruction, and rehabilitation of the City’s street network.

Process for Complete Street Planning and Design

	DEFINITION	GOAL	TOOLS
1. PLAN	<ul style="list-style-type: none"> Identify and prioritize candidate projects. Begin preliminary project scoping. 	<ul style="list-style-type: none"> Project prioritization Preliminary project scoping 	<ul style="list-style-type: none"> Complete Streets Audit Tool Project Scope Tool
2. CONCEPTUALIZE	<ul style="list-style-type: none"> Establish design priorities for the project, develop a vision for what the complete street design will look like, and engage with key internal and external stakeholders. 	<ul style="list-style-type: none"> Develop project scope. Develop preliminary vision for the street including cross-sections and plan profiles. Coordination between City divisions and agencies. 	<ul style="list-style-type: none"> Project Scope Tool Chapter 3 Chapter 4 Chapter 5 Checklist for Designers and Reviewers Stakeholder Map Complete Streets Audit Tool

3. DESIGN

4. IMPLEMENT

5. MONITOR

DEFINITION	GOAL	TOOLS
<ul style="list-style-type: none"> Develop preliminary and detailed design of the complete street, balance trade-offs, priorities and stakeholder feedback while documenting the rationale for design decisions. 	<ul style="list-style-type: none"> Retain consultant, if applicable Prepare 30%, 60%, 90%, and 100% design drawings. Document rationale for design decisions. 	<ul style="list-style-type: none"> Project Scope Tool Chapter 3 Chapter 4 Chapter 5 Stakeholder Map Desired Conditions Matrix Street Element Decision-making Tool
<ul style="list-style-type: none"> Tender and construct the final complete street design and communicate with key stakeholders. 	<ul style="list-style-type: none"> Tender project If applicable, build design. 	<ul style="list-style-type: none"> Construction management and administration plan.
<ul style="list-style-type: none"> Evaluate the complete street's performance against project goals and document lessons learned for future projects. 	<ul style="list-style-type: none"> Monitor to optimize operations and maintenance. Document and communicate lessons learned for future projects. 	<ul style="list-style-type: none"> Update the audit tool with lessons learned.

Roadway projects that are led by developers are subject to the four-stage Developer-Led review process, shown on the following page. These projects could include new public streets designed and built as part of a development, such as in a new subdivision, or could include augmentations to an existing public roadway, such as traffic calming to mitigate impacts from an in-fill project.

The process ensures that developers incorporate Complete Streets design principals in new development sites and subdivisions. The workflow for developer-led projects shown in the following table is intended to be integrated with the five-stage Complete Streets design process outlined earlier.



Open house engagement processes are an important step in the design process.

Draft Process for developer-led Complete Street Planning and Design projects

	GOALS	TOOLS
1. INITIAL PROPOSAL REPORT AND CONSULTATION	<ul style="list-style-type: none"> Ensure that developer has incorporated Complete Streets design criteria in the initial proposal that reflect the goals of this design manual, the Rural or Urban Hamilton Official Plan, and any relevant Secondary Plans. 	<ul style="list-style-type: none"> Chapter 3 Chapter 4 Chapter 5 Stakeholder Map Street Classification Priorities Tool Street Element Decision-making Tool
2. DRAFT APPROVAL	<ul style="list-style-type: none"> Exceptions to ROW widths or design guidelines meet or exceed the requirements of this manual and the relevant Official Plan. Ensure that the draft plan approval conditions include appropriate Complete Streets design criteria and guidelines. 	<ul style="list-style-type: none"> Checklist for Designers and Reviewers Desired Conditions Matrix Street Element Decision-making Tool
3. FOCUSED DESIGN STUDIES / CONSOLIDATED DRAWING REVIEW	<ul style="list-style-type: none"> Ensure that applicable design studies and servicing plans include Complete Streets design criteria and guidelines. 	<ul style="list-style-type: none"> Chapter 3 Chapter 4 Chapter 5 Stakeholder Map Desired Conditions Matrix Street Element Decision-making Tool
4. FINAL APPROVAL	<ul style="list-style-type: none"> Application approved and advances to construction 	<ul style="list-style-type: none"> Stakeholder Map

2.2 PLANNING

Generally, street improvement projects can be initiated and prioritized through one of the following means:

- **State of good repair:** such as a planned street resurfacing or rehabilitation;
- **Performance improvement:** aimed at improving safety and road operations or upgrading existing streets such as introducing a two-way centre turn lane, improving active transportation and transit, or widening sidewalks; or
- **Greenfield development:** new street(s) constructed as part of a development.

These types of street improvement projects should be initiated with a Complete Streets lens. For example, improvements should be critically considered through multi-modal, Climate Emergency, and Vision Zero lenses, acknowledging that complete street design can help support mobility, safety, and sustainability objectives. For this reason, a vision and goal-setting exercise should be completed in the early stages of a Complete Streets project to clearly outline the City's desired outcomes for the corridor.

The following processes, tools, and strategies should also be considered at the project identification and prioritization phases. This coincides with Step 1 of the process outlined on pages 14-15.

2.2.1 NETWORK PLANNING

Network planning and gap identification involves analyzing the City's street network and identifying missing links in the pedestrian, cycling, transit, or freight networks. The City's Transportation Master Plan (TMP) and Cycling Master Plan Update (CMP), approved by City Council in 2018, were developed to guide future transportation-related studies and projects to address these gaps in the network for all modes.

2.2.2 SECONDARY PLANS

Secondary Plans are policy documents that are a part of the Official Plan and support the City-wide policies of the Official Plan by providing more detailed land use planning guidance for specific geographic areas. City staff should work to ensure that Complete Streets design principles are incorporated when Secondary Plans are developed or updated. Secondary Plans can provide policy support for area-specific TMPs and can be used to identify key planned Complete Streets improvements in policy.

2.2.3 SUBDIVISION AND SITE PLANS

Subdivision and site plans are typically part of development applications that City staff need to review and approve and are a key project input to the planning process. Since these types of plans will impact the street network for their corresponding areas, staff reviewing the plans should work to ensure that Complete Streets design principles are incorporated into the plans.

2.2.4 SETTING CORRIDOR VISION AND GOALS

A vision and set of goals for the identified corridor leveraging the existing and future planning and policy context and the Design Principles are laid out in [Chapter 1](#). The goals should include desired outcomes for all modes and street elements.

They should also seek to engage public and private stakeholders early on to support the development of the project vision. This can help set the stage for ongoing proactive engagement throughout the project life cycle. [Section 2.3.2](#) includes a list of common stakeholders that should be engaged for projects.

2.2.5 ENVIRONMENTAL ASSESSMENTS

The municipal Class Environment Assessment (EA) is a key tool for municipalities planning infrastructure projects such as road improvements. Municipal Class EAs ensure that governments and public organizations consider the potential environmental impacts of a project and plan mitigation strategies before the implementation.

The EA process typically aligns with the Conceptualization and Design phases of a project, while completed EAs can help staff identify and prioritize Complete Streets investments to inform the planning stage. This coincides with the steps of the process outlined on pages 14-15.

2.2.6 PLANNING FOR EQUITY

Transportation planning decisions have significant impacts on the quality of life of residents. They can impact access to housing, education and employment, determine who has access to green space and the outdoors, and define where externalities such as poor air quality are located. But these impacts are not felt equally across the spectrum of marginalization. An extra block driven by an able-bodied person in a car has much less impact than requiring a senior who uses a cane to walk an extra block

to access a safe crossing. Equitable multimodal planning requires consideration of the impacts of design choices on people who have experienced marginalization and a proactive effort to remove barriers for those whose mobility is currently impeded. To accomplish this, designers should consult local demographic data to determine if there are higher concentrations of equity-deserving communities near the project, and should prioritize engagement with those communities as they are initiating the design project.

2.2.7 COORDINATION OF CAPITAL WORKS

The coordination of capital works is a critical step in the capital budget process that involves gathering information from different City divisions for various infrastructure projects. These projects provide opportunities to improve the streetscape and multi-modal performance. As such capital coordination will play a crucial role in coordinating the relevant internal stakeholders.

The key outputs of the process are asset renewal timelines that set the stage for when roads will be rehabilitated or reconstructed. These timelines will be key inputs in the planning and prioritization stage of the project.

2.2.8 PILOT PROJECTS

Many elements of Complete Streets can be tested and implemented as pilot projects with minimal civil work or construction. Pilot projects can be considered when a complete street redesign can be accommodated within the existing curb-to-curb space. Leveraging quick build materials such as paint, bollards, or planters to reconfigure the street would allow staff to test a Complete Streets design, collect data to monitor and evaluate performance, and collect stakeholder feedback to inform decision making for reconstruction projects.



The Danforth pilot project in Toronto proved successful in beautifying the street while also providing extensive active transportation infrastructure.

Pilot projects provide the public with an opportunity to interact with the street and have a tangible experience with the Complete Streets design and provide more informed feedback on the design. Pilot projects can provide the flexibility of improving the design iteratively based on monitoring and feedback.

2.2.9 TREND ANALYSIS

Monitoring trends involves collecting and tracking municipal data to identify and prioritize projects. Data such as collision data, traffic volume (AADT), transit ridership, and pedestrian and cycling activity can be used to determine high risk locations around the City for vulnerable road users that can be prioritized for Complete Streets improvements.

Data can also be used to evaluate the outcomes of a Complete Streets project. In the planning stage, practitioners should develop site-specific performance indicators to measure how the project meets the established vision and goals. Before implementation, baseline data for the relevant indicators should be collected to compare to post-installation data to evaluate the effects of the project.

2.2.10 AUDITING AND AUDIT TOOL

Auditing a corridor involves evaluating the “completeness” of specific street segments to help designers determine which Complete Streets elements should be prioritized. Input conditions for the audit can be based on public and stakeholder feedback, data

analysis, or reviewing as-built designs. Auditing the candidate corridor will help guide the development of the project scope.

The Complete Streets Audit Tool evaluates and visualizes how the existing or proposed design of the street segment balances the Complete Streets elements. The tool is integrated with the Street Element Condition Definitions ([Section 2.2.10](#)) which defines a level of 1 (low) to 5 (high) for each street element based on the level of accommodation for that element. The corresponding Desired Conditions Matrix shows the target level for each street element by typology.

The Audit tool is an interactive tool that allows users to select the relevant typology, assess current or proposed street conditions for each street element and evaluate them based on the desired conditions for that typology. The steps to use the tool are outlined on the following pages.

- 1. Input Data:** users provide information about the street being reviewed to inform the selection of the typology. Input data includes the street name, location, functional classification, land use context, right-of-way width, traffic volumes, posted speed limits, Potential for Safety Improvement (PSI) value, and whether the street is on the BLAST network.

STEP 1: INPUT DATA

Provide some information about the street you're reviewing. The functional classification and context are used to inform the Complete Streets Typology

Street name	Anywhere Street	Right-of-way width (m)	25
Location	Neighbourhood	Traffic volume (ADT)	4,000
Functional classification	Local	On BLAST network?	No
Context	Urban	PSI	2.38

- 2. Select Typology:** based on the input data provided in Step 1, select the preferred typology for the street. Once the typology is selected, the tool will automatically populate the Desired Roadway Conditions for each Complete Street element for that typology.

STEP 2: SELECT TYPOLOGY

Select the preferred Complete Streets Typology, considering the information provided in Step 1. Suggested typologies are highlighted

Complete Streets Typologies

- Urban Avenue
- Transitioning Avenue
- Main Street
- Connector
- Industrial Street
- Neighbourhood Street**
- Rural Road
- Rural Settlement Road

Neighbourhood streets provide direct access to residential areas. They accommodate safe and comfortable pedestrian and cycling movement, and are not intended to serve through traffic

- 3. Assess Current/Proposed Street Conditions:** refer to the Street Element Condition Definitions below for definition of condition values for each street element and assign a score based on the current or potential future conditions.

STEP 3: ASSESS CURRENT / PROPOSED STREET CONDITIONS

Enter a value from 1 to 5 for each of the street elements, considering either current conditions or proposed conditions if evaluating a proposed Complete Streets design. Refer to the Condition Definitions for a description of each of the condition values.

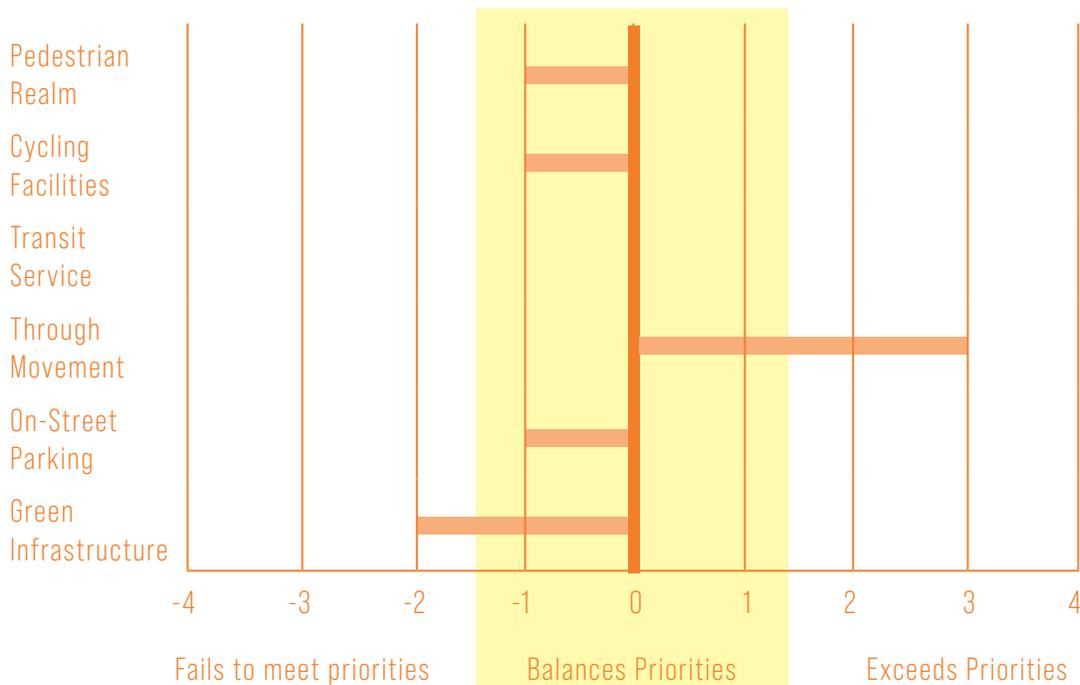
Pedestrian Realm	2	Through Movement	4
Cycling Facilities	1	On-Street Parking	2
Transit Services	1	Green Infrastructure	2

- 4. Review Results:** once step 3 is completed; the table will automatically calculate the difference between desired and existing conditions. The bar graph visualizes where existing conditions exceed, meet, or fail to meet priorities for the specific typology. Priorities are considered balanced if all street elements fall within the shaded area. If some street elements exceed priorities, consider reallocating street space to improve conditions for elements that are failing to meet priorities. Return to Step 3 and adjust until a satisfactory result is achieved.

STEP 4: REVIEW RESULTS

Review the results shown below. Priorities are balanced if all street elements fall within the shaded area. If some street elements exceed priorities, consider reallocating street space to improve conditions for elements that are failing to meet priorities. Return to Step 3 and make adjustments until a satisfactory result is achieved.

	Pedestrian Realm	Cycling Facilities	Transit Service	Through Movement	On-Street Parking	Green Infrastructure
Desired Condition for CLB Typology	3	2	1	1	3	4
Current / Proposed Condition	2	1	1	4	2	2
Exceeds / Fails to Meet Priorities	-1	-1	0	3	-1	-2



This diagram demonstrates the effectiveness of various street elements in meeting conditions for Complete Streets as set out by this manual.

2.2.11 STREET ELEMENT CONDITION DEFINITIONS

The Street Element Condition Definitions below are used to describe the relevant desired conditions per typology and to audit an existing street. Ratings for each element are graded from 1 to 5. The rating reflects the level of accommodation or level of service for that street element.

Pedestrian Realm

	URBAN	RURAL
1	<ul style="list-style-type: none"> No sidewalk or multi-use path (MUP)* 	<ul style="list-style-type: none"> Possible granular/soft shoulder
2	<ul style="list-style-type: none"> 1.5 m pedestrian clearway (may be adjacent to curb) 	<ul style="list-style-type: none"> 1.2 m paved shoulder
3	<ul style="list-style-type: none"> 1.8 m pedestrian clearway with 0.5 m edge zone (measured from back of curb) <i>or</i> 3.0 m MUP with 0.6 m edge zone Street trees/furnishing zone if feasible 	<ul style="list-style-type: none"> 1.5 m paved shoulder
4	<ul style="list-style-type: none"> 2.0 m pedestrian clearway with 1.0 m edge zone <i>or</i> 3.5 m MUP with 1.5 m edge zone Street trees and pedestrian amenities in planting/furnishing zone 	<ul style="list-style-type: none"> 3.0 m MUP, physically separated from travelled portion of roadway
5	<ul style="list-style-type: none"> 2.5 m pedestrian clearway with 1.0 m edge zone Animated pedestrian corridor with street trees, pedestrian amenities, active street frontages and public art 	<ul style="list-style-type: none"> 3.0 m MUP, beyond clear zone of road

* Also known as a multi-use trail (MUT)

Cycling Facilities

	URBAN	RURAL
1	<ul style="list-style-type: none"> No cycling facilities, sub-standard facilities, or facilities that are not contextually appropriate (based on Book 18 nomograph) 	<ul style="list-style-type: none"> Possible granular/soft shoulder
2	<ul style="list-style-type: none"> Shared operations, preferably on roadway with no marked centreline. Posted speed: Max 40 km/h (30 km/h preferred) Volume: Max 3,000 ADT (<1,500 ADT preferred) 	<ul style="list-style-type: none"> 1.2 m paved shoulder
3	<ul style="list-style-type: none"> Bike lane, buffered bike lane, or advisory bike lane, in conditions supported by Book 18 nomograph; <i>or</i> Separated bike lane, cycle track, or MUP, minimum 1.5 m (one way), 3.0 m (two way). Separation may be semi-permeable (e.g. flex bollards or mountable curb) 	<ul style="list-style-type: none"> 1.5 m paved shoulder <i>or</i> Advisory bike lane
4	<ul style="list-style-type: none"> Separated bike lane, cycle track, or MUP, minimum 1.8 m (one way), 3.5 m (two way) Separation elements are non-permeable (e.g. barrier curb, low-wall concrete barrier) Minimum 0.6 m buffer or edge zone 	<ul style="list-style-type: none"> Buffered paved shoulder <i>or</i> 3.0 m MUP, physically separated from travelled portion of roadway
5	<ul style="list-style-type: none"> Cycle track or MUP, minimum 2.0 m (one way), 4.0 m (two way) Minimum 1.5 m edge zone (may be reduced to 1.0 m for one-way cycle tracks on 40-50 km/h roads) 	<ul style="list-style-type: none"> 3.0 m MUP, beyond clear zone of road

Transit Service

1	<ul style="list-style-type: none"> • No transit service or transit service where stop has no hard surface pad
2	<ul style="list-style-type: none"> • Local transit service. • Some stops have hard surface pad allowing passenger boarding/ alighting from all doors
3	<ul style="list-style-type: none"> • Frequent local transit service. • Most stops have hard surface pads, shelters and basic amenities
4	<ul style="list-style-type: none"> • Frequent local service or limited stop express service with significant transit priority elements (e.g. queue jump lanes, transit signal priority) • Most stops have enhanced amenities (e.g. interior heating, real-time arrival information, fare vending machines)
5	<ul style="list-style-type: none"> • Rapid transit service with dedicated transit lanes and comprehensive priority measures • Most stops have enhanced amenities consistent with category 4

Through Movement (Vehicles and Freight)

	URBAN	RURAL
1	<ul style="list-style-type: none"> Design treatments promote slow speeds and divert through traffic. No marked centreline. Drivers may need to alternate directions, yielding to oncoming traffic. 	<ul style="list-style-type: none"> Less than 6.0 m pavement No paved shoulder
2	<ul style="list-style-type: none"> Maximum one lane per direction, two lanes total (mid-block). Centreline may or may not be marked. No continuous centre turn lane. May include auxiliary turn lane at intersections. 	<ul style="list-style-type: none"> 6.0 to 7.0 m pavement Centreline may or may not be marked No paved shoulder
3	<ul style="list-style-type: none"> Maximum one lane per direction, three lanes total (mid-block). May include continuous centre turn lane. May include auxiliary turn lanes at intersections. Total mid-block lane width < 10 m (excluding bike lanes and dedicated parking lanes). 	<ul style="list-style-type: none"> Two lane roadway with marked centreline Minimum 1.0 m paved shoulders
4	<ul style="list-style-type: none"> Maximum two lanes per direction, four or five lanes total (mid-block). May include centre median or continuous centre turn lane. May include auxiliary turn lanes at intersections. Total mid-block lane width < 16 m. 	<ul style="list-style-type: none"> Two lane roadway with marked centreline Minimum 1.5 m paved shoulders
5	<ul style="list-style-type: none"> More than two lanes per direction or more than five lanes total. <i>or</i> Two or more left turn lanes at intersections. Total mid-block lane width >= 16 m 	<ul style="list-style-type: none"> Three or more lane roadway

On-Street Parking

1	<ul style="list-style-type: none"> • On-street parking is not provided
2	<ul style="list-style-type: none"> • Permanent or off-peak parking if there is sufficient space in the ROW and demand cannot be met with off-street supply. • Parking may be provided in specific locations only (where needed, or where curbside space is available), and may not be provided on every block. Parking may be on one or both sides of the street.
3	<ul style="list-style-type: none"> • Permanent or off-peak parking is provided. • Parking is provided on most blocks along the majority of the curb on one or both sides of the street.
4	<ul style="list-style-type: none"> • Permanent parking on one side of the street in dedicated parking lane, typically with curb bulb-outs at intersections and crossings. • Passenger drop-off, freight loading, and accessible parking where required
5	<ul style="list-style-type: none"> • Permanent parking on both sides of the street in dedicated parking lane with curb bulb-outs at intersections and crossings. • Passenger drop-off, freight loading, and accessible parking where required.

Green Infrastructure

1	<ul style="list-style-type: none"> • Street trees and stormwater management practices are not actively provided. • Tree canopy fails to meet coverage guideline. • Planting arrangement has substandard soil volumes and planting configuration
2	<ul style="list-style-type: none"> • Tree canopy at maturity meets coverage guideline in some locations. • Design incorporates low impact development features where possible.
3	<ul style="list-style-type: none"> • Tree canopy at maturity meets coverage guideline in most locations. • Species diversity is achieved. • Design incorporates low impact development features where possible
4	<ul style="list-style-type: none"> • Tree canopy at maturity exceeds coverage guideline. • Species diversity is achieved. • Design incorporates low impact development features
5	<ul style="list-style-type: none"> • Tree canopy at maturity exceeds coverage guideline • Sustainability, resilience and ecological principles are primary themes of the design. • Low impact development features incorporated in a comprehensive manner

Note: refer to any tree canopy coverage requirements during any street audits

2.2.12 DESIRED CONDITIONS MATRIX

The following matrix can be used to determine the desired conditions for each street typology. This uses a scale of 1-5, with 1 being the least and 5 being the most desired condition.

	Pedestrian Realm	Cycling Facilities	Transit Service	Transit Service (on BLAST network)	Through Movement	On-Street Parking	Green Infrastructure
Urban Avenue	4	4	4	5	3	2	3
Transitioning Avenue	5	5	4	5	4	1	3
Main Street	4	4	3	4	2	4	4
Connector	4	4	3	3	2	2	4
Industrial Street	4	4	3	3	3	1	2
Neighbourhood Street	3	2	1	1	1	3	4
Rural Road	1	4	1	3	4	1	2
Rural Settlement Road	4	3	2	3	3	3	3

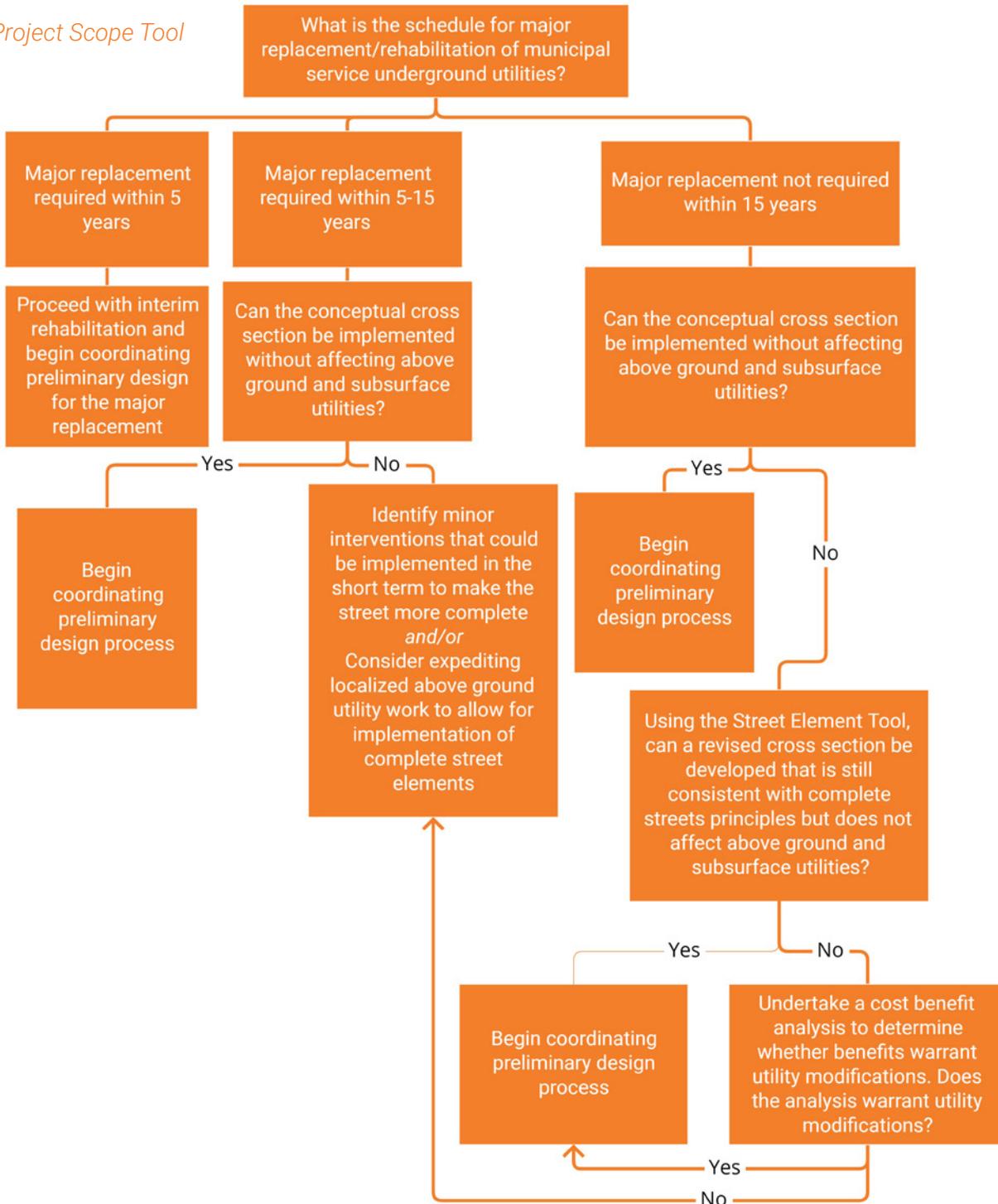
2.2.13 PROJECT SCOPE TOOL

The Project Scope Tool flowchart, shown on the following page, is designed to help staff determine the appropriate project scale for a specific roadway or corridor. The key parameter of the Project Scope Tool is the schedule for major replacement of

utilities and municipal services as these generally have the greatest impact on the scope of a road reconstruction project.

This tool is intended to be used for the initial planning stages of a Complete Streets project. As the project moves from planning to conceptualization and practitioners uncover more details on potential utility impacts of the project, this tool should be revisited to ensure the project is appropriately scoped.

Project Scope Tool



2.3 CONCEPTUALIZATION

Following the selection of a corridor for Complete Streets improvements, practitioners should begin to develop the conceptual design for the street. The cross sections for the relevant street typology in [Chapter 4](#) and the corresponding intersection treatments outlined in [Chapter 5](#) can serve as a starting point for the conceptual design. The concept should then be refined based on core design considerations such as the accommodation of user groups and services, adjacent land uses, network considerations, and relevant user considerations (e.g., cost, land requirements). The Checklist for Designers and Reviews shown below is a tool to help project managers and designers keep track of the key steps and considerations while adjusting the conceptual design for the street.

Checklist for Designers and Reviews

1	SELECT CORRESPONDING CROSS SECTION FROM CHAPTER 4
	Review design elements and target dimensions for the relevant typology and advance to step 2.
2	SELECT CORRESPONDING INTERSECTION DESIGN EXAMPLES FROM CHAPTER 5
	Review design principles and features that should be incorporated at intersections along the corridor and advance to step 3.
3	REVIEW DESIGN CONSIDERATIONS FOR EACH STREET ELEMENT
	Accommodation of the elements below are key design considerations. <ul style="list-style-type: none">• If the answer is no to any of the questions below, the project lead should provide a rationale for why the variance is being proposed and how the proposed concept design will be consistent with Complete Streets principles.• If yes, advance to the next element in this section. <p>Pedestrian Realm and Placemaking Are the pedestrian elements contained in the corresponding Chapter 4 cross section incorporated into the proposed concept design? Do they have similar size/width, distribution along the corridor, and positioning as the elements in the cross section?</p>

Cycling Facilities

Are the cycling and multi-use facility elements contained in the corresponding Chapter 4 cross section incorporated into the proposed concept design? Do they have similar size/width, distribution along the corridor, and positioning as the elements in the cross section?

Transit Facilities

Are the transit supportive elements contained in the corresponding Chapter 4 cross section incorporated into the proposed concept design? Do they have similar size/width, distribution along the corridor, and positioning as the elements in the cross section?

Motor Vehicles

Are the motor vehicle supportive elements contained in the corresponding Chapter 4 cross section incorporated into the proposed concept design? Do they have similar size/width, distribution along the corridor, and positioning as the elements in the cross section?

Green Infrastructure

Are the green infrastructure elements contained in the corresponding Chapter 4 cross section incorporated into the proposed concept design? Do they have similar size/width, distribution along the corridor, and positioning as the elements in the cross section? Do they meet applicable guidelines in terms of soil volume and lateral placement?

Utilities and Municipal Services

Are the utilities and municipal services contained in the corresponding Chapter 4 cross section incorporated into the proposed concept design? Do they have similar depth, configuration, and positioning as the elements in the cross section and do they meet the City's guidelines?

The rationale for any no responses to the questions above should be evaluated with a combination of professional judgment and engagement with relevant stakeholders to determine if the proposed variance is acceptable. For example, if proposed planting zones have narrower widths than target values in the guideline, a Parks and Forestry representative should be consulted to determine if the soil volumes would be adequate and that the variance is acceptable in this case.

Note: Designers should review the existing conditions of the roadway. If the existing lane widths are narrower than what is prescribed in the Chapter 4 cross section and there are no traffic operations issues, consider maintaining the existing lane widths and adjusting other elements in the cross section accordingly.

4 REVIEW AND REVISE THE CONCEPTUAL CROSS SECTION BASED ON LAND USE CONSIDERATIONS

Area Specific Policies and Plans

Are there any place type provisions or Secondary Plans that would affect the design of the corridor?

If yes, determine whether any modifications are appropriate based on the provisions and document the rationale for any changes.

Utilities and Municipal Services

Are any of the utilities along the corridor scheduled for replacement or construction?

If yes, update inputs in the Project Scope Tool and explore opportunities to align project scopes, design parameters, and construction phasing.

Urban Design Guidelines and Streetscape Master Plans

Are there any area-specific Urban Design Guidelines or Streetscape Master Plans that may influence the design (e.g. Bayfront Industrial Area, Dundas)? Refer to the City-Wide Urban Design Guidelines and its support appendices.

If yes, determine whether any modifications are appropriate based on the provisions and document the rationale for any changes.

Heritage Conservation Districts (HCDs) or Special Character Road

Are any segments of the corridor located within an HCD or been identified as a Special Character Road ?

If yes, ensure a heritage planner is engaged as a stakeholder for the project and determine if any streetscaping or right-of-way related HCD or Special Character Roads policies apply.

Community Improvement Plans (CIPs)

Are any segments of the corridor located within a CIP area?

If yes, ensure the appropriate contacts are involved as stakeholders. Determine if any streetscaping or right-of-way related CIP policies apply.

Conservation Authority (CA)

Are any segments of the corridor located within a Conservation Authority's regulated area?

If yes, ensure the appropriate CA representative is engaged as a stakeholder and determine if any relevant watercourse or natural resource area protection policies apply.

Indigenous Communities

Are there First Nations, Métis and/or Inuit communities that have treaty rights in the project area, or whose lands will be affected?

If yes, contact relevant First Nations, Métis, and/or Inuit representatives to ask about their preferences for engagement, and engage them as project partners and rightsholders.

Rail Authorities

Do any segments of the corridor intersect with railway facilities?

If yes, consult with the relevant railway authority to determine if there are existing regulations or future plans that could influence design considerations.

Future Development

Is development activity anticipated along the corridor?

If yes, consult with the appropriate land use planner to identify parcels with existing or expected development applications, consult with key landowners, and analyze future needs and travel patterns along the corridor.

5 REVIEW AND REVISE THE CONCEPTUAL CROSS SECTION BASED ON NETWORK CONSIDERATIONS

Cycling and Pedestrian Network

Are any segments of the corridor aligned with the planned cycling network outlined in the City's Cycling Master Plan, Recreational Trails Master Plan or major active transportation corridor?

If yes, prioritize comfortable cycling facilities and associated amenities such as bicycle parking and intersection treatments, and connections to trails or major active transportation corridors.

Transit Network

Are any planned or existing transit routes or emergency transit detour routes aligned to any segment of the corridor? Whether or not the corridor is part of a current transit route, consider appropriate accommodations for Accessible Transportation Services (ATS), increased ridesharing activity, or other emerging technologies.

If yes, coordinate with relevant transit agencies to review existing and forecasted routing and ridership, and provide rider amenities and transit priority treatments as appropriate. Ensure roadway geometry accommodates transit vehicles.

Freight/Truck Route Network

Are any segments of the corridor aligned with truck route network outlined in the City's Truck Route Network?

If yes, review roadway geometry parameters to ensure that trucks are appropriately accommodated along the corridor.

Wildlife Corridors

Do any segments of the corridor abut or intersect with known wildlife corridors?

If yes, review corridor to determine potential wildlife crossing locations.

Operational and Traffic Calming Issues

Are there known issues regarding motorist behaviour or road operations along any segments of the corridor?

If yes, determine if appropriate geometric changes and / or traffic calming measures can be incorporated into the design.

6 REVIEW AND REVISE THE CONCEPTUAL CROSS SECTION BASED ON USER CONSIDERATIONS

Complete Streets should be designed to accommodate all users. However, in areas where specific users groups are anticipated to be more prevalent, it may be appropriate to adjust the design to support the user group, such as widening sidewalks near medical facilities or incorporating traffic calming measures near schools. If the groups listed below are prevalent along the corridor, they should influence the project's design considerations.

Children (i.e. proximity to a school)

If yes, consider providing wider sidewalks, designated pick-up and drop-off areas, traffic calming, and in-boulevard cycling facilities.

Post-secondary students

If yes, consider providing wider sidewalks, providing high capacity cycling facilities, and increasing transit priority in proximity to post-secondary institutions and student housing.

Individuals with disabilities and age-friendly considerations

If yes, provide wider sidewalks, frequent seating opportunities and shaded areas, and well-designed accessible transit drop-off areas near seniors' residences, hospitals, and related facilities.

Underserved communities and equity considerations

If yes, prioritize safe and comfortable links to common social, employment, and civic destinations used by low income and vulnerable communities.

Businesses without off-street parking or laneways

If yes, consider providing loading zones to facilitate deliveries and pick-ups for local businesses.

2.3.2 ENGAGEMENT AND COLLABORATION

Designing streets through a Complete Streets lens requires integrating different street elements that are the responsibility of various divisions and agencies at the City. For example, adding street trees to a corridor will require consideration of utility placement, clearances from adjacent travel lanes, cycle tracks, or sidewalks, and maintaining sight lines for motorist and cyclist safety. As the design concept is being developed, staff should engage relevant stakeholders to gain an understanding of stakeholder needs, processes, availability of resources, and asset conditions. Building off the vision and goal setting from the Planning stage, practitioners should engage the public during this phase to get input on design priorities and to identify and document usage patterns and local issues.

The Stakeholder List below outlines key municipal and agency partners, and private and public stakeholders that should be involved early and continuously throughout the process. Staff should establish primary and secondary stakeholders and develop a consultation plan to address each stakeholder's needs through the duration of the project. The International Association for Public Participation's (IAP2) spectrum of public participation and the city's Public Engagement Charter should be applied to determine the appropriate approach for each stakeholder group. This list is not exhaustive; there may be other stakeholder groups that should be consulted depending on the location and context.



Engagement event in Brampton.

Stakeholder List

Internal Stakeholders

Hamilton Public Works

- ◇ Transportation Operations and Maintenance
- ◇ Engineering Services
- ◇ Infrastructure Planning
- ◇ Landscape Architecture
- ◇ Forestry and Horticulture
- ◇ Hamilton Water
- ◇ Transit (HSR & ATS)

Planning & Economic Development

- ◇ Transportation Planning and Parking
- ◇ Growth Management
- ◇ Placemaking, Public Art and Projects
- ◇ Heritage and Urban Design
- ◇ Sustainable Communities Section

Healthy and Safe Communities Departments

- ◇ Public Health Services - Healthy Environments Division
- ◇ Hamilton Fire Department, Emergency Management

Other Governing Authorities

Ontario Provincial Government

- ◇ Ministry of Transportation (MTO)
- ◇ Ministry of Infrastructure
- ◇ Ministry of Municipal Affairs and Housing (MMAH)

Adjacent Municipalities

Adjacent Indigenous Nations

External Stakeholders

Utilities and Railways

- ◇ Bell
- ◇ Rogers
- ◇ Telus
- ◇ Cogeco
- ◇ MTS Allstream
- ◇ Hydro One
- ◇ Alectra
- ◇ Hamilton Community Energy
- ◇ Metrolinx
- ◇ CN
- ◇ CP

Organizations and Advisory Committees of Council

- ◇ Urban Indigenous Organizations
- ◇ Cycling and Environmental Organizations
- ◇ Municipal Advisory Committees (e.g. Cycling Advisory Committee, Advisory Committee for Persons with Disabilities, Seniors Advisory Committee)
- ◇ Hamilton Bike Share
- ◇ Environmental and Social Justice Organizations
- ◇ Conservation Authorities
- ◇ Mobility Lab

Community Associations and Councils

Business Improvement Areas Chambers of Commerce

Education

- ◇ Local school boards and schools
- ◇ Post-secondary institutions
- ◇ School Travel Planning teams

2.4 DESIGNING

Following the planning and conceptualization phase, the design concept is refined further by balancing the priorities outlined in the previous phases with physical and budgetary constraints, public feedback, and other context-sensitive considerations. Collaboration among City divisions and other key stakeholders is crucial at this phase to make and document trade-offs to ensure that the designs are feasible and meet relevant technical standards.

2.4.1 DESIGN PHASES

The design process is typically split into two phases:

- **Preliminary design:** outcomes include the project scope, a preferred cross section and 30% design drawings, preliminary construction cost estimates, and documented design rationale.
- **Detailed design:** outcomes include a tender package with 100% drawings, specifications, and the final construction cost estimate.

2.4.2 PROJECT LEVELS

Projects are typically classified as one of the following three levels:

- **Rehabilitation:** involves minimal construction or civil work. Typically changes are made between the existing curbs. This can include resurfacing and reconfiguring the road platform to introduce Complete Streets elements. For these types of projects, designers should seek to minimize the impacts to existing trees and utilities as much as possible.
- **Reconstruction:** typically involves reconstruction of the road platform and may involve reconstructing the boulevard. Reconstruction presents a significant opportunity to introduce Complete Streets elements to the corridor. Designers should coordinate with relevant utility stakeholders for these types of projects to determine if any utilities should be modified or replaced as part of the project.
- **Major projects:** large-scale capital projects, for example new developments within the City that include constructing new infrastructure. These projects present the most significant opportunity to reimagine the existing streets or construct new streets with a Complete Streets lens.

2.4.3 CONSULTATION

Consultation is a key component of the design process. As described in the Conceptualization section, designers should seek to engage the public early and often throughout the design process. Designers should also note that when projects are planned in communities with a high proportion of equity-deserving groups, additional engagement will likely be required. The City should identify priority populations to be consulted, and structure plans to deliberately engage with that audience. Special care should be taken to build trust and capacity within these communities, which may require additional engagement resources, especially as the preliminary design concepts are being developed and the overall vision for the project has not yet been finalized. Having previously marginalized communities see their priorities reflected in the direction of the project will help to build trust, and is a vital step towards more equitable implementation of this Manual.

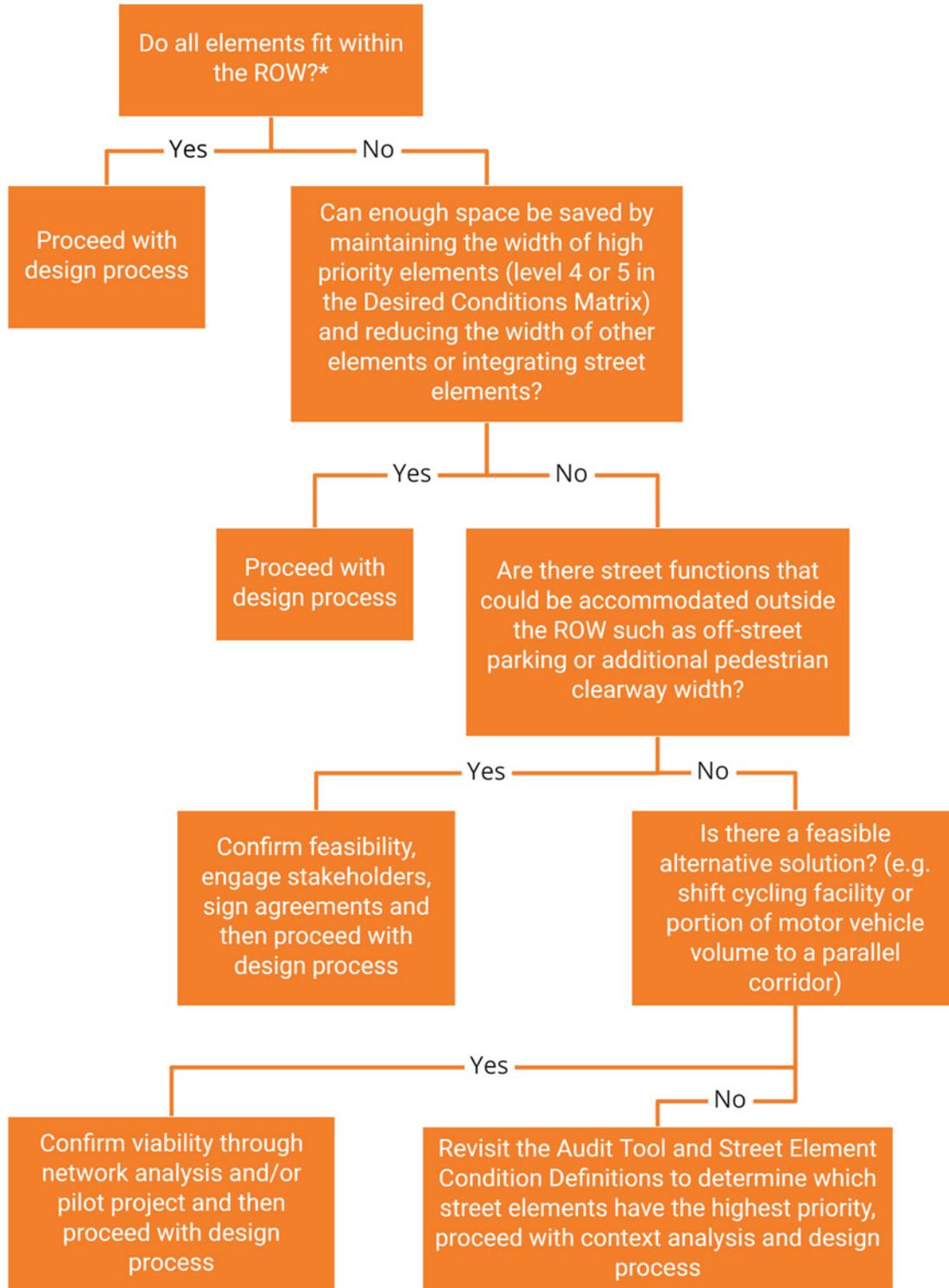
2.4.4 STREET ELEMENTS

In some cases, corridors in the City may be narrower than the right-of-way widths shown in the Chapter 4 cross sections. The following Street Element Decision-Making Tool aims to guide practitioners in balancing the Complete Streets priorities when the right-of-way is not as wide as the corresponding cross section.



Cannon Street in Hamilton.

Street Element Decision-Making Tool



*Refer to the note regarding motor vehicle lane widths in Step 3 of the Checklist for Designers and Reviewers above (section 2.3.1)

2.4.5 DIMENSIONS

Designers should seek to meet target dimensions for each element as outlined in Chapters 3 and 4, however in constrained situations, designers may have to use minimum widths for specific elements.

In this case, practitioners should keep the following in mind when designing with minimum widths:

- Anticipated user volumes
- Relative priority of each element by street classification and project objectives
- Operations and maintenance impacts
- Need for physical separation of vulnerable roads users and motorists
- Permanence of facility and ability to modify it down the line (For example: pilot projects or rehabilitation on a road scheduled for full reconstruction within a few years).

If minimum dimensions are required, designers should consider:

- Assessing whether a posted speed limit reduction is appropriate
- Alerting and guiding road users through pinch points and constrained areas through signage and pavement markings
- Maintaining appropriate sightlines to ensure safety for all road users
- Monitoring user behaviour in the area following implementation to determine if project goals are being met and re-evaluate if not

In a constrained right-of-way, integrating street elements to use the same space for multiple street functions may be necessary to avoid designing to minimum dimensions. This can include integrating elements in the same space such as the Shared Cycle Track Platform Stop described in Chapter 3. Curbside management is another approach to integrate different uses within the same zone within the ROW. For example alternating on-street parking or flex zones with curb bump outs to provide space for street tree planters or transit shelters. The cross sections in Chapter 4 show other examples on how different street elements can be integrated to maximize the use of space.

2.5 IMPLEMENTING

Once the detailed design is completed, the implementation phase begins through tendering and construction of the complete street.

2.5.1 TENDERING

Contractors should be familiar with Complete Streets design elements through the tendering process. Maintaining open lines of communication and fostering a collaborative project workflow between designers, contractors, and contract administrators will help ensure that the design is constructed to meet the vision and goals set out in the planning stage.

2.5.2 OUTREACH AND EDUCATION STRATEGY

Some design elements of Complete Streets may be new to Hamiltonians. Staff should develop a public communication strategy to educate users prior to the opening of a new facility to minimize confusion and ensure designs are used as intended by all road users. This can include online or in-person outreach events, signage along the corridor to explain the new features and their intended use, social media campaigns, and updating relevant content on the City's website.

2.5.3 MAINTENANCE STRATEGY

Plans for ongoing maintenance of the facility should be developed as part of the capital budget submission for the project. Operating costs, maintenance standards, and divisional responsibilities should be identified and included in the relevant operating budgets.

2.6 MONITORING AND REVIEW

2.6.1 AREAS OF LEARNING

Re-imagining streets through a Complete Streets lens is a relatively new approach for planning, engineering, and design. Best practices are being refined and adapted to local contexts as more jurisdictions redesign streets through a Complete Streets lens. As the City adopts these practices, lessons learned should be documented and distributed to relevant divisions and stakeholders to ensure that practitioners gain knowledge from past experiences to benefit future projects.

- **User behaviour and comprehension:** tracking the behaviour patterns of users on different facilities to determine if they are being used as intended. This can help practitioners determine whether a public education campaign is warranted or if adjustments to the design need to be made.
- **Conflict mitigation:** documenting interventions made to mitigate conflicts between different road users and tracking their impact on safety and user behaviour will help the City develop a catalogue of context appropriate interventions.
- **Operations and maintenance:** documenting any operations and maintenance issues following the construction of a complete street and identifying strategies to address them for existing and future designs.
- **Multi-modal mobility:** leveraging tools such as the Multi-Modal Level of Service (MMLoS) to evaluate the project's performance for all modes of transportation can help inform whether designs contribute to meeting the City's multi-modal goals. Refer to OTM Book 18 for more guidance on MMLoS.
- **Constructability / implementation cost and asset life cycle:** documenting project costs and any challenges throughout the project life cycle can help inform cost benefit analysis at early stages of future projects.
- **Equity and quality of life outcomes:** recording the reported changes to quality of life in the local community including improvements to roadway safety, changes to multi-modal access to public services and economic opportunities, and increased opportunities for marginalized communities.

2.6.2 MONITORING STRATEGY

As discussed in the Planning Stage, practitioners should seek to develop a monitoring strategy to understand and communicate the outcomes of Complete Streets designs and document lessons learned to apply to future projects. The flowchart below outlines the basic steps to develop a monitoring strategy for a Complete Streets project.



Determine key metrics: During the Planning stage, staff should decide which metrics will be used to evaluate the project. The metrics selected should relate to the project vision and goals. For example, collision frequency and severity can be a key metric for projects where road safety improvements are a primary goal.

Develop a baseline: Baseline data should be collected prior to any construction work to develop an understanding of the “before” conditions. Data collected after the project completion can then be compared to the baseline data to interpret the impacts of the project on each metric.

Monitor usage patterns regularly: Implementing Complete Streets across the City’s transportation network will affect overall usage patterns. Since usage patterns may not change immediately after a facility opens, it is important to consider performing monitoring activities over long periods of time to allow traffic patterns to stabilize (for example three to six months after implementation). Evaluation of operations in both the summer and winter are important to ensure the road operates adequately in all seasons. It is important to consider both existing and latent demand when observing usage patterns. Complete Streets may address latent demand that is currently unmet by the existing design such as cycling demand on roads that previously had no dedicated cycling facilities. Staff should consider leveraging survey tools throughout the project to gauge interest in the Complete Streets design and gain a better understanding of latent demand for different modes.

Communicate findings and lessons learned: Communicating the outcomes of a project to stakeholders and the public is an important step to close the engagement loop after completing the project. The findings from the monitoring stage should be presented in a way that is easy to understand and digest. Lessons learned from the project should be incorporated in the planning stages of future projects.

2.7 UPDATING POLICY

Provincial and municipal policies provide direction for the development of the Complete Streets policies. The Provincial Policy Statement and the Growth Plan for the Greater Golden Horseshoe support a Complete Streets approach to road design to achieve a multi-modal transportation system that allows for safe movement of people and goods. The City's Transportation Master Plan emphasizes the "City's interest and investment in developing streets that are safe, accessible, accommodating of multiple modes and provide an attractive public realm are consistent with best practices".¹ The TMP highlights the development of this Design Manual as a key step in the Complete-Livable-Better (CLB) Streets policy theme to provide detailed guidance to be applied during the design and implementation phases to shape the look and feel of Hamilton's network of Complete Streets.

These guidelines are meant to inform the design of Complete Streets and are meant to supplement the City's existing engineering standards. Following the publishing of this manual, the TMP outlines supplementary actions including harmonizing existing guidelines and road classifications in the Official Plans with the Complete Streets Design Manual guidelines, while also updating engineering standards, so that policy and standards follow Complete Streets principles.

Practitioners should leverage this Design Manual to implement Complete Streets throughout the City while documenting lessons learned and context-specific adjustments made for each project. The experience gained from the Complete Streets implementations will support City staff in reviewing and updating standards and policies to incorporate Complete Streets design principles.

1 City of Hamilton Transportation Master Plan: Review and Update, 2018

2.8 FINANCIAL CONSIDERATIONS

Despite a common misconception that Complete Streets always cost more than conventional streets, they are often comparable to traditionally planned roads. Many transportation projects in Hamilton are already being designed with a Complete Streets philosophy in mind, incorporating elements included in Council-approved plans and policies such as the Pedestrian Mobility Plan, Cycling Master Plan, and the Recreational Trails Master Plan, among others.

The cost of building Complete Streets is typically comparable to constructing a conventional road, given that many of the elements proposed in the manual are required today. The “missing” Complete Street elements on existing streets are typically integrated into baseline designs produced today and therefore do not necessarily represent an incremental cost compared to traditional designs. Incorporating AODA-compliant sidewalks and crossings, new cycling facilities along existing or planned bike routes, and new street trees are some of the elements that Council has previously approved that are being integrated into design work.

A study undertaken by the Charlotte Department of Transportation compared the cost of Complete Streets projects against traditional road projects over a five-year period.² The research found a slight increase in cost by adding sidewalks and bike lanes, which were partially offset by the lower costs of narrower lanes. However, Charlotte found that overall market fluctuations in construction costs played a more significant role in the costs of a project than the costs for incorporating complete street elements. They concluded that “considering the small percentage of project budgets required to include complete street elements and the significant fluctuation in historical project construction costs, the authors of this paper make the argument for continuing to include Complete Streets items in project scopes and budgets.”

As discussed in the planning, conceptualization and design steps above, a context-specific approach is necessary to reflect unique attributes when completing existing roadways. For instance, it may not be pragmatic to relocate utilities underground or widen an existing bridge/tunnel due to their high costs. While these situations can pose unique challenges, these should not pose a barrier to implementing Complete Streets, as the Manual’s cross-sections, elements, and processes are adaptable to these novel situations.

2 Shapard, J., & Cole, M. (2013). Do Complete Streets Cost More than InComplete Streets? Transportation Research Record, 2393(1), 134–138. <https://doi.org/10.3141/2393-15>

3

CHAPTER THREE ELEMENTS

3.1 PEDESTRIAN REALM AND PLACEMAKING

Walking remains the most affordable and accessible form of transportation. It promotes physical activity and social interaction, and is emissions-free, making it a climate-friendly and healthy mode of travel.

The key to promoting walkability lies in **attentive and intuitive urban design**. Streets designed at a human scale with slowed or limited vehicle traffic can naturally attract pedestrians for the comfort they provide. This largely depends on the design of the pedestrian realm: the part of the street that provides physical space for pedestrian activity, including sidewalks, street trees, and other amenities. A thriving pedestrian realm leads to a strong sense of place; cities designed for walkability are seen as attractive, vibrant, and comfortable by residents and visitors alike.

This section outlines the principles and design practices that bolster a healthy pedestrian realm and sense of place. Further guidance can be found in the Design of Public Spaces Standards under the Ontario Integrated Accessibility Standards regulations, Ontario Traffic Manual (OTM) Book 15: Pedestrian Crossing Treatments, the National Association of City Transportation Officials' (NACTO) Urban Street Design Guide, and NACTO's Designing Streets for Kids Guide.

3.1.1 DESIGN PRINCIPLES

Design for universal accessibility. "Pedestrians" are a broad group of street users, and include people of diverse abilities who may be using strollers or mobility aids such as wheelchairs, canes, or guide dogs. Many built environment standards have historically been written to accommodate the needs of able-bodied men. In reality, these users comprise a minority of the population. Women, children, seniors, and people with disabilities are all users whose needs may be overlooked in the traditional design process. Street design should remove existing accessibility barriers and avoid creating new ones.

Safe and comfortable for all ages. Pedestrians are the most vulnerable road users and especially children and seniors. Where the pedestrian realm interacts with traffic at intersections and driveways, designs need to prioritize safety for these users. Along the street, horizontal separation from live traffic lanes can enhance comfort and safety.

Create well-designed places and spaces. Pedestrians are not always in motion. Well-designed pedestrian realms encourage people to move at their own pace, with the potential for stopping and socializing or sitting to watch the world go by. Each street has its own uniqueness, and the pedestrian realm should complement those features to create a vibrant and enjoyable environment.

Complement adjacent land uses. Certain land uses and development types are more conducive to pedestrian activity. For example, a restaurant might use extra sidewalk space for a patio or an area for customers to line up, while a store might use its outdoor frontage for displaying products. In other cases, the pedestrian realm might be built to complement future anticipated land use, such as along transitioning avenues that are expected to densify.



Streetscape renewal of Gore Park.

3.1.2 PEDESTRIAN ZONES

The pedestrian realm can be organized into four defined zones: the **buffer zone**, the **street tree / furniture zone**, the **walkway zone**, and the **frontage zone**. These zones work together to support the design of Complete Streets. On some streets, some zones may be combined, or the boundaries between zones may not be formally defined. In designing the pedestrian realm, it is important to consider the function of each zone and to ensure that the zones function as a cohesive whole.

The **buffer zone** begins at the edge of the curb and provides a safety buffer between moving traffic and people walking or cycling. It may also accommodate signs, poles, and snow storage. A wider buffer zone should be considered along streets with higher traffic speeds and volumes, especially where speeds are 50 km/h or higher, or volumes

exceed 4,000 vehicles per day. On streets with cycle tracks, the cycle track width acts as an additional buffer from traffic for pedestrians, but a buffer zone should still be provided between the roadway and the cycle track.

The **street tree / furniture zone** is where trees and furniture such as benches or transit shelters are placed. It can be located on either side of the walkway zone. Light poles or utilities may also be placed in this zone. When placed between the buffer zone and the walkway zone, this zone can supplement the buffer to provide additional separation between pedestrians and motor vehicle traffic.

The **walkway zone** provides a clear, unobstructed path of travel for pedestrians. Street furniture and other obstructions are not located in this zone. The target width for the walkway zone should be at least 2.0 metres or more. Width should be increased in areas with higher pedestrian traffic volumes or with higher use of power scooters and other mobility devices. In areas with limited pedestrian activity, a minimum of 1.8 metres is acceptable— the absolute minimum width of 1.5 metres should be applied in constrained cases only.

The **frontage zone** provides a transitional space between the street and the adjacent buildings. It provides a buffer to the property line, and in areas where the sidewalk is adjacent to buildings, provides a buffer from opening doors. It may be used as an extension of the public realm, such as for restaurant patios or retail displays. Note that these recommendations may vary by typology or context.



Illustration of pedestrian zones.

Pedestrian Zone Dimensions

ELEMENT	TARGET VALUE	MINIMUM VALUE
BUFFER ZONE	1.0 m	0.5 m
STREET TREE / FURNITURE ZONE	2.0 m to 3.0 m	1.75 m
WALKWAY ZONE	1.8 m to 2.0 m or more	1.8 m
FRONTAGE ZONE	Varies	0.5 m

In some cases, the pedestrian realm may extend beyond the curb into a **flex zone** that is part of the roadway and designed to be flexible in use. This space may be used for on-street parking, patio space, parklets, bike parking, pick-up / drop-off areas, and bike share stations. The flex zone should be designed to provide barrier-free access to the sidewalk, either by constructing it level with the sidewalk or by providing temporary or permanent curb ramps. The flex zone can be framed by a mountable or semi-mountable curb, depending on the context. See [Section 3.5](#) for more details on curbside management.



An example of a flex zone, accommodating a street patio and on-street parking.

3.1.3 INTERSECTION ELEMENTS

Intersections are conflict points between different road users and should be designed to mitigate conflict and create a safe environment for all modes, especially vulnerable road users such as pedestrians. This section outlines key design elements to improve safety and comfort for pedestrians crossing the right of way.

SIGNALIZED INTERSECTIONS

Cycle Length and Signal Timing

Like other road users, pedestrians rely on signalized crossings to cross at many intersections. Signals inevitably create delays for pedestrians, and this should be considered in the planning and design process. Pedestrian delay is minimized when the intersection's cycle length is as short as possible. For example, at a compact urban intersection with a cycle length of 60 seconds, pedestrians typically have to wait an average of 15 seconds to cross. At a large suburban intersection with signalized left turns for vehicles and a cycle length of 140 seconds, that waiting time increases to approximately 50 seconds, nearly a full minute.

At urban intersections with very high volumes of pedestrians, a pedestrian scramble (also known as a pedestrian priority phase) may be considered. This provides a walk phase for pedestrians travelling in all directions while stopping motorists in all directions.

Pedestrian Crossing Time

It is also important to provide adequate pedestrian crossing time. Caregivers with children, elderly pedestrians, and people using mobility devices are all examples of users who may require more time to cross the street. Accommodating these users may involve increasing the crossing interval beyond the minimums outlined in Ontario Traffic Manual (OTM) Book 12.

PEDESTRIAN CROSSOVER (PXO) / MIDBLOCK CROSSINGS

Crossing frequency is a key factor in the walkability of a street. In addition to signalized crossings at major



A mid-block crossing

intersections, midblock crossings should be placed to align with desire lines, such as between a residential tower and a park or a bus stop. Where block lengths are beyond 100–200 metres, consideration should be given for additional midblock crossings, as a lack of crossings either encourages pedestrians to incur risk and cross at potentially unsafe locations or requires them to walk extra distance to reach the nearest crossing. Refer to OTM Book 15 for the planning and design of pedestrian crossings.

On low-volume streets, rather than creating designated crossing points, it may be preferable to design the street for crossability by creating safe crossing conditions along the entire length. Low traffic volumes and speeds—brought about by traffic calming measures such as on-street parking, reducing the number of traffic lanes, or modal filters—are one way to create streets with more universal crossability. Modal filters may also be applied to help lower traffic volumes; these limit the through-journeys of certain transportation modes along a street using permanent or temporary materials.



Bollards acting as a modal filter, limiting through-traffic to pedestrians and cyclists only. Source: Cycling Embassy of Great Britain

CROSSING DISTANCE

Longer crossings, especially those longer than 30 metres, disproportionately affect vulnerable pedestrians—such as children and people with reduced stamina—as they require more energy to use and involve more exposure to traffic. All pedestrians are better off when crossing distances are minimized, which can be accomplished with curb extensions, smaller corner radii, refuge islands, fewer travel lanes, and narrower travel lanes.

3.1.4 EQUITY AND ACCESSIBILITY

NEEDS OF USERS

It is important to consider that people with disabilities do not all have the same experience. Each disability brings a nuanced way of navigating one's environment. As such, designing accessible streetscapes and roadways should take each experience into consideration:

Partially sighted or blind people may use canes or guide dogs to navigate the built environment. They rely on features such as curbs, contrasting materials, and tactile surfaces to orient themselves and identify potential hazards. Guide dogs are trained to stop at curbs and steps, avoid obstacles, and negotiate traffic, but do not have the cognitive function of a human and thus may become confused by complex designs.

People using wheelchairs or mobility devices rely on a built environment that they can physically navigate with ease. Continuously smooth and level surfaces, curb depressions and ramps at crossings points, and a sufficiently wide path of travel are all examples of physical design elements that improve the experience of wheelchair users.



Example of a depressed curb ramp at a pedestrian crossing in Washington State. Source: Pedestrian Safety Guide and Countermeasure Selection System

People with reduced stamina need places to rest more often, as they may have a harder time navigating slopes, walking longer distances, or may walk at a slower speed. Many intersections are timed for a faster walking speed which may not provide enough time for this group of people to cross the street. Consider providing frequent

benches and other rest areas, minimizing intersection crossing distances, and increasing allocated pedestrian phase time to help these users.

Children undergo much of their cognitive development while navigating the built environment. They are more sensitive to traffic danger and more dependent on their caregivers at younger ages. A well-designed built environment not only provides safety for children, but also supports an active independent lifestyle and fosters their desire for spontaneous play. Examples of designs that support children include wider sidewalks and buffers from traffic, improved sightlines to roadways, longer pedestrian signal intervals, and frequent places to pause and linger.

Women tend to experience the built environment much differently than men. Whether they be caretakers or simply walking on their own, women have unique experiences that warrant specific accessibility needs. Similar to wheelchair users, dropped curbs and ramps may benefit women pushing strollers. Many of the design suggestions that would benefit children, such as traffic calming measures or more frequent places to rest, would also help women who must navigate the street with children in tow. Women may also take more frequent trips than men and tend to make more non-work related trips than men, such as transporting children to school or completing household errands.

Finally, many women cite safety concerns as a deterrent to using public transit or walking alone at night. Dark, empty spaces can come across as unsafe, leaving potentially more women vulnerable to attackers. To combat this, consider thoughtful and inclusive design elements like well-lit pathways, more walkable shared streets, and open gathering spaces with shops or vendors. The latter may help contribute to a sense of safety in numbers, or “eyes on the street”, wherein the presence of others, and especially other women, can improve their sense of security in public.

DESIGN ELEMENTS

The following outline some design elements that should be considered when designing for multiple accessibility needs:

Hamilton Urban Braille. Urban Braille is a system of tactile information that uses colour and texture contrast to provide warning signals and orientation cues. The shoreline feature is used to delineate the pedestrian clearway. Generally used downtown, in Business Improvement Areas (BIAs), and designated activity areas such as campuses or hospitals. A shoreline feature may also be used to provide delineation between pedestrian and cycling operating spaces.

Attention Tactile Walking Surface Indicator (TWSI). Used to provide critical safety information to everyone at potentially dangerous locations. They should only be used

to identify potential hazards. Appropriate uses in the context of streets include curb ramps and depressed curbs and entry points to vehicular routes. While attention TWSI's are typically found at crosswalks, **they do not delineate or demarcate crosswalks**. An attention TWSI may be warranted even if a crosswalk is absent, and the extents of the attention TWSI may differ from the crosswalk itself.

Directional TWSI. Used to guide people along an intended path of travel or to key points of interest such as crossings. While attention TWSI's consist of truncated domes, directional TWSI's consist of a pattern of parallel, flat-topped, elongated bars that extend in the direction of travel that are detectable both underfoot and with a long cane. They can be used to help users navigate at complex roadway environments such as roundabouts and mid-block crossings.



A guidance TWSI in grey used to guide pedestrians between a bus stop (left) and a cycle track crossing (right). Red attention TWSI's are used to identify hazards at each end. Ottawa, ON

Curb ramps and dropped curbs. These help people with wheeled mobility aids, pushing strollers, walking bikes, etc. to smoothly transition from the sidewalk to the street. TWSI's are required to warn those with visual disabilities of the traffic road ahead. A clear width of 1.2 metres is required for all curb ramps, with an additional 1.5 metre clear walkway width at the top.

Accessible pedestrian signals (APS). For those with visual disabilities, an APS provides a series of sounds to indicate when it is safe to cross an intersection.



Accessible pedestrian signals help those with visual or auditory disabilities safely cross the street. Source: CBC

Clear width. To accommodate people using wheeled mobility aids, a clear path of travel of at least 1.8 metres is recommended. Minimum clear width of 1.2 metres must be provided between bollards.

Pedestrian refuges. Intended as an intermediary space within a larger pedestrian crossing. They can be located in the middle of the roadway between two travel directions or in the boulevard as part of a setback bicycle crossing. They can also act as traffic calming measures. To be considered a refuge, a space must be at least 1.6 metres wide to allow for passing, 2.1 metres long in the direction of travel, and have attention TWSI's at each end. Refuges larger than these minimum dimensions provide additional manoeuvrability and comfort for users and should be considered, especially in moderate to high volume pedestrian areas. For signalized crossings where the above criteria cannot be met, the pedestrian clearance interval must be long enough to traverse the entire roadway.



An example of a pedestrian refuge between a cycle track and roadway crossing, denoted by attention TWSI's at each end. Ottawa, ON

3.1.5 PLACEMAKING & AMENITIES

Beyond simply considering safety, effective pedestrian design also considers the factors that contribute to a positive user experience. While pedestrians are often thought of as being constantly in motion, people benefit from places to stop, rest, and socialize. Shade, seating, interesting views and storefronts all contribute positively to the pedestrian environment. The pedestrian experience is negatively impacted by traffic noise, pollution, wind and sun exposure, steep grades, poor lighting, and frequent conflicts with vehicles. A few examples of amenities include:

- Street trees, which provide shelter from the sun, absorb some of the noise from vehicles, provide urban cooling, and add variety to the street
- Benches, which provide a place for people to rest
- Parklets and Privately Owned Publicly Accessible Spaces (POPS), which create a space for playing, eating, and socializing
- Lighting, which provides improved security for people traveling at night

Consider the context and surrounding land use when determining the placemaking elements of a street. Active surrounding land uses and building frontages are more likely to attract pedestrians and may warrant more attention to placemaking.

Changes to the traffic environment of a street can also significantly improve the pedestrian experience. For example, reducing the number of lanes or introducing traffic calming can help reduce speeds and volumes, significantly reducing noise levels. Increased buffer zones and street tree / furniture zones also help insulate pedestrians from vehicle traffic noise.



A high quality pedestrian realm including seating and trees, with active storefronts

3.2 CYCLING FACILITY AND DESIGN

Cycling is a healthy, climate-friendly, and affordable form of transportation. Cycling facilities also offer a highly efficient use of space in terms of people-movement capacity. However, many people are reluctant to cycle when faced with stressful interactions with motor vehicle traffic. Going forward, it is critical to provide low-stress conditions to make cycling an attractive option for a wide range of ages and abilities.

This section outlines the principles and features that make up cycling facility design. Detailed guidance on cycling facility design is available in Ontario Traffic Manual (OTM) Book 18.

3.2.1 DESIGN PRINCIPLES

Design for all ages and abilities. This implies designing for comfort and safety. On local streets, focus on slowing traffic and managing motorized vehicle volumes so that cyclists and motorists are travelling at similar speeds. On busy streets, focus on separating cyclists from traffic.

Provide connected cycling facilities to allow people to get to their destinations via a direct route. Work to eliminate missing links and develop a network that connects all parts of the city.

Make cycling attractive by providing well-maintained facilities along the route, such as greenery, good lighting, and smooth pavement. End-of-trip facilities like secure bike parking stations or lockers could incentivize people to switch to cycling for their daily commutes.

3.2.2 FACILITY TYPES

OTM Book 18 provides a detailed description of cycling facilities and selection criteria. Bike facilities are grouped into three categories:

Physically separated facilities, such as cycle tracks and multi-use paths. These may either be located within the roadway with physical measures to separate cyclists from motor vehicles, or above the curb in the boulevard.



This cycle track in Hamilton uses planters to separate cyclists from vehicle traffic.

Source: Canadian Consulting Engineer

Designated facilities, such as conventional bike lanes. These are delineated with pavement markings on the road but no physical separation.



A buffered bike lane demarcated by painted lines. Source: The Hamilton Spectator.

Shared facilities. These do not have separation between cyclists and motor vehicles; they must share the space instead.



A street with shared cycling facilities and vehicle space.

IMPLEMENTATION

Bike facilities should be selected based on the appropriate context and the guidance provided in OTM Book 18. In general, the following guidance should be applied in selecting an appropriate cycling facility.

On lower-speed streets where shared facilities are appropriate, **neighbourhood bikeways** may be implemented. These are a connected network of low-traffic streets that create continuous cycling routes. They often feature wayfinding signage, pavement markings, and speed management measures, and traffic diverters that prevent continuous use by motor vehicle traffic.



A neighbourhood bikeway, also known as a bicycle boulevard, sign indicating that cyclists have priority. Source: NACTO

In new construction, **cycle tracks in the boulevard** are the preferred design approach where separation is needed. They provide more comfort to cyclists and are less costly than building a wider roadway to provide an on-street cycling facility. In retrofit scenarios, consider an on-road separated or designated facility. These facilities can often be implemented cost-effectively if space between the curbs can be reallocated from other uses.



Volume management measures on a low-volume, neighbourhood greenway in Vancouver, BC

In general, **separate cycling facilities and sidewalks** should be preferred. A multi-use path may be considered in areas with low pedestrian volumes and long distances between driveways or intersections. For example, multi-use paths may be appropriate on mountain access routes or to provide longer-distance connections through rural or other lower-density parts of the City.



An urban multi-use path along the Halifax Urban Greenway. Source: City of Halifax

One-way cycling facilities on each side of the street are generally preferred over two-way facilities, especially along two-way streets. Compared to two-way cycling facilities, they usually result in safer outcomes, less complex intersections, and are supportive of providing access to destinations on both sides of the street.



Cross-section of one-way physically separated bike lanes.

Two-way cycling facilities may be appropriate on one-way streets, in situations where the right of way is constrained, where destinations are predominantly located on one side of the street, or along routes with minimal driveway or intersection conflicts –for example, along mountain access routes. When retrofitting one-way streets, it is desirable to introduce two-way cycling facilities to avoid wrong-way cycling along a one-way cycling facility. A two-way cycling facility may be placed on either the right or the left side of a one-way street. Some guidelines suggest that right-side placement may result in better safety outcomes at intersections. However, left-side placement better mitigates conflicts with transit operations along streets with bus service. When considering two-way cycling facilities, safety countermeasures should be applied at intersections and driveways to improve the visibility and safety for cyclists.

3.2.3 WIDTHS & CLEARANCES

FACILITY WIDTHS

When people ride bicycles, they tend to naturally sway back and forth. The target widths of 2.0 m for one-way cycling or 3.5 m for two-way cycling provide adequate width to allow cyclists to manoeuvre around bumps and other unexpected obstacles, and to allow faster cyclists to pass slower moving cyclists. The table below provides a summary of constrained, minimum and target widths for in-boulevard bike facilities.

The suggested minimum width is 1.8 metres for a one way facility or 3.0 metres for a two-way facility. Justification for a decreased width may include:

- Presence of single physical obstacles that are cost-prohibitive to relocate. Keep in mind that providing a facility next to these will increase the risk of bicycle crashes.
- Retrofit conditions where existing road space is being reallocated and adjacent vehicle facilities are also at the lower end of the design range. 1.5 metres is acceptable and 1.2 metres is an absolute minimum at pinch points.

CLEARANCES

Cycling facilities should provide a lateral clearance to help reduce the potential for injuries while cycling, for example due to a handlebar or a pedal striking an obstacle. When clearances are not provided, cyclists will naturally offset themselves from these obstacles, reducing the effective operating space of the facility.

For vertical obstructions between 50 millimetres and 750 millimetres in height (such as a curb), a clearance of 0.2 metres is recommended to avoid bicycle pedals striking these objects. For vertical obstructions greater than 750 millimetres in height (such as a fence or pole), a clearance of 0.5 metres is recommended to avoid bicycle handlebars striking these objects.

3.2.4 CROSS-SECTION ELEMENTS

IN-BOULEVARD FACILITIES

Universal design principles should be applied in the design of in boulevard cycle tracks. In particular, consider the needs of:

- People who are blind or who have low vision that may have difficulty distinguishing between the cycle track and sidewalk
- People using mobility devices who may have difficulty moving across the cycle track to access the curb— for example, to access parking spaces, loading spaces, or bus stops.

When a cycle track is adjacent to a sidewalk, an Urban Braille “shoreline” feature — a tactile and colour-contrasting band — may be used to provide separation between these facilities. Alternatively, a short (50–75 mm) curb may be used to separate the facilities.



Urban Braille feature separating cycle track and sidewalk

An alternative approach places a furnishing or planting zone between the sidewalk and the cycle track to offer more separation between pedestrians and cyclists.



Planting zone between cycle track and sidewalk

In areas with low pedestrian activity, a multi-use path on one or both sides of the street may be considered. The table on the following pages outlines options key considerations for selecting appropriate in-boulevard cycling facilities.

In-boulevard Facility Selection Guide

		PEDESTRIAN VOLUMES	ADDITIONAL CONSIDERATIONS
<p>OPTION 1: SIDEWALKS AND ONE-WAY CYCLING ON BOTH SIDES</p>	 <p><i>Both sides of the street include a sidewalk and a one-way cycle track</i></p>		<p>Preferred option when frequent destinations or driveway conflicts are present on both sides of the street.</p>
<p>OPTION 2: SIDEWALKS ON BOTH SIDES AND A TWO-WAY CYCLING FACILITY ON ONE SIDE</p>	 <p><i>One side of the street has a sidewalk and a two-way cycle track. The opposite side has a sidewalk only.</i></p>	<p>Moderate to high</p>	<p>May be considered on one-way streets, or when conflicts can be minimized by placing the cycling facility on one side of the street. May also be appropriate when the two-way facility provides a network connectivity benefit. See discussion regarding one-way and two-way cycling facilities in Section 3.2.2.</p>

<p>OPTION 3: MULTI-USE PATH ON ONE SIDE AND SIDEWALK ON ONE SIDE</p>	 <p><i>One side of the street has a multi-use path. The opposite side has a sidewalk.</i></p>	<p>Low</p>	<p>May be considered when pedestrian volumes are low. The multi-use path should be placed on the side of the street that minimizes conflicts or that provides the greatest network connectivity benefit. See discussion regarding one-way and two-way cycling facilities in Section 3.2.2 above.</p>
<p>OPTION 4: MULTI-USE PATHS ON BOTH SIDES</p>	 <p><i>Both sides of the street have a multi-use path.</i></p>		<p>May be considered when pedestrian volumes are low and there is rationale to provide cycling connectivity on both sides of the street.</p>

Design Parameters for In-boulevard Cycling Facilities

ELEMENT	TARGET VALUE	MINIMUM VALUE
BUFFER ZONE	1.0 m ¹	0.5 m
CYCLE TRACK (ONE-WAY)	2.0 m	1.8 m (1.5 m) ²
CYCLE TRACK (TWO-WAY)	3.5 m	3.0 m (2.4 m) ²
MULTI-USE PATH	3.5 to 4.0 m	3.0 m
PEDESTRIAN WALKWAY	See pedestrian realm section. When abutting a cycle track, apply shorelines (see Urban Braille standards) or separate with a short height curb.	
<p>Notes:</p> <p>1. Minimum 0.6 m where on-street parking is next to cycling facility; minimum 1.0 m to support utility poles and signage. On 60 km/h or higher streets, a wider buffer is recommended. Refer to OTM Book 18 for guidance.</p> <p>2. Dimensions provided in brackets indicate widths that may be applied in constrained locations for short segments. In these cases, the City should confirm that maintenance equipment will be capable of clearing snow from narrow cycle tracks.</p>		

ON-ROAD FACILITIES

On-street cycling facilities may be physically separated (for example, through the use of bollards, planter boxes, or concrete barriers), or they may be a designated facility such as a conventional bike lane. OTM Book 18 provides guidance on selecting an appropriate facility.

Where on-street parking is present, the cycling facility may be positioned to the left or to the right of parked vehicles. On busy streets with high parking usage and high parking turnover, the cycling facility should be positioned between parked vehicles and the curb. On lower volume streets with less frequent parking turnover, the cycling facility may be positioned to the left of parked vehicles. In either case, painted buffers to separate between cyclists and parked vehicles should be provided to reduce the risk of "dooring".

An advisory bicycle lane configuration may be considered where there is insufficient space for conventional bike lanes. In this case, motor vehicle traffic shares a two-way centre travel lane and vehicles travelling in opposite directions are required to yield to one another by temporarily entering the advisory bike lane. The installation of advisory bike lanes should be followed with an educational campaign teaching cyclists and vehicle drivers how to navigate these lanes.



Advisory Bike Lane with On-Street Parking in Victoria, BC

On low-traffic streets where shared operating space is appropriate, no centreline should be marked on the roadway. This is shown to reduce motor vehicle speeds and to cause drivers to provide greater passing distance when overtaking cyclists. A summary of design parameters for on-street bike facilities is provide in the following table.

Design Parameters for On-street Cycling Facilities

ELEMENT	TARGET VALUE	MINIMUM VALUE
PAINTED BUFFER	1.0 m	0.3 m ¹
BIKE LANE	1.8 m	1.5 m (1.2 m) ²
CONTRAFLOW BIKE LANE	2.0 m	1.8 m (1.5 m) ²
ADVISORY BIKE LANE	1.8 m to 2.0 m	1.5 m
TWO-WAY TRAVEL LANE	3.0 to 4.0 m or 5.0 to 5.7 m	2.7 m
RURAL PAVED SHOULDER	1.5 to 2.0 m plus 0.5 to 1.5 m buffer	1.2 m
<p>Notes:</p> <p>1 - Minimum 0.6 m when adjacent to parked vehicles. Minimum width may vary dependent on the type of physical separation used (if any); consult OTM Book 18.</p> <p>2 - Widths in brackets correspond to widths that may be applied in highly constrained contexts for short segments.</p>		

3.2.5 INTERSECTIONS

A significant number of urban cyclist fatalities occur at intersections. The design for cyclists at intersections focuses on reducing the possibility of collisions and the severity of collisions. Safe intersection design requires attention to the geometric design, the vehicle speeds and turning volumes, and the degree to which cyclists are visible to turning traffic. Extra consideration is required for two-way facilities at intersections which are correlated with a higher rate of injury. Detailed information on intersection design is provided in [Chapter 5](#).

PROTECTED INTERSECTIONS

Protected intersections include several geometric features to improve safety. The cycling facility is set back four to six metres from the motor vehicle lanes, which improves visibility of cyclists to turning motorists. Protected intersections include corner islands that extend the cycling facility's protection into the intersection and create a bicycle queuing area after the crosswalk. The design typically includes pedestrian islands that reduce pedestrian crossing distances and improve visibility. [Chapter 5](#) provides further guidance around designing protected intersections and discusses their benefits in more detail.



A protected intersection where cyclists are protected from vehicle traffic via a setback intersection crossing, curbs, and brightly coloured bike lanes. Source: NACTO

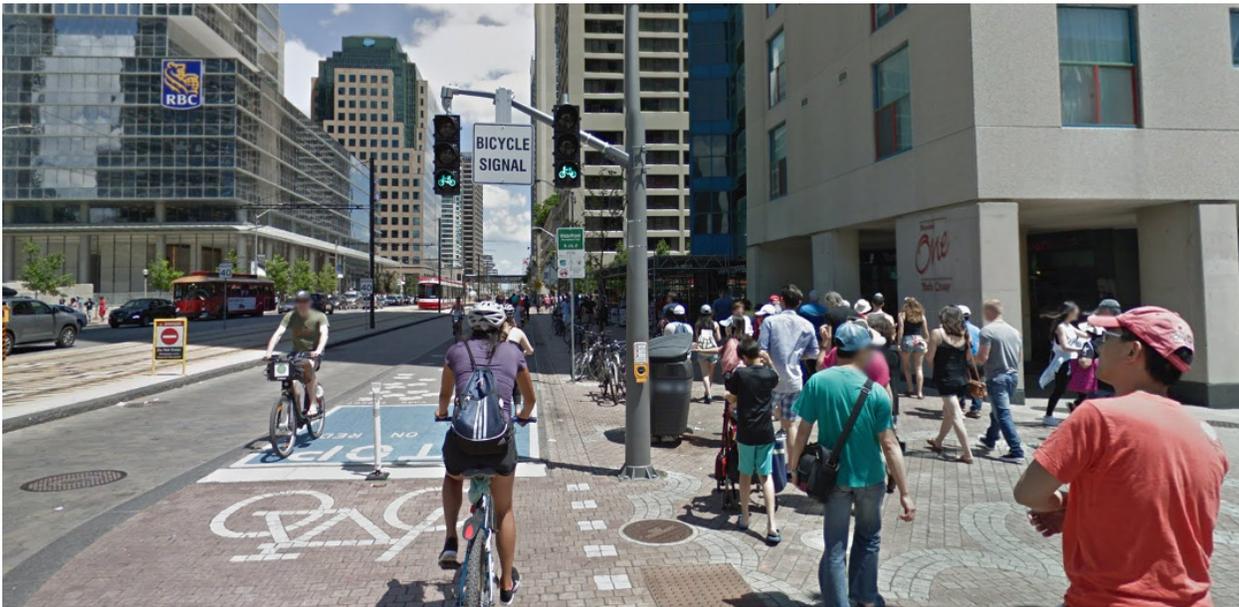
TRAFFIC SIGNAL MEASURES

Leading pedestrian and bicycle intervals give cyclists a head start on the green phase and make them more visible to turning motorists.

Right turn on red restrictions reduce conflicts between turning vehicles and cyclists.

Bicycle signals can be used to signalize bicycle-specific movements, such as leading intervals, and can also be positioned at a height that is more visible to cyclists than a standard traffic signal head. These can also be mounted near-side to further increase visibility.

Protected signal phasing can significantly reduce conflicts between users. However, delays to cyclists should be considered when implementing protected phasing. Long delays may result in poor compliance with the traffic signals.



Bicycle-specific signals, seen here in Toronto, are easier for cyclists to see when installed at the same height as pedestrian signals. Source: Google Maps

3.2.6 OTHER FACILITIES

BICYCLE PARKING

The conversion of on-street parking to bicycle or micro-mobility parking can result in significantly more efficient use of curbside space. It is recommended to provide bicycle parking at popular destinations to incentivize residents to cycle for their daily trips.

"Hammer hoop" style bicycle racks are recommended for their ease of use and level of security they provide. These are recommended to be 70 centimetres tall with a hoop diameter of 30 centimetres. These are to be installed on a 20 centimetre wide plate. Further details for bicycle parking can be found in [Section 3.5.5](#).



Hammer hoop bicycle racks.

BIKE SHARE STATIONS

Bike share stations may occupy either sidewalk or curbside space. The provision of bike share stations allows for residents or tourists to cycle throughout Hamilton without the need for a personal bicycle. These may be useful for short trips throughout the City whether for a daily commute, running errands, or for scenic tours.

Bike share stations should be a minimum length of 8 to 10 metres and a minimum width of 1.8 metres, though 2.0 metres is desired. Refer to [Section 3.5.5](#) for further details on bike share stations for Hamilton.

WAYFINDING

The City of Hamilton is currently looking to update its wayfinding strategies as part of the next Cycling Master Plan update. This will include cycling wayfinding features with Hamilton-specific signage. Refer to OTM Book 18 for detailed recommendations for cycling wayfinding.



Examples of cycling wayfinding signage.

3.3 TRANSIT FACILITY DESIGN

Hamilton Street Railway (HSR) provides efficient, sustainable and affordable access to employment, essential services and recreational destinations for all Hamiltonians. The transit user experience should be considered in street design from start to end of trip, including the journey to the transit stop or station, comfort and safety while waiting for transit, and the efficiency of movement between destinations. To be an attractive option for Hamiltonians, transit should be efficient, reliable, user friendly, and provide access to all major destinations in Hamilton.

3.3.1 DESIGN PRINCIPLES

Provide safe and comfortable transit facilities: Enhancing the transit user experience can help ensure that transit is an attractive option for Hamiltonians. The transit user experience begins and ends at transit facilities. Strategies to improve on the transit user experience include providing accessible, comfortable, and attractive transit stops and stations. Provision of shelters, seating, appropriate space for waiting and manoeuvring, and lighting contribute to the comfort and perceived safety at a transit stop. Transit facilities may provide users with key information about the service such as the schedule, fares, and maps to facilitate trip planning.

Accommodate multi-modal travel: Transit trips are generally multi-modal with trips beginning and ending with users walking, cycling, or driving to and from the transit stop. Enhancing cyclist access at transit stops can help increase the catchment areas of the transit system. Strategies to accommodate multi-modal travel include providing comfortable pedestrian crossings, dedicated bicycle parking or bike share stations, and wayfinding near transit facilities.



Sheltered Bicycle Parking at Hamilton Centre GO Station Source: Ontario Traffic Manual

Facilitate transit efficiency by providing transit vehicles with priority access:

Ensuring efficiency of transit vehicle movements contributes to the reliability of the transit system. Strategies such as transit signal priority, dedicated transit lanes, and frequent service help minimize transit delay and improve the transit user experience.



A transit HSR bus in Hamilton.

3.3.2 STOPS

KEY DESIGN CONSIDERATIONS

Safety and Comfort: Transit stop visibility, lighting, weather protection and street furniture contribute to the level of comfort and safety passengers experience at a transit stop. Transit stop design should aim to reduce conflicts between transit users and other road users by providing clear wayfinding and adequate queueing space for passengers.

Accessibility: Accessibility treatments include tactile walking surface indicators and detectable warning surfaces along platform edges, curb cuts, and raised landing pads. Transit stop design should be in full compliance with the Accessibility for Ontarians with Disabilities Act (AODA), the City's Transit Bus Stop Accessibility Criteria & Guidelines and Section 8.17 of the City's Barrier-Free Design Guidelines.

Integration with Transit Design Vehicle: The intended vehicle type and size, as well as the service frequency will influence the geometry of transit stops. Hamilton Street Railway's Stops and Shelters Technical Manual provides details on design requirements for transit stops including landing pad dimensions for specific transit vehicle sizes, ideal shelter location, and accessibility considerations.

STOP TYPES

Centre Median Platform Stop

Centre median platform stops are positioned between a dedicated transit lane and motor vehicle lanes. This type of platform is typically installed along Bus Rapid Transit (BRT) lines where transit vehicles operate in dedicated lanes. Transit users reach the platform from signalized crossings, reducing conflicts with cyclists and motorists.

Island Boarding Stop

Island boarding stops require transit passengers to cross a cycle track to access the platform. The cycle track is channelized behind the platform, and cyclists are required to yield to pedestrians crossing to reach to the platform.

Conflicts between cyclists and pedestrians reaching the stop can be mitigated by providing dedicated pedestrian crossings across the cycling facility with tactile surface indicators, zebra crossings, sharks teeth and a Cyclists Yield to Pedestrians sign (Rb-73).

If the area has a high volume of passenger movement or cycling volumes at the adjacent cycling facility, a railing should be included to channel pedestrians to the dedicated crossing. Clear distance of 0.5m is preferred between cycling facility and vertical obstacles such as a railing or transit shelter.



Island Boarding Stop in Seattle. Source: NACTO

Island boarding stops can present some challenges for people with disabilities, especially those with visual impairments. Designers should seek to engage the accessibility community early in the design of these stop types to ensure they are accessible to all users. Refer to [Section 2.2.5](#) Equity & Accessibility for additional guidance.

Shared Cycle Track Platform Stop

If there are right of way constraints, the transit platform can be integrated with the cycle track with tactile strips denoting the edge of the platform. In the presence of transit vehicles, cyclists are required to yield to passengers crossing the cycle track. When no transit vehicles are present, passengers wait on the sidewalk behind the cycle track while cyclists continue along the facility without stopping.

Signage and pavement markings should be used to reinforce the requirement for cyclists to yield to pedestrians. The transit shelter, if present, should open onto the sidewalk rather than the cycling facility.



Shared cycle track platform stop in Toronto. Source: NACTO

Shared Space Stop

At these stops, transit vehicles merge into a parallel cycling facility to serve the stop. Pavement markings should be used to warn cyclists that transit vehicles may enter the bike lane. Shelters and other amenities for the stop are placed on the adjacent sidewalk.

While serving the stop, buses will be partially in the motor vehicle lane and partially in the bike lane. Motorists and cyclists are expected to either wait behind the bus at the stop or merge into the adjacent travel lanes to overtake the bus.

This type of stop increases complexity and reduces comfort for cyclists. It is not recommended for use on high-frequency transit routes or along high-volume cycling routes.

Bus Bay Stop

Buses merge into a lay-by or a right turning lane to reach the platform. This stop type is usually not preferred as it requires additional space in the right of way, and may result in delays as buses merge back into traffic after serving the stop.

Bus bay stops are intended for stops that operate as a layover, have high passenger boarding and alighting volumes, or have frequent boardings and alightings by passengers using mobility devices that require a ramp to board and where the bus stopping in the motor vehicle lane could have a significant impact on traffic operation.

Where on-road cycling facilities are present, transit vehicles may have to cross the facility to reach the bus bay, reducing cyclist comfort and safety.



Shared space bus stop in Toronto. Source: Google Earth

Curb Bump-outs

As outlined in Hamilton Street Railway's Stops and Shelters Technical Manual, curb bump-outs can be considered at stops along streets with high pedestrian volumes, on-street parking, and limited waiting space for transit passengers.

Curb bump-outs are an extension of the sidewalk or curb into the roadway that act as a transit landing pad. Curb bump-outs help separate waiting passengers from pedestrian traffic and reduce pedestrian crossing distances when implemented at near- and far-side stops.

This treatment eliminates the need for transit vehicles to change lanes to service a transit stop and can reduce transit delays from having to pull in and out of through lanes.



Shared space bus stop on a curb bump out in San Francisco. Source: NACTO

Design Parameters for Transit Stops

ELEMENT	TARGET VALUE	MINIMUM VALUE
PLATFORM LENGTH	9.0 to 15.0 m	9.0 m
CENTRE MEDIAN STOP WIDTH	3.2 to 3.5 m	1.5 m (plus ramp) 2.0 m (where level boarding is provided)
ISLAND BOARDING STOP WIDTH	3.0 to 3.5 m	2.5 m
TRANSIT SHELTER AND STREET FURNITURE CLEARANCE FROM BIKEWAY	0.5 m	0.3 m
CLEARANCE WIDTH ALONG TRAFFIC CURB EDGE	1.8 m	0.5 m
CURBSIDE TRANSIT STOP WIDTH	3.0 m	2.5 m

AMENITIES

A consistent look and feel is applied to Hamilton's transit stops. Consistency in amenities and stop layout contributes to the transit system's identity. At a minimum, all stops include stop signage and a clear area to accommodate boardings and alightings. Stop amenities enhance a user's experience in taking transit by making the wait experience more comfortable and convenient. Amenities that may be considered for transit stops include lighting, seating, shelters, tactile surface treatments, route and schedule information (static or digital/real time displays), litter receptacle, way finding signage and bicycle storage. Stop amenity requirements vary by typology as described in [Chapter 4](#).

LOCATIONS

Transit stops are typically placed near intersections and pedestrian infrastructure for increased connectivity to side streets and to better facilitate transfers between routes. Near-side stops are stops located before an intersection. Near-side stops are preferred in most contexts as they promote efficient transit operations and visibility of pedestrian and fast-moving vehicles. Far-side (located after an intersection) or mid-block (between intersections) may be preferred over near-side stops to accommodate specific user or operational needs as described in the HSR Stops and Shelters Technical Manual.

3.3.3 TRANSIT LANES

Transit lane treatments can be considered for Complete Street improvements to separate transit vehicles from mixed traffic to improve efficiency and speed of service. Examples of transit lane configurations include dedicated transit lanes, reserved lanes, and queue jump lanes.

DEDICATED TRANSIT LANES

Dedicated transit lanes are marked for exclusive use by transit vehicles with diamond and "bus only" pavement markings, signage, and possibly physical barriers. Emergency and maintenance vehicles are generally permitted on dedicated transit lanes.

Dedicated transit lanes support high volume routes along corridors with high motor vehicle volumes and congestion to improve transit efficiency. This type of treatment is typically seen along Bus Rapid Transit (BRT) corridors.

Consideration should be given for how buses will enter and exit these facilities, especially for short-turn service or if the vehicles are expected to operate on other types of lanes along the route. Turning movements for motor vehicles across dedicated lanes should be managed to reduce transit delays.

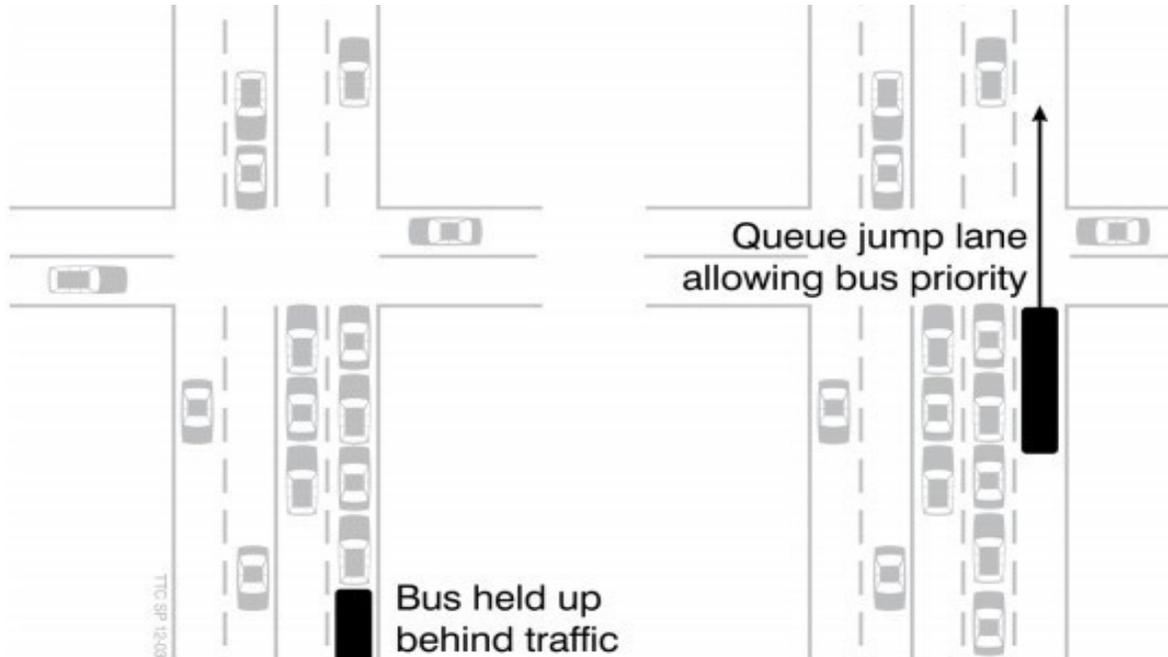


On-street bike lane adjacent to transit-only lane in Vancouver, BC.

3.3.4 INTERSECTIONS

QUEUE JUMP LANES

Queue jump lanes combine a short-dedicated transit facility with transit signal priority to provide transit vehicles opportunities to bypass queued traffic at intersections. Queue jump lanes are typically considered where motor vehicle volumes negatively impact transit efficiency. In some cases, right-turn lanes can double as queue jump lanes.



Queue Jump Lane Concept. Source: Toronto Transit Commission



Right-turn lane used as a queue jump lane for buses in Brampton, ON. Source: Google maps

TRANSIT SIGNAL PRIORITY

Transit Signal Priority (TSP) strategies can improve transit efficiency and speed by reducing dwell time at intersections. These strategies complement other interventions such as reserved lanes or queue jump lanes. TSP strategies include active and passive techniques. Active TSP techniques require hardware to detect the presence of transit vehicles and adjust the signals accordingly. Active techniques include holding green lights longer or shortening the red cycle when transit vehicles are detected. Passive techniques do not require any transit vehicle detection technology. Passive TSP involve signal timing techniques such as coordinating signals across a transit corridor to provide a wave of green lights for transit vehicles or providing a dedicated transit signal at intersections.

TSP can be implemented at individual intersections or across corridors. In the absence of transit vehicle detection technology, keeping signal cycles short at intersections can help improve transit efficiency.

3.4 ROADWAYS

The early streets of Hamilton were designed for the horse and carriage. To reduce congestion and meet the needs of a rapidly growing City, a horse-drawn streetcar service known as the Hamilton Street Railway was introduced in 1874, with electric streetcar service to follow a decade later. Today, the roadway serves many different vehicles including buses, trucks, bikes, and private automobiles.

This section of the manual relates to the portion of the street between the curbs that is generally intended for vehicle travel. Curbside uses such as parking are discussed separately in [Section 3.5 Curbside Management](#).

The roadway serves an important role in providing for efficient goods movement and emergency response, and in allowing people to freely move about the City. Historically, roadways have been assessed using a level of service (LOS) grading systems based on a letter grade system ranging from LOS A to F. LOS A reflects operational conditions that are favourable towards speed, travel time, freedom to manoeuvre, traffic interruptions and comfort and convenience for motor vehicles. Designing towards higher levels of service for motor vehicles may result in excessive space allocated to the roadway and over-prioritization of private motor vehicle travel. This in turn can limit mobility choices, resulting in environmental and public health impacts, and in streets that are neither complete nor livable. From a street design perspective, it is important to balance the needs of all road users and to consider the context of the street within the overall road network.

3.4.1 DESIGN PRINCIPLES

Design reflects context. Streets may serve movement or access functions. Movement-oriented streets prioritize higher speeds, greater separation between road users, and limit access to the roadway. Access-oriented streets prioritize slower speeds, property access, and curbside activity. Roadways that attempt to prioritize higher-speed movement while also incorporating frequent property access are prone to high volumes of collisions.

Prioritize safety. Minimize conflict likelihood and severity through design choices to help create safer streets as a proactive preventative measure, rather than a retroactive reactionary one.

Multimodal roads. Curb to curb space dedicated for motor vehicles means less space available for other users within the overall right of way. The allocation of space should be considered for all users rather than designing a road to meet a target LOS for motor vehicles.

3.4.2 SPEED

Streets should be designed with a contextually-appropriate target speed in mind. Streets that feature interactions with vulnerable road users should be designed for slower speed operations. For example, in environments with high pedestrian activity or where cyclists are operating in a shared roadway, speeds should be no greater than 30 to 40 km/h. Where there is high potential for right-angle conflicts between motorized vehicles, speeds should be no greater than 50km/h.

Roadways designed for high-speed traffic are incompatible with a complex multi-modal environment. When drivers increase their speed, their field of vision becomes narrower, and it becomes more difficult to notice and react to events happening within or adjacent to the street. Collisions at higher speeds are more likely to result in severe injuries and fatalities. Finally, noise increases with motor vehicle speeds. This noise pollution has a negative impact on the livability of a street.

From a Complete Street perspective, the preferred approach is to start with a target speed and implement design measures that help to ensure motor vehicles will travel at those speeds. Techniques that introduce "visual friction" or that visually narrow the roadway can be highly effective in promoting slower speeds. These include on-street parking, street-oriented buildings, and elements such as trees planted close to the street. Narrow lanes and traffic calming features may also be considered.

DESIGN AND TARGET SPEEDS

- Target speeds should be selected in a context-sensitive manner.
- For urban streets with a posted speed up to and including 50 km/h, the design speed should be set equal to the target / posted speed.

3.4.3 DESIGN AND CONTROL VEHICLES

Design and control vehicles are used to inform the selection of various design elements including but not limited to, intersections, sight access configurations, vertical clearance and widths of elements within the roadways and curbside management zones. Design vehicles refer to “regular users” that reflects the largest turning radius frequently required to manoeuvre a turn at intersections. Control vehicles reflects the largest vehicle that is occasionally required to manoeuvre a turn at an intersection corner but are relatively low in frequency and may have less available space to manoeuvre. Appropriate design and control vehicles should be selected so that the roadway is not over-designed – for example, with wider lanes or curb radii than necessary.

3.4.4 LANES

Lanes provide the required amount of space for vehicles to travel along the roadway. It has consistently been found that narrower lane widths are associated with a reduction in travel speed. As such, narrower lanes, in the range of 3.0 to 3.3 metres, are preferred in most urban and suburban contexts.

Wider lanes increase the roadway crossing distance for pedestrians and consume excess cross section width, reducing available space for sidewalks, cycling facilities, landscaping, and other amenities. Even in contexts with fewer space constraints or in greenfield developments, wider lanes are still not preferred due to their impact on travel speeds and safety. The same lane width targets should be applied in both retrofit and new construction contexts.

In most contexts, 3.0 metre lanes are the target. On truck routes and on streets with regular transit service, the target width of the curb lane (or right-most travel lane) is 3.3 metres. Wider lanes are appropriate in a limited number of situations, including higher-speed rural roadways and roads with significant horizontal curvature such as mountain access roads. Note that lane widths are measured from curb to curb rather than from the edge of the gutter.

The target width for parking lanes is 2.2 to 2.5 metres, inclusive of any gutters. Additional guidance for on-street parking is provided in [Section 3.5](#).

Lane Width Parameters

ELEMENT	TARGET VALUE	MINIMUM VALUE
THROUGH LANES AND TURN LANES	3.0 m ¹	3.0 m
CURB LANES	3.0 to 3.3 m	3.0 m
PARKING LANES	2.2 to 2.5 m	2.0 m ²
<p>Note:</p> <p>1 – Applicable to urban, suburban, and low-volume rural contexts with a target speed less than 60 km/h. Refer to TAC Geometric Design Guide for Canadian Roads for guidance in other contexts.</p> <p>2 – 3.3 m recommended on truck routes and on streets with regular transit service</p> <p>Minimum 6 m clear width is required per the Ontario Building Code for emergency vehicle access</p>		

3.4.5 MEDIANS

Medians offer several benefits to the roadway:

- They reduce midblock turning conflicts by restricting access.
- They reduce the potential for head-on collisions, which are much more likely to result in serious injury or death at speeds above 70 km/h .
- They provide landscaping and associated traffic calming benefits.
- They provide a place to install signage, signal poles, and light standards.
- When used as part of a pedestrian crossing, they act as a pedestrian refuge that reduces the street crossing into two separate crossings, helping to reduce stress and improve safety.

By way of increasing the width of the roadway, medians can take away road space that could otherwise be allocated to other modes. For this reason, their benefits should be carefully weighed against their drawbacks. For example, if insufficient space is available in the boulevards to provide comfortable separation between traffic, pedestrians, and cyclists, wider boulevards should be prioritized over medians.

MEDIANS

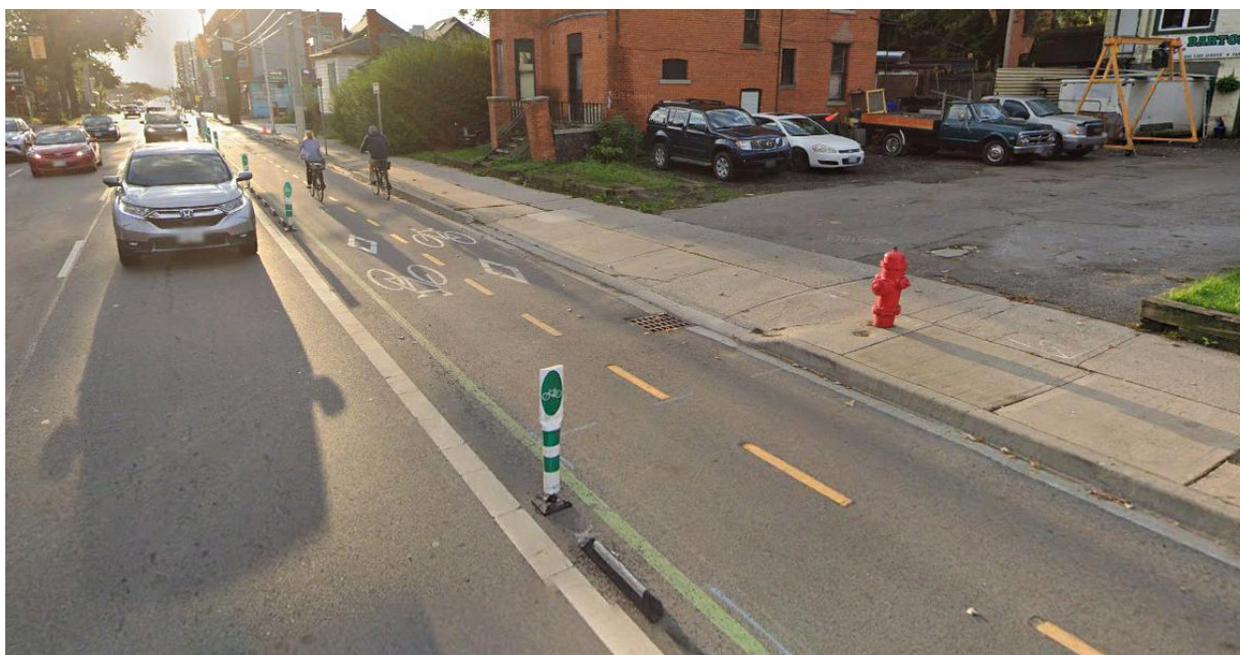
Continuous medians should generally be avoided on compact urban streets. They should be considered at pedestrian crossing locations, and may be used continuously on higher-speed mobility-oriented streets to eliminate mid-block left-turn conflicts.

3.4.6 DRIVEWAYS

Driveways, though necessary to facilitate access to and from properties, introduce conflicts on roadways.

For pedestrians and cyclists, the extra caution needed when crossing driveways can add extra stress to their travel experience. Therefore, driveways should be minimized and avoided where possible along mobility-focused streets. Instead, access should be provided via side streets.

Where driveways cannot be eliminated or shifted to side streets, their impact on the street should be minimized as much as possible. The curb cut should be no larger than necessary, with the sidewalk and cycle track continuing across the driveway rather than being discontinued.



Driveway across Cannon Street bikeway. Source: Google Earth

3.5 CURBSIDE MANAGEMENT

Planning for curbside activities is key to designing vibrant streetscapes. Successful curbside management can help improve street access for people and businesses alike. Apart from accommodating pedestrians, cyclists, transit customers, and vehicle users, streets must also be designed to accommodate curbside uses such as snow removal, waste collection, and pickup or drop-off activities. Equitable and efficient allocation of curbside space can help to meet a street's design objectives.

3.5.1 DESIGN PRINCIPLES

Flexible Space. Where possible, curbside space should be designed for flexible uses to allow for iterative, interactive design, or for uses that vary at different times of day or different times of the year. Temporary installations or pop-ups could help City staff test different configurations and uses for the curbside, providing an opportunity for observation and real-time feedback from residents.

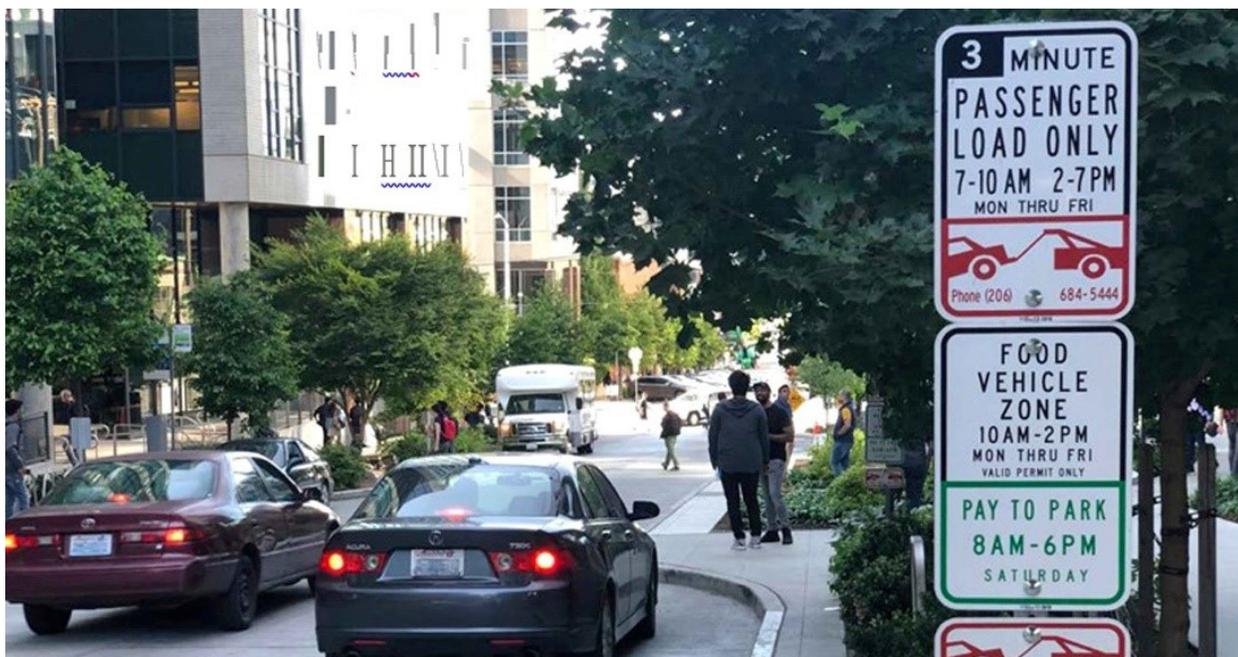


Temporary seating added as part of the King Street Pilot Project in Toronto. The success of this pilot led to the permanent installation of seating and planting along the curbside. Source: City of Toronto

Ensure safety for all road users. Providing the proper space for curbside activities can contribute to increased visibility and space for various road users; transit users can

benefit from designated disembarking and boarding spaces separated from live traffic, while delivery drivers would feel much safer stopping in a loading area rather than in a drive lane. Consider the context of the street when planning for safe curbside spaces.

Balance competing uses. The demands for curbside spaces have increased with ride hailing services, online retail, and micro-mobility and bike share services. Where curbside space is limited, proper curbside management, such as dedicated pickup and drop-off areas, has the potential to help reduce conflicts between competing uses and support multimodal access.



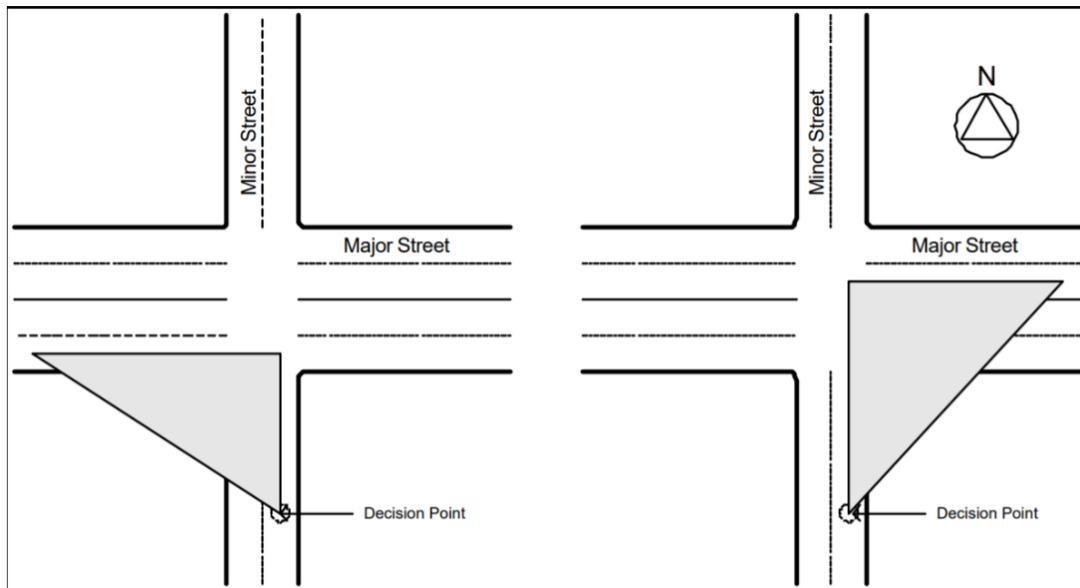
Loading signs for passenger pickup and food vehicle uses. Source: Washington State Transportation Center

3.5.2 ON-STREET PARKING

On-street parking is one of the most common uses for the curbside. Whether for private vehicles or commercial loading zones, it is important to design a roadway to allow for smooth transitions for parked or moving vehicles while minimizing disruption to other road users.

	TARGET VALUE	MINIMUM VALUE
ON-STREET PARKING WIDTH, INCLUSIVE OF GUTTER	2.2 to 2.5 m	2.0 m

In order to maintain sightlines near intersections, on-street parking should be restricted for at least 9 metres from an intersection, 6 metres from a crosswalk, or 45 cm from driveways. Angled parking is not recommended in any context.



An example of sightline triangles for motorists approaching an intersection for entry. Source: NACTO

In most cases, on-street parking should be provided within dedicated lay-by areas, framed with curb extensions at each end. This approach allows parking restrictions to be reinforced through design, reducing the number of on-street parking spaces in favour of extended space for pedestrians or street furniture. Curb extensions also results in shorter pedestrian crossing distances at intersections and crossings. The use of through lanes for off-peak parking is not recommended.



A curb extension at an intersection offers additional seating and an extended pedestrian crossing.

Available on-street parking can reduce the need for off-street parking for nearby businesses. In addition, on-street parking can act as a traffic calming tool by increasing the perceived “friction” for drivers, thereby encouraging lower travel speeds and buffering pedestrians and cyclists from traffic. However, for Complete Streets features to be added as part of retrofits, some or all on-street parking may have to be removed.

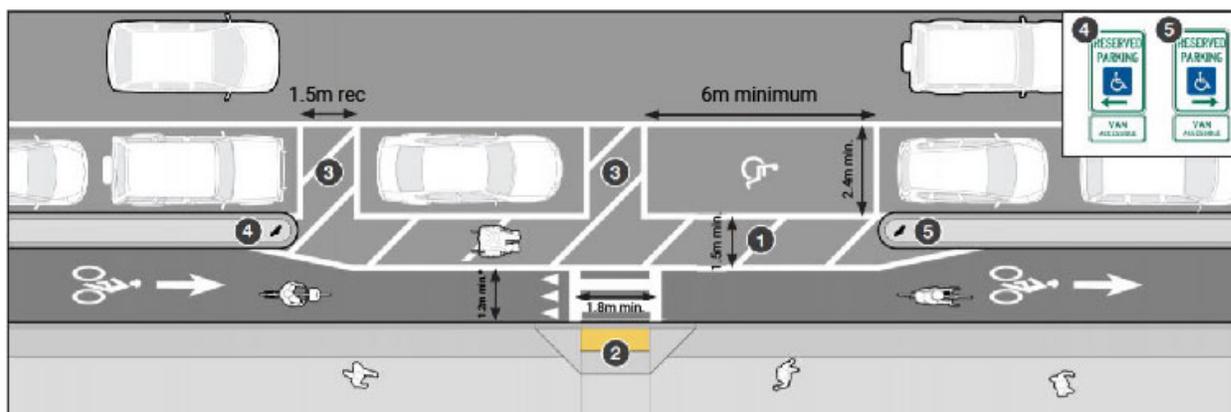
When adding on-street parking spaces it is important to first understand parking demand along a corridor. Providing more on-street parking than required could lead to empty parking lanes being perceived as an extension of motor vehicle through lanes, thus promoting increased travel speeds and reducing the overall safety of the corridor.

Conflicts may arise between drivers pulling in and out of parking spaces and other road users, such as cyclists riding in adjacent cycling facilities or drivers in adjacent through lanes. As such, on-street parking on high-speed streets with posted speed limits of 60 km/h or greater is not recommended.

3.5.3 ACCESSIBLE PARKING AND LOADING

All accessible parking and loading spaces should meet the minimum standards outlined in the Accessibility for Ontarians with Disabilities Act (AODA). As per AODA, when providing on-street accessible parking and loading spaces, the City must consult with relevant municipal accessibility advisory committees to determine the appropriate number of spaces to provide, ideal location, and design.

A buffer of 1.5 to 2.5 metres is required at both ends of an accessible parking space. Hard spaces should be provided in the adjacent boulevard and accessible curb ramps should be provided at the front or rear of the space. The location of accessible parking and loading spaces should be as close to nearby accessible residential and commercial entrances as possible.



Accessible On-Street Motor Vehicle Parking Example. Source: MassDOT

In cases where cycling facilities are adjacent to accessible parking and loading spaces, treatments such as providing additional buffer width between the parking lane and the cycling facility, narrowing the cycling facility in a constrained right of way, and clearly marking pedestrian crossing areas across the facility should be implemented.

3.5.4 CURBSIDE LOADING

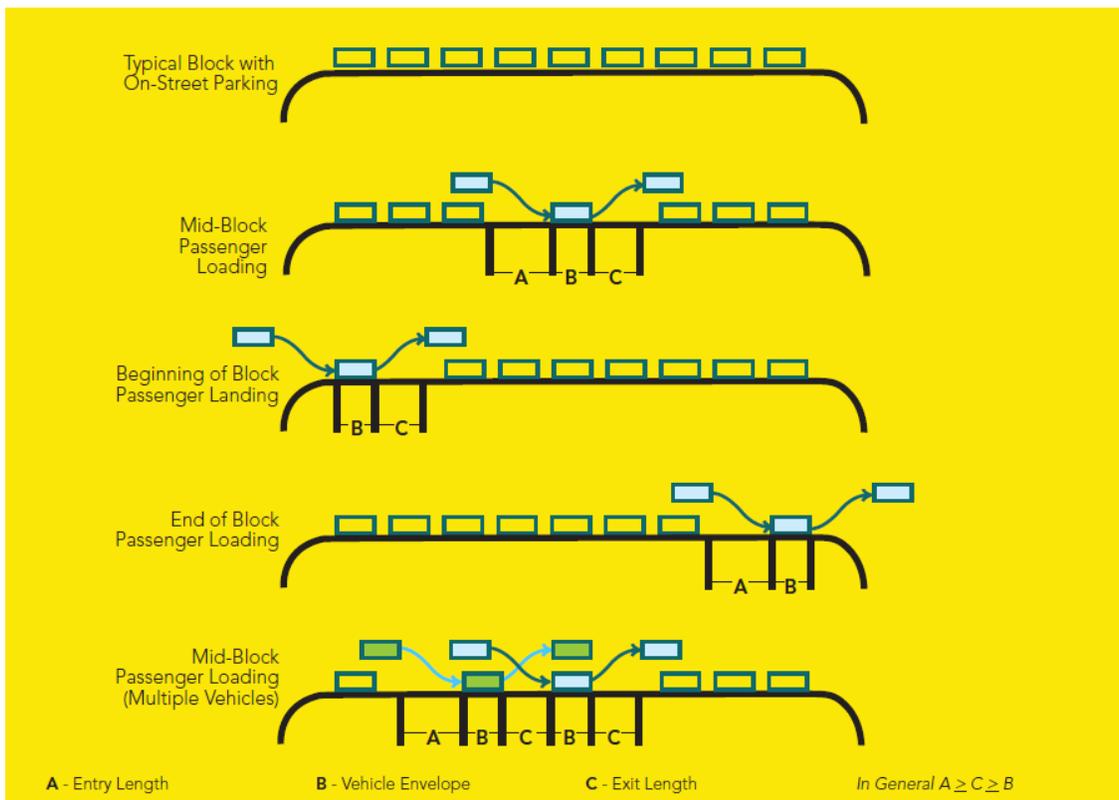
PASSENGER LOADING

With the growth of ride sharing services, the need for safe spaces for passenger loading and unloading is becoming more prevalent. Without dedicated pick-up or drop-off areas, rideshare vehicles or taxis are more likely to double park or cruise along streets while looking for a place to load or unload. This may lead to conflicts with

other road users or unnecessarily increase vehicle kilometres travelled. Proactive provision of dedicated pick-up and drop-off zones can help mitigate conflicts between ridesharing vehicles and other road users.

Entry distances for vehicles entering a loading zone are generally longer than their exit distances. For this reason, providing passenger loading zones at the beginning of a block is generally more space efficient than providing the loading zone in the middle or at the end of a block.

Where passenger loading zones are provided adjacent to cycling facilities, consider increasing the width of the buffer between the loading zone and the cycling facility to allow pedestrians to wait within the buffer or walk along it to reach the nearest crossing. For a more detailed review of buffer widths for cycling facilities, refer to [Section 3.2](#) of the manual.



This diagram demonstrates the curb space required for passenger loading in different configurations. Source: ITE

FREIGHT LOADING

Freight loading is typically provided for businesses in commercial areas where on-site loading or rear alleys are not available. However, with the increased demand for deliveries due to a growth in online retail, freight loading should also be considered in residential and industrial areas. These freight loading zones should be planned as a shared resource with adjacent private and public uses within a neighbourhood. Loading and curbside delivery zones would typically be designed to accommodate single-unit trucks for periods of 30 minutes or less.

Careful consideration should be made for the placement and design of freight loading zones along corridors with cycling facilities to minimize conflicts between cyclists and people loading and unloading the vehicles. Strategies include locating loading zones on intersecting streets or designing loading zones with similar features to accessible on-street parking zones

In 2017, following a recommendation by the Music Industry Working Committee (MIWC) and Hamilton Live Music Venue Alliance, the City introduced signs stating "Musicians Welcome" to be installed outside live music venues to provide space for musicians to load and unload their equipment. Loading zones in commercial areas for live music venues should continue to apply music loading zone areas.

Four of Hamilton's live music venues with nearby loading zones were provided with these signs to lend to musicians and their teams. These were to be placed in their vehicle's windshield to allow them to use the loading zones without risk of incurring a ticket.



Welcome Signs for musicians to load and unload by live music venues.

3.5.5 BICYCLE AND MICRO-MOBILITY PARKING

Curbside space may be used to support multi-modal mobility by providing on-street bike racks, bike corrals, or bike share stations. Space may also be reserved for dock-less bicycles and e-scooters so as to provide parking locations that do not obstruct the sidewalk. The conversion of on-street parking to bicycle or micro-mobility parking can result in significantly more efficient use of curbside space: typically, eight to fourteen bicycle parking spaces can fit within the area of one to two vehicle parking spaces. From a maintenance perspective, it can be desirable to re-purpose the on-street parking used for micromobility devices into poured concrete extensions.

To maximize usage, bicycle and micro-mobility parking should be located along corridors with a high volume of bicycle traffic. Locating parking along main streets has the added benefit of offering riders quick and easy access to nearby businesses and amenities. Safe and secure bicycle parking may further incentivize people to use their bicycles for everyday trips.



Bike corrals in Berkeley, CA (left) and on-street bike share station in Hamilton (right). Sources: City of Berkeley, CBC

3.5.6 PARKLETS & PATIOS

Curbside space may also be used to extend the pedestrian realm and activate the streetscape through the construction of parklets or restaurant patios. Parklets and pop-up patios gained popularity in many cities across Canada during the COVID-19 pandemic as a way to provide outdoor dining space for restaurants and cafés when indoor dining was prohibited. The City of Hamilton's On-Street Patio program, established in 2016, allows businesses to install temporary patios and seating areas in on-street parking areas within Business Improvement Areas (BIAs) and Community Improvement Plan Areas (CIPAs). These installations typically include green space, seating, dining areas, or public art installations. To maximize their usage, parklets should be considered on streets with high pedestrian volumes, such as on Main Streets.

As an extension of the public realm, parklets should also be designed with accessibility in mind. Ensuring sidewalks are regularly cleaned with minimal slope and cracks, as well as ensuring a wide, accessible route connecting the sidewalk to the parklet entry can help create accessible public parklets. Within the parklet itself, the entry should be

located facing an unobstructed area with minimal slope along the sidewalk and curb. The parklet's surface should be firm, stable, and slip resistant, and additionally have adequate turning and resting space for a wheelchair, in order to accommodate those with mobility issues.



Temporary parklets installed in Hamilton. Source: Pop-Up Street Patios

Parklets are often temporary spaces that can be used seasonally and converted to parking or snow storage during the winter months. Regardless, parklets should be installed and maintained in collaboration with local Business Improvement Areas (BIAs) and neighbourhood groups to ensure their long-term viability.

3.5.7 OTHER CURBSIDE USES

FOOD TRUCKS

As a way to further activate the streetscape, consider designating curb space to allow Food Service Vehicles to operate in the right of way. The location of designated curbside spaces should follow City by-law no. 07-170 for the proper stopping distances for Food Service Vehicles.

In addition, food truck curbside areas should provide an extra 1 metre clearance in front of and behind the vehicle for fire safety and to allow for circulation.



Food trucks provide an opportunity to activate the public and pedestrian realm. Source: Food and Wine

FLEX ZONES

In addition to fixed uses, curbside space designation can be flexible to serve different uses. This can be achieved through time-of-day restrictions, by allowing multiple uses in the same space, or designating different uses along a single block face. ITE offers dynamic curbside management options such as the Curbside Management Tool to help temporally and spatially prioritize demand and allocation of the curb usage. Refer to this tool or the Curbside Management Practitioners Guide for more details.

EV CHARGING

With a rise in Electric Vehicle (EV) users over the past few years, the City may benefit from installing charging stations along the curbside. The City of Hamilton's Parking Master Plan includes recommendations to develop a comprehensive strategy for EV charging. The plan recommends considering curbside charging, a pricing strategy, and enforcement for designated EV spaces.

Siting a charger requires physical space in a location that maintains adequate clearance for other road users, particularly in constrained areas such as Business Improvement Areas, as well as electricity connections. As a result, the need for charging stations should be determined early in the scoping process.

EV charging stations along the curbside in commercial areas have the potential to incentivize current EV users to frequent those shopping areas knowing there are guaranteed charging spots. On the other hand, EV charging stations along residential curbsides could help EV users without private parking spots where they can install their own charger.



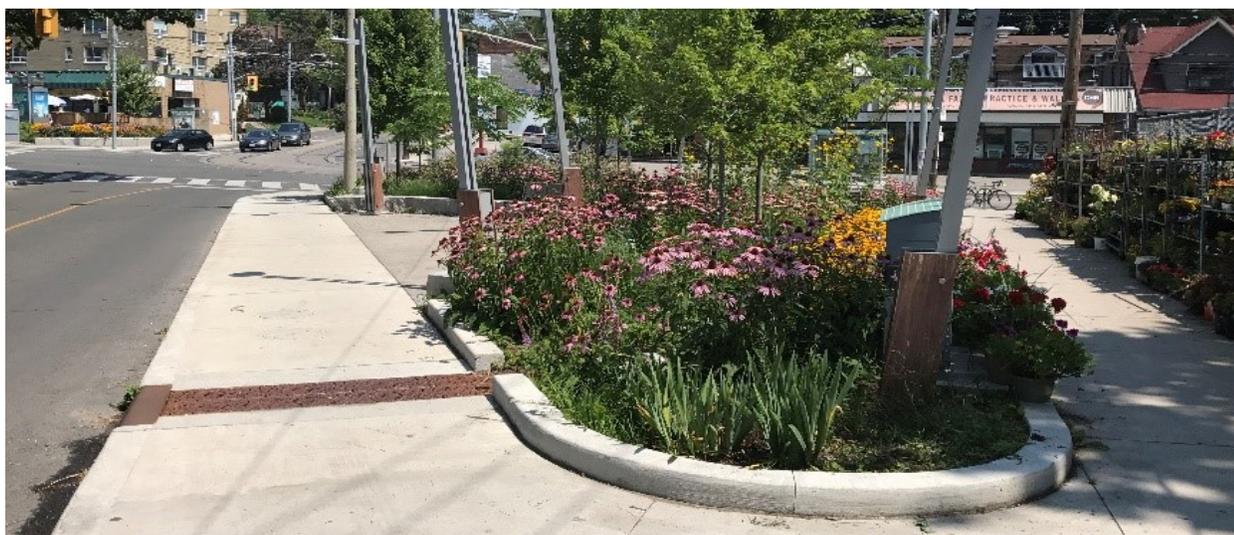
New York's first curbside electric vehicle (EV) charging station, installed in June 2021. Source: The Wall Street Journal

3.6 GREEN INFRASTRUCTURE

Green infrastructure refers to elements of streets that provide ecological and hydrological functions. These functions include mitigating urban heat island effect, improving biodiversity, air quality, energy efficiency, and stormwater management. Green infrastructure also contributes to the aesthetics of the streetscape and can improve comfort for people walking, rolling, cycling, and waiting for transit. Access to nature within the City can improve mental health outcomes for residents. Green infrastructure such as street trees and stormwater management systems within the right of way can also act as traffic calming measures.

Green infrastructure can help the City in addressing environmental objectives. Increasing vegetation and light surfaces in urban environments helps mitigate the urban heat island effect caused by surfaces that absorb solar thermal energy, such as asphalt. Paved areas with little vegetation absorb solar energy, increasing temperatures and making them less comfortable on hot summer days, thus increasing the demand for air conditioning. Introducing green infrastructure can help reflect solar energy, reducing local temperatures and thus improving outdoor comfort and the energy efficiency of nearby buildings. Street trees and other vegetation also help improve air quality and enhance biodiversity by providing habitats that support a variety of different species.

Green infrastructure can support the City in reaching its environmental and sustainability goals as outlined in the Rural and Urban Hamilton Official Plans, the Stormwater Management Master Plan, Hamilton's Design and Preservation Standards for Public Property (Draft), Hamilton's Urban Forestry Strategy (Draft), and the Hamilton Biodiversity Action Plan (Draft), Community and Energy Emissions Plan (Draft).

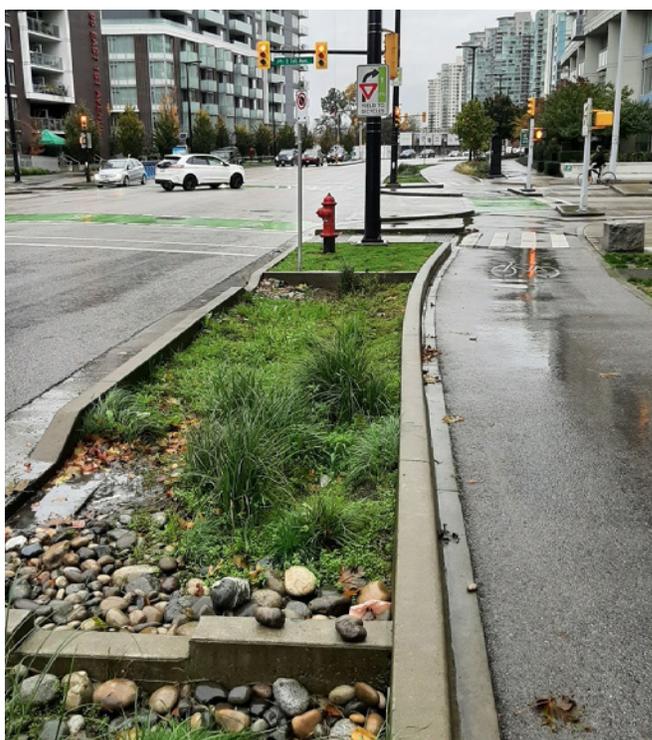


Rain garden in a parklet in Toronto. Source: City of Toronto

3.6.1 DESIGN PRINCIPLES

Prioritize low impact stormwater management features. Low Impact Development (LID) aims to mimic natural movement of water to manage stormwater runoff after rain or snowfall. Stormwater runoff is precipitation that does not evaporate or is not absorbed into the ground and instead runs over ground surfaces carrying pollutants into local water sheds. Unmanaged, stormwater runoff can cause erosion, flooding, and can degrade water quality of streams, lakes, and other wetlands. Green infrastructure such as rain gardens and permeable pavements complement the traditional storm sewer system by filtering, storing, and reducing runoff near the source of precipitation. Stormwater management features can help reduce the burden on the stormwater sewer system and improve water quality.

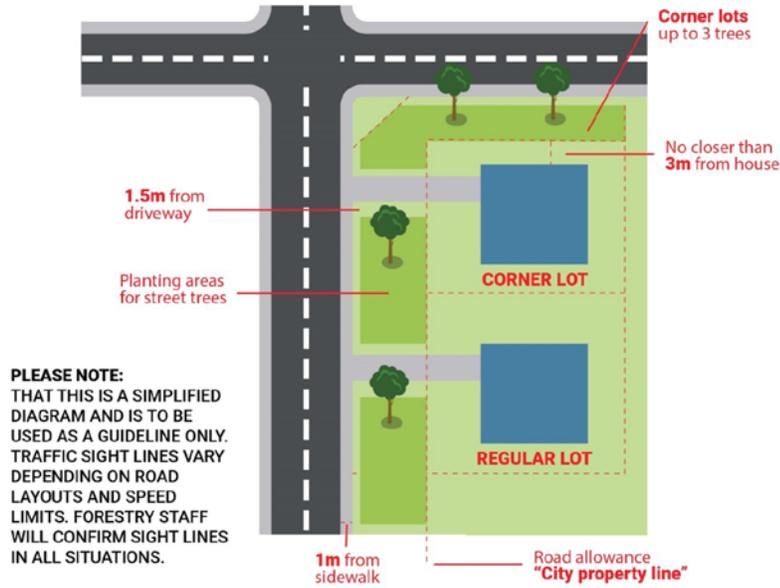
Complement sustainable and active transportation. Providing greenery in the urban landscape is important to encourage residents to walk and cycle more often. The shading, cooling, and noise reduction provided by an expanded tree canopy increases the comfort of people walking, cycling, or waiting for transit, further promoting the use of sustainable modes of transportation. Location of street trees and other green infrastructure can help reduce motor vehicles speeds, further improving road safety. The placement and maintenance of vegetation should ensure visibility and appropriate sightlines are maintained to ensure safety for all road users.



Low impact development feature separating a protected bike lane from automobile traffic, in Vancouver, BC

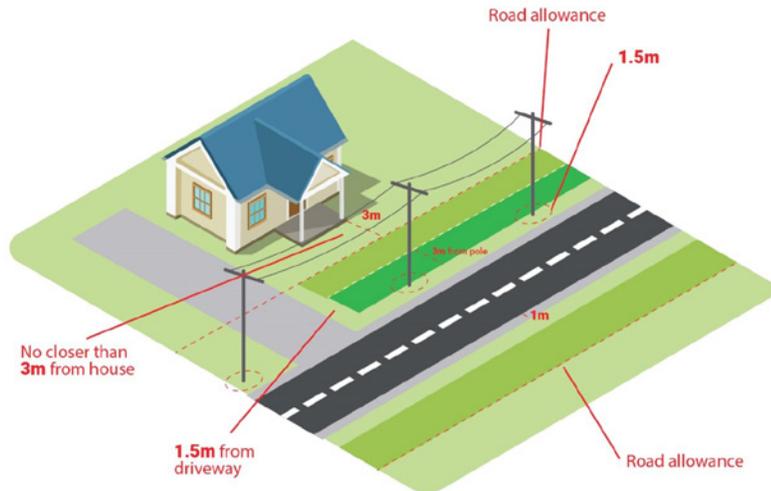
SITE REQUIREMENT OVERVIEW

ROAD ALLOWANCES VARY THROUGHOUT GREATER HAMILTON.
FORESTRY STAFF WILL CONFIRM MEASUREMENTS AT THE TIME OF INSPECTION.



PLEASE NOTE:
THAT THIS IS A SIMPLIFIED
DIAGRAM AND IS TO BE
USED AS A GUIDELINE ONLY.
TRAFFIC SIGHT LINES VARY
DEPENDING ON ROAD
LAYOUTS AND SPEED
LIMITS. FORESTRY STAFF
WILL CONFIRM SIGHT LINES
IN ALL SITUATIONS.

SITE REQUIREMENT SIDE VIEW



PLEASE NOTE: THIS IS A SIMPLIFIED
DIAGRAM TO BE USED AS A GUIDELINE
ONLY. FORESTRY STAFF WILL CONFIRM
PLANTING LOCATIONS IN ALL SITUATIONS.

- UPRIGHT SPECIES
- SMALLER SPECIES
UP TO 6.0M

Setbacks and allowances for street trees. Source: City of Hamilton Design and Preservation Standards for Public Property (Draft)

3.6.2 STREET TREES

According to Hamilton’s Urban Forest Strategy, the Urban Hamilton Official Plan sets a target to reach 30% canopy cover. The canopy cover in the City in 2018 was approximately 21%. Of the 5.2 million trees in Hamilton’s urban forest, 168,000 (3.2%) of them are street trees. Street trees provide numerous environmental, economic, and social benefits.

ENVIRONMENTAL	ECONOMIC	SOCIAL
<ul style="list-style-type: none"> •Improved local air and water quality •Biodiversity conservation •Reduced flooding •Mitigate urban heat island effect •Carbon sequestrain and storage 	<ul style="list-style-type: none"> •Reduced pressure on stormwater infrastructure •Extended pavement life •Increased residential property values •Improved visitor perception •Lower energy costs for heating and cooling •Improved climate resiliency 	<ul style="list-style-type: none"> •Mental health benefits •Shade and cooling •Increased physical activity •Better walking environments •Noise reduction •Solace and a sense of place •Cultural heritage values

Benefits of Urban Trees from the Hamilton Urban Forestry Strategy (2020). Source: Tree Canada, Benefits of Urban Trees

The City of Hamilton’s Forestry and Horticulture Draft Design and Preservation Manual for Public Property outlines details on permitting, plans, and planting guidelines for street trees and other green infrastructure. Street trees can be planted directly into the boulevard where there is continuous soft surface width of at least 1.75m. The width of the tree planting area and sufficient soil volumes contribute to the health and longevity of the street trees. The Draft Design and Preservation Standards for Public Property outline minimum requirements for soil volumes based on tree type and planting medium. The document also outlines site requirements and desired setbacks from nearest buildings, driveways, sidewalks, and adjacent utilities and municipal services.

When hard surfaces surround tree planting areas, strategies such as soil cells, soil corridors, or root bridges should be considered to ensure the health of the root system. In each case, the system should include proper drainage to mitigate the impact on nearby sub-surface utilities. Refer to the Forestry and Horticulture Draft Design and Preservation Manual for Public Property and consult with the City of Hamilton’s Forestry and Horticulture department ensure designs adhere to the City’s best practices and design and preservation standards.

“Right tree, right place” best practices should be used in selecting species. Local context and street typologies should be considered when selecting tree species.

According to the Urban Forestry Strategy Report, there is an overabundance of maple species making street trees more vulnerable to pest outbreaks. Ensuring species diversity can help mitigate this vulnerability.

Selection of street tree placement should include coordination with utilities early in the process to ensure root space is maximized and will not interfere with subsurface utilities. Maintenance plans for any vegetation should be developed in the preliminary design phase as well to ensure that the canopy does not interfere with overhead wires and street lighting and that the plants can thrive over many years.



Continuous growing trench in streetscape. Source: Draft Design and Preservation Standards for Public Property

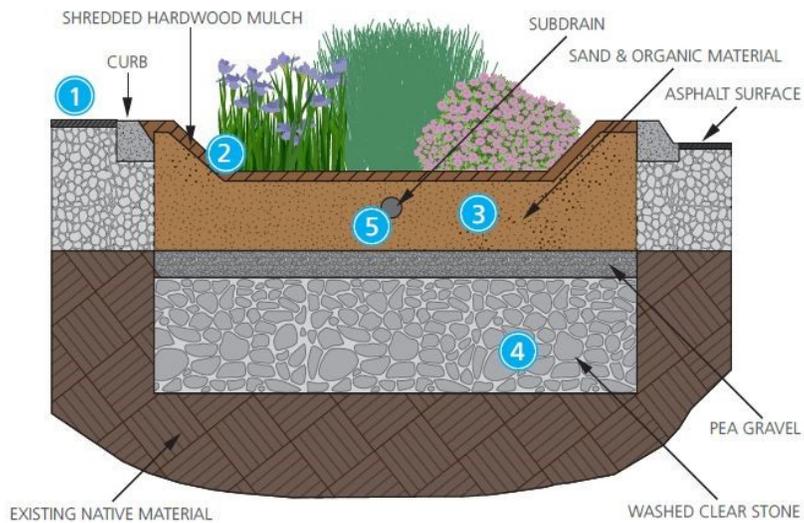
3.6.3 STORMWATER MANAGEMENT

Low-impact development (LID) features such as rain gardens, cisterns, permeable pavement, and grassed swales provide soft surfaces to collect, store, and filter stormwater runoff along roads and boulevards. Managing stormwater closer to the source can help reduce runoff volume, erosion, and flooding in the city, can reduce the burden on the City's storm sewer system. This may help the City manage operating costs and energy efficiency of the storm water system. LID features also manage stormwater runoff quality by filtering sediments and pollutants before entering Hamilton's waterways. As with other green infrastructure, LID features contribute to mitigating the urban heat island effect and increasing the attractiveness of the streetscape. Rain gardens installed as curb bump-outs in the right of way can also act as traffic calming devices.



A Rain Garden Traffic Calming Bump-out installed at the intersection of Bay Street North and Simcoe Street in Hamilton. Source: Google Maps

Many landscaped features along the right of way can be designed as rain gardens to collect runoff. Grassy surfaces, planters, and planting garden can all act as rain gardens. Consideration for grading of hard surfaces and the location of inlets near LID features should be given early in the design process to lead stormwater towards LID features.



Cross Section of Bay Street North and Simcoe Street West Traffic Calming LID Bump-Out.

Rain gardens should contain a mix of native perennials and grasses that can tolerate wet and dry conditions throughout the seasons as well as winter salt. Including plants that attract pollinators in LID features also strengthen and enhance Hamilton's biodiversity.

Rain gardens should be designed to drain within 1-3 days of heavy rainfall and should include an overflow system such as a subdrain that filters excess water into nearby stormwater sewers. Where the right of way is constrained or where raingarden maintenance is not feasible, subsurface LID systems to collect stormwater such as perforated pipes, third pipe systems or infiltration galleries can be installed.

As with street trees, maintenance plans should be developed during the preliminary design phase of stormwater management features, in consultation with Horticulture staff, to ensure that vegetation does not encroach in the pedestrian clearway, that the spaces remain attractive, and that the LID landscapes continue to provide the intended level of stormwater treatment.



LID feature incorporated into the sidewalk in Montreal.

3.7 UTILITIES AND MUNICIPAL SERVICES

Utilities and municipal services comprise essential services such as water supply, sewers, electricity and telecommunications, lighting, and gas supply the residents of Hamilton rely on every day. Utilities and municipal services are generally accommodated within the public right of way and, as such, are critical elements in the design and maintenance of Complete Streets. The design of Complete Streets should consider utility placement to maximize infrastructure investments, facilitate access for maintenance and repair, mitigate the impacts of extreme weather caused by climate change, and contribute to placemaking.

This section outlines the principles and consideration for the installation of utilities and municipal services on public roads. Further information on utility specifications, permitting, and installation can be found in the City's Comprehensive Development Guidelines and the Right-of-Way Utility Installation and Permit Manual.

3.7.1 DESIGN PRINCIPLES

Follow existing processes. Designing, installing, and maintaining utilities and municipal services are complex processes involving different stakeholders from the City to individual utility providers. The Hamilton Public Utilities Coordination Committee (HPUCC), a group that coordinates utilities work between the City and private utility companies, should be included in the design process. Utility and municipal service design should adhere to the City's engineering standards such as the Right-of-Way Utility Installation and Permit Manual and the Comprehensive Development Guidelines.

Facilitate access to underground utilities. Underground utilities should be positioned under soft surfaces wherever possible to provide easy access for maintenance and repair. Horizontal and vertical clearance between utilities should be sufficient to avoid interfering with adjacent utilities when accessing another one for maintenance or replacement. Where possible, communication and electrical utilities should be combined into a single utility trench to make effective use of space within the right of way.

Design should be driven by surface level uses, not utilities. Lateral placement of underground utilities is often more flexible than street-level elements. The needs of all road users and surface operations should be considered first when designing Complete Streets. Utility placement should be coordinated to fit into a given design unless specific constraints require consideration of utility placement first.

Aesthetic treatment. Placemaking and the public realm are key elements of Complete Streets. As such, the placement of above ground utilities should include provisions to minimize negative visual impacts to the surface level street-design to ensure an attractive streetscape.

3.7.2 SEWERS

STORM SEWERS

Storm sewers collect precipitation runoff from buildings, roadways, and other hard surfaces through drains and catch basins.

Maintenance holes and grates should be positioned outside the wheel path of motor vehicles and bicycles – for example in the centre of a lane or in between lanes – to avoid degradation of the surrounding pavement. Catch basins should be located upstream of pedestrian crosswalks and should be avoided in driveway curb depressions to keep them clear of stormwater and ice.

Careful design of catch basin grates should be considered for catch basins that fall within a cycling facility to ensure that cyclists wheels do not get caught in the gaps. Herringbone openings and similar design with gaps, such as side-inlet catch basins, that do not run parallel to the path of travel can help mitigate that risk.

SANITARY SEWERS

Sanitary sewers collect wastewater from residential, commercial, and industrial buildings to be treated at wastewater treatment facilities before being discharged into a receiving body of water.

Maintenance holes and grates should be positioned outside the wheel path of motor vehicles and bicycles – for example in the centre of a lane or in between lanes – to avoid degradation of the surrounding pavement.



Maintenance holes positioned outside the wheel path of motor vehicles and bicycles on Garth St. in Hamilton. Source: Google Maps

3.7.3 WATERMAINS AND WATER SERVICES

Watermains supply drinking water to fire hydrants and residential, commercial, and industrial buildings throughout the city.

Emergency access is an important consideration when determining the alignment of watermains. Broken watermains can result in losing water services for nearby buildings and can cause flooding. If not addressed rapidly, flooding can cause significant infrastructure damage increasing safety risks. To facilitate access, the preferred location for a watermain is below soft surface over asphalt or concrete.

3.7.4 ELECTRICAL AND COMMUNICATIONS UTILITIES

Electrical and communication wires can be installed either overhead or underground. Overhead electrical and communication utility wires allow for low cost implementation and easier access for maintenance than underground wires. Overhead wires are, however, more susceptible to weather conditions including ice, falling tree branches, and heavy wind than underground wires.

An abundance of overhead wires can also have a negative impact on the aesthetics of the public realm and can conflict with street trees. Careful considerations of these impacts should be kept in mind when evaluating the installation of utility wires. Traffic signal, street lighting, and utility pole installation should be coordinated to minimize the number of utility poles in the public right of way.

Installing electrical and communication utilities below ground can help mitigate

the downsides of overhead wiring however can significantly increase the cost and complexity of implementation and maintenance. Underground wiring requires transformer boxes to be installed at surface level. Transformer boxes should be located as close to the property line as possible and away from snow storage areas. The alignment of underground wires should be consistent with the alignment of the right of way as much as possible. Wherever possible and financially feasible, the City should work to install electrical and communication wires underground.

3.7.5 STREET LIGHTING

Streetlights are crucial to enable the safe movement of pedestrians, cyclists, and motorists at night through the city. Street lighting contributes to enhancing safety and accessibility of public space. The design of street lighting should consider the pedestrian experience while balancing the City's energy and climate goals. For example, light fixtures should be dark sky compliant and focus most of the light towards the ground to minimize light pollution and energy consumption. Careful consideration should be made to minimize blind spots along the right of way that can create unsafe conditions for people walking. The location of new streetlights should be coordinated with the location of street trees to minimize light obstruction from trees.

3.7.6 GAS MAINS

Gas mains supply natural gas throughout the City as a primary source of fuel and heating for residential and commercial markets.

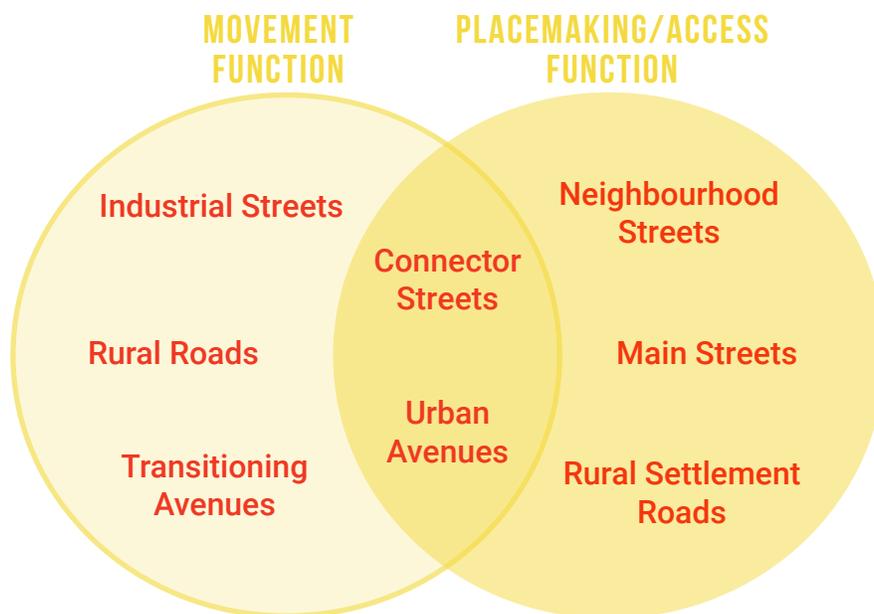
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CHAPTER FOUR TYPOLOGIES

4.1 DEFINING TYPOLOGIES

All streets serve a mobility function and placemaking function to different degrees. Mobility-focused streets prioritize safely providing a high degree of people–or goods–movement capacity. On these streets, there is greater separation between different road users, more limited access to the roadway, and higher motor vehicle operating speeds. In contrast, streets with a placemaking or a property access function prioritize the role of the street as a means of accessing destinations, and often prioritize the role of the street as an important public space with an attractive environment built at the pedestrian scale. These may take the form of quiet neighbourhood streets or vibrant main streets. On these streets, there are slower motor vehicle speeds with less separation between different road users, more frequent property access, and more curbside activity.

Defining the primary function of a street is an important principle of a Vision Zero or Sustainable Safety approach to road safety. Roadways that attempt to simultaneously provide a high degree of mobility and a high degree of property access are often prone to higher frequencies of serious collisions. Existing roads that feature high traffic volumes and frequent property accesses should be critically evaluated on a case-by-case basis to determine their desired long-term function, while temporary measures to improve safety should also be considered.



Hamilton's Complete Streets approach includes eight typologies, approved by Council in 2021, which represent the diverse range of streets found throughout the city.

Urban Avenues are vibrant pedestrian-oriented streets that provide a high amount of people-movement capacity, located in highly urban areas of the City.



Transitioning Avenues are mobility-oriented streets that extend across urban areas of the City. They are high-traffic streets and often important goods movement corridors.



Main Streets are placemaking-oriented streets, and include many historic main streets found in urban parts of the City. They are pedestrian-oriented with slow motor vehicle speeds and small-or medium-scale mixed-use buildings.



Connectors serve to link neighbourhood streets with Urban Avenues and Transitioning Avenues. They accommodate moderate volumes of vehicle traffic in a lower speed environment.



Industrial Streets provide direct land access to industrial and commercial employment areas. They are found in industrial areas of the City and may accommodate significant truck traffic.



Neighbourhood Streets provide direct access to residential dwellings. They are low-volume and low-speed streets that are not intended to serve a through traffic function for motor vehicle traffic.



Rural Roads are mobility-oriented streets within agricultural, natural, or industrial areas of the City. They provide a high motor vehicle capacity and may be important goods movement corridors.



Rural Settlement Roads are found where Rural Roads pass through small communities throughout the rural areas of Hamilton. Although through-traffic is accommodated, these road segments are focused on property access and placemaking.



The Complete Street Typologies expand upon Hamilton's existing functional road classification system, which classifies streets as arterials (major, minor, and rural), collectors (residential and rural), or local (residential and rural). The typologies consider the function of the street along with its context (for example, urban, industrial, or rural) to provide context-sensitive design guidance for each of the different categories of streets commonly found throughout the City. Chapter 2, [Section 2.2.11](#) includes a matrix outlining the desired conditions for each street element per typology. Municipal Parkways are not included within the typology framework given their unique operating characteristics. The typologies are an overlay to the functional road classifications in the City's Rural/Urban Official Plans and are not meant to replace the existing classification.

This chapter explores the various aspects of street design for each of the eight Complete Street typologies. In each section, illustrations demonstrate the typical right of way configurations and accompanying streetscape elements that make up each typology. These illustrations are intended to demonstrate the general design features of each typology and may vary when applied to the location-specific context of each street. Note that all utilities and underground service locations are conceptual only. Depending on the specific context, it may not be possible to apply the typical cross-section due to various constraints, such as one-way or two-way traffic flow, available right of way, natural features, utilities or building setbacks. In these situations, practitioners should refer to Chapter 3, which provides design guidance.

The content shown in the following chapter is presented on two-way roadways, however the principles, space allocation, and designs are applicable to one-way streets with some modification. Ultimately, conversion of streets from one-way to two-way are subject to further study, consultation, and Council approval. Some roadways in the City have been identified for two-way conversion to improve safety outcomes. The City's Transportation Master Plan provides a framework for considering one-way to two-way conversions.

Note that examples for each typology are given as potential locations for the application of each typology but may not be representative of the desired conditions in its current form.

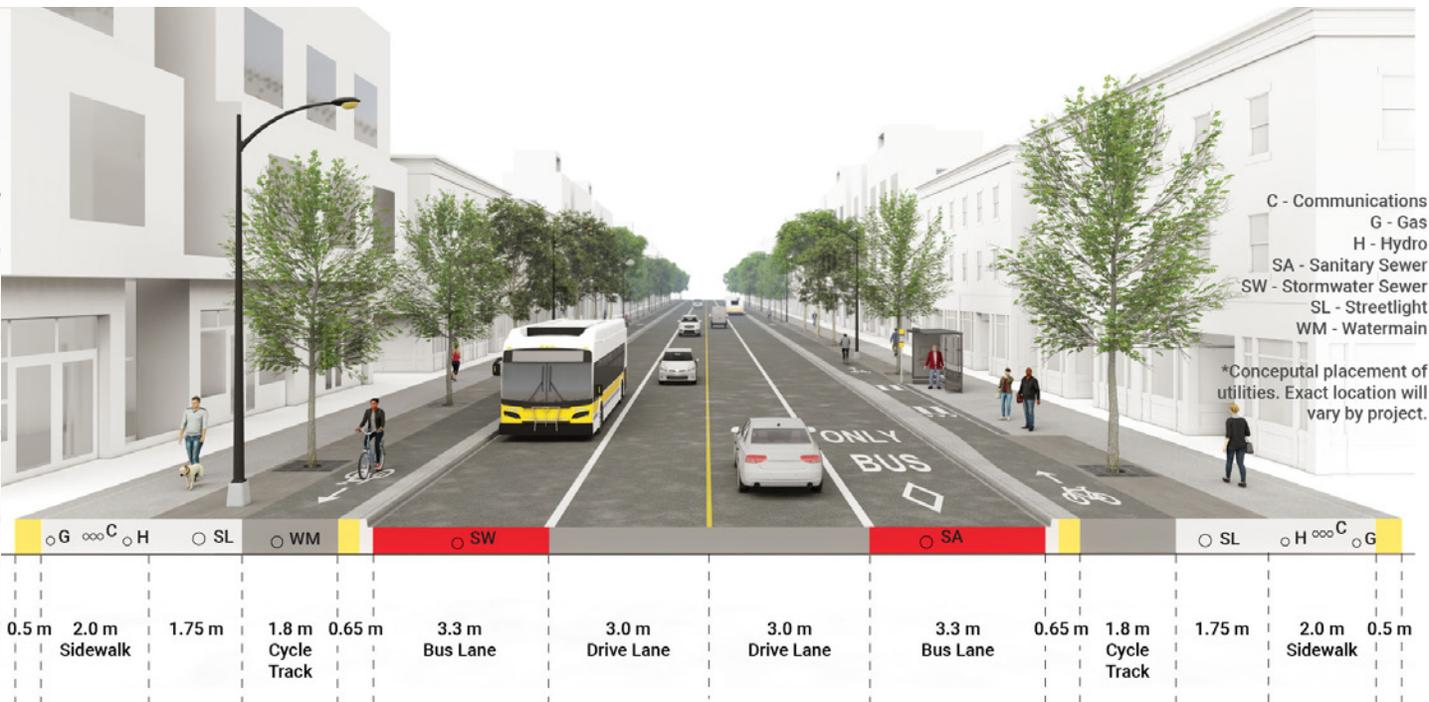
	URBAN AVENUES	TRANSITIONING AVENUES	MAIN STREETS	CONNECTORS	INDUSTRIAL STREETS	NEIGHBOURHOOD STREETS	RURAL ROADS	RURAL SETTLEMENT ROADS
CONTEXT	Urban	Urban / Suburban / Industrial	Urban	Urban / Suburban	Industrial	Urban / Suburban	Rural	Rural
PRIMARY STREET FUNCTION	Mobility and place-making	Mobility	Placemaking and access	Mobility and access	Access	Access	Mobility	Access
TYPICAL RIGHT OF WAY	20–26 m	36 m	18–20 m	20–26 m	26–30 m	15–20 m urban 20–26 m rural	26–36 m	20–26 m
NUMBER OF LANES	2–4	4	2	2	3	1–2	2	2
TARGET SPEED	40–50 km/h	50–60 km/h	30–40 km/h	30–40 km/h	40–50 km/h	30–40 km/h	60–80 km/h	40–50 km/h
CYCLING FACILITIES	Cycle tracks	Cycle tracks or multi-use paths	Shared lanes	Cycle tracks	Cycle tracks or multi-use paths	Mixed traffic or contraflow lane	Shared lanes, paved shoulder, or multi-use path	Bicycle lanes, cycle tracks, or multi-use paths
WALKWAY ZONE WIDTH	2.0–3.5 m	1.8–2.5 m	2.0–3.5 m	1.8–2.0 m	2.0 m	1.8 m	n/a	1.8 m

4.2 URBAN AVENUES



Typical Urban Avenue Cross Section (20m ROW)

Urban avenues are mobility-oriented streets found in highly urban areas of the City. They provide a high volume of people-movement capacity with an emphasis on active transportation and transit service. These are vibrant pedestrian-oriented streets with frequent building entrances and commercial activity fronting onto sidewalks, and an attention to street trees and other green infrastructure elements. However, there is limited property access (for example, driveways) from the roadway, and curbside activity such as parking or loading is minimized. These may also accommodate higher-order transit corridors, particularly in the lower city.



Typical Urban Avenue Cross Section (26m ROW).

CONTEXT	Urban
STREET FUNCTION	Mobility and placemaking
RIGHT OF WAY	20–26 m
NUMBER OF LANES	2–4
TARGET SPEED	40–50 km/h
CYCLING FACILITIES	Cycle tracks, typically one-way on each side of the street
PEDESTRIAN CLEAR ZONE WIDTH	2.0 m, up to 3.5 m adjacent to high pedestrian generators

RIGHT OF WAY

The right of way (ROW) range for Urban Avenues largely depends on context. In older areas of the City, many existing streets are 20 metres wide. Through redevelopment, 26 to 30 metre ROWs can be achieved if heritage constraints and existing built form allow. Wider ROWs allow for increased capacity and more generous sidewalk, street furniture, and tree planting space. However, the core elements of an Urban Avenue can be implemented within a 20 metre ROW.

GOALS

- Prioritize transit and active transportation to maximize capacity. Dedicated bus lanes or bus queue jumps can help improve the efficiency of public transit along these avenues. Cycle tracks may be located next to the sidewalk, separated by an urban braille feature.
- In narrow corridors, maximize functionality and mobility by varying the design along the length of the corridor. For example, space that is used for a planting zone in one location may be re-purposed to provide a loading zone where needed or to allow for a turn lane to be introduced at an intersection.
- Provide frequent pedestrian crossing opportunities. Consider mid-block crossings in areas with high pedestrian traffic.
- Provide generous plantings and furnishing zones where space permits. These can help beautify and enliven the streetscape, making it more appealing for pedestrians to visit.

EXAMPLE STREETS

- John Street (St. Joseph's Drive to Cannon St.)
- Barton Street (through Barton Village)
- Cannon Street (Hess St. N. to Sherman Ave. N.)

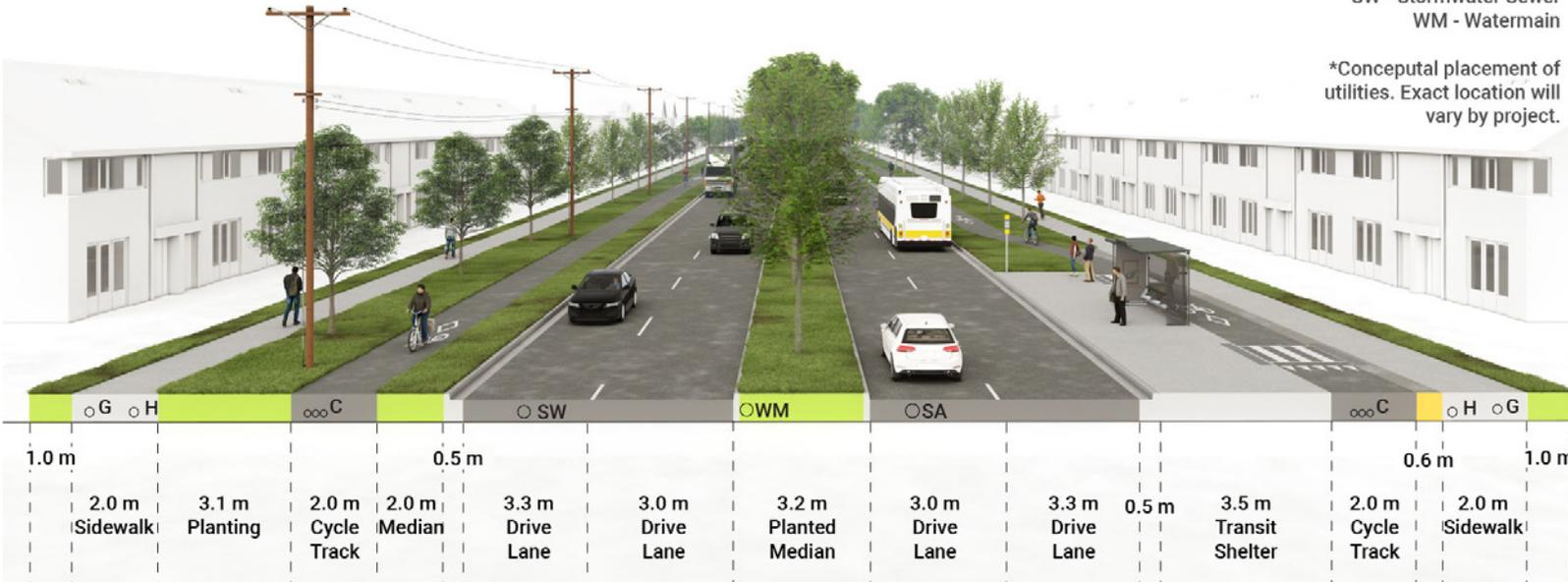


Cannon St. W in Central Hamilton.

4.3 TRANSITIONING AVENUES

C - Communications
G - Gas
H - Hydro
SA - Sanitary Sewer
SW - Stormwater Sewer
WM - Watermain

*Conceptual placement of utilities. Exact location will vary by project.

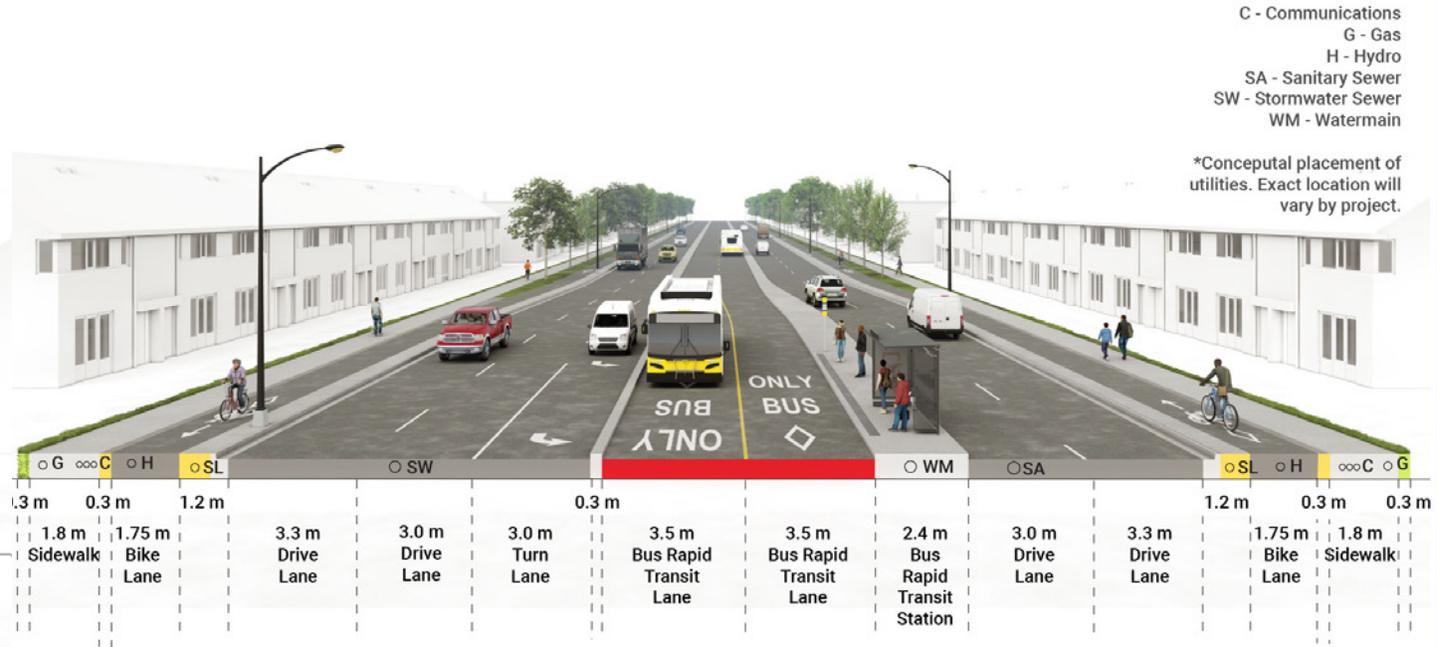


Typical Transitioning Avenue Cross Section (36m ROW).

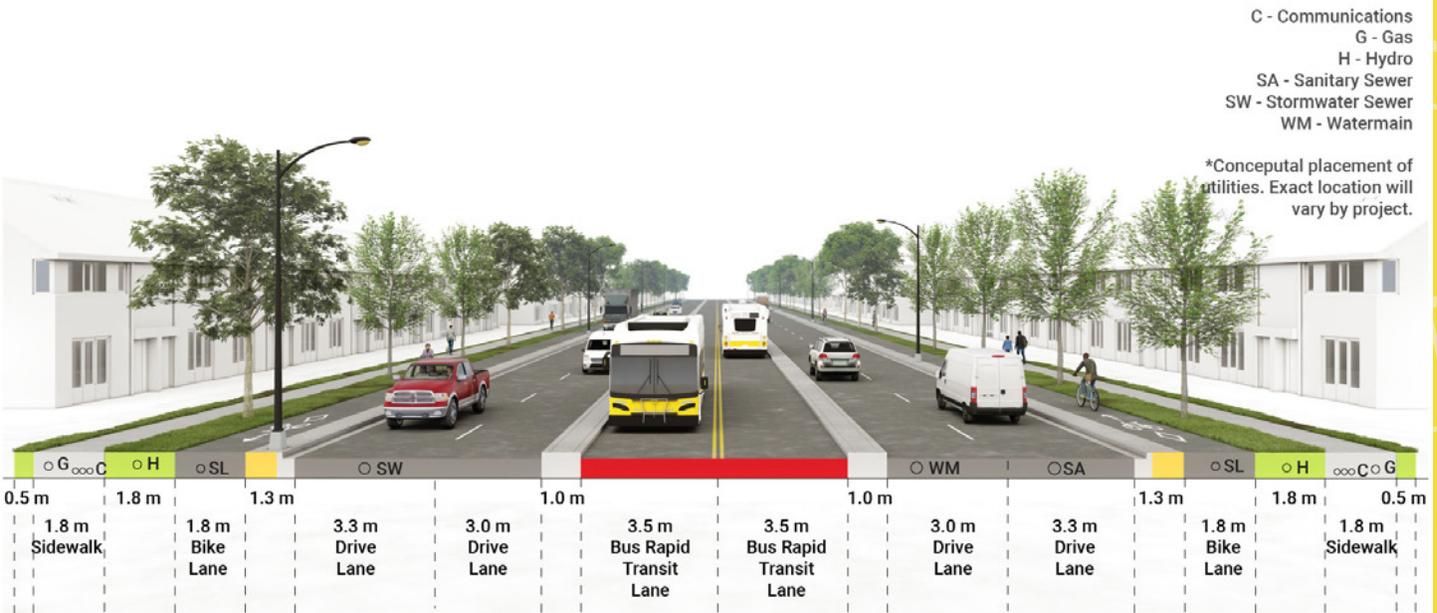
Transitioning Avenues are major streets that cross the city. They are high traffic streets that focus on mobility, whether that be people or goods movement. They are often, but not exclusively, located in areas of the City that are expected to transition to a more urbanized and mixed-use context over the coming years. This may include a transition from low-density residential and retail to medium or high-density mixed-use development. New developments are expected to be more street-oriented than in the past, with higher priority on the access and movement of pedestrians.

As these are high-capacity mobility-oriented streets, land access to these streets is limited and curbside activity is minimized. Driveways generally front onto intersecting streets, and not directly onto a Transitioning Avenue. Intersections are widely spaced and centre medians or dedicated turning lanes are used to improve safety and capacity. Some existing Transitioning Avenues feature frequent driveway accesses. Through redevelopment, efforts should be made to remove or consolidate accesses.

Any environmental assessment or feasibility studies for transitioning avenue projects along higher order transit corridors should investigate opportunities for increasing the right of way to enhance public realm and green infrastructure within the right of way.



Typical Transitioning Avenue Cross Section with centre-running BRT approaching intersection and transit stop (36m ROW).



Typical Transitioning Avenue Cross Section with centre-running BRT at mid-block (36m ROW).

Some Transitioning Avenues form part of Hamilton’s higher order transit network and may eventually feature higher order transit lanes. Active transportation facilities including cycle tracks or multi-use paths are provided in a manner that facilitates network connectivity and access to destinations along these corridors.

CONTEXT	Urban/Suburban/ Industrial
STREET FUNCTION	Mobility
RIGHT OF WAY	36 m
NUMBER OF LANES	4 plus turning lanes
TARGET SPEED	50–60 km/h
CYCLING FACILITIES	Cycle tracks or multi-use paths (see Chapter 3 for selection criteria)
PEDESTRIAN CLEAR ZONE WIDTH	1.8 m, 2.5 m adjacent to high pedestrian activity generators

RIGHT OF WAY

The desired ROW for a Transitioning Avenues is 36 metres, which provides sufficient space for dedicated higher-order transit lanes and active transportation facilities. Wider right of ways may be desirable or necessary in some situations. Along higher-order transit corridors, localized widening may be considered at station locations to provide wider platforms or boulevards, or to accommodate additional turning lanes.

GOALS

- Limit vehicle accesses (e.g. driveways); consolidate accesses at controlled intersections.
- Limit on-street parking, loading, and other curbside uses.
- Use planting zones to buffer pedestrians and cyclists from motor vehicle traffic. Additional street furniture, public art or plantings in the buffer zones can also beautify the street, making the streetscape more appealing and contributing to place-making strategies.
- Provide centre-running transit in dedicated lanes on rapid transit corridors to maximize transit capacity, where planned.
- Select contextually appropriate active transportation facilities. In choosing between cycle tracks or multi-use paths, consider expected pedestrian and cyclist activity levels. In determining the placement of facilities (which side of the street), consider the frequency of conflicts, access to destinations, and network connectivity.

EXAMPLE STREETS

- Rymal Road (east of Garth Street)
- Wilson Street W (west of McClure Road)
- Upper James Street (Queensdale Avenue to Rymal Road)



Rymal Road East in October 2020. Source: Google Maps

4.4 MAIN STREETS

G - Gas
H - Hydro
SA - Sanitary Sewer
SW - Stormwater Sewer
WM - Watermain

*Conceptual placement of utilities.
Exact location will vary by project.



Typical Main Street Cross Section (20m ROW).

Main Streets are often the traditional shopping streets found in urban areas of the City. They are highly pedestrian-oriented with small-scale mixed-use buildings. They often contain heritage buildings or are designated as Heritage Conservation Districts in their own right. Development along Main Streets is heavily street-oriented and often surrounded by residential neighbourhoods. Main Streets exist in each of the former municipalities that make up Hamilton.

The Main Street typology has an urban cross-section with an emphasis on streetscaping and placemaking. Pedestrians should be prioritized with slower traffic, wide sidewalks, enhanced pedestrian amenities, and flexible curbside uses, which may include small parks (parklets) or outdoor dining. The flex zones can be framed by a mountable or non-mountable curb, depending on the context. Motor vehicle traffic is slowed through the intentional use of narrow vehicle lanes and a high intensity of curbside activity along with curb extensions at intersections. Street amenities include wide sidewalks and furnishing zones, pedestrian-oriented lighting, street trees, transit amenities, and opportunities for public art.

CONTEXT	Urban
STREET FUNCTION	Placemaking and access
RIGHT OF WAY	18–20 m
NUMBER OF LANES	2
TARGET SPEED	30–40 km/h
CYCLING FACILITIES	Shared lanes
PEDESTRIAN CLEAR ZONE WIDTH	2.0 m, 3.5 m adjacent to high pedestrian generators

RIGHT OF WAY

These roads historically have narrow ROWs which is important in maintaining pedestrian-scale street. A 20 metre ROW is desirable. However, 18 metre ROWs can be found on some streets.

GOALS

- Prioritize the pedestrian realm. Provide amenities like plantings, furniture, or creative curbside uses such as extended seating, restaurant and café patios, or parklets to attract pedestrians and encourage them to linger on the Main Street. Curb-less street sections may also be considered to provide more space for street plantings and furniture in place of parking.
- Provide street design that allows for frequent pedestrian crossing opportunities. Consider mid-block crossings with curb extensions to allow convenient crossings with minimal crossing distance.
- Apply passive traffic calming to promote slow travel speeds. This may include narrow vehicle travel lanes, on-street parking or flex zones, mid-block crossings, curb bulb-outs.
- Provide convenient and frequent bicycle parking or bike share stations to support access to destinations by bicycle.
- Collaborate with local BIAs to introduce and maintain placemaking features and activities tailored to the unique context of each street.

EXAMPLE STREETS

- Locke Street (Hunter St. to Herkimer St.)
- Kenilworth Ave North (north of Roxborough Ave.)
- Wilson Street (Rousseaux St. to Sulphur Springs Rd.)



Locke St. in Central Hamilton. Source: Google Maps

4.5 CONNECTORS



Typical Connector Cross Section (26m ROW)–New Construction.

Connectors are an important component of the street network, linking residential neighbourhoods to the major street network and to other areas of the city. Connectors are commonly found in residential areas and accommodate a moderate volume of traffic. Buildings are generally set back from the street and front onto a wide boulevard.

Connectors support transit and active transportation with wide sidewalks and dedicated cycling facilities. These streets include ample soft landscaping and mature trees to create a pleasant environment for walking and cycling. Sidewalks may be more than 1.8 metres in areas with high pedestrian traffic.

Connectors typically feature one lane of traffic per direction with on-street parking on one or both sides of the street. A narrow roadway with parking in dedicated lay-bys promotes slower travel speeds. Existing streets with wider roadways may be retrofit with advisory bike lanes and curb extensions at intersections to achieve similar objectives.



C - Communications
G - Gas
H - Hydro
SA - Sanitary Sewer
SW - Stormwater Sewer
SL - Streetlight
WM - Watermain

*Conceptual placement of utilities. Exact location will vary by project.

Typical Connector Cross Section (26m ROW)–Retrofit Scenario.



C - Communications
G - Gas
H - Hydro
SA - Sanitary Sewer
SW - Stormwater Sewer
SL - Streetlight
WM - Watermain

*Conceptual placement of utilities. Exact location will vary by project.

Typical Connector Cross Section (26m ROW)–Retrofit Scenario.

CONTEXT	Urban/Suburban
STREET FUNCTION	Mobility and access
RIGHT OF WAY	20–26 m
NUMBER OF LANES	2
TARGET SPEED	30–40 km/h
CYCLING FACILITIES	Cycle tracks (on-street lanes, advisory lanes, or shared lanes may be appropriate depending on context)
PEDESTRIAN CLEAR ZONE WIDTH	1.8m, 2.0m adjacent to high pedestrian generators

RIGHT OF WAY

The preferred ROW width for Connectors is 26 metres, though it is still applicable in more constrained areas. The 26 metre ROW provides enough space for dedicated cycle tracks and on-street parking along with generous planting zones.

GOALS

- Use passive traffic calming measures to promote slower speeds. This may include narrow vehicle travel lanes, on-street parking, mid-block crossings, and curb bulb-outs. More intrusive calming measures such as speed humps or raised crosswalks may be appropriate at pedestrian crossing locations or where specific concerns exist.
- Emphasize green infrastructure. Providing space for street trees and native plantings can enable a continuous tree canopy and provide shade for people walking and cycling.

Planters or green bump-outs may also help beautify the street while acting as passive traffic calming measures and managing stormwater at the source.

- Provide separated cycling facilities. These streets often serve as cycling routes to schools and may be used by children and less experienced cyclists. Cycle tracks completely separated from traffic offer the highest degree of safety and comfort for people cycling.
- Formalize space for motor vehicle parking, where warranted. On-street parking introduces "friction" which can encourage slower driving speeds. However, a wide roadway with low parking utilization will have the opposite effect. Curb extensions should be added at the beginning and end of blocks to help frame parking areas and reduce crossing distances for pedestrians.
- Driveways should be consolidated where possible along mobility-focused streets, such as those with high pedestrian movement. Instead, provide access via side streets.

EXAMPLE STREETS

- Stonehenge Drive
- Windwood Drive
- Bendamere Avenue
- Sterling Street



Sterling Street. Source: Google Maps

4.6 NEIGHBOURHOOD STREETS

- C - Communications
- G - Gas
- H - Hydro
- SA - Sanitary Sewer
- SW - Stormwater Sewer
- SL - Streetlight
- WM - Watermain

*Conceptual placement of utilities. Exact location will vary by project.



Typical Neighbourhood Street Cross Section (20m ROW).

Neighbourhood Streets provide direct access to residential areas. They have low traffic volumes and are most often used by people residing within the neighbourhood—they are not intended to serve as a through-traffic connection for motor vehicles. As Neighbourhood Streets are surrounded by residential uses, traffic calming measures and minimizing through-traffic are important considerations. Neighbourhood Streets can also double as bicycle boulevards.

Neighbourhood Streets provide comfortable and safe pedestrian and cyclist movement with sidewalks, plantings, street lighting and a low-traffic and low-speed roadway. A mature street tree canopy can add to the attractiveness of a street. Tree canopies provide shade, a sense of place, and can improve mental well-being. Neighbourhood Streets may be located in school zones where speed limits are 30 km/h.

These streets also offer the opportunity for social interactions and community activities. Yard sales, block parties, or children’s play should be encouraged along Neighbourhood Streets. As such, it is important to ensure that Neighbourhood Streets are slow streets that feel comfortable and safe for travellers.

CONTEXT	Urban/Suburban
STREET FUNCTION	Access
RIGHT OF WAY	15–20 m urban 20–26 m rural
NUMBER OF LANES	1–2
TARGET SPEED	30–40 km/h
CYCLING FACILITIES	Mixed traffic or contraflow lane
PEDESTRIAN CLEAR ZONE WIDTH	1.8 m

RIGHT OF WAY

The typical ROW for a Neighbourhood Street is 20 metres, though many existing ROWs may be as narrow as 10 to 15 metres, especially in the lower city. The recommended roadway width varies depending on the demand for on-street parking, and whether it is provided on one or both sides of the road. Roads with parking on one side typically have a 6 to 7 metre curb-to-curb width, whereas roads with parking on both sides have an 8 to 8.5 metre width. Some streets may be one-way only for motor vehicle traffic, however, contraflow cycling should be allowed where beneficial for network connectivity.

It is important to understand the local context with regards to parking demand so that the roadway may be properly sized. If there is low parking demand and the on-street parking lane is largely empty, it can result in a wide roadway with excessive speeding that may require future traffic calming interventions. In areas with high demand for on-street parking, curbside parking can create friction and encourage motor vehicle traffic to proceed slowly, thus acting as a traffic-calming measure.

GOALS

- Sidewalks on both sides by default. Consider other features such as green infrastructure, landscaping, and pedestrian scaled lighting to improve the pedestrian realm.
- Consider passive traffic calming measures to keep speeds low, making the roadways safe and comfortable for pedestrians and cyclists. This may include narrow vehicle travel lanes, on-street parking, mid-block crossings, and curb bulb-outs. Refer to the TAC Canadian Guide to Traffic Calming for further details on traffic calming measures.
- Make the roadway comfortable for cycling. Consider features like barriers to reduce through traffic for vehicles, also known as modal filters. Painted dedicated cycle tracks are preferred, however shared roadways may also be encouraged if space is limited. Bike boulevards can help prioritize movement for bicycles while also limiting vehicle speed and traffic.

EXAMPLE STREETS

- Many residential streets
- Bay Street (north of Cannon Street)
- Pearl Street South (south of Main Street)
- South Bend Road East (east of Upper Wellington Street)

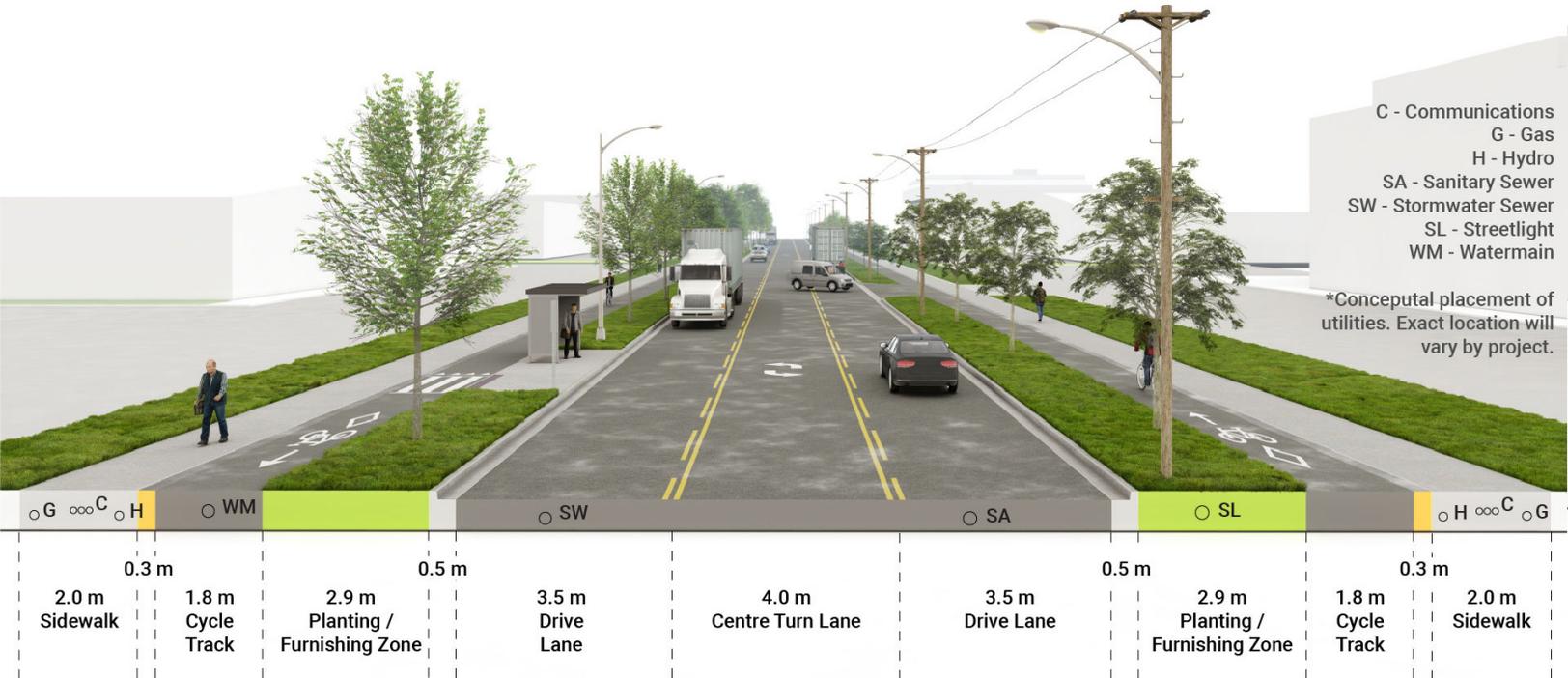


Brian Boulevard. Source: Google maps



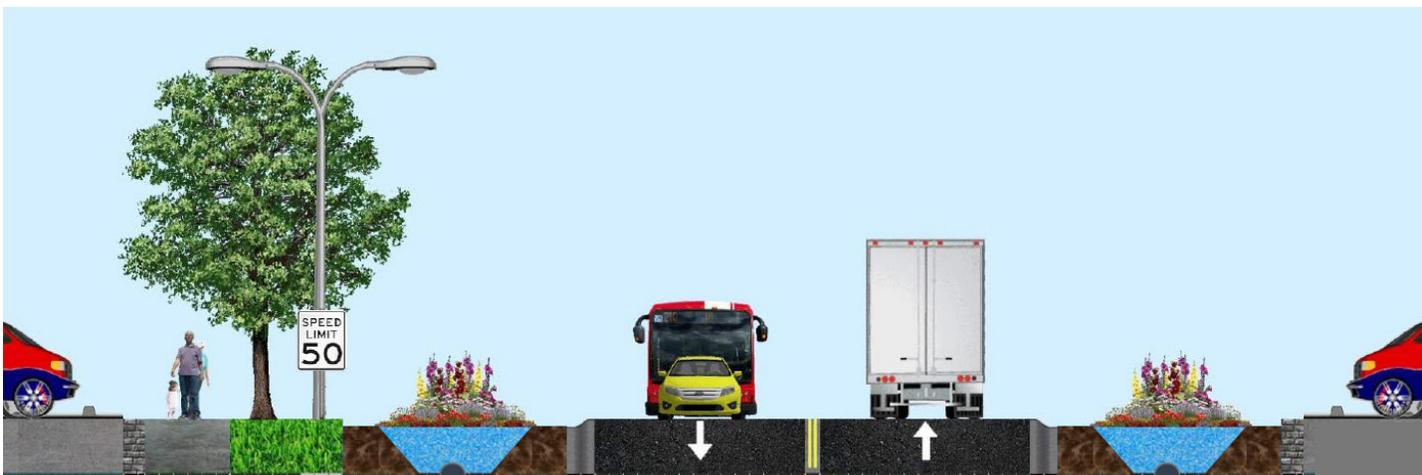
Pearl Street South. Source: Google Maps

4.7 INDUSTRIAL STREETS



Typical Industrial Street Cross Section (26m ROW).

Industrial Streets provide access to industrial and commercial employment areas. They are not through-trucking routes but are often the origin or destination of a truck trip and feature high volumes of heavy vehicles. Buildings and lots are often set back from the roadway with landscaping or parking lots separating the buildings from the street. There are many frequent and large driveways along these streets that provide access to destinations.



Emerging Industrial Road with LID features. Subject to further refinement and approvals as part of the AEGD Transportation Master Plan

Industrial Streets play a major role in supporting goods movement and employment activity. The destinations along these streets are also workplaces for significant numbers of people. As such, this typology aims to provide a range of mobility options for people travelling to work, while providing appropriate accommodation for large trucks.

Sidewalks, cycle tracks (or alternatively, multi-use paths) and transit shelters are provided on Industrial Streets. The choice of cycle tracks or multi-use paths is dependent on network connectivity considerations and the location of destinations along the corridor. The roadway consists of a three-lane cross-section with a centre turn lane to facilitate access to driveways on both sides of the street. Lane widths are appropriately sized for large trucks. On-street parking is not recommended since adjacent land uses generally provide ample surface parking.

The City is adopting standards that incorporate greater levels of LID features, such as bioswales, within the right-of-way in the Airport Employment Growth District (AEGD) and other employment areas. This can increase the right-of-way required.

CONTEXT	Industrial
STREET FUNCTION	Access
RIGHT OF WAY	26–30 m
NUMBER OF LANES	3
TARGET SPEED	40–50 km/h
CYCLING FACILITIES	Cycle tracks or multi-use path
PEDESTRIAN CLEAR ZONE WIDTH	2.0 m

RIGHT OF WAY

A 26 metre ROW is desirable for the base typology. This provides sufficient space for active transportation facilities, bus shelters, planting zones, and motor vehicle travel lanes and two-way centre turn lanes. As the City adopts LID standards in the AEGD and other employment areas, additional width may be required. This can increase the required right-of-way by up-to 10 metres for a local or collector road.

GOALS

- Appropriately sized travel lanes accommodate goods movement and transit vehicles. Transit vehicles generally operate in mixed traffic.
- Provide vehicular access to industrial and commercial driveways on both sides of the street through the use of two-way centre turn lanes.
- Provide attractive mobility options for workers. This may be through enhanced transit service, sheltered transit stops, setback sidewalks on both sides of the street, or dedicated cycling infrastructure. Cycle tracks, or multi-use paths may all be considered to improve safety and connectivity of the cycling network.
- Enhance pedestrian and cyclist comfort, especially between transit stops. Consider full protected bus shelters and benches, dedicated cycle tracks separate from traffic, street plantings, and pedestrian-oriented lighting. Where the distance between intersections is lengthy, consider mid-block pedestrian crossings, especially at transit stop locations.

EXAMPLE STREETS

- Nebo Road (north of Rymal Road)



Nebo Road at Lansing. Source: Google Maps



Nebo Road at Rymal Road. Source: Google Maps

4.8 RURAL ROADS



Typical Rural Road Cross Section (26m ROW)

Rural Roads are located outside Hamilton’s urban area, primarily in agricultural and natural areas. Their primary function is motor vehicle and goods movement. They may also serve slow-moving agricultural vehicles, people cycling for recreation or to travel to other communities, and in some cases may feature transit service.



Rural Road Cross Section—Multi-Use Path Option (26m ROW)

Paved shoulders should be appropriately sized, with consideration of motor vehicle speeds and volumes. On routes identified in the Cycling Master Plan, shoulders should be sized in accordance with OTM Book 18 guidance. These roads may be provided in a shared lane configuration (on lower speed and lower volume roads) or on a paved shoulder. Where speeds and volumes of motor vehicles are expected to be high, a multi-use path parallel to the roadway may be considered. Curbs are not provided on rural roads to allow for drainage through adjacent ditches. Where transit is provided, buses operate in mixed traffic. The edges of rural roads should also include naturalized drainage swales.

CONTEXT	Rural
STREET FUNCTION	Mobility
RIGHT OF WAY	26–36 m
NUMBER OF LANES	2
TARGET SPEED	60–80 km/h
CYCLING FACILITIES	Shared lanes, paved shoulder, or parallel multi-use path
PEDESTRIAN CLEAR ZONE WIDTH	n/a

RIGHT OF WAY

Within the Rural Official Plan, Rural Roads are classified as either Arterial or Collector Roads. Typical ROW widths range from 26 to 36 metres.

GOALS

- Prioritize the movement of people and goods.
- Ensure that road design is appropriate for vehicle speed and volume.
- Enhance environment with landscaping features such as buffer plantings and drainage swales on the edge of the roadway.
- Support active transportation and slow moving farm vehicles with wide shoulders.
- Accommodate cyclists with paved shoulders, advisory bike lanes and separated multi-use paths as appropriate.

EXAMPLE STREETS

- White Church Road East
- Fletcher Road



White Church Road. Source: Google Maps



Fletcher Road. Source: Google Maps

4.9 RURAL SETTLEMENT ROADS

- C - Communications
- G - Gas
- H - Hydro
- SA - Sanitary Sewer
- SW - Stormwater Sewer
- WM - Watermain

*Conceptual placement of utilities. Exact location will vary by project.



Typical Rural Settlement Area Cross Section (26m ROW)

Rural Settlement Roads are portions of Rural Roads that pass through settlement areas and rural areas of Hamilton, providing services for local residents as well as through-traffic. Rural Settlement Roads are often centred around an intersection or a section of highway and may include residential frontages, a small number of commercial or other uses that serve the community.

C - Communications
G - Gas
H - Hydro
SA - Sanitary Sewer
SW - Stormwater Sewer
WM - Watermain

*Conceptual placement of utilities. Exact location will vary by project.



Typical Rural Settlement Area Cross Section with a Multi-Use Path (26m ROW)

In contrast with the rest of a Rural Road, Rural Settlement Roads feature a slower speed context with a focus on property access. These roads will be designed to support the local community and calm traffic as they transition into a community setting through the use of gateway features, such as lane narrowing and signage reflecting lower speed limits. As they are associated with clusters of low density residential or commercial development, boulevards should include sidewalks, street trees, cycling facilities, on-street parking, and other amenities to support local residential and retail activity.

CONTEXT	Rural
STREET FUNCTION	Access
RIGHT OF WAY	20–26 m
NUMBER OF LANES	2
TARGET SPEED	40–50 km/h
CYCLING FACILITIES	Bike lanes, cycle tracks, or multi-use paths
PEDESTRIAN CLEAR ZONE WIDTH	1.8–2.4 m

RIGHT OF WAY

26 metres is the preferred ROW width to accommodate two vehicle lanes, on-street parking, bike lanes, and wide sidewalks. Depending on space, bike lanes may be implemented as a shared roadway configuration or dedicated cycle track. Note that this may be constrained, particularly in historic cores of these settlements.

GOALS

- Prioritize the pedestrian realm. Provide sidewalks on both sides of the road as well as mid-block and intersection pedestrian crossings. Landscaping, wide planting areas, wide sidewalks, pedestrian-scaled lighting, and other amenities can all contribute to the attractiveness of the public realm.
- Support local residential and retail activity. Ensure side streets provide connectivity to the main retail locations through the use of sidewalks and a cycling network.
- Design for slower speeds. As lanes transition from the highway to the village setting, narrow the roadway and introduce visual “friction” such as on-street parking

or planting zones. Other traffic calming measures like curb extensions and medians may also be considered to help slow traffic.

- Emphasize green infrastructure and landscaping. Planters or street trees can help beautify the public realm and provide shade for pedestrians and cyclists.
- Provide on-street parking where appropriate. It is important to consider the local demand and context for a particular street when assessing the amount of on-street parking to provide. On narrower right of ways, cycle tracks may be implemented in exchange for parking on streets with low parking demand.

EXAMPLE STREETS

- Old Highway 8, Rockton
- Binbrook Road (Southbrook Dr. to Royal Winter Drive)
- Jerseyville Road (in Jerseyville)



Old Highway 8.. Source: Google Maps



Jerseyville Rd W. Source: Google Maps

5

CHAPTER FIVE

INTERSECTIONS

5.1 STREET DESIGN FOR INTERSECTIONS

Intersections are crucial nodes in the transportation system where road users of different modes interact. Intersections are also major conflict points within the City's transportation system – in Hamilton, over 60% of all collisions and over 70% of collisions involving a pedestrian occur at an intersection.¹ This chapter outlines principles for intersection design to prioritize safety for all road users. Land use context and the intersecting road typologies will impact how intersections should be designed to promote predictable and safe movements through the intersection.

This chapter outlines five sample intersection designs to illustrate how intersection design principles can be applied at common intersection types. The sample intersections covered in this chapter represent common intersection typologies that practitioners may encounter when designing Complete Streets. The designs are not intended to cover every scenario or serve as definitive designs. Designers should apply the design principles based on local context. While the design samples shown are of intersections of two-way roads, the design principles and elements can also be applied for intersections of one-way roads.

Further guidance on intersection design can be found in:

- Ontario Traffic Manual (OTM) Book 12: Traffic Signals
- OTM Book 15: Pedestrian Crossing Treatments
- OTM Book 18: Cycling Facilities
- National Association of City Transportation Officials' (NACTO) Don't Give Up at the Intersection
- Transportation Association of Canada (TAC) Canadian Roundabout Design Guide
- National Cooperative Highway Research Program (NCHRP) Report 672
- Roundabouts: An Informational Guide
- TAC Geometric Design Guide for Canadian Roads, and
- Accessibility for Ontarians with Disabilities Act (AODA) Regulation 191/11: Integrated Accessibility Standards for Exterior Paths of Travel

¹ City of Hamilton: Annual Collision Report 2020: <https://www.hamilton.ca/sites/default/files/media/browser/2021-10-05/2020-annual-collision-report.pdf>

Sample Intersection Designs

INTERSECTION TYPE	STREET TYPOLOGIES
Low Speed Intersection	Neighbourhood Street & Neighbourhood Street
Stop Controlled Intersection	Neighbourhood Street & Connector
Urban Roundabout	Connector & Connector
Compact Urban Intersection	Urban Avenue & Main Street
Major High-Capacity Intersection	Transitioning Avenue & Transitioning Avenue

5.2 DESIGN PRINCIPLES FOR INTERSECTION DESIGN

Prioritize Safety. Safety for all road users should be the first priority when designing intersections. People walking and cycling are more vulnerable than people riding transit or driving. As such, vulnerable road user safety should be prioritized for all intersections. Interactions between conflicting movements, such as a turning vehicle crossing the path of a pedestrian, should occur at slow speeds. Good visibility, short crossing distances, and design that encourages predictable movements all support the goal of improving safety.

When retrofitting existing intersections, safety improvements can often be implemented using quick build measures such as flexible bollards, pavement treatments, or planter boxes. A recent study found that incorporating asphalt art at intersections had a strong positive correlation with improved safety and road user behaviour.² Design interventions that can improve safety outcomes should be key components of intersection redesigns.

Design for Accessibility. Intersections should include curb cuts, tactile walking surface indicators (TWSI), audible signals, and other accessibility features to ensure people with visual and mobility impairments can safely and comfortably navigate the intersection.

Minimize Delay: Traffic signals operations should be designed to minimize delay for all road users. Long cycle lengths which delay pedestrians or cyclists can result in non-compliance by those users, increasing the likelihood of unpredictable movements and reducing safety. Along transit corridors, minimizing transit delay should be a priority.

² Gates, Asphalt Art Safety Study: Historical Crash Analysis and Observational Behavior Assessment at Asphalt Art Sites, April 2022: <https://assets.bbhub.io/dotorg/sites/43/2022/04/Asphalt-Art-Safety-Study.pdf>

5.3 GEOMETRIC CONSIDERATIONS

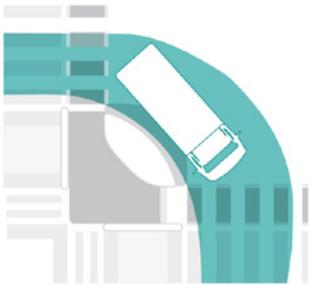
CORNER RADII

The size of corner radii has a relationship with the speed of vehicles at street intersections. The design of corner radii is determined based on design and control vehicles.

The design vehicle is the largest vehicle frequently turning at the intersection. Intersections should be designed to allow the design vehicle to turn with relative ease, typically starting from the curb lane and remaining to the right side of the centreline (or the right half of the roadway, where there is no marked centreline) on the receiving roadway.

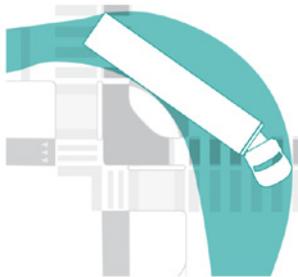
The control vehicle is the largest vehicle that infrequently turns at the intersection. Control vehicles are physically accommodated but may be required to take a wider turning path using adjacent lanes. In some cases, they may encroach into an opposing traffic lane. Control vehicles may be required to travel at a crawling speed of 5 km/h or less to negotiate the intersection.

DESIGN FOR THE DESIGN VEHICLE



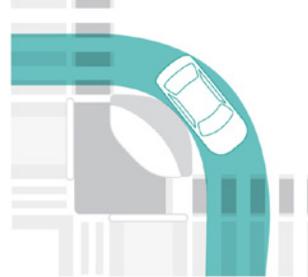
Largest frequent vehicle. Often a delivery truck (MSU), city bus (B-12), or passenger vehicle

ACCOMMODATE THE CONTROL VEHICLE



Largest infrequent vehicle. In urban areas, often a semi-trailer (WB-20). On neighbourhood streets, may be a garbage truck or fire truck

MANAGE THE SPEED OF PASSENGER VEHICLES

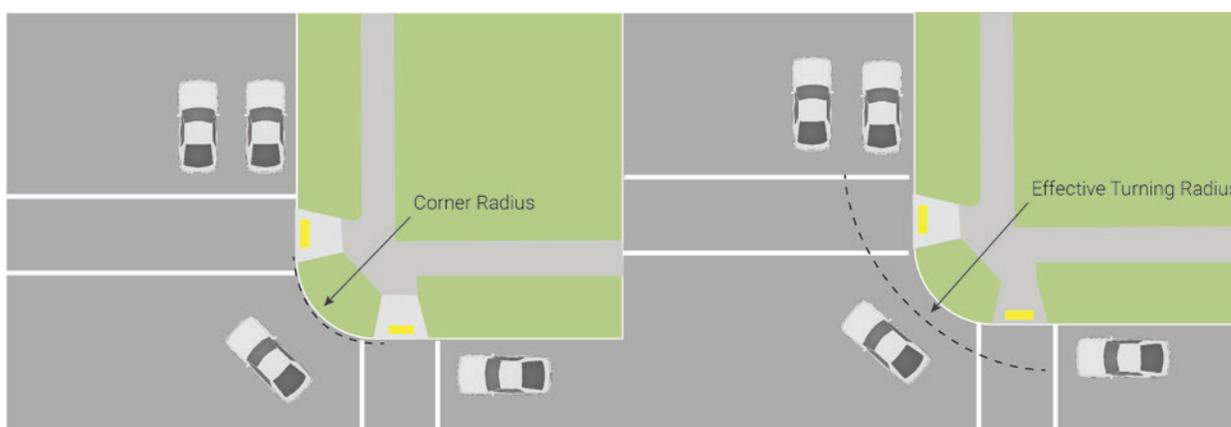


Passenger vehicles are typically the most common vehicle, and are a capable of higher, more dangerous speeds

Corners should accommodate turns made by design vehicles with relative ease. Control vehicles can be expected to encroach on adjacent lanes to complete their turns. (Adapted from NACTO, "Don't Give Up at the Intersection", 2019)

Smaller corner radii help lower vehicle turning speeds, thereby reducing the likelihood and severity of collisions with vulnerable road users at intersections. In areas of older development with small corner radii, the existing radii should generally be maintained, even if the radii do not accommodate the design/control vehicle, unless there is a history of operational concerns.

Corner radii should be selected based on an analysis of effective turning radii. The effective turning radius is based on the travel path typically used by a motor vehicle to navigate around a corner and is larger than the constructed corner radius. On-street parking, bicycle lanes and multiple receiving lanes can contribute to a larger effective turning radius which may allow the use of a substantially smaller physical corner radius.³



Corner Radii and Effective Turn Radii (Source: City of Edmonton Complete Streets Design and Construction Standards)

The following table provides a summary of design and control vehicles and the recommended effective turn radius by intersection type. The most restrictive condition should be applied. For example, at an intersection of a Connector Street and an Urban Avenue, the “Connector / Any Street” condition in the table should be applied. The design and control vehicles listed are defined in the TAC Geometric Design Guideline for Canadian Roads which includes typical dimensions for each vehicle:

- Passenger Cars (P)
- Light single-unit trucks (LSU)
- Medium single-unit trucks (MSU)
- WB-20 tractor / semi-trailers (WB-20)
- Standard transit buses (B-12)

³ Transportation Association of Canada. (2017). Chapter 6 – Pedestrian Integrated Design. Geometric Design Guidelines for Canadian Roads.

INTERSECTION CORNER TYPE (SELECT MOST RESTRICTIVE TYPE)	DESIGN VEHICLE	CONTROL VEHICLE ¹	EFFECTIVE TURN RADIUS TARGET RANGE
Neighbourhood Street	Passenger Vehicle	Waste collection vehicle	4.0 – 6.0 m
Connector	LSU or B-12 ²	B-12	6.0 – 8.0 m
Main Street	MSU or B-12 ²	B-12	6.0 – 8.0 m
Urban Avenue	MSU or B-12 ²	WB-20	6.0 – 10.0 m
Transitioning Avenue	MSU or WB-20 ³	WB-20	8.0 – 15.0 m ⁴
Industrial Street	MSU or WB-20 ³	WB-20	8.0 – 15.0 m ⁴
Rural Settlement Area Street	LSU	B-12	5.0 – 8.0 m
Rural Road	MSU or WB-20 ³	WB-20	6.0 – 15.0 m ⁴

1. City of Hamilton Fire Truck is an additional control vehicles for all cases.
2. Use B-12 design vehicle if regular scheduled HSR service turns at the corner.
3. Use WB-20 design vehicle if peak hour heavy truck turning volumes are 5 or greater.
4. Use a two-centred compound curve or a smart channel channelized right turn if the WB-20 cannot be accommodated with a 15.0 m radius

Practitioners can expect a transition period for motorists to adjust their behaviour when curb radii are reduced at an intersection. Signs of vehicles mounting the curb or encroaching in adjacent lanes to complete right turns in the first weeks following the adjustment should not be viewed as a sign of failure. Redesigned intersections should be monitored over a period of time to determine how the new corner radii affect driver speeds, turning paths, and overall behaviour.

5.4 LOW SPEED INTERSECTION



This intersection of two Neighbourhood Streets may be controlled by stop or yield signs. The intersection is designed to promote slow speed operations and to minimize pedestrian crossing distances.

DESIGN VEHICLE	Passenger Vehicle
CONTROL VEHICLE	Fire Truck and Waste Collection Vehicles



Low Speed Intersection ground view, approaching the intersection.

KEY FEATURES

- 1 Corner radii are designed to minimize motor vehicle turning speeds while allowing occasional larger vehicles such as emergency vehicles, waste collection, and snowplows to use the entire width of the roadway to negotiate turns. Small corner radii and narrow pavement widths at the intersection reduce crossing distances for pedestrians.
- 2 Traffic calming measures, such as the raised intersection shown in this example, are applied to reduce motor vehicle speeds. Other measures such as curb extensions or neighbourhood traffic circles may also be considered. Traffic diverters may also be considered at these intersections to prevent cut-through traffic. Raised intersections and crosswalks can also be constructed with asphalt rather than concrete.
- 3 No specific accommodation for cyclists is needed as they operate in mixed traffic on low-speed Neighbourhood Streets.



Neighbourhood Traffic Circle in Vancouver.

5.5 STOP CONTROLLED INTERSECTION



This intersection between Connectors and Neighbourhood Streets serves as a gateway to the lower speed context of Neighbourhood Streets. The intersection is stop controlled on the minor leg. The design of this intersection aims to clearly convey the priority of all modes on the Connector street which includes dedicated pedestrian and cycling facilities. When designing new streets, three-leg intersections may be appropriate in some situations to help reduce cut through traffic, and reduce conflict points between road users, relative to four-leg intersections.

DESIGN VEHICLE	Passenger Vehicle
CONTROL VEHICLE	MSU, Fire Truck, and Waste Collection Vehicles



Stop Controlled Intersection ground view, approaching the intersection.

KEY FEATURES

- 1** Raised pedestrian and cyclist crossing with the cycle track and sidewalk continued across the local street to clearly designate pedestrian and cyclist priority across the intersection. This design narrows the entry and exit of the local street and reduces barriers for people with mobility and visual impairments. The curb is depressed at the intersection to allow vehicles to enter and exit the Neighbourhood Street. The raised crossing slows vehicles navigating the intersection, increases the visibility of vulnerable road users, and increases the likelihood of yielding by motorists. Designers should consider requiring raised intersections for these typologies wherever feasible.
- 2** A pedestrian crossing across the Connector Street. A pedestrian refuge island serves to calm motor vehicle traffic and reduces pedestrian exposure to motor vehicle traffic while crossing the major road. The refuge island should include a cut-through or ramp that is the same width of the crosswalk to ensure accessibility. OTM Book 15: Pedestrian Crossing Treatments provides further guidance on pedestrian crossing design.
- 3** A dedicated left-turn lane for motorists turning onto the minor street removes left-turning vehicles from the stream of through traffic.

5.6 URBAN ROUNDABOUT



This example of a roundabout intersection between two Connector Streets draws on design guidance from OTM Book 18: Cycling Facilities, the TAC Canadian Roundabout Design Guide, and NCHRP Report 672, Roundabouts: An Informational Guide . Roundabouts typically require more right of way space than traditional intersections, as such property requirement needs should be an early consideration in the roundabout design process.

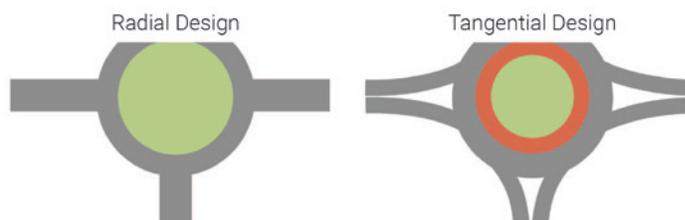
DESIGN VEHICLE	LSU (or B-12 on transit corridors)
CONTROL VEHICLE	B-12
INSCRIBED CIRCLE DIAMETER	32 - 40 m
ENTRY/EXIT DESIGN SPEED	30 km/h



Urban Roundabout ground view, approaching the intersection.

KEY FEATURES

- 1 Intersecting streets approach the circulatory roadway at a perpendicular angle. This approach, known as a radial design, requires vehicles to travel slowly as they enter and exit the roundabout, and in the vicinity of pedestrian and cycling crossings.



Comparison of Radial and Tangential Roundabout Designs (Source: OTM Book 18, 2021)

- 2 Single lane entries and exits reduce exposure to conflicts for pedestrians and cyclists crossing the legs of the roundabout.
- 3 Pedestrians and cyclists operate on the perimeter of the roundabout fully separated from motor vehicle traffic. Any on-road bike lanes on the intersecting streets should transition into the boulevard on the approach. Cycle tracks around the perimeter of the roundabout may operate as two-way facilities if it provides a more direct path of travel.
- 4 Uncontrolled crossings are provided for pedestrians and cyclists at each approach to the roundabout, approximately one or two car lengths (6–12 m) from the roundabout. Research from the Netherlands suggests better safety outcomes by assigning priority to motorists rather than cyclists at these crossings.⁴ Another alternative is to implement a pedestrian crossover (PXO) at the roundabout approaches. However, in this scenario, cyclists would have to dismount to cross the PXO.

⁴ SWOV Institute for Road Safety Research. "Rotondes met vrijgigende fietspaden ook veilig voor fietsers?", 2005. <https://www.swov.nl/sites/default/files/publicaties/rapport/r-2004-14.pdf>

5.7 COMPACT URBAN INTERSECTION



This intersection is an example of a signalized compact urban intersection. The design of this intersection aims to separate and protect vulnerable road users as much as possible to minimize conflicts with motor vehicles. The design shows an intersection of an Urban Avenue and a Main Street. For intersections of two Urban Avenues, the design of the Urban Avenue approaches can be replicated on all four legs of the intersection.

DESIGN VEHICLE	MSU (or B-12 on transit corridors)
CONTROL VEHICLE	B-12



Compact Urban Intersection ground view, approaching the intersection.

KEY FEATURES

- 1 Raised corner islands separate cyclists from motor vehicles and prevent motor vehicles from entering the cycling facility. The corner islands create a queuing space for cyclists and allow for a setback of the crossside. The cycle track setback is introduced as close to the intersection as possible to maximize pedestrian space at the corner.
- 2 Small corner radii promote slow turning movements.
- 3 Left turn stop bar is set back to provide additional maneuvering space for buses and trucks.
- 4 Cyclists transition from shared on-street space to a separated cycle track on the approach to the intersection.
- 5 Bus stops are designed as Shared Cycle Track Platform Stops (see Chapter 3, [Section 3.3.2](#)) due to space constraints.
- 6 Crossing distances are minimized to support shorter traffic signal cycle lengths.



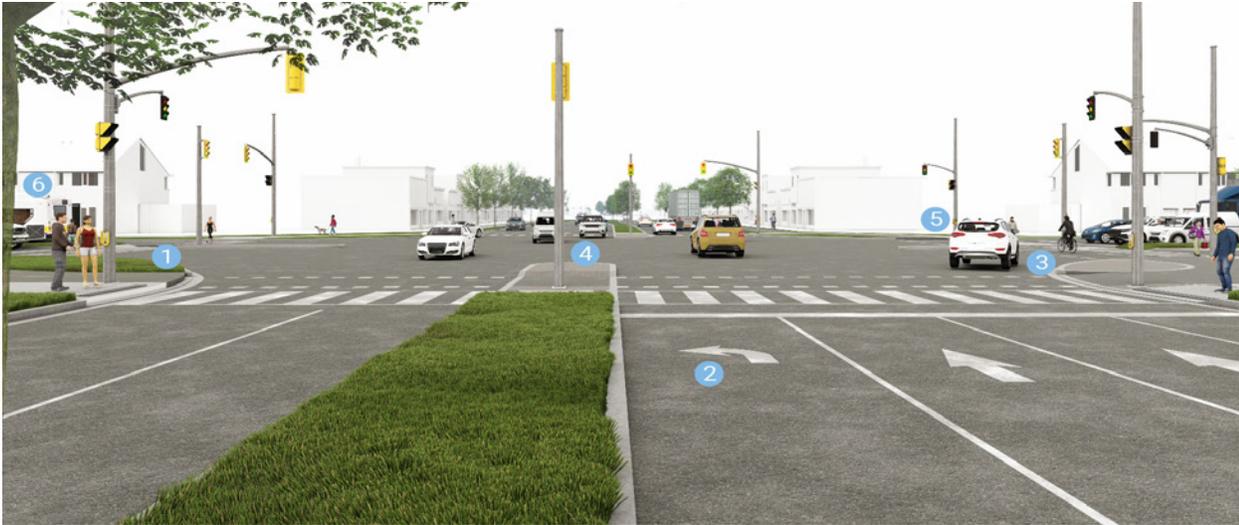
Compact urban intersection in Vancouver, BC

5.8 MAJOR HIGH CAPACITY INTERSECTION



This intersection is an example of two high-capacity Transitioning Avenues intersecting. Due to the high volumes of turning vehicles expected at these intersections, the intersection is designed with the intention that protected signal phasing will be used to minimize conflicts between turning vehicles and pedestrians or cyclists.

DESIGN VEHICLE	MSU or WB-20
CONTROL VEHICLE	WB-20



Major High Capacity Intersection ground view, approaching the intersection.

KEY FEATURES

- 1 Corner radii are larger than at other intersection types to allow for efficient movement of right turning vehicles. Protected signal phasing is recommended to separate high-volume right-turning movements from pedestrian and cyclist movements. For lower-volume movements, leading pedestrian intervals (LPI) and leading bicycle intervals (LBI) should be applied to give pedestrians and cyclists a head start over motor vehicles.
- 2 The corridor widens at the intersection to provide dedicated left and right turn lanes, which increases capacity and provides flexibility to operate turning movements on protected signal phases.
- 3 Corners are designed as protected intersection corners with small islands that physically separate active transportation facilities (MUPs, cycle tracks, and/or sidewalks) from turning motor vehicles.
- 4 The centre medians are extended into the intersection to provide refuge islands for pedestrians. The median refuges also allow flexibility for two-phase pedestrian crossings, which should be employed strategically to minimize overall pedestrian crossing delay. The median extensions also serve as a form of left turn calming to prevent motorists from making sweeping high-speed left turn movements.
- 5 A small sidewalk adjacent to the MUP mixing zone serves as a refuge and waiting space for pedestrians.
- 6 Bus stops with full amenities are provided along transit corridors.



Large protected intersection in Ottawa (Source: Google Earth)



A

APPENDIX

APPENDIX A

GLOSSARY OF TERMS, ABBREVIATIONS, AND ACRONYMS

ABBREVIATION	TERM	DEFINITION
AADT	Annual Average Daily Traffic	The average 24 hour, two way traffic on a roadway for the period from January 1st to December 31st within a single calendar year.
AODA	Accessibility for Ontarians with Disabilities Act	Provincial legislation and associated regulations that set targets and provide for the development of standards for making the Province accessible to all Ontarians by 2025.
APS	Accessible pedestrian signals	Auxiliary devices that supplement traffic control signals to aid pedestrians with vision losses (and those with both visual and hearing impairments) in their road crossing. Information is communicated in non-visual format such as audible tones, verbal messages, and/or vibrotactile indications to provide cues at both ends of a crossing when activated.
ATS	Accessible Transportation Services	Intended for people with physical or functional disabilities or health conditions who are unable to access fixed-route public transit. Eligibility is considered on a case-by-case basis and is not based on a particular disability, or income level.
B-12	Standard Single-Unit Buses	Typical bus size on Hamilton streets

ABBREVIATION	TERM	DEFINITION
BIA	Business Improvement Area	An association of commercial property owners and tenants within a defined area who work in partnership with the City to create thriving, competitive, and safe business areas that attract shoppers, diners, tourists, and new businesses.
BLAST Network	HSR's Planned Rapid Transit Network	A planned frequent rapid transit system in Hamilton, with a plan for five routes including one light rail transit and four bus rapid transit lines.
BRT	Bus Rapid Transit	A high-quality bus-based transit system that delivers fast and efficient service that may include dedicated lanes, busways, traffic signal priority, off-board fare collection, elevated platforms and enhanced stations.
CIPA	Community Improvement Plan Area	A Community Improvement Plans or CIP is a tool that allows a municipality to direct funds and implement policy initiatives toward a specifically defined project area (CIPA).
CMP	Cycling Master Plan	Intended to guide the development and operation of its cycling infrastructure for the next twenty years.
EA	Environmental Assessment	The environmental assessments process ensures that governments and public bodies consider potential environmental effects before an infrastructure project begins.
EV	Electric Vehicle	Vehicles that are either partially or fully powered on electric power.

ABBREVIATION	TERM	DEFINITION
HCA	Hamilton Conservation Authority	The Hamilton area's largest environmental management agency dedicated to the conservation and enjoyment of watershed lands and water resources.
HCD	Heritage Conservation District	A defined geographical area within a municipality that is protected under a local bylaw to ensure conservation of its existing heritage character.
HOT	Higher Order Transit	Transit that operates in whole or in part in a dedicated right of way, including heavy rail, light rail and buses.
HOV	High-Occupancy Vehicle	A motor vehicle carrying more than a specified minimum number of people and therefore permitted to use a traffic lane reserved for such vehicles.
HPUCC	Hamilton Public Utilities Coordination Committee	A group that coordinates utilities work between the City and private utility companies.
HSR	Hamilton Street Railway	Hamilton's public transport agency.
ITE	Institute of Transportation Engineers	An international educational and scientific association of transportation professionals who are responsible for meeting mobility and safety needs
LBI	Leading Bicycle Interval	Gives people on bikes a head start in front of turning vehicles, providing a priority position in the right of way.

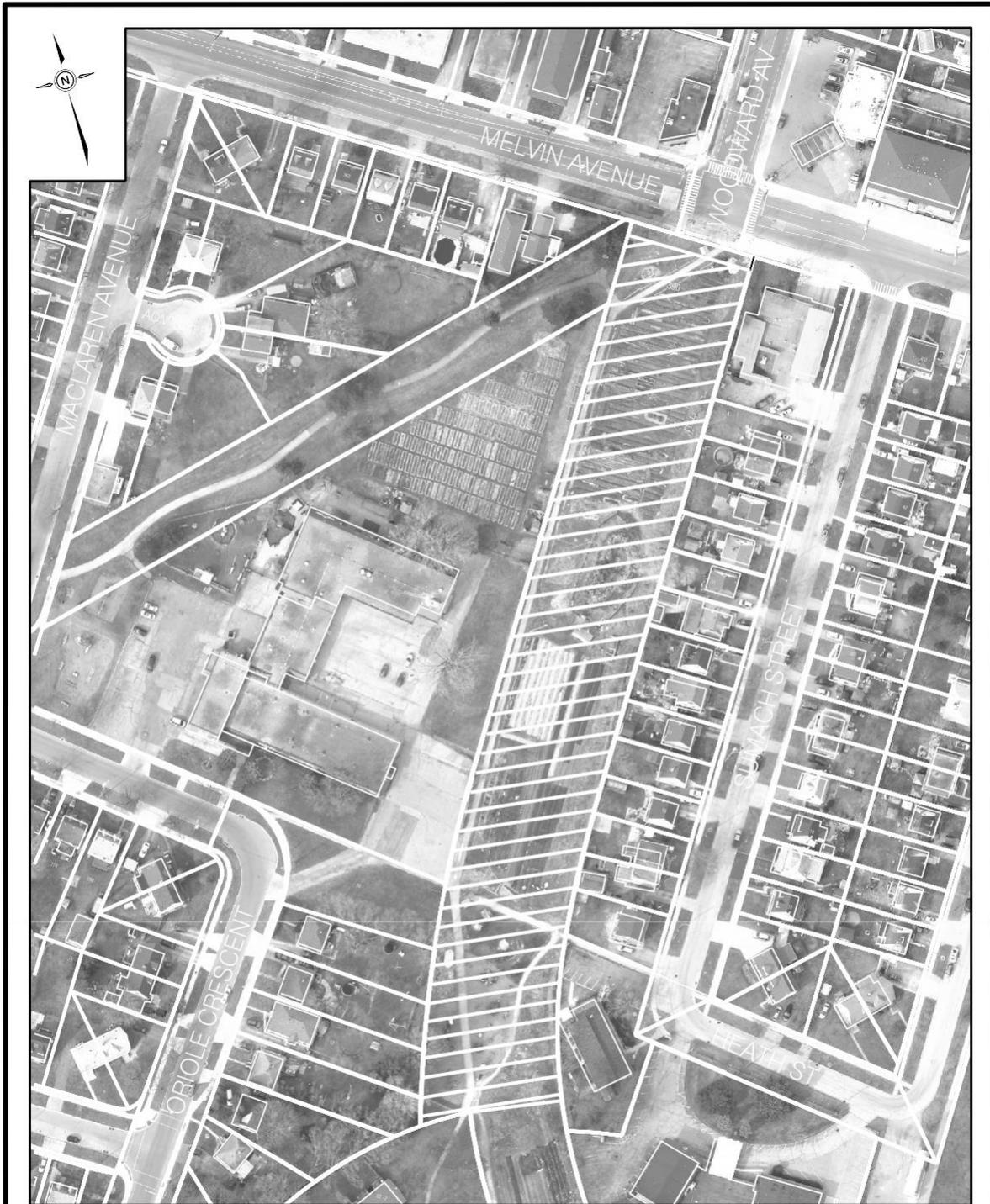
ABBREVIATION	TERM	DEFINITION
LID	Low Impact Development	An innovative approach to land development that mimics the natural movement of water in order to manage stormwater (rainwater and urban runoff) close to where the rain falls.
LOS	Level of Service	A qualitative measure of traffic flow at an intersection dependent upon vehicle delay and vehicle queue lengths at the approaches. Specifically, Level of Service criteria are stated in terms of the average stopped delay per vehicle for a 15-minute analysis period.
LPI	Leading Pedestrian Interval	A form of an exclusive pedestrian phase where a walk indication (generally around 4 to 6 seconds in duration) is provided in advance of the corresponding vehicle green indications to give pedestrians a head start on parallel or turning traffic.
LSU	Light Single-Unit Trucks	Vehicle configurations designed to transport property, where the cargo carrying capability of the vehicle is integral to the body of the vehicle. LSU's typically weigh 14,000 lbs and under.
MIWC	Music Industry Working Committee	A committee designed to advise the city on the goals and priorities of the music industry, as well as on marketing strategies.
MMLOS	Multi-Modal Level of Service	Similar to LOS but also applicable to transit, bicycle, and pedestrian levels of service.

ABBREVIATION	TERM	DEFINITION
MSU	Medium Single-Unit Trucks	Vehicle configurations designed to transport property, where the cargo carrying capability of the vehicle is integral to the body of the vehicle. LSU's typically weigh between 14,000 and 26,000 lbs
MUP	Multi-Use Path	A shared pedestrian and cycling facility that is physically separated from motor vehicle traffic by a hard-surfaced splash pad or by a grass strip. It is often referred to as part of a boulevard within the roadway or highway right-of-way.
NACTO	National Association of City Transportation Officials	An association of 89 major North American cities and transit agencies formed to exchange transportation ideas, insights, and practices and cooperatively approach national transportation issues.
NCHRP	National Cooperative Highway Research Program	Conducts research in problem areas that affect highway planning, design, construction, operation, and maintenance in the United States.
OP	Official Plan	An official plan describes an upper, lower or single tier municipal council or planning board's policies on how land in a community should be used.
OTM	Ontario Traffic Manual	Publications providing information and guidance to transportation practitioners and to promote the uniformity of treatment in the design, application and operation of traffic control devices and systems across Ontario.

ABBREVIATION	TERM	DEFINITION
OTM Book 12	Ontario Traffic Manual: Book 12, Traffic Signals	Provides some elementary instructions to beginners and a reference for experienced persons for the design and operation of traffic signals.
OTM Book 15	Ontario Traffic Manual: Book 15, Pedestrian Crossing Treatments	Provides guidelines for justification, treatment system selection and treatment system design for new pedestrian crossovers on low-speed and low-volume roads.
OTM Book 18	Ontario Traffic Manual: Book 18, Cycling Facilities	Provides practical guidance on the planning, design and operation of cycling facilities in Ontario.
P	Passenger Cars	A road motor vehicle, other than a motor cycle, intended for the carriage of passengers and designed to seat no more than nine persons (including the driver).
PXO	Pedestrian Crossover	Any portion of a roadway distinctly indicated for pedestrian crossing by signs on the highway and lines or other markings on the surface of the roadway as prescribed by the regulations and the Highway Traffic Act.
ROW	Right of Way	Allocation of right of movement to a road user, in preference over other road users; The width of the road allowance from the property line on one side to the property line on the opposite side of the roadway is also known as right-of-way.

ABBREVIATION	TERM	DEFINITION
TAC	The Transportation Association of Canada	A not-for-profit, national technical association that focuses on road and highway infrastructure and urban transportation. While TAC does not set standards, it is a principle source of guidelines for planning, design, construction, management, operation, and maintenance of road, highway, and urban transportation infrastructure systems and services.
TMP	Transportation Master Plan	A comprehensive strategic planning document that defines policies, programs and infrastructure improvements required to address transportation and growth needs.
TSP	Transit Signal Priority	Transit Signal Priority (TSP) tools modify traffic signal timing or phasing when transit vehicles are present either conditionally for late runs or unconditionally for all arriving transit.

ABBREVIATION	TERM	DEFINITION
TWSI	Tactile Walking Surface Indicators	<p>A colour contrasting and tactile surface treatment that is used for one of two purposes:</p> <ol style="list-style-type: none"> 1. Tactile Attention Indicator (TAI): A TWSI comprising truncated domes that alert people to the presence of a hazard or a decision making point, such as a street crossing, impending change in elevation, or conflicts with other transportation modes. 2. Tactile Direction Indicator (TDI): A TWSI that uses elongated, flat-topped bars to facilitate wayfinding in open areas, including guiding pedestrians with vision loss or other disabilities to crosswalks or transit stops. The elongated bars indicate the travel direction. <p>In this manual, unless otherwise specified, the term "TWSI" is used to refer to an attention indicator.</p>
WB-20	WB-20 Tractor Semitrailers	Large tractor semi-trailer truck



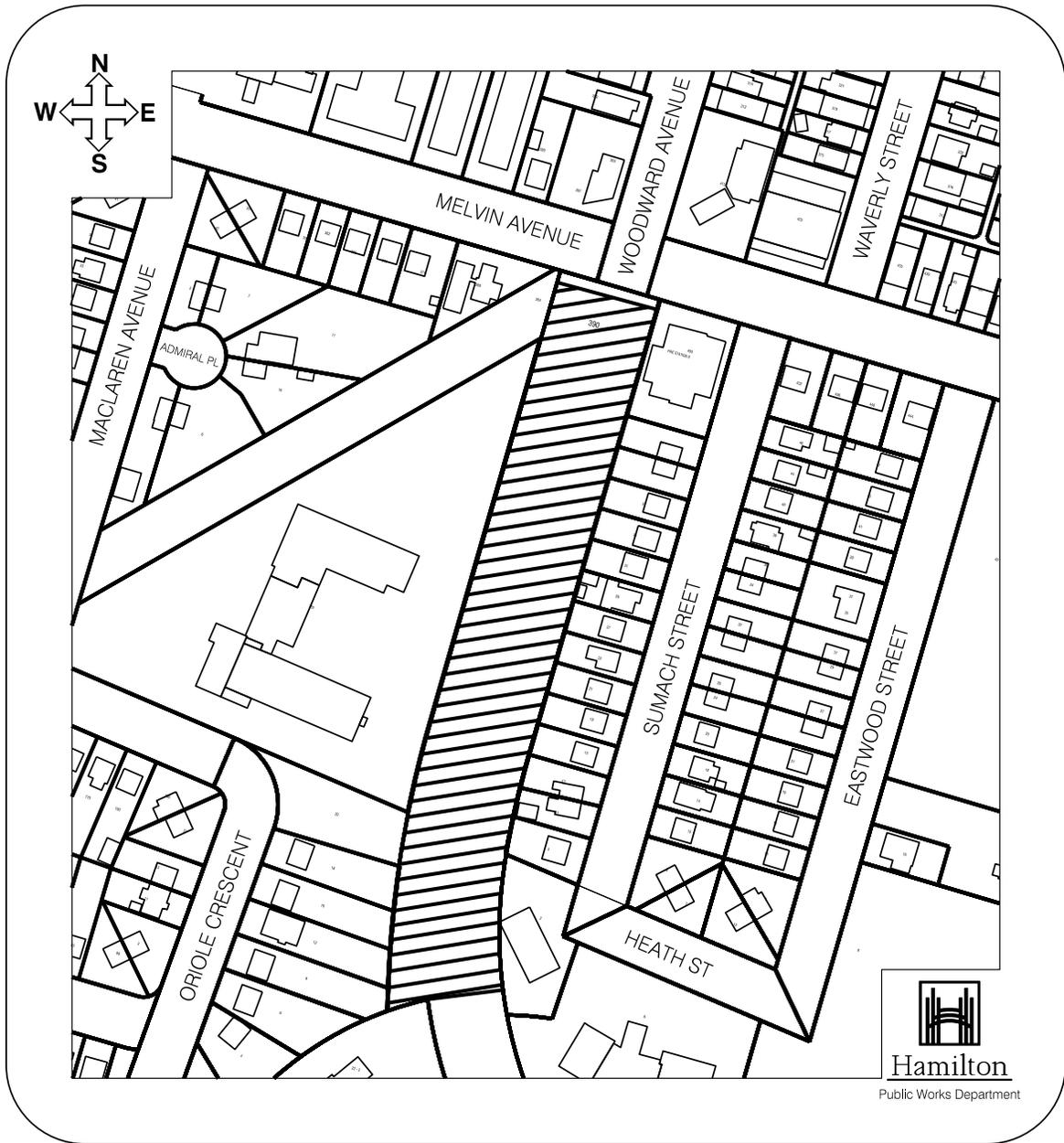
PROPOSED CLOSURE OF PORTION OF
390 MELVIN AVENUE, HAMILTON

Geomatics & Corridor Management Section
Public Works Department

LEGEND

 **Lands to be Closed**

NTS | 01/03/2022 | Sketch By: CF



LOCATION PLAN

PROPOSED CLOSURE OF
PORTION OF
**390 MELVIN AVENUE,
HAMILTON**

CITY OF HAMILTON
PUBLIC WORKS DEPARTMENT

LEGEND



SUBJECT LANDS

DATE: March 1, 2022

Not to Scale | Sketch By: CF

REFERENCE FILE NO : PW22_

Parts, Supplies, Equipment and Services for the City of Hamilton Environmental Laboratory

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts/ Consumables	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts/ Consumables	Est. Annual Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
AirOn HVAC Services Ltd.	x	x		Provide all Labour, Equipment and Materials necessary to service and maintain the Laboratory's only Makeup Air Unit (MAU).	The Makeup Air Unit (Engineered Air) is the sole unit that provides exhaust for all laboratory chemical hoods. AirOn has been providing this service and now has an intimate history of the needs and complexity, as well as AirOn technicians are specially trained to maintain and service this MUA unit, until a replacement unit can be provided under separate contract. In addition AirOn is the Sole OEM authorized service provider for Honeywell Niagara (WAS Building) and Bistech (TSB Building) products.		\$ 115,000.00	\$250,876.73	\$75,629	\$ 175,519.15	\$ 74,650.00	\$ 58,612.00
Eurofins Abraxis Inc	x		x	Supplies, Certified Reference Materials and Laboratory Performance Testing samples for microcystins ELISA analysis.		Sole worldwide manufacturer and North American distributor of Supplies for microcystins ELISA analysis including Certified Reference Materials and Laboratory Performance Testing samples.	\$ 11,000.00	\$ 9,967.47	\$ 9,552.00	\$ 8,892.72	\$ 6,675.00	\$ 9,300.00
Agilent Technologies Canada, Inc.	x	x	x	Instruments, parts, supplies and service for trace metal (Agilent 5900 ICPOES and Agilent 7900 ICPMS) and trace organic analysis (Agilent GC/MSD System, Tekmar Purge and Trap auto sampler with Agilent GC/MSD System, Agilent GC/MS/MS System, SPME PAL Autosampler with GC/MSD System).	Sole OEM - Preventive Maintenance, Repair, Service Contract for existing equipment utilized by the Environmental Laboratory for the analysis of drinking water, wastewater and biosolids	Sole OEM - spare parts, brokerage fees, supplies for existing equipment utilized by the Environmental Laboratory for the analysis of drinking water, wastewater and biosolids	\$ 110,000.00	\$ 105,158.82	\$ 55,472.32	\$ 63,855.20	\$ 64,948.96	\$ 66,610.49
The Canadian Association for Laboratory Accreditation Inc. (CALA)		x			On site ISO 17025 Laboratory Assessments utilized by the Environmental Laboratory to maintain ISO 17025 Accreditation. Accreditation is mandatory in order for the City of Hamilton Environmental Lab to be able to continue to analyze drinking water. The Lab has been accredited by CALA since 1996. There are 2 accreditation bodies in Canada, however, CALA specializes in Laboratory accreditation. CALA assessors have current hands on lab experience enabling them to bring unsurpassed technical expertise to each on-site audit.		\$ 24,000.00	\$ 11,559.90	\$ 12,346.58	\$ 24,265.70	\$ 27,550.00	\$ 22,400.00
IDEXX Distribution, Inc./IDEXX Laboratories Canada Corp./IDEXX Laboratories, Inc.	x		x	Parts and supplies for Microbiological analysis of regulated drinking waters, wastewaters and biosolids.		Sole Provider of supplies, and spare parts for existing equipment utilized by the Environmental Laboratory for the analysis of total coliforms and E.coli in drinking water and wastewater.	\$ 36,000.00	\$ 33,263.14	\$33,420.61	\$ 7,513.26	\$ 16,805.00	NA

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts/ Consumables	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts/ Consumables	Est. Annual Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Magnetar Corporation	x	x		Maintenance and Upgrades to SIRIUS Laboratory Information Management System (LIMS) database.	Sole provider of Maintenance and Upgrades to SIRIUS Laboratory Information Management System (LIMS) database.		\$ 45,000.00	\$ 40,402.10	\$ 39,246.67	\$ 25,432.48	\$ 22,245.76	\$ 21,601.37
Millipore (Canada Ltd.)/ EMD Millipore Corporation/Millipore Canada Ltd.	x		x	Parts, supplies for Laboratory water purification systems for trace analysis.		OEM - Consumables for existing water treatment equipment utilized by the Environmental Laboratory. The PM agreement gives the laboratory a discount off the list price of Millipore-EMD OEM consumables if purchased directly from Millipore-EMD. This is a significant cost savings provided to the City of Hamilton. Millipore will guarantee that the waters produced by their systems will meet the stated specifications for the equipment only if OEM Millipore brand parts are used. The lab has specific quality guidelines for the RO and LGW and must ensure that the equipment is capable of producing this quality, therefore it is essential to use OEM parts.	\$ 25,000.00	\$ 21,960.00	\$ 14,147.42	\$ 16,488.92	\$ 17,671.00	\$ 14,692.36
PTC Proficiency Testing Canada (Proficiency Testing Canada Inc.)			x			Provider of specialized standards utilized by the Environmental Laboratory to maintain ISO 17025 Accreditation. Double-blind Proficiency Testing (PT) water & wastewater samples are required to maintain accreditation for environmental laboratories involved in the analysis of drinking water. It is recommended that these PT study samples continue to be purchased from PTC because they are the sole provider who meets all of the requirements -are an accredited PT Provider, on the accreditation body's list of approved vendors, able to provide whole volume samples including microbiology, can provide all required standards as 4 whole volume samples twice a year, able to provide 4 samples per analyte at different concentration ranges from low to high, guarantee a minimum of 20 participants per study, evaluation of data for bias, statistical analysis of data, takes into account the method detection limits, compares study data to historical study data to ensure standard deviations are fit for purpose.	\$ 13,000.00	\$ 11,367.80	\$10,600.00	NA	NA	NA

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts/ Consumables	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts/ Consumables	Est. Annual Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
SGS Canada Inc.		x			Laboratory Analytical Services accredited to ISO 17025 and MECP Licensed for regulatory drinking water analysis for specific tests listed in their scope of analysis. The last time this service was tendered, there were no successful applicants. The tender was re-issued with only one successful applicant who passed the mandatory requirements. The tender was re-issued a third time and there were 2 more applicants that passed the pre-qualification. However, SGS Canada Inc. was the only applicant who passed the performance sample testing requirement. The process took 4 years. This vendor is being used to analyse regulatory drinking water and must be competent to do the analysis.		\$ 145,000.00	\$ 222,136.77	\$ 123,171.35	\$ 34,059.27	\$ 38,738.10	\$ 109,074.00
Skalar Inc.	x	x	x	Instruments parts, supplies and service for Skalar Automated Chemistry Systems.	Sole authorized service provider - Preventive Maintenance, Repair, Service Contract for existing equipment utilized by the Environmental Laboratory for the analysis of drinking water, wastewater and biosolids	Sole distributor- spare parts, brokerage fees, supplies for existing equipment utilized by the Environmental Laboratory for the analysis of drinking water, wastewater and biosolids.	\$ 30,000.00	\$ 29,837.20	\$ 19,334.95	\$ 13,377.95	\$ 22,071.50	\$ 17,794.00
Systems Plus (1936100 Ontario Inc.)	x		x	Provider of pre-cleaned, pre-labelled Laboratory Bottles for all analytical analysis including regulatory drinking water.		Provider of pre-cleaned, pre-labelled Laboratory Bottles for all analytical analysis including regulatory drinking water. Sole bottle supplier who can offer us the wide range of ready to use suitable sample bottles required by CHEL that meet all of the requirements	\$ 50,000.00	\$ 47,574.84	\$ 35,568.82	\$ 34,607.66	\$ 31,150.74	\$ 44,404.30
VWR (A Part of Avantor) / VWR International Co./Avantor Delivered by VWR			x	Provider of specialized standards utilized by the Environmental Laboratory to maintain ISO 17025 Accreditation.		The City of Hamilton Environmental Lab requires the delivery of a set of specialized QC reference standards each month which are used to provide assurance of the quality of data produced by the Lab. It is recommended that these be purchased from ERA through VWR (A Part of Avantor). ERA is able to provide the complete set of standards required and VWR is the only Canadian distributor for ERA QC reference Standards.	\$ 30,000.00	\$ 25,085.93	\$ 25,895.00	\$ 21,000.00	\$ 20,616.00	\$ 23,342.00
Environmental Resource Associates (ERA)	x											
Linde Canada Inc	x		x	Provider of Specialty Gases for Laboratory Analysis using Analytical Instrumentation		There are currently only 3 suppliers of specialty gases in the market, however, only Linde is able to provide ultrapure helium at this time. Currently, the bulk argon tank located inside the laboratory building belongs to Linde.	\$ 30,000.00	\$ 28,979.00				
TOTALS							\$ 664,000.00	\$ 838,169.70	\$ 454,384.72	\$ 425,012.31	\$ 343,122.06	\$ 387,830.52

Parts, Supplies, Equipment and Services for Plant Operations and Plant Maintenance and Technical Services												
Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
3M	x			OEM for DBI-Sala Davit Guard System and associated parts.			\$ 24,000.00		\$ 6,000.00	\$ -	\$ -	\$ -
ABB Inc.	x	x	x	OEM for analyzers and transmitters, as well as GE Industrial Solutions electrical control equipment, switchgear, soft starters, variable frequency drives, power distribution equipment / Centrifuge power and control equipment.	OEM, sole authorized service provider for ABB and GE Industrial Solutions products.	OEM, sole distributor for ABB and GE Industrial Solutions products.	\$ 15,000.00		\$ -	\$ 2,280.00	\$ 13,768.00	\$ -
ACI Instrumentation Ltd.			x			ACI Instruments LTD is the exclusive Ontario distributor for Pulsar, Greyline, Hydroflow and Krohne products.	\$ 35,000.00	\$ 13,405.20	\$ 21,000.00	\$ 22,589.00	\$ 79,319.00	\$ 8,679.88
Active Scale Manufacturing Inc.	x	x	x	OEM for the truck weigh scale system used at the Woodward WWTP.	OEM, sole authorized service provider for Active Scale products.	OEM, sole distributor Active Scale products.	\$ 2,500.00	\$ 8,285.79	\$ 3,872.00	\$ 850.00	\$ -	\$ 1,564.00
Additel	x			OEM for process measurement and calibration monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Advantech	x			OEM for Advantech equipment			\$ -		\$ -	\$ -	\$ -	\$ -
Aerzen Canada Inc.	x	x	x	OEM for Aerzen blowers and associated parts.	OEM, sole authorized service provider for Aerzen Canada products.	OEM, sole distributor for Aerzen Canada products.	\$ 4,000.00	\$ 1,437.81	\$ -	\$ 1,788.90	\$ 1,040.60	\$ 11,437.31
Alfa Laval Canada Inc.	x	x	x	OEM for centrifuges, gravity thickener belts (formerly Ashbrooks) and associated parts.	OEM, sole authorized service provider for Alfa Laval Canada Inc. and Ashbrooks products.	OEM, sole distributor for Alfa Laval Canada Inc. and Ashbrooks products.	\$ 75,000.00	\$ 159,225.34	\$ 10,035.00	\$ 26,114.02	\$ 315,342.13	\$ 59,451.79
Aqua Technical Sales Inc.			x			Sole authorized distributor for Smith & Loveless line of sewage pumping, headworks, screening, treatment equipment, Shade Aides and associated parts for the above.	\$ -		\$ -	\$ -	\$ -	\$ -
Anderson Electronics		x	x		Anderson Electronics is the preferred service provider to the City of Hamilton for all battery bank systems and chargers, including high voltage switch gear, UPS's for the RTC gates, and Protection control for all of the electrical. They also provide ongoing data trending, battery condition history and perform any required repairs. As they have all the history in their proprietary data base, an alternate provider would not have the history and trending data to make the appropriate repair recommendations.	Anderson Electronics is the preferred parts provider to the City of Hamilton for all battery bank systems and chargers, including high voltage switch gear, UPS's for the RTC gates, and Protection control for all of the electrical. They also provide ongoing data trending, battery condition history and perform any required repairs. As they have all the history in their proprietary data base, an alternate provider would not have the history and trending data to make the appropriate parts recommendations.	\$ 50,000.00	\$ 8,679.28				
Armstrong Fluid Technology	x			OEM for pumps, hot water recirculation systems and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Asco	x			OEM for regulatory flow control and pressure control equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Asea Brown Boveri (ABB)	x			OEM for regulatory water quality monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
ASI Group		x			Provision of underwater services for inspection, repair, cleanout, or installation of equipment. They are a specialized local and readily available firm to respond to emergency situations, they have extensive experience performing diving work with the City which mitigates safety risks and have a vast understanding of the City's water infrastructure.		\$ 10,000.00		\$ -	\$ 8,151.00	\$ 25,111.00	\$ 7,113.00
ATI	x			OEM for gas monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Auma Actuators Inc	x			OEM for actuators.			\$ -		\$ -	\$ -	\$ -	\$ -
Avensys Solutions Inc.		x	x		OEM authorized service provider for Teledyne ISCO and QCEC automatic water/wastewater sampling and flow monitoring equipment for Canada.	sole distributor for Fluid Components International LLC products for the Province of Ontario and sole distributor for Teledyne ISCO and QCEC automatic water/wastewater sampling and flow monitoring equipment for Canada.	\$ 50,000.00	\$ 34,782.01	\$ 70,571.00	\$ 34,219.00	\$ 48,481.45	\$ 28,872.00
AW Chesterton Co Ltd	x	x	x	OEM for Mechanical Seal Pumps and Valve Packing, Engineered Polymer Hydraulic/Pneumatic Seals, Sheet Gaskets, Metal and Concrete Composite Coating Systems, MRO and Production Cleaners and Degreasers, Lubricants and Metal Working Fluids.	OEM, sole authorized service provider for AW Chesterton Co. Ltd products and Inpro/Seals® Custom Engineered Bearing Protection and Process Seals.	Exclusive distributor for Inpro/Seals® Custom Engineered Bearing Protection and Process Seals.	\$ 4,500.00		\$ 1,542.00	\$ 3,666.60	\$ 10,560.00	\$ 1,027.00
Aysix Analytical Equipment	x			OEM for process monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Baker Hughes Energy Services Canada (Formerly Bentley Nevada And General Electric Canada)	x	x	x	OEM for pump monitoring equipment.	OEM, sole authorized service provider for Bentley Nevada products and services, including but not limited to vibration monitoring equipment and associated parts.	OEM, sole distributor for Bentley Nevada products, including but not limited to vibration monitoring equipment and associated parts.	\$ 5,000.00	\$ 13,891.53	\$ -	\$ -	\$ 6,442.50	\$ 7,272.50
Benshaw	x	x	x	OEM for soft starters, variable frequency drives and switchgear equipment.	OEM, sole authorized service provider for Benshaw products including soft starters variable frequency drives and switchgear products and services.	OEM, sole distributor for Benshaw products including soft starters variable frequency drives and switchgear products and services.	\$ 4,000.00		\$ -	\$ -	\$ 8,028.00	\$ 3,320.00
Blue-White	x			OEM for chemical metering and dosing equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Brentwood Industries	x			OEM for clarifier tank equipment and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
C & M Environmental Technologies Inc.		x	x		Sole authorized service provider for Brentwood, WesTech Engineering LLC and SPIRAC products.	Sole authorized distributor for Brentwood, WesTech Engineering LLC, and SPIRAC products. Sole authorized sales representative for WesTech Engineering sludge mixers and associated parts (eg. draft tubes, motors, etc.) in Ontario.	\$ 150,000.00		\$ -	\$ -	\$ 21,376.88	\$ 86,831.29
Can-Am Instruments Ltd.		x	x		Sole OEM authorized service provider for Hach open channel flow meters and sensors in Ontario.	Sole authorized distributor of Hach open channel flow meters, sensors and parts in Ontario.	\$ 12,000.00	\$ 6,647.22	\$ 10,073.00	\$ 1,958.34	\$ 13,955.76	\$ 22,082.60
Cancoppas Limited		x	x		Sole Original authorized service provider for Aysix Technologies products, and GWFTechnologies.	Sole authorized distributor for Aysix Technologies products in Canada, and GWF Technologies products in Ontario.	\$ 30,000.00	\$ 9,076.55	\$ 62,427.00	\$ 24,677.25	\$ 19,765.42	\$ 2,617.24
Caterpillar Inc.	x	x		OEM for engines, generators, controllers and associated parts for back-up power equipment.	Exclusive service provider for proprietary components including PLCs, Switchgear, and controllers, etc., specifically for the Woodward Electrical Power Centre.	Exclusive Distributor for proprietary OEM components including PLCs, Switchgear, and controllers, etc., specifically for the Woodward Electrical Power Centre.	\$ 20,000.00		\$ -	\$ -	\$ -	\$ -
Chemline	x			OEM for pressure control devices, valves, control and regulation devices, piping, tubing and flow meters.			\$ -		\$ -	\$ -	\$ -	\$ -
Cla-Val	x			OEM for specialty valves and flow monitoring.			\$ -		\$ -	\$ -	\$ -	\$ -
Conval Process Solutions Inc.			x			Sole authorized distributor for GA Industries products in Ontario.	\$ 15,000.00	\$ 15,751.61	\$ -	\$ 23,618.45	\$ 13,709.00	\$ 14,400.72
Crane Pumps And Systems Canada	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Cummins	x			OEM for engines, generators, controllers and associated parts for back-up power equipment.			\$ 10,000.00		\$ -	\$ 1,094.34	\$ -	\$ -
Danfoss / Vacon	x			OEM for Vacon soft starters and variable frequency drives.			\$ -		\$ -	\$ -	\$ -	\$ -
Devine & Associates Ltd.		x	x		Sole authorized service provider of Cla-Val products in Ontario.	Sole authorized distributor of Cla-Val products in Ontario.	\$ 10,000.00	\$ 20,568.97	\$ 4,086.00	\$ 22,324.00	\$ 4,216.00	\$ 7,751.86
Dezurik	x			OEM for valves.			\$ -		\$ -	\$ -	\$ -	\$ -
Directrik Inc.			x			Sole authorized distributor for Seepex, Armstrong, Vogelsang, Flowserve (including but not limited to Worthington, Pacific, Ingersoll-Rand, Byron Jackson, Durco, Innomag and Sihj), Trillium Flow Technologies (WEMCO, WSP), and Hidrostal pumps and products for the province of Ontario	\$ 6,000.00	\$ 18,286.11	\$ 18,067.00	\$ 24,572.53	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Eaton Industries Canada Company	x	x		OEM parts for electrical distribution equipment covering Eaton Industries Canada, Cutler-Hammer, Westinghouse, Cooper, and Klockler Moller brand names. Parts include motor control, soft starter/VFD's, transformers, switchgear, breakers, relays/power meters, Ct's, Pt's, surge protectors and other associated electrical distribution parts.	OEM authorized service/maintenance provider for power distribution systems (including any parts required for service/maintenance) covering Eaton Industries Canada, Cutler-Hammer, Westinghouse, Cooper, and Klockler Moller brand names. Field service and technical support for the uninterrupted power supply units for the power distribution system. Eaton Industries Canada has an intimate history of the needs and complexity, as well as technicians are specially trained to maintain their own OEM equipment at various locations across Hamilton Water.		\$ 150,000.00	\$ 34,645.27	\$ 77,000.00	\$ 127,341.00	\$ 124,004.43	\$ 235,974.10
Eaton Powerware	x	x		OEM Equipment Manufacturer of UPS systems ranging from 600v to 110v.	OEM authorized service/maintenance provider for UPS systems (including any parts required for service/maintenance) covering Eaton Powerware Equipment. Eaton Powerware has an intimate history of the needs and complexity, as well as technicians are specially trained to maintain their own OEM equipment at various locations across Hamilton Water.		\$ 60,000.00					
Endress+Hauser	x			OEM for process measurement and optimization monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Envirocan Wastewater Treatment		x	x		Sole OEM authorized service provider for JWC Environmental, Muffin Monster®, Channel Monster®, Auger Monster®, Screenings Washer Monster®, Honey Monster® and Monster Screening Systems® products.	Sole authorized distributor for JWC Environmental, Muffin Monster®, Channel Monster®, Auger Monster®, Screenings Washer Monster®, Honey Monster® and Monster Screening Systems® products.	\$ 60,000.00		\$ -	\$ -	\$ 36,300.00	\$ 138,764.00
ESC Automation Inc.		x	x		Sole authorized service provider for Delta Controls products.	Sole authorized distributor for Delta Controls products.	\$ 60,000.00	\$ 21,808.75	\$ 54,000.00	\$ 43,937.00	\$ 82,887.00	\$ 46,458.00
Evoqua Water Technologies Ltd (Formerly US Filter)	x		x	OEM for intake screens, clarification systems, separation systems and associated parts as well as disinfection, water quality and regulatory monitoring equipment.		Sole authorized distributor for Wallace & Tiernan equipment and associated parts.	\$ 120,000.00	\$ 20,697.11	\$ 120,508.00	\$ 21,104.99	\$ 549,684.42	\$ 26,081.07
Fag Bearings	x			OEM for bearing as identified on motor name plates.			\$ -		\$ -	\$ -	\$ -	\$ -
Floval Equipment Ltd.		x	x		Sole authorized service provider for HydraTite Seal products.	Sole authorized distributor for BNW Valve, HydraTite Seal, DeZURIK, APCO, Willamette, and Hilton parts and equipment.	\$ 20,000.00	\$ 25,710.74	\$ -	\$ 14,000.00	\$ 19,723.00	\$ 588.73
Flowmotion	x		x	OEM for chemical metering equipment.		Sole Authorized distributor for all Flowmotion Systems products in Ontario	\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Flygt	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Gerrie Electric Wholesale Limited		x	x		Sole authorized service provider for Rockwell Automation, Allen Bradley, Endress+Hauser brand electrical distribution systems and parts	Sole authorized distributor for Rockwell Automation, Allen Bradley, and Endress+Hauser brand electrical distribution systems and parts. Rockwell Agent for technical support under Rockwell technical support agreement.	\$ 130,000.00	\$ 262,137.64	\$ 243,200.00	\$ 135,000.00	\$ 126,582.97	\$ 7,633.48
Golden Anderson (GA Industries)	x			OEM for valves and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Gorman Rupp	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Gratec	x			OEM for mixers and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
GrayMatter Systems		x	x		Gray Matter Systems, Inc. is the Partner of Record for the City of Hamilton and currently the only third-party entity GE Digital allowed to quote and provide support under the GE Digital AP agreement. Provides support for the GE Historian and Alarm and Events software		\$ 250,000.00		\$ 20,483.00	\$ -	\$ -	\$ -
Grundfos	x			OEM for submersible pumps and related parts, and chemical dosing equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
H2Flow Equipment, Inc.		x	x		H2Flow Equipment Inc. is the sole authorized service provider for Environmental Dynamics International parts and equipment.	H2 Flow is the sole authorized Representative for all UGSI Chemical Feed, Inc. PolyBlend® and Dynablend™ polymer activation products, aeration diffusers systems and associated parts. H2Flow Equipment Inc. is the sole authorized distributor for parts supplied and provided by Environmental Dynamics International (EDI)	\$ -	\$ 17,697.88	\$ -	\$ -	\$ -	\$ -
Hach Sales & Service LP	x	x	x	OEM for regulatory analyzers, monitoring and control devices.	OEM authorized service provider for repair, start-up service, warranty repair or maintenance on HACH instruments.	OEM and sole source distributor for all Hach branded products with the exception of open channel flow meters and sensors which is through Can-am Instruments LTD.	\$ 130,000.00	\$ 196,539.21	\$ 259,394.00	\$ 89,537.75	\$ 73,963.28	\$ 89,869.32
HCE Telecom Inc.	x	x	x	City authorized SCADA ISP provider	HCE provides remote SCADA access and communications to Hamilton Water Infrastructure including fibre optic cable installation and repair services.	HCE is the distributor for communication hardware, routers, switches and cellular antennas.	\$ 50,000.00		\$ 29,000.00	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Hitachi Energy (ABB/GE)	x	x		Hitachi purchased certain divisions of ABB and GE. Now Hitachi is the OEM for analyzers and transmitters, as well as GE Industrial Solutions electrical control equipment, switchgear, soft starters, variable frequency drives. Power distribution equipment / Centrifuge power and control equipment.	Hitachi formerly ABB/GE has been providing this service (including any parts required for service/maintenance) and now has an intimate history of the needs and complexity, as well as Hitachi technicians are specially trained to maintain their OEM equipment. In addition Hitachi is the Sole OEM authorized service provider for analyzers and transmitters, as well as GE Industrial Solutions electrical control equipment, switchgear, soft starters, variable frequency drives. Power distribution equipment / Centrifuge power and control equipment.		\$ 25,000.00					
Hydroflow	x			OEM for electronic water conditioners and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Hydromatic Pumps	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Hydrovision (GWF Technologies)	x			OEM for flow monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
John Brooks Company Limited		x	x		Sole OEM authorized service provider for Vaughan Co. Inc. (including Chopper, Triton, and Rotomix brands).	Sole authorized distributor for Engineered Systems, Gorman Rupp Pumps (Municipal) , Vaughan Co. Inc. (Chopper, Triton, Rotomix), Goulds Water Technology.	\$ 15,000.00	\$ 2,652.93	\$ 10,186.00	\$ 270.60	\$ 75,878.02	\$ 3,096.00
John Crane	x			OEM for seals, packing, couplings and associated parts.			\$ 12,500.00	\$ 6,156.55	\$ 22,400.00	\$ 9,455.84	\$ 13,829.20	\$ 3,698.50
JSM Controls Inc		x			Service for chlorination equipment and gas chlorination systems in water/wastewater treatment applications. JSM controls maintains the correct TSSA licensing and credentials to preform this work and are well versed with the process and equipment within Hamilton Water's facilities. Their ongoing sole source service support ensures that timely service is received for highly regulated process equipment, to support regulatory compliance and continuance of operations.		\$ 25,000.00	\$ 9,996.26	\$ 19,000.00	\$ 40,000.00	\$ 40,894.00	\$ 132,000.29
JWC Environmental - Muffin Monster	x			OEM for grinders and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Krohne	x			OEM for flow meters.			\$ -		\$ -	\$ -	\$ -	\$ -
KSB Pumps Inc. (Canada)	x	x	x	OEM for pumps and associated parts.	Sole OEM authorized service provider in Canada for all KSB manufactured pumps, valves, parts and service.	OEM and sole goods provider in Canada for all KSB manufactured pumps, valves, parts and service.	\$ -		\$ -	\$ -	\$ 9,102.26	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Lakeside Process Controls Ltd		x	x		Sole OEM authorized service provider for Emerson Automation Solutions products including, but not limited to, MicroMotion flowmeters, Fisher Control Valves, DeltaV® Control Systems, and the Rosemount brand of measurement devices.	Sole authorized distributor for Emerson Automation Solutions products including, but not limited to, MicroMotion flowmeters, Fisher Control Valves, DeltaV® Control Systems, and the Rosemount brand of measurement devices.	\$ 10,000.00	\$ 36,997.58	\$ 32,463.00	\$ 4,638.06	\$ 1,280.00	\$ 9,116.08
Landmark Municipal Services ULC (LMS)		x			Providers of specialized services including the annual aircraft lighting and maintenance on water towers, and water tower maintenance including cleaning. Landmark maintains the required qualifications for tower climbing, harnessing, safety planning, rescue planning documentation and personnel, etc.		\$ 45,000.00		\$ 10,350.00	\$ 18,500.00	\$ 9,800.00	\$ 9,361.05
Link Belt	x			OEM for bearings, seals and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Lonestar	x			OEM for aeration blowers and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Lotowater Technical Services Inc		x			Lotowater is the preferred service provider to the City of Hamilton for potable water wells assessment, maintenance and repair. They are local and readily available to respond to emergency situations, they have profound experience with each of the City's communal drinking water wells, and they maintain an inventory of repair parts and equipment at their facility.		\$ 35,000.00	\$ 6,906.46	\$ 135,230.00	\$ -	\$ -	\$ 33,178.69
Mccrometer	x			OEM for regulatory flow monitoring devices.			\$ -		\$ -	\$ -	\$ -	\$ -
Micro Pump	x			OEM of pumps for regulatory water quality sampling systems and chemical metering equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Moyno Inc	x			OEM for pumps, grinders and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
MSA	x			OEM for gas monitoring equipment, self contained breathing apparatus, confined space entry equipment, and associated replacement parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Myers Pumps	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
NatPro - DXP			x			Sole authorized distributor for Pentair and Aurora products.	\$ -		\$ -	\$ -	\$ -	\$ -
Netzsch	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Nexom	x			OEM for aeration diffusers and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Nivus	x			OEM for flow and level control equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Pentair	x			OEM for Aurora, Aurora Fire, Fairbanks, Nijhuis, Layne-Vertiline, VTSH (vertical turbine solid handling) products and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Philadelphia Mixing Solutions	x			OEM for mixers and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
PMC Engineering	x			OEM for level, pressure and process monitoring and control equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Powell Electric (Formerly GE)	x	x		Powell Electric is the OEM for PowerVac, PowlVac, Powell Breaker, Powell Flextrol, Powell Flexgear, Powell Arc Whipper, Delta/Unibus, Powell Bus electrical equipment.	Powell Electric (Formerly GE) has been providing this service (including any parts required for service/maintenance) and now has an intimate history of the needs and complexity, as well as Powell Electric technicians are specially trained to maintain their OEM equipment. In addition Powell Electric is the Sole OEM authorized service provider for analyzers and transmitters, as well as GE Industrial Solutions electrical control equipment, switchgear, soft starters, variable frequency drives. Power distribution equipment / Centrifuge power and control equipment.		\$ 80,000.00	\$ 9,957.24				
Pro Aqua Inc			x			Sole authorized distributor of Schwing Bioset Parts in Ontario and Evoqua's wastewater treatment equipment and parts with the exception of odour control equipment.	\$ 2,500.00		\$ 2,336.00	\$ 2,080.70	\$ 110,566.68	\$ -
Prominent	x			OEM for regulatory water quality monitoring and disinfection equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Pro-Tech Power			x			Pro-Tech Power Sales Inc. is the exclusive authorized sales representative for Schweitzer Engineering Laboratories, Inc. (SEL) and its Canadian Subsidiary in the province of Ontario.	\$ 50,000.00					
Pulsar	x			OEM for level monitoring equipment.			\$ -		\$ -	\$ -	\$ -	\$ -
Red Valve	x			OEM for valves and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Rockwell Automation (Allen-Bradley)	x			OEM for soft starters, variable frequency drives and SCADA components.			\$ -		\$ -	\$ -	\$ -	\$ -
Ro-Flo Compressors LLC	x			OEM for compressors and associated parts.			\$ -		\$ -	\$ -	\$ 60,558.00	\$ -
Rotork Controls (Canada) Ltd.	x	x	x	OEM for actuators.	OEM authorized service provider.	OEM authorized distributor.	\$ 15,000.00	\$ 36,656.31	\$ 13,000.00	\$ 17,205.05	\$ 43,910.66	\$ 28,903.43
S&C Electric Canada	x	x		OEM parts for electrical distribution equipment covering S&C Electric. Parts include all controls, battery chargers, all power distribution/communication equipment for the loop ring power center/ELU and other associated electrical distribution parts.	OEM authorized service/maintenance provider for power distribution systems (including any parts required for service/maintenance) covering S&C Equipment. S&C now has an intimate history of the needs and complexity, as well as S&C technicians are specially trained to maintain their own OEM equipment at the Woodward Electrical Power Centre.		\$ 20,000.00		\$ -	\$ -	\$ -	\$ -
SCG (Formerly Metcon Sales & Engineering Limited)			x			Sole authorized distributor for Spencer Equipment, Watson Marlow Fluid Technology and ProMinent Canada products.	\$ 55,000.00	\$ 98,534.04	\$ 70,000.00	\$ 37,965.04	\$ 52,918.46	\$ 61,045.42
Schneider Electric Canada	x	x		OEM for Federal Pioneer Ltd., Square D, Merlin Gerin power systems; Telemecanique Automation & Control Products; APC UPS Equipment, ASCO Transfer Switches, Powerlogic Metering Equipment, ION Metering parts, relays and other associated electrical distribution parts.	OEM authorized service/maintenance provider for power distribution systems (including any parts required for service/maintenance) covering all products identified. Schneider Electric Canada now has an intimate history of the needs and complexity, as well as Schneider Electrical technicians are specially trained to maintain their own OEM equipment.		\$ 20,000.00		\$ -	\$ -	\$ 10,874.44	\$ -
Schweitzer Engineering Laboratories	x			OEM for protective relay and communications equipment associated with the power distribution system. Distributor - Pro-Tech listed above			\$ -		\$ -	\$ -	\$ -	\$ -
Schwing Bioset Inc	x		x	OEM for biosolids equipment and associated parts.		Schwing Bioset, Inc. is the Exclusive/ Sole Source Distributor of original Schwing spare parts used on the Schwing supplied Material Handling equipment such as sludge pumping systems, sliding frame silos, push floor systems, power units, screw feeders, and lubrications systems, etc.	\$ 50,000.00	\$ 24,681.36	\$ 34,000.00	\$ 230,598.16	\$ 26,132.50	\$ -
Seepex	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Siemens Canada Limited	x	x		OEM for power distribution, soft starters and variable frequency drives and other associated electrical distribution parts.	OEM authorized service/maintenance provider for power distribution systems (including any parts required for service/maintenance) covering all products identified. Siemens Canada has an intimate history of the needs and complexity, as well as their technicians are specially trained to maintain their own OEM equipment.		\$ 40,000.00	\$ 2,533.83	\$ -	\$ -	\$ 58,480.04	\$ 50,617.00
Singer Valve	x			OEM for valves and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
SKF Bearings	x			OEM for bearings as identified on motor name plates.			\$ 10,000.00		\$ -	\$ 12,500.00	\$ -	\$ -
Smart Turner Pumps Inc.	x		x	OEM for pumps and associated parts.		OEM and sole distributor of Smart Turner Pumps.	\$ 20,000.00		\$ 58,370.00	\$ -	\$ -	\$ -
Smith & Loveless, Inc.	x		x	OEM for pumps, grit classification systems and associated parts.		OEM and sole distributor of Smith & Loveless products and equipment.		\$ 75,006.30				
Spaans Babcock	x		x	OEM for screws, gear box and associated parts.		OEM and sole distributor Spaans Babcock products and training / inspection services in North America. The products include Spaans Babcock screw pumps, hydro screw generators, fine screen and any spare parts related to these products.	\$ 20,000.00	\$ 15,610.01	\$ 71,125.00	\$ 8,455.00	\$ 7,833.00	\$ 16,925.14
SPD Sales Limited		x	x		Sole OEM authorized service provider for Grundfos, Kurz Instruments, Vega, Xylem Brands (YSI IQ Sensornet, Royce Technologies & WTW products).	Sole authorized distributor for MSA Safety Inc. (for Fixed Gas and Flame Detection products), ATI products, Blue- White Industries Pro-Series M product line, Nivus, Grundfos, Kurz Instruments, Chemtrac, and Xylem Brands (YSI IQ Sensornet, Royce Technologies & WTW products).	\$ 50,000.00	\$ 113,558.43	\$ 115,781.00	\$ 79,801.75	\$ 43,437.00	\$ 59,499.57
Spirac	x			OEM for grit handling augers and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
SPX Flow	x			OEM for mixer process systems and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
SRP Control Systems Limited		x	x		Sole Authorized Service Provider for PMC Engineering Legacy Series Industrial Pressure Transmitters, PMC Engineering Versaline Series Submersible Depth and Level Sensors, and STS Series Precision Pressure Sensors.	Sole authorized distributor of PMC Engineering Legacy Series Industrial Pressure Transmitters, PMC Engineering VersaLine Series Submersible Depth and Level Sensors, and STS Series Precision Pressure Sensors in Canada.	\$ 15,000.00	\$ 11,977.17	\$ 19,920.00	\$ 16,215.00	\$ -	\$ -
Suez Treatment Solutions Inc. (formerly Infilco Degremont)	x		x	OEM for Climber Screen ®, and associated parts.		OEM and sole distributor of Climber Screen ®, and associated parts.	\$ 150,000.00		\$ 52,547.00	\$ -	\$ 357,720.21	\$ 176,538.71

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Summa Engineering Limited		x			SUMMA Engineering Ltd. as the SCADA Service vendor for the Electrical Power Center (EPC) housing the 13.8 kV switchgear Automation System project at the Woodward Avenue WWTP. Summa Engineering Ltd. being the Designer of the SCADA for this automation system are the preferred service provider for the SCADA graphics system for this complete system. SUMMA has in the past, successfully completed similar work and has completed SCADA Graphics OEM Projects. Also, SUMMA, being the designer of the SCADA graphics system, can provide expert service seamlessly without delays. SUMMA has the experience and dedicated staff to provide SCADA service.		\$ 5,000.00					
Syntec Process Equipment Ltd.		x	x			Sole authorized distributor for Banyan Products, Fontaine- Aquanox, Master Distributor of Chemline Plastics, J&S Valve Inc., Netzsch, Singer, Limitorque Products for Simons Automation and Supply, Trueline Valve Corp., Val-Matic, Trumbull Mfg. Ltd.	\$ 100,000.00	\$ 191,135.08	\$ 172,381.00	\$ 197,007.30	\$ 129,690.50	\$ 75,000.96
T.D. Rooke Associates Limited			x			Sole authorized distributor for SPXFLOW – Lightning Mixers, and associated parts.	\$ 25,000.00		\$ -	\$ -	\$ -	\$ 55,300.28
Teledyne (ISCO)	x			OEM for auto samplers.			\$ -		\$ -	\$ -	\$ -	\$ -
Toromont Industries Ltd.		x			Toromont programmed the PLC control according to customer Plant Control Narrative and performed the commissioning and customer witness test for the supplied system. Toromont is the sole service provider for the standby power PLC control, full commissioning, testing, training and inspection for the Woodward Power Center. Consequently Toromont is the Sole service provider (and associated parts required for the service) for C175 diesel generator sets with remote radiators, Local and manual control panels (LMCP), and System Automation System (SAS) Panels at Woodward WWTP.		\$ 30,000.00	\$ 10,000.00	\$ -	\$ 1,094.34	\$ -	\$ -
Tyco Integrated Fire and Security Canada, Inc. o/a Johnson Controls	x	x		Tyco Integrated Fire and Security Canada, Inc. o/a Johnson Controls collaboratively designed, commissioned and verified the newly installed Fire Alarm and fire protection equipment installed at 700 Woodward Avenue Hamilton. This system contains a Simplex-brand fire alarm panel and Tyco sprinkler products.	Johnson Controls International is the manufacturer of the Simplex brand of fire alarm panels. JCI's technicians undergo factory training in all Simplex fire alarm products.		\$ 5,000.00					

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
Johnson Controls		x			An ISO 17025 accredited organization that is able to provide on-site calibration services on a multitude of different instruments. Standardization ensures timely and expert response to regulatory instrumentation equipment.		\$ 17,500.00	\$ 18,126.66	\$ 16,745.00	\$ 16,844.00	\$ 21,835.34	\$ 15,847.54
Trillium Measurement and Control		x	x		Sole authorized service provider for Varec digester equipment and associated parts.	Sole authorized distributor for Varec digester equipment and associated parts.						
Trojan Technologies		x	x	OEM for disinfection process equipment.	OEM service provider must be used to ensure performance guarantee is maintained.	Parts must be purchased through OEM to maintain warranties and guarantee equipment performance	\$ 50,000.00	\$ 21,470.95	\$ -	\$ -	\$ -	\$ -
Troy-Ontor Inc		x	x		Sole OEM authorized service provider for Auma products in Canada.	Sole authorized distributor for Auma products in Canada.	\$ 20,000.00	\$ 37,990.52	\$ 18,000.00	\$ 19,431.92	\$ 29,897.00	\$ -
UGSI Chemical Feed, Inc	x			OEM for PolyBlend ® product line of equipment, including but not limited to pumps, mixers, drives, controllers, ORP, Stranrol Analyzers and Chemical Feed/mixing systems.			\$ -		\$ -	\$ -	\$ -	\$ -
United Electric Controls	x			OEM for pressure control and monitoring devices.			\$ -		\$ -	\$ -	\$ -	\$ -
USP Technologies		x			USP is the sole source service provider for the Binbrook Odour Control Pilot using Hydrogen Peroxide to control sewer odours. The pilot began in 2020 and is approximately 3 years in length.		\$ 100,000.00	\$ 92,000.00				
Val-Matic	x			OEM for valves and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Varec Biogas	x	x		OEM for digester equipment and associated parts.	Only a qualified field service engineer from Varec Biogas is authorized to perform any service on Varec Biogas equipment, otherwise this will void any existing warranties for the product.		\$ -		\$ -	\$ -	\$ -	\$ -
Vaughan Pumps	x			OEM for pumps and associated parts.			\$ -		\$ -	\$ -	\$ -	\$ -
Vega	x			OEM for process control equipment.			\$ -	\$ 47,146.58	\$ -	\$ -	\$ -	\$ -
Wajax Industrial Components		x	x		Sole OEM authorized service provider for Moyno Progressing Cavity Pumps in Ontario.	Sole authorized distributor for Moyno Progressing Cavity Pumps in Ontario.	\$ 50,000.00	\$ 51,519.56	\$ 101,000.00	\$ 23,845.51	\$ 325,401.64	\$ 14,323.32
Wallace And Tiernan	x			OEM for disinfection process equipment.			\$ -		\$ -	\$ -	\$ -	\$ -

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017	
WesTech Engineering LLC	x		x	OEM of the ExtremeDuty™ Internal Sludge Mixer Model SMI1.ster mixers		Sole distibuter of the ExtremeDuty™ Internal Sludge Mixer Model SMI1.ster mixers	\$ 600,000.00		\$ -	\$ -	\$ -	\$ -	
Westech Industrial Ltd		x	x		Westech Industrial, Ltd. is the sole authorized service provider for Emerson and Varec equipment used in digester gas or biogas processing in Canada.	Westech Industrial, Ltd. is the sole authorized Distributor for Emerson and Varec equipment used in digester gas or biogas processing in Canada.	\$ 10,000.00	\$ 49,633.54	\$ -	\$ -	\$ 5,013.00	\$ -	
Westin Technology Solutions (Formerly Eramosa Engineering)	x	x		Consultant and provider of the SCADA architecture , DATA, Network under SCADA master plan , SCADA _E network provider.	Sole OEM authorized service agent in North America for existing eRIS software users. SCADA support service and maintenace provider for network,data, SCADA architecture core system level.		\$ 300,000.00	\$ 34,426.12	\$ 221,826.00	\$ 90,331.64	\$ 430,455.47	\$ 612,110.71	
Xylem Canada Company	x	x	x	Sole authorized distributor of Flygt branded submersible pumps and mixers, and associated parts. Sole authorized distributor of Godwin branded pumps and accessories.	Sole OEM authorized service provider for Flygt branded submersible pumps and mixers. Sole OEM authorized service provider Godwin branded pumps and accessories.	Sole authorized distributor of Flygt branded submersible pumps and mixers, and associated parts. Sole authorized distributor of Godwin branded pumps and accessories.	\$ 300,000.00	\$ 234,631.17	\$ 330,861.00	\$ 293,238.49	\$ 452,639.62	\$ 302,944.69	
YSI	x			OEM for regulatory analyzers, monitoring and control devices.			\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	
							Totals	\$3,939,500	\$ 2,128,578.67	2,538,779	1,748,303	3,912,408	2,467,297

Parts, Supplies, Equipment and Services for Additional Hamilton Water Sections

Vendor Name	Original Equipment Manufacturer (OEM)	Provider for Service	Distributor for Parts	(OEM) Description of Goods and/or Service Supplied	Provider for Service	Distributor for Parts	Estimated Spend 2022	Annual Spent 2021	Annual Spent 2020	Annual Spent 2019	Annual Spent 2018	Annual Spent 2017
AECOM Canada Ltd		x			National vendor responsible for National Water Wastewater Benchmarking Initiative (NWWBI), through the Canadian Infrastructure Benchmarking Initiative (CIBI)		\$50,000	\$ 48,229.19	\$ 50,773.00	\$ 49,895.00	\$ 49,895.00	\$ 37,421.00
Devine & Associates Limited			x			Sole authorized distributor of Cla-Val products in Ontario	\$50,000					
E.H. Wachs	x	x	x	Supply of OEM spare parts, equipment for ERV- 750 Valve Exerciser - Automated Valve Operator, VMT2 Valve and Hydrant Maintenance Trailer, WACHS Vital (valve data collection) software system, WACHS Hydrant Cutter, and guillotine saw.	Sole Supply of OEM spare parts, equipment and services for ERV-750 Valve Exerciser - Automated Valve Operator, VMT2 Valve and Hydrant Maintenance Trailer, WACHS Vital (valve data collection) software system, WACHS Hydrant Cutter, and guillotine saw.	Sole Supply of OEM spare parts, equipment and services for ERV- 750 Valve Exerciser - Automated Valve Operator, VMT2 Valve and Hydrant Maintenance Trailer, WACHS Vital (valve data collection) software system, WACHS Hydrant Cutter, and guillotine saw.	\$50,000	\$ 57,696.93	\$ 86,681.00	\$ 57,421.44	\$ 30,000.00	\$ 29,749.58
Evans Supply Limited (Utility & Municipal Products)			x			Sole distributor of hydrant Adaptor stands.	\$10,000	\$ 22,333.91	\$ 33,233.00	\$ 22,088.85	\$ 15,573.50	\$ 4,847.00
Flowpoint Environmental Systems LP	x	x		Supply of OEM spare parts, equipment and to maintain existing Flow Point terminals.	Annual Licensing and Technical support for existing terminals and software for the City's Bulk Water Filling and Hauled Waste sewage stations. This software system allows customers to create an account online and make online purchases.		\$30,000	\$ 7,219.88	\$ 18,901.00		\$ 12,575.00	
Industrial Scientific Corporation	x	x	x	Supply of OEM spare parts, equipment for Ventis gas detectors and accessories used by Hamilton Water.	Sole service provider to maintain existing Ventis gas detectors and accessories used by Hamilton Water.	Sole distributor of spare parts, and equipment to maintain existing Ventis gas detectors and accessories used by Hamilton Water.	\$ 65,000.00	\$ 53,928.38	\$ 56,179.00	\$ 57,840.91	\$ 52,155.72	
Kronos Inc.	x	x	x	Supply of OEM spare parts, equipment for Kronos In Touch 9000 terminals.	OEM support services to maintain existing Kronos In Touch 9000 terminals.	Sole provider of OEM parts and equipment to maintain existing Kronos In Touch 9000 terminals.	\$10,000		\$ 46,632.00	\$ 25,116.98	\$ 10,000.00	\$ 30,000.00

Van Essen Instruments - Canada	x		x	Supply of OEM spare parts, equipment for Diver dataloggers		Sole distributor of spare parts, equipment to maintain Diver dataloggers and related component	\$10,000		\$1,680.00	\$ 24,355.50	\$ 30,000.00	\$ 11,050.11
Totals							\$275,000.00	\$189,408.29	\$ 294,079.00	\$236,718.68	\$200,199.22	\$113,067.69



AUDIT, FINANCE AND ADMINISTRATION COMMITTEE REPORT 22-013

**9:30 a.m.
July 7, 2022
Council Chambers
Hamilton City Hall**

Present: Councillors M. Pearson (Chair), B. Clark, L. Ferguson, B. Johnson, R. Powers, and A. VanderBeek (Vice-Chair)

Absent: Councillor M. Wilson - Personal

THE AUDIT, FINANCE AND ADMINISTRATION COMMITTEE PRESENTS REPORT 22-013 AND RESPECTFULLY RECOMMENDS:

- 1. Tax and Rate Operating Budgets Variance Report as at April 30, 2022 - Budget Control Policy Transfers (FCS22042) (City Wide) (Item 8.1)**
 - (a) That the Tax and Rate Operating Budgets Variance Report, as at April 30, 2022, attached as Appendices “A” and “B”, respectively, to Report FCS22042, be received;
 - (b) That the City of Hamilton continue to participate in efforts with other municipalities seeking financial support from senior levels of government for the unfunded financial pressures of the COVID-19 pandemic response and recovery;
 - (c) That, in accordance with the “Budgeted Complement Control Policy”, the 2022 complement transfer transferring complement from one department / division to another with no impact on the levy, as outlined in Appendix “A” to Audit, Finance and Administration Committee Report 22-013, be approved.

- 2. Governance Review Sub-Committee Report 22-003 - June 23, 2022 (Item 10.2)**
 - (a) Renaming of Committees (FCS22055) (City Wide) (Item 10.1)**
 - (i) That the following Committees be renamed as follows:
 - (1) Waste Management Advisory Committee be renamed to the Waste Management Sub-Committee;
 - (2) Agricultural and Rural Affairs Advisory Committee be renamed the Agricultural and Rural Affairs Sub-Committee;

- (3) Business Improvement Area Advisory Committee be renamed the Business Improvement Area Sub-Committee; and
 - (4) Community Benefits Protocol Advisory Committee be renamed the Community Benefits Protocol Sub-Committee.
 - (ii) That the Waste Management Sub-Committee; Agricultural and Rural Affairs Sub-Committee and Business Improvement Area Sub-Committee Terms of Reference, attached as Appendix 'B' to 'D' to Audit, Finance and Administration Committee Report 22-013, be approved.
- (b) Mandatory COVID-19 Vaccination Verification Policy for Members of Council and Members of Council Appointed Committees (FCS22016 / HR22010) (City Wide) (Item 10.2)**
 - (i) That Council approve the amendments to the City of Hamilton Policy respecting the Appointment of Citizens to the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees as outlined in Appendix "E" to Audit, Finance and Administration Committee Report 22-013;
 - (ii) That Appendix "F" to Audit, Finance and Administration Committee Report 22-013 the Mandatory COVID-19 Vaccination Verification Policy, be amended:
 - (1) to include, Option 'B' as the preferred option to administer Council Appointees who are not compliant with the Mandatory COVID-19 Vaccination Verification Policy, that reads as follows:

Members of Council appointed committees will be presumed to have an Approved Exemption and shall work with the City to develop a reasonable and appropriate accommodation plan including health and safety measures to protect all Employees and Committee Members, up to the point of undue hardship
 - (2) to relocate Section 6. of Terms and Conditions - Non-compliance with the Mandatory COVID-19 Vaccination Verification Policy as Section 2., renumbering the remaining sub-sections accordingly; and
 - (iii) That Council approve the proposed amendments to the Mandatory COVID-19 Vaccination Verification Policy, substantially in the form attached as amended Appendix "F" to Audit, Finance and Administration Committee Report 22-013, Mandatory COVID-19 Vaccination Verification Policy.

(c) Establishment of the City of Hamilton's Citizen Advisory (Volunteer) and Sub-Committees for the 2022-2026 Term of Council (FCS22056) (City Wide) (Added Item 10.3)

(i) That the membership within the Terms of Reference of the following Committees be amended as follows, for quorum purposes:

(1) Hamilton-Wentworth Catholic District School Board (HWCDSB) Liaison Committee:

MEMBERSHIP

Total of three Trustees (Chair of the Board and two Trustees)
Total of three Members of Council (Mayor and two members of Council)

(Appendix 'G' – Hamilton-Wentworth Catholic District School Board (HWCDSB) Liaison Committee – Terms of Reference (revised))

(2) Hamilton-Wentworth District School Board (HWDSB) Liaison Committee:

MEMBERSHIP

Total of three Trustees (Chair of the Board and two Trustees)
Total of three Members of Council (Mayor and two members of Council)

Total of two HWDSB Secondary Students (non-voting, rotating basis, invited by HWDSB)

(Appendix 'H' - Hamilton-Wentworth District School Board (HWDSB) Liaison Committee – Terms of Reference (revised))

(ii) That the following Sub-Committees, be disbanded:

- (1) Affordable Housing Site Selection Sub-Committee;
- (2) Government Relations Sub-Committee;
- (3) Steel Sub-Committee;
- (4) Rental Housing Sub-Committee;
- (5) Hamilton Utilities Corporation Joint Advisory Committee and Working Group;
- (6) Capital Projects Work In-Progress Review Sub-Committee; and
- (7) Multi-Year Budget Planning Sub-Committee;

(iii) That the Sub-Committees listed in Appendix "I" to Audit, Finance and Administration Committee Report 22-013, be established for the 2022 – 2026 Term of Council (November 16, 2022 to November 15, 2026);

- (iv) That the establishment of the Physician Recruitment and Retention Steering Committee be postponed until after Council has considered the review of the Physician Recruitment and Retention process in the Fall of 2022;
- (v) That the establishment of the Citizen Advisory Committees (Volunteer) with the exception of the Advisory Committee for Persons with Disabilities, be postponed and considered by the 2022–2026 Council, so that the Citizen Advisory Committees (Volunteer) may be aligned with the 2022–2026 Council Term priorities; and
- (vi) That the Advisory Committee for Persons with Disabilities, be established for the 2022 – 2026 Term of Council (November 16, 2022 to November 15, 2026), as per section 29(1) of the *Accessibility for Ontarians with Disabilities Act, 2005, S.O. 2005, c. 11*.

FOR INFORMATION:

Councillor VanderBeek, Vice Chair assumed the Chair in Councillor Pearson's absence.

(a) CHANGES TO THE AGENDA (Item 2)

The Committee Clerk advised of the following changes to the agenda:

6. DELEGATION REQUESTS (Item 6)

- 6.1. Delegation Requests respecting Item 10.1 - Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Services Board selection process, for today's meeting:
 - 6.1(a) Marlene Dei-Amoah and Taimur Qasim, Committee Against Racism
 - 6.1(b) Kojo Dampety
 - 6.1(c) Kim Martin, Social Planning and Research Council and No Hate In the Hammer
 - 6.1(d) Lyndon George, Hamilton Anti-Racism Resource Centre
 - 6.1(e) Rabbi Hillel Lavery-Yisraeli

The agenda for the July 7, 2022 Audit, Finance and Administration Committee meeting was approved, as amended.

(b) DECLARATIONS OF INTEREST (Item 3)

There were no declarations of interest.

(c) APPROVAL OF MINUTES OF PREVIOUS MEETING (Item 4)

(i) June 16, 2022 (Item 4.1)

The Minutes of the June 16, 2022 meeting of the Audit, Finance and Administration Committee were approved, as presented.

(d) DELEGATION REQUESTS (Item 6)

The following Delegation Requests respecting Item 10.1 - Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Services Board selection process, were approved for today's meeting:

- (i) Marlene Dei-Amoah and Taimur Qasim, Committee Against Racism (Added Item 6.1)
- (ii) Kojo Dampsey (Added Item 6.2)
- (iii) Kim Martin, Social Planning and Research Council and No Hate In the Hammer (Added Item 6.3)
- (iv) Lyndon George, Hamilton Anti-Racism Resource Centre (Added Item 6.4)
- (v) Rabbi Hillel Lavery-Yisraeli (Added Item 6.5)

(e) STAFF PRESENTATIONS (Item 8)

(i) Tax and Rate Operating Budgets Variance Report as at April 30, 2022 - Budget Control Policy Transfers (FCS22042) (City Wide) (Item 8.1)

Mike Zegarac, General Manager of Finance and Corporate Services, addressed the Committee with a presentation on the Tax and Rate Operating Budgets Variance Report as at April 30, 2022 - Budget Control Policy Transfers.

The staff presentation respecting the Tax and Rate Operating Budgets Variance Report as at April 30, 2022 - Budget Control Policy Transfers, was received.

Councillor Pearson assumed the Chair.

For further disposition of this matter, refer to Item 1.

(f) PUBLIC HEARINGS / DELEGATIONS (Item 9)

(i) Delegations respecting Item 10.1 - Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Services Board selection process (Added Item 9.1)

The following delegations address the Committee respecting Item 10.1 - Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Services Board selection process:

- (a) Marlene Dei-Amoah and Taimur Qasim, Committee Against Racism (Added Item 9.1)
- (b) Kojo Dampsey (Added Item 9.2)
- (c) Kim Martin, Social Planning and Research Council and No Hate In the Hammer (Added Item 9.3)
- (d) Lyndon George, Hamilton Anti-Racism Resource Centre (Added Item 9.4)
- (e) Rabbi Hillel Lavery-Yisraeli (Added Item 9.5)

The following Delegations respecting Item 10.1 - Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Services Board selection process, were received:

- (a) Marlene Dei-Amoah and Taimur Qasim, Committee Against Racism (Added Item 9.1)
- (b) Kojo Dampsey (Added Item 9.2)
- (c) Kim Martin, Social Planning and Research Council and No Hate In the Hammer (Added Item 9.3)
- (d) Lyndon George, Hamilton Anti-Racism Resource Centre (Added Item 9.4)
- (e) Rabbi Hillel Lavery-Yisraeli (Added Item 9.5)

For disposition of this matter, refer to Item (g)(i).

(g) DISCUSSION ITEMS (Item 10)

(i) Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Service Board selection process (Item 10.1)

That the Committee Against Racism - Citizen Committee Report - Recommendations for changes to the Hamilton Police Service Board

selection process, be referred to staff for a report back to the Governance Review Sub-Committee on the feasibility of implementing the following recommendations within the Citizen Committee Report:

That the Committee Against Racism recommends the City of Hamilton's Selection Committee for Agencies, Boards and Sub-Committees be restructured to include the following for recruitment to the Hamilton Police Services Board:

- (a) The Selection Committee for Agencies, Boards and Sub-Committees be Co-chaired by a member of City Council and a representative from the Hamilton Anti-Racism Resource Centre (HARRC);
- (b) The Selection Committee for Agencies, Boards and Sub-Committees be comprised of 60% community representatives and 40% Council representatives with full voting privileges;
- (c) The recommended community representatives for the Selection Committee for Agencies, Boards and Sub-Committees include:
 - (1) Hamilton Anti-Racism Resource Centre (HARRC)
 - (2) Hamilton Black Leadership Community
 - (3) Hamilton Jewish's leadership community
 - (4) Hamilton's Muslim leadership community
 - (5) Hamilton's Indigenous leadership community
 - (6) Hamilton's LGBTQ2AI community
 - (7) Youth representative
 - (8) And other equity-seeking communities
- (d) The Selection Committee for Agencies, Boards and Sub-Committees recommend 2 citizen members to Hamilton City Council for consideration as the new citizen appointee for the Hamilton Police Service Board;
- (e) The Selection Committee for Agencies, Boards and Sub-Committees consult with and solicit recommendations for selecting candidates, including questions for candidates, from the City of Hamilton's Volunteer Advisory Committees, including:
 - (1) Hamilton Women and Gender Equity Advisory Committee
 - (2) Indigenous Advisory Committee
 - (3) LGBTQ Advisory Committee
 - (4) Committee Against Racism Advisory Committee
 - (5) Advisory Committee for Persons with Disabilities

(h) PRIVATE AND CONFIDENTIAL (Item 14)

Committee determined that discussion of Item 14.1 was not required in Closed Session, therefore, the item was addressed in Open Session, as follows:

(i) Closed Minutes – June 16, 2022 (Item 14.1)

- (a) The Closed Session Minutes of the June 16, 2022 Audit, Finance and Administration Committee meeting, were approved as presented; and,
- (b) The Closed Session Minutes of the June 16, 2022 Audit, Finance and Administration Committee meeting, remain confidential.

(i) ADJOURNMENT (Item 15)

There being no further business, the Audit, Finance and Administration Committee adjourned at 11:38 p.m.

Respectfully submitted,

Councillor Pearson, Chair
Audit, Finance and Administration
Committee

Councillor VanderBeek, Vice-Chair
Audit, Finance and Administration
Committee

Angela McRae
Legislative Coordinator
Office of the City Clerk

**CITY OF HAMILTON
BUDGET AMENDMENT SCHEDULE**

STAFF COMPLEMENT CHANGE

Complement Transfer to another division or department ^(1,2)

ITEM #	TRANSFER FROM				TRANSFER TO			
	Department	Division	Position Title (2)	FTE	Department	Division	Position Title (2)	FTE
1.1	Planning and Economic Development	Transportation Planning and Parking	Transportation Technologist	1.00	Planning and Economic Development	Transportation Planning and Parking	Project Manager, Transportation Planning Approvals	1.00
	Explanation: Conversion of Transportation Technologist (Grade L) to a Project Manager (Grade 5) required to provide more responsibility and technical ability to support development approvals and guide junior positions. Change in pay band can be accommodated through available gapping dollars.							
1.2	Public Works	Environmental Services	Co-Op Student	0.66	Public Works	Environmental Services	Urban Forestry Health Tech	1.0
	Public Works	Environmental Services	Summer Student	0.34	Explanation: To approve the transfer of budgeted FTE from DeptID 445026 Forestry Co-Op Student (0.66 FTE) and Summer Student Local 5 from DeptID 446026 to DeptID 445005 Urban Forestry Health Technician CUPE 5167 Grade K. Budget cost variance will be absorbed within the current operating budget.			
1.3	Public Works	Transportation Operations & Maintenance	Operations Svcs Rep-Student	1.00	Public Works	Transportation Operations & Maintenance	Operations Svcs Rep	1.0
	Explanation: To approve the transfer of an Operations Svcs Rep-Student (1.0 FTE) DeptID 466005 to an Operations Svcs Rep (1.0 FTE) DeptID 466005, CUPE 5167 Grade G. Budget cost variance will be absorbed within the current operating budget.							
1.4	Corporate Services	Financial Services Taxation & Corp Controller	Pension Clerk II	1.00	Corporate Services	Financial Services Taxation & Corp Controller	Pension Clerk 1	1.00
	Explanation: The duties of the Pension Clerk II position (Grade E) have decreased over the years while the duties of the Pension Clerk 1 position (Grade G) have increased. It now makes sense to have two Pension Clerk 1 positions and combine the duties and split the workload evenly between the positions. Variance will be funded from gapping in Accounts Payable and Business Application Support.							

Note - Complement transfers include the transfer of corresponding budget.

(1) - All other budgeted complement changes that require Council approval per Budgeted Complement Control Policy must be done through either separate report or the budget process (i.e. Increasing/decreasing budgeted complement).

(2) - If a position is changing, the impact of the change is within 1 pay band unless specified.



Hamilton

TERMS OF REFERENCE

Waste Management Sub-Committee

(Formerly the Waste Management Advisory Committee)

1. INTRODUCTION

1.1 Committee Name

Waste Management Sub-Committee (WMSC)

1.2 Statement of Purpose

To assist the City of Hamilton with the implementation of the 2012 Solid Waste Management Master Plan (SWMMP), 2020 SWMMP Action Items, and to discuss / make recommendations on other solid waste management initiatives.

1.3 Committee Mandate

The mandate of the Waste Management Sub-Committee shall be to:

- a) Give overall guidance and direction during the implementation and maintenance of the City's long-term Solid Waste Management Master Plan,
- b) Give overall guidance and direction during the preparation and implementation of other solid waste management initiatives; and
- c) Advise Council through the Public Works Committee of the progress and to receive feedback, advice and direction, as appropriate.

1.4 Accountability

- a) WMSC is a Sub-Committee that advises Council through the Public Works Committee.
- b) Members of the WMSC are responsible for complying with the Procedural By-law and the Advisory Committee Handbook.

2. COMMITTEE STRUCTURE

2.1 Membership

The Waste Management Sub-Committee shall be comprised of up to five (5) members, as follows:

- a) Up to three members of City Council; and
- b) Two citizen members.

2.2 Attendance and Vacancies

If a member is absent for three (3) meetings in a calendar year without approval from the WMSC, the member may be subject to replacement.

2.3 Term of Office

The membership term will coincide with the term of Council or until such time as successors are appointed by Council.

3. SUPPORT SERVICES

3.1 The City's Waste Management Division shall provide for the administrative costs of operating the Waste Management Sub-Committee, including the cost of meeting places and clerical support services.

3.2 The City's Waste Management Division shall provide the Waste Management Sub-Committee with reasonable access to the City's consultants and facility operators.

4. MEETINGS

4.1 The Waste Management Sub-Committee shall meet bi-monthly or at the call of the chair.

AGRICULTURE AND RURAL AFFAIRS SUB-COMMITTEE COMMITTEE

TERMS OF REFERENCE (DRAFT)

MANDATE

The Agriculture and Rural Affairs Sub-Committee (ARASC) is a Sub-Committee established by City Council in accordance with these adopted Terms of Reference. The ARASC will provide input to Council and City staff on issues that impact agriculture and rural communities, and act as the liaison between those communities and Council, helping to preserve, maintain and advance an economically viable agriculture base in the City of Hamilton.

SCOPE OF ACTIVITIES

The ARASC's scope of activities, as it relates to their mandate, are as follows:

- To solicit and coordinate the interests and concerns of Hamilton's farming industry, agricultural organizations, farm families and non-farm rural residents, presenting recommendations before City Council as necessary;
- To serve as a community forum for the exchange of information on agricultural and rural affairs matters and encourage public awareness and education of agricultural and rural issues;
- To review and provide advice on federal, provincial and municipal strategies, policies, studies, plans, by-laws and proposal related to agriculture and rural affairs;
- To provide advice on agricultural and rural affairs matters, as they arise, at the request of Council or staff; and,
- To serve as the Tree By-Law Committee, as set forth in By-law No. R00-054, the Regional Municipality of Hamilton-Wentworth Woodland Conservation By-law, for the purpose of hearing applicants who are refused a permit for minor exception, and recommending whether the permit should be issued or denied with or without terms and conditions.

REPORTING STRUCTURE

The Agriculture and Rural Affairs Sub-Committee reports to City Council, through the Planning Committee.

COMPOSITION & TERMS

The Agriculture and Rural Affairs Sub-Committee shall be comprised of up to 19 voting members and up to five key stakeholders/advisors (non-voting) as follows:

Voting Members:

- Three members appointed by the Hamilton-Wentworth Federation of Agriculture;
- One member appointed by the Wentworth-Brant Christian Farmers Association;
- One member appointed by the Hamilton-Wentworth Women's Institute;
- One rotating member appointed by the Agricultural Societies (Rockton, Binbrook and Ancaster);

- Two members appointed by the Wentworth Soil & Crop Improvement Association;
- Up to five citizen members appointed by Council; and,
- Up to six Councillors appointed by Council from Wards 9, 10, 11, 12, 13, and/or 15.

Key Stakeholders/Advisors (Non-voting Members):

- The Member Service Representative for Zone 5 from the Ontario Federation of Agriculture;
- One staff representative from the Ontario Ministry of Agriculture, Food and Rural Affairs;
- One member appointed by farmers markets in Hamilton;
- One member appointed from the Chamber of Commerce (Flamborough, Stoney Creek, Hamilton); and,
- One member appointed from the Wentworth Junior Farmers Association.

Quorum shall be a half of the voting membership rounded up to the nearest whole number. Non-voting members are not counted in determining the number required for quorum or in determining whether or not quorum is present.

Volunteer citizen members of the ARASC will be selected as per the City of Hamilton's Policy respecting the Appointment of Citizens to the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees for the Term of Council (4 years) or until such time as a successor is appointed by Council.

A Chair and Vice-Chair of the ARASC shall be elected for the Term of Council (4 years) or until such time as a successor is appointed.

COMMITTEE SUPPORT

The following resources will be made available to the Agriculture and Rural Affairs Sub-Committee:

- The City Clerk's Division will provide legislative support and be responsible for the administrative costs of operating the Committee meetings;
- City staff liaison(s) from the Economic Development Division and Planning Division; and,
- The Committee may request information or support from other City Divisions and Departments or experts in the field to assist in formulating appropriate decisions and recommendations.

MEETINGS AND GENERAL INFORMATION

Meetings shall be held at the call of the Chair at an appropriate meeting space and time. Regular meetings will not be scheduled during the months of July, August or December, but special meetings may be called during these months.

The Terms of Reference shall be reviewed by the Committee within each Term of Council.

**Business Improvement Area Sub-Committee
Terms of Reference**

(approved by Council on February 13, 2013)

Mandate

To provide a forum for individual BIAs which encourages and facilitates receiving and sharing of information relating to initiatives affecting small businesses and business districts in the City of Hamilton.

Role of Sub-Committee Members

It shall be the role of the members of the Committee to share with their individual BIAs information received at meetings and consult with, and seek input when required, from its members on matters referred to the Sub-Committee by City Council.

Membership

1. Membership will be open to each BIA who will submit the name of one representative and one alternate to attend meetings of the Sub-Committee
2. One Member of Council
3. Membership is for the Term of Council

Election of Chair and Vice-Chair

A Chair and Vice Chair will be selected from amongst the Sub-Committee members through a nomination process.

City of Hamilton Staff

A member of the City Clerk's Legislative Team will provide legislative support to the Sub-Committee, and the Sub-Committee will follow all processes and procedures established by the City of Hamilton.

Departmental staff will act as Liaisons and provide updates and presentations on matters referred to the Sub-Committee by Council and Senior Management Team.

Meetings

Meetings will be on the second Tuesday of each month.

Reporting Structure

Sub-Committee to report to the General Issues Committee.

Corporate Policy	 Hamilton	Policy Alignment: <i>Municipal Act 2001, as amended</i>
Hamilton City Council - Appointment of Citizens to the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees		Council Approved: March 2017 Revisions: September 18, 2018; December 15, 2021 and July 8, 2022
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Eligibility

1. The Selection Process is open to all residents and business owners of the City of Hamilton who are at least 18 years of age, unless otherwise stated (Note: Additional requirements may be requested by the individual Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee, if they are governed by separate legislation, policies or mandates);
2. ***As a condition of participation within the Selection Process, you are required to provide proof that you are fully vaccinated against COVID-19 or provide proof of valid exemption satisfactory to the City in compliance with the City’s Mandatory COVID-19 Vaccination Verification Policy prior to your start date. You must acknowledge and agree to comply with Mandatory COVID-19 Vaccination Verification Policy and any future vaccine policy amendments as an ongoing condition of participation at the City of Hamilton;***
3. City Council wishes to ensure that its Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees reflect the diverse nature of the City of Hamilton’s population and encourages all residents to apply for appointment opportunities.

Public Notice

4. The City Clerk’s Office advertises for all citizen member vacancies on the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees in the Hamilton Spectator and/or relevant Community Newspapers, on the City’s website and through other appropriate methods.

Recruitment

5. Citizen membership on all of the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees, with the exception of those terms of office defined by Provincial or Federal legislation, will be to serve for a period of up to four years, which coincides with the Term of Council, or until a successor is appointed by Council.
6. Citizens are permitted to apply for membership on no more than two (2) of the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees or Sub-Committees.

Corporate Policy	 	Policy Alignment: <i>Municipal Act 2001, as amended</i>
Hamilton City Council - Appointment of Citizens to the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees		Council Approved: March 2017 Revisions: September 18, 2018; December 15, 2021 and July 8, 2022
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7. Applications and information regarding all the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees or Sub-Committees (i.e. Terms of Reference, Mandate, approximate number of meetings per year, etc.) are made available at the City Clerk's Office, at all Municipal Service Centre locations and on the City's website (www.hamilton.ca);
8. Completed application forms are to be returned to the City Clerk's Office or any of the Municipal Service Centres by the application deadline (date and time), as set out in the advertised Public Notice. Applications received after the deadline will not be considered for appointment.
9. Completed applications may be submitted by one of the following methods:
 - (a) Online Application Process on the City's website;
 - (b) Hand delivered or mailed to the Office of the City Clerk, 1st Floor, 71 Main Street West, Hamilton, Ontario, L8P 4Y5;
 - (c) Delivered to any Municipal Service Centre;
 - (d) Scanned and forwarded via e-mail to the contact person listed in the Advertisement; or,
 - (e) By Facsimile Transmission at (905) 546-2095
10. Applications shall be kept on file by the City Clerk's Office for the Term of Council. In the event of a vacancy, the Selection Committee or Interview Sub-Committee may consider interviewing applicants whose applications are on file for that term, and the appointment would be for the balance of that Term of Council only.
11. Incumbents who are eligible and willing to seek reappointment to a City Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee must reapply in the same manner as other applicants.

Information Session(s)

12. An Information Session(s) is scheduled during the beginning of the initial recruitment process (end of the previous Term of Council) and although attendance is not mandatory, attendance is strongly encouraged for new applicants.

Corporate Policy	 	Policy Alignment: <i>Municipal Act 2001, as amended</i>
Hamilton City Council - Appointment of Citizens to the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees		Council Approved: March 2017 Revisions: September 18, 2018; December 15, 2021 and July 8, 2022
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At the Information Session(s), citizens are provided with information regarding the various Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees, and are afforded the opportunity to ask questions of the Staff Liaisons.

Interested citizens may also fill out and submit an application during the Information Session(s).

Selection Process for Agencies, Boards, Commissions and Sub-Committees

13. A minimum of five (5) members of Council are appointed to the Selection Committee whose mandate will be to:
- (i) Review citizen member applications for the City’s Agencies, Boards, Commissions and Sub-Committees;
 - (ii) Applicants are shortlisted with assistance of staff, where appropriate, based on the applicant information provided;
 - (iii) Interview candidates ***that have provided confirmation of compliance with the City’s Mandatory COVID-19 Vaccination Verification Policy;***
 - (iv) Make recommendations to City Council for the appointment of citizens to the various Agencies, Boards, Commissions and Sub-Committees.

Selection Process for Advisory (Volunteer) Committees

14. A minimum of three (3) members of each Standing Committee (plus two alternates) are appointed to the respective Standing Committee Interview Sub-Committee whose mandate will be to:
- (i) Review citizen member applications for the City’s Advisory (Volunteer) Committees;
 - (ii) Applicants are shortlisted with assistance of staff, where appropriate, based on the applicant information provided;
 - (iii) Interview candidates ***that have provided confirmation of compliance with the City’s Mandatory COVID-19 Vaccination Verification Policy;***

Corporate Policy	 	Policy Alignment: <i>Municipal Act 2001, as amended</i>
Hamilton City Council - Appointment of Citizens to the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees		Council Approved: March 2017 Revisions: September 18, 2018; December 15, 2021 and July 8, 2022
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- (iv) Make recommendations to the respective Standing Committee for the appointment of citizen members to the various Advisory (Volunteer) Committees. These recommendations are ratified by Council.

Selection Process for the Indigenous Advisory Committee

15. The local Hamilton Indigenous Community leadership, will recommend to Council the appointment of residents from the Hamilton Indigenous community, to sit on the Indigenous Advisory Committee. *(Approved by Council on July 10, 2015)*

Selection Process for Agencies, Boards, Commissions, Advisory (Volunteer) Committees or Sub-Committees Established during the Term of Council

16. When an Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee is established during the Term of Council, the respective process outlined in Section **13** and **14** will be followed.

Interview Process

17. The Selection Committee or Interview Sub-Committee may, at its discretion, with the assistance of staff, shortlist candidates using the following criteria:
- (i) Related competencies;
 - (ii) Previous committee experience; and,
 - (iii) Number of citizens who applied for vacancy(ies).
18. Interviews will be conducted, where required, with those applicants who are most suited to serve on the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees or Sub-Committees as follows:
- (i) Applicants will be notified verbally or by e-mail of their interview date and time by the City Clerk's office;
 - (ii) The allotted time for each interview will be approximately ten (10) minutes in length;

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- (iii) An established set of interview questions will be developed by staff in relation to the mandated role of the Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee in consultation with the City Clerk's Office;
 - (iv) The City Clerk's Office will notify all applicants in writing once the appointments have been approved by Council;
 - (v) Applicants may be required to have background checks.
19. One (1) City of Hamilton Legislative Coordinator or Staff Liaison to the Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee, shall attend the interviews and serve as a resource person.

Roles and Responsibilities of Appointed Citizen Members of the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees

20. Citizen members of the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees are encouraged to make themselves familiar with the Terms of Reference, the Roles, Responsibilities and Expectations of New Members and mandated activities of the Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee(s) to which they are making application to.
21. Citizen members of the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees:
- (i) are required to attend and participate fully in the meetings;
 - (i) who miss more than three (3) meetings without Committee without Committee approval, may be subject to replacement on the Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee and may not be eligible for re-appointment; and
 - (ii) upon appointment, are required to sign a Committee Member Acknowledgement Form (attached hereto as Schedule "B"), provided by the City Clerk's Office, prior to attending the first meeting of the Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee to which they are appointed. Such declaration will remain on file in the Office of the City Clerk for the duration of the citizen's appointment.

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22. Citizen members of the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees are bound by the *Municipal Conflict of Interest Act* found at the following link: <https://www.ontario.ca/laws/statute/90m50> and attached as Appendix “D” to the Advisory Committee, Procedural Handbook.
23. Citizen members of the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees are to comply with the Hamilton Advisory Committee/Task Force Code of Conduct (attached hereto as Schedule “D”).

Appointment at Pleasure of Council

24. ***Despite the set term of appointment of up to 4 years or until a successor is appointed, all citizen members are appointed at the pleasure of City Council and City Council retains the right to remove any citizen appointed member at any time and for any reason, unless legislation provides otherwise.***

Filling of Vacancies

25. Vacancies on the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees can occur throughout the Term of Council, due to a member’s resignation, should a vacancy occur during the Term of Council, the following process will be followed:
- (i) The Committee member who is resigning shall do so formally in writing by providing a completed and signed copy of the Committee Member Resignation Form (attached hereto as Schedule “C”) to the appropriate Legislative Coordinator, in the City Clerk’s office, stating which Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee the Committee member is resigning from and general reasons why (the inclusion of private/personal information is not required).
 - (ii) The Committee Member Resignation Form will be placed on a Standing Committee or Council Agenda to be formally received by Committee/Council; and, consideration of the vacancy shall be forwarded to the Selection Committee or Interview Sub-Committee for review.
 - (iii) The Selection Committee or respective Interview Sub-Committee will consider whether to fill the vacancy from those applicants who applied in the initial call for applications, or to re-advertise.

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- (iv) If the vacancy occurs within 12 months of the end of the Term of Council, and there are insufficient applicants on file to fill the vacancy the vacancy will not be filled, and the quorum of the Agency, Board, Commission, Advisory (Volunteer) Committee or Sub-Committee will be adjusted accordingly.

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Schedule “A”

SELECTION COMMITTEE & INTERVIEW SUB-COMMITTEE GUIDING PRINCIPLES

1. The Selection Committee and Interview Sub-Committees are committed to fulfilling the recruitment and selection of its citizens to the City’s Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees in an open, transparent and equitable manner.
2. The Selection Committee and Interview Sub-Committees are committed to a public recruitment process which is communicated well in advance and which encourages a broad range and diverse participation of citizens, free of barriers.
3. The Selection Committee and Interview Sub-Committees are committed to a competitive recruitment process which seeks suitable candidates evaluated on interest, merit and related competencies.
4. The Selection Committee and Interview Sub-Committees are committed to unbiased decision making essential to a fair and impartial selection process.

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Schedule "B"



COMMITTEE MEMBER ACKNOWLEDGEMENT FORM

I _____ in consideration of the City of Hamilton appointing me to the _____, for the _____ term, acknowledge, undertake and agree as follows:

1. I will make all reasonable efforts to attend all meetings of this body to which I have been appointed and to participate in an impartial manner with the understanding that:
 - (a) If I am absent from more than three meetings, I may be subject to replacement on the Committee and not be eligible for re-appointment.
2. I will exercise all of the roles and responsibilities of a member of the body to which I have been appointed.
3. I shall respect and co-operate with the other Committee members and City staff.
4. I shall not disclose to any member of the public any confidential information, acquired by virtue of my position.
5. As a volunteer Committee member, I have received, read and have a general understanding of the City of Hamilton Advisory Committee Procedural Handbook **and Hamilton Advisory Committee/Task Force Code of Conduct (Schedule D of this policy)**.
6. ***I agree to comply with the City's Mandatory COVID-19 Vaccination Verification Policy as amended as an ongoing condition of participation within the Selection Process or as an Appointee to the City's Agencies, Boards, Commissions, Advisory (Volunteer) Committees and Sub-Committees.***

Dated at City of Hamilton, in the Province of Ontario this _____ day of _____, 20____.

Applicant:

Witness (Must be at least 18 years if age):

(Please Print Your Name)

(Please Print Your Name)

(Please Sign)

(Please Sign)

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Schedule "C"



Hamilton

COMMITTEE MEMBER RESIGNATION FORM

I, _____, would like to submit my resignation, effective _____, 20____, from the _____, for the following reason(s):

- My circumstances have changed and I know longer have the time to effectively participate on the Committee.
- Personal reasons.
- Other (please explain briefly):

Additional Comments (optional)

Signature

Date

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Schedule “D”

HAMILTON ADVISORY COMMITTEE/TASK FORCE CODE OF CONDUCT

Council has adopted this Code of Conduct for the guidance of Appointees to Advisory Committees and Task Forces providing recommendations to Standing Committees and to assist Appointees in performing their duties in a manner which will promote the public’s confidence in these Advisory Committees and Task Forces operating with integrity, transparency and courtesy.

It is recognized that the Code of Conduct cannot anticipate all possible fact situations in which Appointees may be called upon to exercise judgement as to the appropriate standard of conduct. When this occurs, Appointees are to ensure that their decisions maintain the Advisory Committee or Task Force’s integrity, transparency and courtesy.

This Code of Conduct does not apply to Members of Council who are subject to the Council Code of Conduct.

Failure to comply with this Code of Conduct may result in the Advisory Committee or Task Force:

- (1) requesting an apology from the Appointee; and/or
- (2) removing the Appointee from the Advisory Committee or Task Force for a portion or all of their term.

1. GOOD CONDUCT

Appointees shall act with honesty and integrity including:

- acting in a manner that contributes to the public’s confidence in the Advisory Committee or Task Force; and
- not engaging in conduct that may, or may appear to, constitute an abuse of their position as an Appointees.

2. MEETINGS

Appointees shall maintain proper control over meetings demonstrating respect for everyone who is involved in a proceeding.

Appointees are expected to attend all meetings of the Advisory Committee or Task Force. If an Appointee misses more than three meetings during their term, the Chair, after hearing and considering any explanation provided by the Appointee, may remove the Appointee from the Advisory Committee or Task Force for the remainder of their term.

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3. COLLEGIALITY

Appointees shall respect and co-operate with other Appointees and the Advisory Committee or Task Force staff.

4. GIFTS OR BENEFITS

Appointees shall not accept a gift or benefit that may appear as being offered because they are Appointees.

5. CONFIDENTIAL INFORMATION

Appointees shall not disclose to any member of the public any confidential information acquired by virtue of their position.

6. MEDIA COMMUNICATION

Except for the Chair, who may accurately communicate a recommendation or direction, Appointees shall not comment to the media.

Should the media contact an Appointee directly, the Appointee shall refer the media to the Chair or, in the absence of the Chair, to the Vice Chair.

Approved by Council on March 8, 2017
Amended by Council on September 12, 2018
Amended by Council on December 15, 2021 (Section 25 (iv))

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<p>Policy No: HR-66-21 Page 1 of 13</p>		<p>Approval: TBD</p>

<p><i>Mandatory COVID-19 Vaccination Verification Policy</i></p>	
<p>POLICY STATEMENT</p>	<p>The City of Hamilton (the City) has an obligation under Provincial legislation to take all necessary precautions to protect the health and safety of its workforce and is committed to providing excellent service to our community and to building trust and confidence in local government.</p> <p>To help reduce the risk of COVID-19 transmission, this mandatory workplace vaccination verification policy is an important measure that complements other workplace health and safety measures in place including such measures as daily health screening, masking, physical distancing, hand hygiene and enhanced cleaning.</p> <p>This policy is in line with Public Health guidance and supports the direction that vaccines provide a high level of protection against COVID-19 and related variants. Getting fully vaccinated against COVID-19 is the best defense against the virus, including the variants.</p> <p>The City is demonstrating its commitment to promoting vaccinations to ensure the health and safety of all members of its workforce and the broader City of Hamilton community.</p>
<p>PURPOSE</p>	<p>The purpose of this policy is to outline the City's requirements with regard to COVID-19 vaccination and provide direction to Employees, Members of Council and members of Council appointed committees on the requirement to receive a COVID-19 vaccination and provide proof of vaccination or a bona fide exemption.</p>
<p>SCOPE</p>	<p>This policy applies to all employees of the City of Hamilton, including full-time, part-time, permanent, temporary, casual, volunteers; and students ("Employees"), and including Members of Council</p>

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	<p>and members of Council appointed committees. New Employees will also be subject to this policy as a condition of their employment contract, volunteer position or student placement with the City of Hamilton.</p> <p>It requires Employees, Members of Council and members of Council appointed committees to be fully vaccinated against the COVID-19 virus and requires everyone to provide proof of vaccination satisfactory to the City.</p> <p>Vaccination requirements are subject to bona fide medical and human rights exceptions. All exemption requests will be reviewed on a case by case basis and are subject to the City’s accommodation process (“Approved Exemptions”).</p> <p>In addition to this policy, some workplace groups may have additional mandates or directives or reporting requirements from provincial or federal authorities.</p>
<p>DEFINITIONS</p> <p>COVID-19</p> <p>Vaccine</p> <p>Fully Vaccinated</p>	<p>The following terms referenced in this Policy are defined as:</p> <p>A virus belonging to a large family called coronavirus which includes the virus that causes the common cold and more severe disease such as Severe Acute Respiratory Syndrome (SARs) and Middle East Respiratory Syndrome (MERS-COV). The virus that causes COVID-19 is a novel coronavirus, named SARS-CoV-2.</p> <p>For the purposes of this Policy, a Vaccine is defined as a substance used to stimulate the production of antibodies and provide protection from SARS-CoV-2</p> <p>Having received the full series of a COVID-19 vaccine or combination of COVID-19 vaccines as</p>

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<p>Proof of Vaccination</p>	<p>defined by the Ontario Ministry of Health and Long-term Care and the employer.</p>
<p>Proof of Medical Exemption</p>	<p>Documentation issued by the Ontario Ministry of Health, other province or territory or international equivalent indicating individual immunization status against the COVID-19 virus.</p>
<p>Members of Council Appointed Committees</p>	<p>Written proof of a medical reason, provided by a physician or nurse practitioner in the extended class that sets out: (i) a documented medical reason for not being fully vaccinated against COVID-19, and (ii) the effective time-period for the medical reason.</p>
<p>TERMS & CONDITIONS</p>	<p>Citizen Members of Committees, Boards and Tribunals of Council who have been selected and appointed by Council</p> <p>The following apply to this Policy:</p> <p>1. Mandatory COVID-19 Vaccinations</p> <p><i>Employees</i></p> <ul style="list-style-type: none"> All City of Hamilton Employees are required to be fully vaccinated against COVID-19 to access a City facility for the purpose of conducting work, unless subject to an Approved Exemption by September 30, 2022. Employees who have not provided proof that they are fully vaccinated or an Approved Exemption satisfactory to the employer are required to participate (or continue to participate) in the City’s rapid antigen testing program until September 30, 2022. Unvaccinated Employees, and unvaccinated Employees who have provided an Approved Exemption, will only be allowed into a City facility for the purposes of working if they submit to regular antigen testing for

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	<p>COVID-19 and demonstrate a negative result, at intervals to be determined by the employer.</p> <ul style="list-style-type: none"> As a condition of employment, new hires are required to provide proof that they are fully vaccinated or provide proof of an Approved Exemption satisfactory to the employer prior to their start date. By signing the conditional offer letter, they acknowledge and agree to comply with any future vaccine policy requirements as an ongoing condition of employment at the City. <p><i>Members of Council</i></p> <ul style="list-style-type: none"> All Members of Council are required to be fully vaccinated against COVID-19, unless subject to an Approved Exemption by September 30, 2022. Unvaccinated (or non-disclosing) Members of Council will be presumed to have an Approved Exemption and shall work with the City to develop a reasonable and appropriate accommodation plan including health and safety measures to protect all Employees, up to the point of undue hardship. <p><i>Members of Council Appointed Committees</i></p> <ul style="list-style-type: none"> All members of Council appointed committees are required to be fully vaccinated against COVID-19 unless subject to an Approved Exemption by September 30, 2022. Members of Council appointed committees will be presumed to have an Approved Exemption and shall work with the City to develop a reasonable and
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	<p><i>appropriate accommodation plan including health and safety measures to protect all Employees and Committee Members, up to the point of undue hardship</i></p> <ul style="list-style-type: none"><i>All new members of Council appointed committees are required to provide proof that they are fully vaccinated or provide proof of Approved Exemption satisfactory to the City prior to their start date. By participating in the Council appointed committee, they acknowledge and agree to comply with any future vaccine policy requirements as an ongoing condition of participation.</i> <p>2. Non-compliance with the Mandatory COVID-19 Vaccination Verification Policy</p> <p><i>Employees</i></p> <ul style="list-style-type: none">In accordance with City Human Resources policies, collective agreements and applicable legislation, directives, and policies, any non-compliance with the requirements of the Mandatory COVID-19 Vaccination Verification Policy will result in disciplinary measures, up to and including termination of employment. <p><i>Members of Council</i></p> <ul style="list-style-type: none"><i>Unvaccinated Members of Council shall comply with their approved accommodation plan from the City and failure to comply may result in a complaint to the Integrity Commissioner.</i> <p><i>Members of Council Appointed Committees</i></p> <ul style="list-style-type: none"><i>Members of Council appointed committees will be presumed to have an</i>
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Approved Exemption and shall work with the City to develop a reasonable and appropriate accommodation plan including health and safety measures to protect all Employees and Committee Members, up to the point of undue hardship

3. Operational Area Exemptions:

- Employees in the following three operational areas will be subject to the following differential terms and conditions under the policy as follows:
 - Employees at the Lodges remain required to provide proof of vaccination in order to attend at work, with those employees failing to provide such evidence to remain on an unpaid leave of absence until September 30, 2022, and with employees at the Lodges required to continue participation in daily Rapid Antigen Testing until further notice;
 - Employees at Hamilton Paramedic Services who have failed to disclose proof of vaccination remain required to participate in Rapid Antigen Testing prior to commencing each shift until September 30, 2022;
 - Employees at the Red Hill Child Care Centre who have failed to disclose proof of vaccination remain required to participate in Rapid Antigen Testing three times per week until September 30, 2022;
- Should circumstances warrant, or should the City receive guidance, direction, or recommendations from public health, the Province, or the Federal government, the City may alter the rapid antigen testing

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	<p>process in place in any one of the above operational areas (i.e. by changing testing frequency, the method of testing, reporting requirements, etc.) with appropriate notice to Employees.</p> <ul style="list-style-type: none"> • For clarity, Rapid Antigen Testing will only continue beyond September 30, 2022 for employees at the Lodges (who will continue to undergo daily testing until further notice). The Rapid Antigen Testing Program will otherwise end for all other employees after September 30, 2022. <p>4. Providing Proof of COVID-19 Vaccination Status</p> <ul style="list-style-type: none"> • Employees, Members of Council and members of Council appointed committees are required to provide proof of their vaccination series approved by Health Canada and recommended by Ontario Ministry of Health by providing one of the following: <ul style="list-style-type: none"> ○ Proof of COVID-19 vaccine administration as per the following requirements: <ul style="list-style-type: none"> ○ Proof of all required doses of a COVID-19 vaccine approved by Health Canada and recommended by Ontario Ministry of Health. <p>5. Providing Proof of An Approved Exemption</p> <ul style="list-style-type: none"> • The City will comply with its human rights obligations and accommodate Employees, Members of Council and members of Council appointed committees who are legally entitled to accommodation.
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	<ul style="list-style-type: none"> • Exemptions will be made for grounds protected by the Ontario Human Rights Code which includes confirmed medical reasons. Human Resources will assist with accommodation questions, concerns and requests. • Employees, Members of Council and members of Council appointed committees are required to provide proof of their medical exemption by providing one of the following: <ul style="list-style-type: none"> ○ Written proof of a medical reason, provided by either a physician or nurse practitioner in the extended class that sets out: <ul style="list-style-type: none"> - That the person cannot be vaccinated against COVID-19 and; - The effective time period for the medical reason (i.e., permanent or time- limited). • The City will work with those who receive an exemption to connect them with the appropriate resources to develop a reasonable and appropriate accommodation plan including health and safety measures to protect all workers, up to the point of undue hardship. <p>6. Vaccination Status Reporting and Documentation</p> <ul style="list-style-type: none"> • Vaccination status information will be collected and protected in accordance with relevant legislation. <p>7. Access to the Rapid Antigen Test Program</p> <ul style="list-style-type: none"> • In the event that an Employee cannot participate in the City’s rapid antigen testing program as a result of the inability of the City
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	<p>to provide rapid test kits, the Employee will be placed upon a paid leave of absence until they are able to meet the requirements of the next test date in the program (i.e. the City has made test kits available).</p> <ul style="list-style-type: none"> • Employees are required to return to work at the earliest possible opportunity once testing kits are made available, and the Employee can test, on the next applicable testing date. <p>8. Access to COVID-19 Vaccination Clinics</p> <ul style="list-style-type: none"> • Reasonable arrangements will be made to allow for Employees to attend COVID-19 vaccination clinics during work time. • Employees must have approval from their supervisor in advance before attending a clinic during work time. • All efforts should be made to allow the Employee to use time at the beginning or end of their shift day or to extend lunch and break times as operationally feasible with work schedules to attend vaccination clinics. • Where Employees are unable to attend a vaccination clinic during their regularly scheduled work hours, the City will compensate staff for a period of 30 minutes outside of work time to receive a vaccination subject to any limitations under a collective agreement or policy. <p>9. Continued Adherence to Public Health Measures</p> <ul style="list-style-type: none"> • Fully vaccinated Employees are required to practice Public Health measures to control the spread of COVID-19. • Employees must adhere to the City’s health and safety protocols while in the workplace, including daily health screening, mandatory
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	<p>masking, physical distancing, hand hygiene, enhanced cleaning and the use of Personal Protective Equipment as required by their position.</p> <ul style="list-style-type: none"> • In the event that circumstances change, such that Public Health, the province, the federal government, and/or other sources of information indicate that reintroduction of elements of this Policy are required or recommended, such changes will be proposed and brought before Council for Direction.
<p>RESPONSIBILITIES (if applicable)</p>	<p>The following positions and/or departments are responsible for fulfilling the responsibilities detailed in this Policy as follows:</p> <p>Management/Supervisors</p> <ul style="list-style-type: none"> • Follow and comply with any federal or provincial mandates or directives regarding the vaccination of Employees • Continue to enforce workplace precautions that limit the spread of COVID-19 virus • Ensure that Employees are aware of the importance of getting vaccinated against COVID-19 • Provide Employees with access to information on COVID-19, health and safety precautions, and on the efficacy of the COVID-19 vaccine • Provide Employees with information on location and scheduling of vaccination clinics when available • Where feasible, support time from regular duties for staff to attend vaccination clinics in accordance with relevant collective

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	<p>agreement language and/or Corporate Policy</p> <p>Employees</p> <ul style="list-style-type: none"> • Continue to follow health and safety protocols to ensure personal safety and prevent the spread of COVID-19 before and after vaccination. • Remain informed about COVID-19 and COVID-19 vaccination as it relates to your role, personal health and/or professional requirements. • Adhere to any additional mandates or directives or reporting requirements from provincial or federal authorities. • Identify opportunities to obtain COVID-19 vaccination through community clinics or from health care professionals • If additional booster doses of the COVID-19 vaccine are required, ensure subsequent doses are also received. • Employees not fully vaccinated against COVID-19 or who have not yet disclosed their vaccination status are required to participate (or continue to participate) in the City's rapid antigen testing program until September 30, 2022 (when the program will come to an end, with the exception of the operational areas identified where testing will continue beyond this date). • In the event of an interruption to the City's rapid antigen testing program (e.g. due to a lack of supply) and an employee is placed on a paid leave of absence; the employee is expected to return to active duty at the employer's determination once testing kits are made available, and the employee can
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Corporate Human Resources Policy Health, Safety and Wellness	 Hamilton	Content Updated: 2022-05-25 Supersedes: Pandemic Response Procedure - Staff Vaccination for COVID-19 - COH-P-018
Policy No: HR-66-21 Page 12 of 13		Approval: TBD

	<p>test, on the next applicable testing date.</p> <p>Human Resources</p> <ul style="list-style-type: none"> • Assist management with any labour relations and health and safety issues arising from application of this policy including liaising directly with Union leadership. • Create procedure for collecting and storing documentation on employee vaccination status. • Assist management with accommodation questions, concerns and requests. <p>Occupational Health Nurse</p> <ul style="list-style-type: none"> • Provide information and guidance to leaders and employees on COVID-19 vaccination. • Ensure that any records of COVID-19 vaccination held by the City are stored and used in compliance with privacy legislation and corporate policies. <p>City Clerk</p> <ul style="list-style-type: none"> • <i>Confirm that all members of Council appointed committees are fully vaccinated or have an Approved Exemption.</i> • <i>Create necessary policies and procedures for removing/replacing members of Council appointed committees as necessary in accordance with this policy.</i> • <i>Create necessary policies and procedures to ensure all new members of Council appointed committees provide proof of full vaccination or an Approved Exemption.</i>
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Corporate Human Resources Policy Health, Safety and Wellness	 Hamilton	Content Updated: 2022-05-25 Supersedes: Pandemic Response Procedure - Staff Vaccination for COVID-19 - COH-P-018
Policy No: HR-66-21		
Page 13 of 13		Approval: TBD

HISTORY	<p>The following stakeholders were consulted in the creation or revisions made to this Policy:</p> <p>City of Hamilton’s Senior Leadership Team External Legal Counsel Human Resources Leadership Team Union Representatives from each of the City’s unions.</p> <p>This policy replaces the former policy named Mandatory COVID-19 Vaccination Verification Policy dated 2022-01-06.</p> <p>This policy was amended by Council on May 25, 2022, to extend the compliance date to September 30, 2022 for Employees</p> <p><i>This Policy was amended by Council on (Date) to provide staff direction on the administration of the Policy for Members of Council and Council Appointees.</i></p>
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HAMILTON-WENTWORTH CATHOLIC DISTRICT SCHOOL BOARD (HWCDSB) LIAISON COMMITTEE

MANDATE

To strengthen the relationship among City Council and the Hamilton-Wentworth Catholic District School Board by:

Addressing issues affecting governing bodies

Promoting increased co-operation, synergies and efficiencies between City Council and each School Board

Exploring common interests (i.e. transportation, environmental issues, aging issues)

Develop a better understanding and co-ordination of services (i.e. recreation, social and public health issues)

Increasing and maintaining regular communication

Speaking in a united voice on issues of common interest to other levels of government

Working in a spirit of co-operation to further the mission of each of the school boards and the city.

MEMBERSHIP

Total of three Trustees (Chair of the Board and two Trustees)

Total of **three** Members of Council (Mayor and **two members** of Council)

Duration: The term of membership on the committee shall be consistent with the practices of each of the respective bodies.

Reporting to: General Issues Committee

Stipend: None

Meeting Schedule: Meetings will be held at regularly scheduled dates agreed to by members. The City and the Boards can request a meeting outside these times.



JOINT CITY OF HAMILTON AND HAMILTON-WENTWORTH DISTRICT SCHOOL BOARD (HWDSB) LIAISON COMMITTEE

TERMS OF REFERENCE

MEMBERSHIP

Total of three Trustees (Chair of the Board and two Trustees)

Total of **three** Members of Council (Mayor and **two members** of Council)

Total of two HWDSB Secondary Students (non-voting, rotating basis, invited by HWDSB)

City and Board Staff will be invited to attend meetings as required.

The term of membership on each committee shall be consistent with the practices of each of the respective bodies.

CO-CHAIR

The Co-Chair of each meeting will alternate between the Mayor of the City of Hamilton and the Chair of HWDSB.

MANDATE

- (1) To strengthen the relationship between the City and HWDSB
- (2) To assist in addressing issues affecting the governing bodies
- (3) To promote increased co-operation, synergies and efficiencies between City Council and HWDSB
- (4) To explore common interests
- (5) To better understand and co-ordinate services
- (6) To increase and maintain regular communication
- (7) To work in a spirit of co-operation to further the mission of the City and HWDSB

Page 2

- (8) Joint initiatives and outcomes will align
- (9) To develop ongoing collaborative and timely public communication strategies reflective of common purpose, work in progress and outcomes
- (10) To monitor the work developed by staff on the Joint Property Asset Committee (JPAC) as outlined in the Terms of Reference of JPAC and attached as Appendix A.

REPORTING MECHANISM

The members of the Liaison Committee will be responsible to report back to their respective reporting structures.

DECISION MAKING PROCESS

Decisions will be arrived at through consensus and if a vote is required a majority from each participating body's approval is required.

MEETING FORMAT

- 1. Four business days written notice will be given of meetings. Meetings will be held at regularly scheduled dates agreed to by members. The City and HWDSB can request a meeting outside these times. Meetings will be held at either City Hall or the HWDSB Board Office.
- 2. Agenda

The agenda will be mutually developed and agreed upon by the City and HWDSB with items that fall within the mandate of the Liaison Committee.
- 3. Resources

The Committee will be clerked by the City of Hamilton.
- 4. Quorum

Quorum will be four (4) made up of two trustees and two councillors.

SUB COMMITTEES
2022-2026

SUB COMMITTEE	MEMBERSHIP REQUIRED
Accessibility Transit Services Review Sub Committee	3 Councillors
Advisory Committee for Persons with Disabilities	1 Councillor <i>(18 Citizen Members are also appointed)</i>
Agricultural and Rural Affairs Sub-Committee (Voting Members)	<p style="text-align: center;">6 Councillors (Wards 9, 10, 11, 12, 13, and/or 15) <i>(Three members appointed by the Hamilton-Wentworth Federation of Agriculture; One member appointed by the Wentworth-Brant Christian Farmers Association; One member appointed by the Hamilton-Wentworth Women's Institute; One rotating member appointed by the Agricultural Societies (Rockton, Binbrook and Ancaster); Two members appointed by the Wentworth Soil & Crop Improvement Association; Up to five citizen members appointed by Council)</i></p> <p>Non-Voting Members: <i>The Member Service Representative for Zone 5 from the Ontario Federation of Agriculture; One staff representative from the Ontario Ministry of Agriculture, Food and Rural Affairs; One member appointed by farmers markets in Hamilton; One member appointed from the Chamber of Commerce (Flamborough, Stoney Creek, Hamilton); and, one member appointed from the Wentworth Junior Farmers Association.</i></p>
Airport Sub-Committee	8 Councillor and the Mayor (ex-officio)
Business Improvement Area Sub-Committee	1 Councillor <i>(one Member from each BIA are also appointed)</i>
Cleanliness and Security in the Downtown Core Task Force	Ward 2 Councillor & 1 additional Councillor
Community Benefits Protocol Sub-Committee <i>Staff were directed to prepare the terms of reference on June 27, 2018</i>	2 Councillors
Cross-Melville District Heritage Committee (Dundas)	Ward 13 Councillor <i>(3 citizens are also appointed (1 ecclesiastical representative from the District; 1 member of the Hamilton Municipal Heritage Committee (Dundas); 1 member of the Municipal Heritage Permit Review Sub-Committee (Dundas))</i>
Development Charges Stakeholders Sub-Committee	6 Councillors
Expanding Housing and Support Services For Women, Non-Binary and Transgender Community Sub-Committee	<p style="text-align: center;">4 Councillors</p> <p><i>(two (2) representatives from the Women's Housing Planning Collaborative (WHPC); three (3) Council appointed volunteer citizens with experience and interest in homelessness prevention and the provision of affordable housing; and, a minimum of one (1) staff representative from the City's Homelessness Policy and Programs Team, Housing Services Division will also sit on the Sub-Committee as a non-voting member)</i></p>

Appendix '1' to Item 2 of AF&A Report 22-013

SUB COMMITTEE	MEMBERSHIP REQUIRED
Facility Naming Sub-Committee	4 Councillors
Glanbrook Landfill Co-ordinating Committee	1 Councillor <i>(4 Citizen Members are also appointed)</i>
Governance Review Sub-Committee	6 Councillors
Grants Sub-Committee	6 Councillors
Greater Bay Area Sub-Committee	Mayor's from the City of Hamilton and City of Burlington Two members of Burlington City Council Two members of Hamilton City Council
Hamilton Future Fund Board of Governors	5 Councillors <i>(13 Citizen Members are also appointed)</i>
Hamilton Municipal Heritage Committee	1 Councillors <i>(11 Citizen Members - 1 resident of the former Town of Ancaster; 1 resident of the former Town of Dundas; 1 resident of the former Town of Flamborough; 1 resident of the former Town of Glanbrook; 1 resident of the former Town of Stoney Creek; 4 residents of the former City of Hamilton and 2 citizens at large are also appointed)</i>
Hamilton - Oshawa Port Authority - City of Hamilton Liaison Committee	Mayor and 3 Councillors <i>(Chair of Port Authority Board, 2 members of the Port Authority Board are also members)</i>
Hamilton Water Sub-Committee	4 Councillors
Hamilton-Wentworth Catholic District School Board (HWCD SB) Liaison Committee	Mayor and 2 Councillors <i>(3 representatives of the HWCD SB (Chair of the Board and 2 Trustees) are also members)</i>
Hamilton-Wentworth District School Board (HWDSB) Liaison Committee	Mayor and 2 Councillors <i>(3 representatives of the HWDSB (Chair of the Board and 2 Trustees) and 2 HWDSB Secondary Students (non-voting, rotating basis, invited by the HWDSB), are also members)</i>
HMRF/HWRF Pension Administration Committee	3 Councillors <i>(3 HMRF representatives and 3 HWRF representatives are also members)</i>
Interview Sub-Committee to the Audit, Finance and Administration Committee	3 Councillors (Minimum) & 2 Alternates <i>(members of the Audit, Finance and Administration Committee)</i>
Interview Sub-Committee to the Board of Health	3 Councillors (Minimum) & 2 Alternates <i>(members of the Board of Health)</i>
Interview Sub-Committee to the Emergency and Community Services Committee	3 Councillors (Minimum) & 2 Alternates <i>(members of the Emergency and Community Services Committee)</i>
Interview Sub-Committee to the General Issues Committee	3 Councillors (Minimum) & 2 Alternates <i>(members of the General Issues Committee)</i>
Interview Sub-Committee to the Public Works Committee	3 Councillors (Minimum) & 2 Alternates <i>(members of the Public Works Committee)</i>

Appendix 'I' to Item 2 of AF&A Report 22-013

SUB COMMITTEE	MEMBERSHIP REQUIRED
Light Rail Transit Sub-Committee	<p>Mayor and Councillors <i>(from Wards along the B-Line corridor, in addition to Councillors with various interests (a Councillor of the mountain and the rural/suburban wards as well) (unlimited))</i> <i>(key stakeholders from but not limited to the Chamber of Commerce, BIA's, HHHBA, institutional and business partners are invited as advisors)</i></p>
Mayor's Intelligent Community Forum Task Force	<p>Mayor and 2 Councillors <i>(community partners and experts, to include McMaster University, Mohawk College, Hamilton Health Sciences, St. Joseph's Healthcare Hamilton, and Innovation Factory, are also members)</i></p>
Non-Union Compensation Sub-Committee	<p>3 Councillors</p>
Open for Business	<p>6 Councillors <i>(Representatives from the Hamilton-Halton Home Builders Association and the Hamilton, Flamborough and Stoney Creek Chambers of Commerce, the Business Improvement Area Sub-Committee (BIASC) and the Realtors Association of Hamilton-Burlington and the Hamilton Burlington Society of Architects, will participate as ex-officio, non-voting as key advisors of the Sub-Committee)</i></p>
Procurement Sub-Committee	<p>4 Councillors <i>(3 industry representatives with 1 representative from the OGCA, 1 representative from the HHCA and 1 representative from the architectural association, are also members (voting members). The membership shall be amended as required, to include representation from providers of goods and services to the City, as appropriate)</i></p>
School Board Properties Sub-Committee	<p>6 Councillors</p>
Selection Committee for Agencies, Boards and Sub-Committees	<p>8 Councillors</p>
Transit Area Rating Review Sub-Committee	<p>6 Councillors <i>(3 three urban and 3 three suburban Councillors)</i></p>
Truck Route Sub-Committee	<p>6 Councillors</p>
Waste Management Sub-Committee	<p>3 Councillors <i>(2 citizen members are also appointed)</i></p>
Wentworth Lodge Heritage Trust Fund	<p>Ward 13 Councillor <i>(1 Wentworth Lodge resident, 1 Alternate Lodge Resident, 1 Wentworth Lodge Family Council representative and 1 Dundas Senior Citizens' Club representative are members)</i></p>
West Harbour Development Sub-Committee	<p>4 Councillors</p>



EMERGENCY & COMMUNITY SERVICES COMMITTEE REPORT 22-011

1:30 p.m.
Thursday, July 7, 2022
Council Chambers
Hamilton City Hall
71 Main Street West

Present: Councillors B. Clark (Chair), N. Nann and T. Jackson

Regrets: Councillors E. Pauls, S. Merulla and T. Whitehead – Personal

Also Present: Councillor J.P. Danko

THE EMERGENCY & COMMUNITY SERVICES COMMITTEE PRESENTS REPORT 22-011 AND RESPECTFULLY RECOMMENDS:

1. Consent Items (Item 7.1-7.3)

That the following Consent Items, but received, as presented:

- (a) Canada Mortgage and Housing Corporation (CMHC) Rapid Housing Initiative (RHI) Update (HSC22027) (City Wide) (Item 7.1)
- (b) Community Housing Renewal Implementation (HSC22034) (City Wide) (Item 7.2)
- (c) Interview Sub-Committee for the Emergency and Community Services Committee Minutes - April 5, 2022 (Item 7.3)

2. Relief for Not-For-Profit Housing Developments (Added Item 9.1)

That the General Managers of the Healthy and Safe Communities and Corporate Services Departments work with other City staff to review the challenges facing not-for-profit housing developments, such as the Hamilton is Home project at 60 Caledon Avenue, regarding the requirements of the City of Hamilton Development Charges By-Law and of Federal funding and report back to the August 12, 2022 Council meeting with a viable solutions for that project and other known or expected in-year requests.

3. Community Warning System for Persons with Special Needs (Added Item 9.3)

WHEREAS, we have all become aware of the recent tragic death of Draven Graham, an 11-year-old child on the autistic spectrum, after walking away from the family home;

WHEREAS, the Amber Alert has been very effective in announcing to the public children who have been abducted; and

WHEREAS, a similar alert program for those who have special needs who leave family custody would seem to be warranted in the Province of Ontario;

THEREFORE, BE IT RESOLVED:

That the Council of the City of Hamilton request that the Province of Ontario explore the feasibility of developing and implementing a community warning program similar to, or part of, Amber Alert, for those persons of special needs or circumstance who leave the care givers or locations and potentially put themselves at risk.

4. Recreation Master Plan – Facility Provision Decision Making Framework (HSC22014(a)) (City Wide) (Item 10.1)

That the Facility Provision Decision-Making Framework as detailed in Appendix “A” to Emergency and Community Services Committee Report 22-011, be approved as an evidence-based decision framework to guide the identification and prioritization of capital projects within the Recreation Master Plan.

5. Additional Funding for CityHousing Hamilton's Residential Conversion at 263 Main St. E. (HSC22028) (Ward 3) (Item 10.2)

- (a) That up to \$182,000 in unused funding from project ID 6731841101 “Coordinated Access System” be used to fund additional costs in CityHousing Hamilton’s project to convert commercial space to deeply affordable housing at 263 Main St. E.;
- (b) That the General Manager of the Healthy and Safe Communities Department, or their designate, be authorized and directed to enter into an agreement with CityHousing Hamilton for up to \$182,000 to fund the remaining costs in its project to convert commercial space to deeply affordable housing at 263 Main St. E., with terms and conditions to the satisfaction of the General Manager of the Healthy and Safe Communities Department, and in a form satisfactory to the City Solicitor; and,
- (c) That the remaining balance in project ID 6731841101 “Coordinated Access System” be transferred as follows and the project be closed:
 - (i) \$171, 000 to Social Housing Apartment Improvement Program (SHAIP) Admin Project ID 6731841704; and

(ii) Balance to Social Housing Transition Reserve #112244.

6. Adaptation and Transformation of Services for People Experiencing Homelessness Update 5 (HSC20020(e)) (City Wide) (Item 10.3)

That Report HSC22020(e), respecting Adaptation and Transformation of Services for People Experiencing Homelessness Update 5, be received.

7. Financial Support for CityHousing Hamilton to Remove and Replace Existing Cameras at 44 Martha Street and 799-847 Roxborough Avenue, (A11C - Martha St/Roxborough Avenue), Hamilton, Ontario (Item 11.1)

WHEREAS, CityHousing Hamilton owns the property located at 44 Martha Street and 799-847 Roxborough Avenue, (A11C - Martha St/Roxborough Avenue), Hamilton, Ontario - a 69-unit multi-residential townhouse complex built in 1968;

WHEREAS, CityHousing Hamilton endeavours to increase the monitoring of safety and security issues at the complex;

WHEREAS, the existing camera system at the property requires a digital video recorder (DVR) to be in locked cabinets in the basement of individual tenant units;

WHEREAS, the placement of the DVR is within tenant units, access may not always be easily available when needed;

WHEREAS, the nature of the existing camera system is disruptive to the lives of tenants when staff obtain camera footage required for safety and security purposes, as units must be entered to obtain camera footage;

WHEREAS, CityHousing Hamilton has obtained a quote to remove five old cameras and install five new cameras at the property. The new cameras will be wireless and will be able to stream video to the Building Attendant's unit, where there is currently a DVR unit. Furthermore, the signal will be able to be streamed to a site office at 10 St Andrews Dr. therefore eliminating disruption to tenants and provide immediate access to the footage;

WHEREAS, CityHousing Hamilton commits to overseeing the installation, work and ongoing maintenance of the camera system;

WHEREAS, CityHousing Hamilton has no budgeted amount for this safety and security improvement;

WHEREAS, the scope of work requires \$6960.50 (plus HST) to complete the removal and upgrade of the cameras;

THEREFORE, BE IT RESOLVED:

- (a) That the proposed cost of \$6960.50 to an upset limit of \$7,500 (for contingency for unknowns) for the removal and upgraded replacement of the camera system at 44 Martha Street and 799-847 Roxborough Avenue, (A11C - Martha St/Roxborough Avenue), Hamilton, Ontario, to be funded from the Ward 4 Capital Reinvestment Account 3302109400; and,
- (b) That the Mayor and City Clerk be authorized and directed to execute any required agreement(s) and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

8. Funding for Roots to Justice Initiative at J.C. Beemer Park (Item 11.2)

WHEREAS, the COVID-19 pandemic has deepened social and economic disparities among equity seeking groups;

WHEREAS, young people have struggled from the negative impacts of the pandemic regarding mental health, isolation, and from inconsistent access to social, cultural and recreational programs;

WHEREAS, the pandemic has exacerbated food insecurity and the cost of food is continues to rise;

WHEREAS, the Roots to Justice program engages youth and racialized residents to cultivate and care for their community and environment, share knowledge gained through programming with their communities to build resilience and community care while also supporting households in need to be able to access food and other supports they would otherwise be unable to obtain;

WHEREAS, this program, in partnership with the Hamilton Centre for Civic Inclusion, will provide hand-on programming directed toward, but not limited to, low-income racialized youth in the Hamilton core to build skills and knowledge around gardening and food security; and

WHEREAS, Roots to Justice has met with City of Hamilton staff to develop a plan for the implementation and maintenance of garden boxes at J.C. Beemer Park;

THEREFORE, BE IT RESOLVED:

- (a) That \$5000 be allocated from the Ward 3 Bell Cell Tower Account Number 3301609603 to the Hamilton Centre for Civic Inclusion as the fiscal sponsor for the implementation of the Roots to Justice initiative at J.C. Beemer Park; and
- (b) That the Mayor and City Clerk be authorized and directed to execute any required agreement(s) and ancillary documents, with such terms and conditions in a form satisfactory to the City Solicitor.

9. Appointment to Seniors Advisory Committee (Item 14.1)

That the recommendation respecting an Appointment to the Seniors Advisory Committee be approved and released publicly following approval by Council.

FOR INFORMATION:

(a) APPROVAL OF AGENDA (Item 2)

The Committee Clerk advised the following changes to the agenda:

6. DELEGATION REQUESTS

- 6.2(c) Bill Johnston, Hamilton Alliance for Tiny Shelters (HATS), respecting an update on HATS and HATS research (for today's meeting)
- 6.2(d) Paula Whitlow, Hamilton-Wentworth Chapter Native Women Inc., respecting Hamilton Alliance for Tiny Shelters (for today's meeting)
- 6.3. Nancy Silva Khan, New Democratic Party Federal Riding Association, respecting a motion to request the Province of Ontario develop and implement a community warning program similar to Amber Alert (for today's meeting)

The agenda for the July 7, 2022 Emergency and Community Services Committee meeting was approved, as amended.

(b) DECLARATIONS OF INTEREST (Item 3)

There were no declarations of interest.

(c) APPROVAL OF MINUTES OF PREVIOUS MEETING (Item 4)

The Minutes of the June 16, 2022 meeting of the Emergency and Community Services Committee, were approved, as presented.

(d) DELEGATION REQUESTS (Item 6)

The following Delegation Requests, were approved, for today's meeting:

- (i) Graham Cubitt and Tricia Lewis, Hamilton is Home, respecting our upcoming projects, beginning in 2022-2024 (Item 6.1(a));
- (ii) Lori-Anne Gagne, Caledon Community Collaborative, respecting proposed affordable housing development at 60 Caledon Avenue (Item 6.1(b))

- (iii) Ed Fothergill, Hamilton Alliance for Tiny Shelters, respecting an update of the Hamilton Alliance for Tiny Shelters (HATS) initiative (Item 6.2(a))
- (iv) Tony Stortz, Hamilton Alliance for Tiny Shelters, respecting Person with Lived Experience Report (Item 6.2(b))
- (v) Bill Johnston, Hamilton Alliance for Tiny Shelters (HATS), respecting an update on HATS and HATS research (Added Item 6.2(c))
- (vi) Paula Whitlow, Hamilton-Wentworth Chapter Native Women Inc., respecting Hamilton Alliance for Tiny Shelters (Added Item 6.2(d))
- (vii) Nancy Silva Khan, New Democratic Party Federal Riding Association, respecting a motion to request the Province of Ontario develop and implement a community warning program similar to Amber Alert (Added Item 6.3)

(e) PUBLIC HEARINGS / DELEGATIONS (Item 9)

- (i) Tricia Lewis and Sarah Borde, Hamilton is Home, respecting their upcoming projects, beginning in 2022-2024 (Added Item 9.1(a))**

Tricia Lewis and Sarah Borde, Hamilton is Home, addressed the Committee respecting their upcoming projects, beginning in 2022-2024, with the aid of a presentation.

- (ii) Lori-Anne Gagne, Caledon Community Collaborative, respecting proposed affordable housing development at 60 Caledon Avenue (Added Item 9.1(b))**

Lori-Anne Gagne, Caledon Community Collaborative, addressed the Committee respecting proposed affordable housing development at 60 Caledon Avenue, with the aid of a presentation.

- (iii) Ed Fothergill, Hamilton Alliance for Tiny Shelters, respecting an update of the Hamilton Alliance for Tiny Shelters (HATS) initiative (Added Item 9.2(a))**

Ed Fothergill, Hamilton Alliance for Tiny Shelters, addressed the Committee respecting an update of the Hamilton Alliance for Tiny Shelters (HATS) initiative, with the aid of a presentation and a handout.

(iv) Tony Stortz, Hamilton Alliance for Tiny Shelters, respecting Person with Lived Experience Report (Added Item 9.2(b))

Tony Stortz, Hamilton Alliance for Tiny Shelters addressed the Committee respecting Person with Lived Experience Report, with the aid of a presentation and a handout.

(v) Bill Johnston, Hamilton Alliance for Tiny Shelters (HATS), respecting an update on HATS and HATS research (Added Item 9.2(c))

Bill Johnston, Hamilton Alliance for Tiny Shelters (HATS), addressed the Committee respecting an update on HATS and HATS research, with the aid of a presentation.

(vi) Paula Whitlow, Hamilton-Wentworth Chapter Native Women Inc., respecting Hamilton Alliance for Tiny Shelters (Added Item 9.2(d))

Paula Whitlow, Hamilton-Wentworth Chapter Native Women Inc., addressed the Committee respecting Hamilton Alliance for Tiny Shelters.

(vii) Nancy Silva Khan, New Democratic Party Federal Riding Association, respecting a motion to request the Province of Ontario develop and implement a community warning program similar to Amber Alert (Added Item 9.3)

Nancy Silva Khan, New Democratic Party Federal Riding Association, addressed the Committee respecting a motion to request the Province of Ontario develop and implement a community warning program similar to Amber Alert.

The following Delegations, were received:

- (a) Tricia Lewis and Sarah Borde, Hamilton is Home, respecting our upcoming projects, beginning in 2022-2024 (Added Item 9.1(a));
- (b) Lori-Anne Gagne, Caledon Community Collaborative, respecting proposed affordable housing development at 60 Caledon Avenue (Added Item 9.1(b))
- (c) Ed Fothergill, Hamilton Alliance for Tiny Shelters, respecting an update of the Hamilton Alliance for Tiny Shelters (HATS) initiative (Added Item 9.2(a))
- (d) Tony Stortz, Hamilton Alliance for Tiny Shelters, respecting Person with Lived Experience Report (Added Item 9.2(b))
- (e) Bill Johnston, Hamilton Alliance for Tiny Shelters (HATS), respecting an update on HATS and HATS research (Added Item 9.2(c))

- (f) Paula Whitlow, Hamilton-Wentworth Chapter Native Women Inc., respecting Hamilton Alliance for Tiny Shelters (Added Item 9.2(d))
- (g) Nancy Silva Khan, New Democratic Party Federal Riding Association, respecting a motion to request the Province of Ontario develop and implement a community warning program similar to Amber Alert (Added Item 9.3)

For further disposition of this matter, refer to Items 2 and 3.

(f) GENERAL INFORMATION / OTHER BUSINESS (Item 13)

(i) Amendments to the Outstanding Business List (Item 13.1)

The following amendments to the Outstanding Business List, were approved:

- (a) Items Considered Complete and to be Removed
 1. Canada Mortgage and Housing Corporation (CMHC) Rapid Housing Initiative (RHI) (HSC20056(a)) (City Wide)
Item on OBL: 210D
Addressed as Item 7.1 on today's agenda
 2. Transitional Funding to Secure Short Term Housing Project
Item on OBL: 21-H
Addressed as Item 7.1 on today's agenda

(g) PRIVATE AND CONFIDENTIAL (Item 14)

The Emergency and Community Services Committee determined that it was not necessary to move into Closed Session respecting Item 14.1.

For disposition of this matter, refer to Item 9.

(h) ADJOURNMENT (Item 15)

There being no further business, the Emergency and Community Services Committee was adjourned at 4:25 p.m.

Respectfully submitted,

Councillor B. Clark
Chair, Emergency and Community Services
Committee

Tamara Bates
Legislative Coordinator
Office of the City Clerk

Recreation Master Plan: Facility Provision Decision-Making Framework Summary

Overview

The Recreation Master Plan is currently in development. One component of the Plan is the identification of short and long-term facility requirements, such as community recreation centres, aquatic facilities, sports fields, and courts. Many of these amenities are needed to address growth-related needs, while others respond to emerging demands and make it easier for residents to access the services they require. An evidence-based decision framework based on demonstrated needs and projected future requirements will be used to guide the identification and prioritization of capital projects. The framework uses provision targets (population-based and/or service-based) that allow for long-term predictability. It also prioritizes equity and access across the entire city to ensure that areas of highest need are prioritized.

Decision-Making Framework

For most facility types, the model involves four steps (shown below):

1. The needs assessment process starts with the examination of the current state of facility assets to establish a basis for identifying facility renewal or repurposing opportunities (“Opportunity Scan”).
2. This is followed by the creation of provision targets to determine geographic gaps (across and within each of the nine Recreation Planning Areas), growth-related needs (to 2051), and opportunities to improve and optimize existing facilities.
3. Strategies and priorities are then examined to determine the preferred approach for delivering on needs.
4. And finally, a fluid implementation stage begins as the City puts the Master Plan into action. In some cases, project-specific feasibility studies may be required prior to significant facility expansion, re-purposing, or development projects.

Recreation Master Plan: Facility Provision Decision-Making Framework



Factors Considered

The framework considers a range of factors and is aligned with the Master Plan’s guiding principles. Specific inputs considered throughout include:

- a. current supplies and levels of provision;
- b. facility size, capacity, condition, accessibility, level of amenity, utilization;
- c. public and stakeholder input (facility demand, willingness to travel, etc.);
- d. geographic distribution;
- e. areas and timing of future growth;
- f. recreation participation trends;
- g. socio-demographic trends and under-served populations;
- h. availability of other notable facility and service providers;
- i. targets and recommendations from previous facility studies;
- j. benchmarking against large urban communities in Ontario;
- k. alignment with complementary strategies and initiatives; and
- l. financial viability and partnership potential.

Recommended facilities will be assigned a general priority and timeframe. Timing may be influenced by the project’s status, population growth, and alignment with other civic initiatives or partnerships. Projects in advanced stages of planning, such as those that have initiated their design process, will proceed as planned.

CITY OF HAMILTON

NOTICE OF MOTION

Council: July 8, 2022

MOVED BY COUNCILLOR M. PEARSON.....

SECONDED BY COUNCILLOR.....

Amendment to Item 11 of the Audit, Finance & Administration Committee Report 22-010, respecting the Grants Sub-Committee Report 22-002, which was approved by Council on May 25, 2022

WHEREAS, the name of the Grant recipient for Urban Moorings Two needs to be corrected to include “Nora Hutchinson” in order to process the approved funding.

THEREFORE, BE IT RESOLVED:

That Category ART-D20, shown in Appendix “A” (attached hereto) to Item 11 of the Audit, Finance & Administration Committee Report 22-010, respecting the Grants Sub-Committee Report 22-002 – Report GRA22-002, 2022 City Enrichment Funding Recommendations, be **amended** by adding the name “**Nora Hutchinson**” to the organization name of Urban Moorings Two, to read as follows:

ART-D20	Urban Moorings Two (Nora Hutchinson)
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Motion, as **Amended**, to read as follows:

- 11. **Grants Sub-Committee Report 22-002 - May 9, 2022 (Item 10.4)**
 - (a) **2022 City Enrichment Funding Recommendations (GRA22002) (City Wide) (Item 5.1)**
 - (i) That, the 2022 City Enrichment Fund recommended funding allocation, in the amount of \$6,098,732 (as outlined in the attached **Amended** Appendix “A” to AF&A Report 22-010), be approved;
 - (ii) That, Appendix “B” to Report GRA22002, Application Summary, be received;
 - (iii) That, Appendix “B” to AF&A Report 22-010, Payment Plan, be approved;
 - (iv) That, the Grants for the following organizations with outstanding arrears with the City Enrichment Fund, as of December 31, 2021 be approved and funding be withheld pending the full settlement of such arrears:
 - (1) AGR-A4 Golden Horseshoe Beekeepers' Association \$282.00;

- (2) AGR-A5 Hamilton-Wentworth 4-H Association \$897.50;
 - (3) AGR-A10 Hamilton Wentworth Soil and Crop Improvement Association \$2,461.51;
 - (4) CCH-A12 Hamilton Folk Arts Heritage Council \$41,482.00;
 - (5) CCH-A14 South Asian Heritage Association of Hamilton & Region \$3,115.00;
 - (6) CCH-A40 Festitalia Corporation \$5,952.00; and,
 - (7) CCHA-53 Hamilton Santa Claus Parade Committee \$32,779.00;
- (v) That, the following grants be approved, and funding be withheld, pending the submission of additional information, by July 20, 2022:
- (1) AGR-15 Wentworth District Women's Institute;
 - (2) ART-A30 Industry Performance Makers Arts Organization;
 - (3) ART-D26 Reverend Marco D'Andrea;
 - (4) ART-B3 Greater Hamilton Arts and Events;
 - (5) CCH-A36 Stoney Creek BIA;
 - (6) CCH-B1 Pride Hamilton;
 - (7) SAL-A3 & SAL-B6 The Hamilton and District Soccer Association;
 - (8) SAL-A4 Hamilton Ringette Association;
 - (9) ART-A34 Chamber Music Hamilton;
 - (10) CCH-A50 Flamborough Santa Claus Parade;
 - (11) CCH-A51 Hamilton Waterfront Trust;
 - (12) CCH-A52 Polish Symfonia Choir; and,
 - (13) CCH-A53 Hamilton Santa Claus Parade Committee;
- (vi) That staff be directed to report back, before summer, with respect to the status of the organizations whose 2022 City Enrichment Fund grants are being withheld, pending full settlement of outstanding arrears, as shown below:
- (1) AGR-A4 Golden Horseshoe Beekeepers' Association \$282.00
 - (2) AGR-A5 Hamilton-Wentworth 4-H Association \$897.50

- (3) AGR-A10 Hamilton Wentworth Soil and Crop Improvement Association \$2,461.51
 - (4) CCH-A12 Hamilton Folk Arts Heritage Council \$41,482.00
 - (5) CCH-A14 South Asian Heritage Association of Hamilton & Region \$3,115.00
 - (6) CCH-A40 Festitalia Corporation \$5,952.00
 - (7) CCHA-53 Hamilton Santa Claus Parade Committee \$32,779.00;
- (vii) That staff be directed to report back, before summer, with respect to the status of those organizations whose 2022 City Enrichment Fund Grant is being withheld, pending the submission of additional Information, as shown below:
- (1) AGR-15 Wentworth District Women's Institute
 - (2) ART-A30 Industry Performance Makers Arts Organization
 - (3) ART-D26 Reverend Marco D'Andrea
 - (4) ART-B3 Greater Hamilton Arts and Events
 - (5) CCH-A36 Stoney Creek BIA
 - (6) CCH-B1 Pride Hamilton
 - (7) SAL-A3 & SAL-B6 The Hamilton and District Soccer Association
 - (8) SAL-A4 Hamilton Ringette Association
 - (9) ART-A34 Chamber Music Hamilton
 - (10) CCH-A50 Flamborough Santa Claus Parade
 - (11) CCH-A51 Hamilton Waterfront Trust
 - (12) CCH-A52 Polish Symfonia Choir
 - (13) CCH-A53 Hamilton Santa Claus Parade Committee; and,
- (viii) That, should an additional Grants Sub-committee meeting not be possible prior to summer, the Legislative Coordinator be permitted to place the forthcoming report directly on the Audit, Finance & Administration agenda.

2022 City Enrichment Fund SUMMARY

	Category	No. of Apps	2022 Budget (Total)	2022 Requested	2022 Approved	Budget vs Approved	
	Community Services						
CS - A	Hunger/Shelter	9		\$ 395,825	\$ 330,838		
CS - B	Everyone Safe	9		\$ 302,172	\$ 286,846		
CS - C	Everyone Thrives	9		\$ 296,164	\$ 268,321		
CS - D	No Youth Left Behind	6		\$ 197,192	\$ 149,608		
CS - E	Everyone Age in Place	19		\$ 474,008	\$ 431,797		
CS - F	Community Capacity Grows	10		\$ 212,973	\$ 179,392		
CS - G	Someone to Talk to	3		\$ 92,688	\$ 72,496		
CS - H	Emerging Needs	46		\$ 1,527,859	\$ 517,389		
	Community Services Total	111	\$ 2,164,360	\$ 3,498,881	\$ 2,236,687	-\$ 72,327	-3.34%
	Agriculture						
AGR A	Program & Events	15		\$ 152,804	\$ 109,389		
	Agriculture Total	15	\$ 143,360	\$ 152,804	\$ 109,389	\$33,971	23.70%
	Arts						
ART - A	Arts - Operating	35		\$ 3,968,178	\$ 2,478,271		
ART - B	Arts - Festival	4		\$ 190,000	\$ 116,010		
ART - C	Arts - Capacity Building	2		\$ 28,850	\$ 12,500		
ART - D	Arts - Creation & Presentation	28		\$ 237,569	\$ 114,793		
	Arts Total	69	\$ 2,770,540	\$ 4,424,597	\$ 2,721,573	\$48,967	1.77%
	Environment						
ENV-A	Capacity Building	2		\$ 48,000	\$ 29,750		
ENV-C	Project and Programs	8		\$ 181,000	\$ 109,577		
	Environment Total	10	\$ 146,390	\$ 229,000	\$ 139,327	\$7,063	4.82%
	Communities, Culture & Heritage						
CCH - A	CCH - Events	55		\$ 964,120	\$ 542,320		

	Category	No. of Apps	2022 Budget (Total)	2022 Requested	2022 Approved	Budget vs Approved	
CCH - B	CCH - New Projects	7		\$ 156,815	\$ 49,500		
CCH - C	CCH - Capacity Building	5		\$ 60,288	\$ 9,737		
	CCH Total	67	\$ 567,700	\$ 1,181,223	\$ 601,557	-\$33,857	-5.96%
	Sport and Active Lifestyles						
SAL-A	Long Term Athlete Dev (LTAD)	7		\$ 51,575	\$ 32,800		
SAL-B	Sport Awareness	6		\$ 44,565	\$ 31,938		
SAL-C	Capacity Building	8		\$ 61,831	\$ 37,816		
SAL-D	Sport for Development/Inclusion	4		\$ 27,450	\$ 27,450		
SAL-E	Accessibility	6		\$ 50,019	\$ 36,976		
SAL-F	Active for Life	12		\$ 84,974	\$ 60,673		
SAL-G	Multi-Sport Hosting	2		\$ 51,500	\$ 12,546		
	Sport and Active Lifestyles Total	45	\$ 245,990	\$ 371,914	\$ 240,199	\$5,791	2.35%
	Total Traditional Grant Requests	317	\$ 6,038,340	\$ 9,858,419	\$ 6,048,732	-\$10,392	-0.17%
	CEF Administration		50,000		50,000	-	
	Total City Enrichment Fund		\$ 6,088,340	\$ 9,858,419	\$ 6,098,732	-\$10,392	-0.17%

2022 City Enrichment Fund

Community Services

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CS-A1	Neighbour to Neighbour Centre	Neighbour to Neighbour Food Bank	A	\$30,151	\$32,291	\$30,151	\$272,745	\$0
CS-A2	Mission Services of Hamilton	Good Food Centre	A	\$28,695	\$48,421	\$28,695	\$704,856	\$0
CS-A3	Neighbour to Neighbour Centre	Home Delivery Program	A	\$17,669	\$18,557	\$17,669	\$90,218	\$0
CS-A4	YWCA Hamilton	Transitional Living Program	A	\$29,979	\$22,419	\$22,419	\$1,095,540	-\$7,560
CS-A5	Good Shepherd Centre Hamilton	Marketplace	A	\$77,915	\$79,473	\$77,915	\$1,251,900	\$0
CS-A6	Housing Help Centre - Hamilton & Area	Housing Stabilization	B	\$67,000	\$67,000	\$67,000	\$641,330	\$0
CS-A7	Good Shepherd Centre Hamilton	Community Hot Meals	B	\$36,772	\$37,507	\$36,772	\$885,250	\$0
CS-A8	The Governing Council of the Salvation Army in Canada	Salvation Army Food Bank	B	\$23,627	\$51,510	\$23,627	\$171,700	\$0
CS-A9	The Governing Council of the Salvation Army of Canada and Bermuda (Salvation Army Booth Centre)	The Salvation Army Street Ministry	B	\$26,590	\$38,647	\$26,590	\$128,823	\$0
No one is hungry Total				\$338,398	\$395,825	\$330,838		-\$7,560
CS-B1	Sexual Assault Centre (Hamilton and Area)	Abuse Prevention Program	A	\$6,565	\$6,997	\$6,565	\$131,694	\$0
CS-B2	Sexual Assault Centre (Hamilton and Area)	Diverse Communities Outreach Program	A	\$11,951	\$12,739	\$11,951	\$151,198	\$0
CS-B3	YWCA Hamilton	Phoenix Place VAW Residential Program	A	\$15,000	\$15,000	\$15,000	\$155,788	\$0
CS-B4	Sexual Assault Centre (Hamilton and Area)	Crisis Support Program	A	\$15,242	\$16,247	\$15,242	\$160,812	\$0
CS-B5	Sexual Assault Centre (Hamilton and Area)	Counselling and Advocacy Program	A	\$20,155	\$21,483	\$20,155	\$248,555	\$0
CS-B6	Thrive Child and Youth Trauma Services	Child and Youth Trauma Services (CYTS)	A	\$86,060	\$91,493	\$86,060	\$391,862	\$0
CS-B7	Good Shepherd Centre Hamilton	2nd Stage Housing	B	\$59,619	\$61,408	\$59,619	\$772,900	\$0
CS-B8	Interval House of Hamilton	Community Outreach, Counselling and Telephone Crisis	B	\$25,240	\$26,502	\$25,240	\$1,044,077	\$0
CS-B9	Interval House of Hamilton	Women's Centre of Hamilton	B	\$47,014	\$50,303	\$47,014	\$362,285	\$0
Everyone feels safe Total				\$286,846	\$302,172	\$286,846		\$0
CS-C1	Immigrants Working Centre	IWC Childcare Program	A	\$23,976	\$25,679	\$23,976	\$984,751	\$0
CS-C2	Wesley Urban Ministries Inc.	Children & Family Programs	B	\$43,872	\$44,750	\$43,872	\$219,200	\$0
CS-C3	BGC Hamilton Halton (Hamilton East Kiwanis Boys & Girls Club)	Community Outreach Program	B	\$39,065	\$40,500	\$39,065	\$171,400	\$0
CS-C4	BGC Hamilton Halton (Hamilton East Kiwanis Boys & Girls Club)	Parent Outreach	B	\$51,664	\$52,000	\$51,664	\$175,000	\$0
CS-C5	Elliott Heights Baptist Church	Larch After School Program	B	\$5,202	\$20,000	\$5,202	\$117,111	\$0

2022 City Enrichment Fund

Community Services

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CS-C6	Living Rock Ministries	Wellness Works	B	\$33,428	\$37,635	\$33,428	\$125,450	\$0
CS-C7	Big Brothers Big Sisters of Halton & Hamilton	Matching Programs	B	\$24,480	\$25,200	\$24,480	\$345,000	\$0
CS-C8	Big Brothers Big Sisters of Halton & Hamilton	Group Mentoring Girls	B	\$23,548	\$25,200	\$23,548	\$85,000	\$0
CS-C9	Big Brothers Big Sisters of Halton & Hamilton	Group Mentoring Boys	B	\$23,086	\$25,200	\$23,086	\$96,000	\$0
Every child & family thrives Total				\$268,321	\$296,164	\$268,321		\$0
CS-D1	AY Alternatives for Youth Hamilton	Street Involved Outreach	A	\$40,625	\$43,510	\$40,625	\$181,438	\$0
CS-D2	AY Alternatives for Youth Hamilton	AY Outreach	A	\$26,347	\$28,218	\$26,347	\$142,639	\$0
CS-D3	Routes Youth Centre (Dundas Youth Chaplaincy)	Routes Youth Centre	A	\$10,506	\$11,500	\$10,506	\$198,400	\$0
CS-D4	Wesley Urban Ministries Inc.	Youth Housing	A	\$40,669	\$41,482	\$40,669	\$557,526	\$0
CS-D5	Living Rock Ministries	Oasis Coffee House Evening Program	A	\$15,801	\$37,982	\$15,801	\$126,608	\$0
CS-D6	Living Rock Ministries	It's a New Day Breakfast Program	A	\$15,660	\$34,500	\$15,660	\$115,000	\$0
No Youth is left behind Total				\$149,608	\$197,192	\$149,608		\$0
CS-E1	Dundas Community Services	Services for Seniors A.C.T.I.V.E at the Villa Adult Day Program	A	\$45,016	\$45,917	\$45,016	\$328,393	\$0
CS-E2	St. Joseph's Villa	Program	A	\$20,400	\$20,808	\$20,400	\$651,914	\$0
CS-E3	Wesley Urban Ministries Inc.	Seniors Outreach Program	A	\$18,918	\$34,363	\$18,918	\$114,542	\$0
CS-E4	VON Hamilton (VON Canada - Ontario Branch)	Meals on Wheels Adult Day Program/Mobile Adult Day Services	A	\$56,852	\$60,888	\$56,852	\$1,242,498	\$0
CS-E5	VON Hamilton (VON Canada - Ontario Branch)	Day Services	A	\$29,238	\$31,314	\$29,238	\$841,870	\$0
CS-E6	Good Shepherd Centre Hamilton	SAM Adult Day Program	A	\$68,940	\$70,394	\$68,940	\$568,108	\$0
CS-E7	VON Hamilton (VON Canada - Ontario Branch)	Volunteer Visiting & Tele-Touch	A	\$52,191	\$53,235	\$52,191	\$472,764	\$0
CS-E8	Glanbrook Community Services (Glanbrook Home Support Program Inc)	Health & Fitness Program	A	\$16,574	\$16,574	\$16,574	\$71,156	\$0
CS-E9	Glanbrook Community Services (Glanbrook Home Support Program Inc)	Meal Supports Program	A	\$23,962	\$23,962	\$23,962	\$287,941	\$0
CS-E10	Glanbrook Community Services (Glanbrook Home Support Program Inc)	Volunteer Assisted transportation Program (VAT)	A	\$12,993	\$12,993	\$12,993	\$87,438	\$0
CS-E11	Flamborough Connects (Flamborough Information and Community Services)	Seniors Support	A	\$3,446	\$3,691	\$3,446	\$12,491	\$0
CS-E12	Glanbrook Community Services (Glanbrook Home Support Program Inc)	Community Supports Program	A	\$11,326	\$11,326	\$11,326	\$93,174	\$0
CS-E13	Ancaster Community Services	Assisted Volunteer Driving Program	B	\$9,417	\$9,605	\$9,417	\$48,495	\$0
CS-E14	Banyan Community Services Inc.	Grocer-Ease Program	B	\$19,145	\$20,504	\$19,145	\$272,935	\$0

2022 City Enrichment Fund

Community Services

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CS-E15	Ancaster Community Services	Meals on Wheels	B	\$5,306	\$5,571	\$5,306	\$116,356	\$0
CS-E16	Ancaster Community Services	Frozen Meals Seniors Intensive Case Management	B	\$7,946	\$7,105	\$7,105	\$74,938	-\$841
CS-E17	Catholic Family Services of Hamilton The Governing Council of The Salvation Army Canada-	Management	B	\$11,768	\$26,459	\$11,768	\$851,878	\$0
CS-E18	Mountberrry Adult Day Services	Reopening with resilience	B	\$6,200	\$6,300	\$6,200	\$693,012	\$0
CS-E19	BGC Hamilton-Halton Adult Day Program (Hamilton East Kiwanis Boys and Girls Club)	BGC Hamilton-Halton Adult Day Program	B	\$13,000	\$13,000	\$13,000	\$273,396	\$0
Everyone can age in place Total				\$432,638	\$474,008	\$431,797		-\$841
CS-F1	Adult Basic Education Association	Lifelong Learning Opportunities and Pathways	A	\$8,500	\$10,000	\$8,500	\$88,840	\$0
CS-F2	Neighbour to Neighbour Centre	Middle East Outreach	A	\$2,818	\$3,000	\$2,818	\$38,867	\$0
CS-F3	Flamborough Connects (Flamborough Information and Community Services)	Community Outreach Services	A	\$48,385	\$40,731	\$40,731	\$165,907	-\$7,654
CS-F4	Ancaster Community Services Hamilton Literacy Council (The Hamilton and District Literacy Council)	Ancaster Community Services- Youth Engagement Program	A	\$17,575	\$15,018	\$15,018	\$97,304	-\$2,557
CS-F5	Literacy Council)	Literacy and Basic Skills for Adults	A	\$5,894	\$6,000	\$5,894	\$300,000	\$0
CS-F6	Ancaster Community Services	Community Outreach	B	\$35,991	\$37,700	\$35,991	\$239,450	\$0
CS-F7	Dundas Community Services	Community Outreach	B	\$11,099	\$11,542	\$11,099	\$38,500	\$0
CS-F8	Hamilton Council on Aging	Seniors Engagement	B	\$8,323	\$15,000	\$8,323	\$455,860	\$0
CS-F9	Rotary Club of Hamilton	Rotary Literacy in Action Program	B	\$7,362	\$25,000	\$7,362	\$187,320	\$0
CS-F10	Social Planning and Research Council of Hamilton	Community Social Research and Planning	B	\$43,656	\$48,982	\$43,656	\$512,500	\$0
Capacity grows Total				\$189,603	\$212,973	\$179,392		-\$10,211
CS-G1	Canadian Mental Health Association, Hamilton	Evening Social Recreation Rehabilitation Program	A	\$24,995	\$26,727	\$24,995	\$111,075	\$0
CS-G2	Catholic Family Services of Hamilton	Individual and Family Counselling	A	\$39,901	\$58,209	\$39,901	\$249,725	\$0
CS-G3	Dundas Community Services	Counselling & Referral	B	\$7,600	\$7,752	\$7,600	\$25,923	\$0
Everyone has someone to talk to Total				\$72,496	\$92,688	\$72,496		\$0
CS-H1	Sir Winston Churchill Secondary School	SWC Outfitters Launch Affordable Supportive Housing at the former Royal Oak Dairy	DNQ		\$600		\$2,000	\$0
CS-H2	Indwell Community Homes		A		\$75,000	\$56,250	\$594,575	\$56,250

2022 City Enrichment Fund

Community Services

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CS-H3	AY Alternatives for Youth Hamilton	Parent Education, Support and Skills Development Program	A	\$18,162	\$14,303	\$14,303	\$73,622	-\$3,859
CS-H4	Immigrants Working Centre	IWC Employment Services	A		\$27,850	\$20,888	\$396,011	\$20,888
CS-H5	St. Matthew's House	Seniors in Kitchens (SinKs) Information and Peer Support for Cancer Patients and Families/Caregivers	A		\$40,000	\$30,000	\$674,100	\$30,000
CS-H6	Wellwood Resource Centre of Hamilton (Wellwood)	Families/Caregivers	A	\$27,300	\$27,300	\$27,300	\$173,800	\$0
CS-H7	Cancer Assistance Program	Cancer Assistance Program – Community Engagement in under-resourced areas of Hamilton	A	\$17,500	\$23,760	\$17,500	\$79,200	\$0
CS-H8	Mission Services of Hamilton	Youth Afterschool Meal Program	A	\$27,587	\$35,000	\$27,587	\$129,385	\$0
CS-H9	Mission Services of Hamilton	Willow's Place	A	\$25,000	\$35,000	\$25,000	\$863,226	\$0
CS-H10	Welcome Inn Community Centre	Food Access Program	A	\$16,482	\$25,000	\$16,482	\$150,423	\$0
CS-H11	Thrive Child and Youth Trauma Services	OASIS	A	\$41,700	\$30,222	\$30,222	\$181,032	-\$11,478
CS-H12	Disability Justice Network Of Ontario	Youth Action	A		\$6,000	\$4,500	\$20,000	\$4,500
CS-H13	Neighbour to Neighbour Centre	N2N Wellness and Mindful Movement	A		\$31,452	\$23,589	\$104,843	\$23,589
CS-H14	Neighbour to Neighbour Centre	N2N Food Skills For Kids Program	A		\$31,689	\$23,767	\$105,633	\$23,767
CS-H15	Neighbour to Neighbour Centre	N2N Intercultural Community Kitchen	A		\$18,979	\$14,234	\$63,265	\$14,234
CS-H16	Rygiel Supports for Community Living BGC Hamilton Halton (Hamilton East Kiwanis Boys & Girls Club)	Senior Wellbeing project	A		\$27,000	\$20,250	\$90,000	\$20,250
CS-H17	City Kidz Ministry	McQuesten BGC	A		\$24,400	\$18,300	\$84,600	\$18,300
CS-H18	Liberty For Youth	CityKidz Cares 2022	A	\$0	\$19,685	\$14,764	\$367,100	\$14,764
CS-H19	Neighbour to Neighbour Centre	Bright Choices Program	A		\$46,943	\$35,207	\$156,478	\$35,207
CS-H20	Neighbour to Neighbour Centre	Garden Skills Programming	A		\$36,893	\$27,670	\$122,978	\$27,670
CS-H21	Wayside House of Hamilton	Helping Men Recover: 13-Week Program for Men With Lived Experience and their Family Caregivers	A	\$21,000	\$30,000	\$21,000	\$115,857	\$0
CS-H22	Neighbour to Neighbour Centre	N2N Community Action and Advocacy Program	A		\$30,155	\$1,535	\$100,518	\$1,535

2022 City Enrichment Fund

Community Services

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
		Increasing Access to STEM Education for Marginalized Children						
CS-H23	Scientists in School	in Hamilton	B		\$11,000		\$62,250	\$0
CS-H24	Neighbour to Neighbour Centre	N2N Community Meals and Good Food Markets	B		\$59,322		\$197,742	\$0
CS-H25	Hamilton Jewish Family Services	HJFS Hamilton Kosher Food Bank	B		\$40,000		\$276,313	\$0
CS-H26	Wesley Urban Ministries Inc.	Wesley Day Centre	B		\$160,000		\$540,588	\$0
CS-H27	Learning Disabilities Association of Halton-Hamilton	Mastering Mathematics	B		\$32,720		\$109,067	\$0
CS-H28	Food4Kids Hamilton	Weekends Without Hunger	B	\$9,542	\$50,000	\$9,542	\$877,500	\$0
CS-H29	Neighbour to Neighbour Centre	Hamilton Community Garden	B		\$20,991		\$69,970	\$0
CS-H30	Parkview Church (Parkview Church of the Christian and Missionary Alliance)	Networking Program	B		\$11,000		\$76,582	\$0
CS-H31	Canadian National Institute for the Blind	Compassion Ministries	B		\$6,775		\$22,585	\$0
CS-H32	JA South Western Ontario (Junior Achievement of South Western Ontario Inc)	Peer Support for Hamiltonians with Sight Loss	B		\$2,340		\$7,800	\$0
CS-H33	Dr. Bob Kemp Hospice (Foundation Inc)	Financial literacy programs	B		\$45,000		\$211,505	\$0
CS-H34	Body Brave	Bereavement Supports for All Types of Loss for Children and Families	B	\$37,500	\$45,000	\$37,500	\$211,505	\$0
CS-H35	Neighbour to Neighbour Centre	Body Brave Online Recovery Support Program	B		\$88,380		\$294,600	\$0
CS-H36	Miskha Social Services	N2N Cooking Up Justice Youth Kitchen	B		\$18,979		\$63,265	\$0
CS-H37	Sisters in Sync	Sanad - Case Management Program	B		\$95,400		\$318,000	\$0
CS-H38	Essential Aid and Family Services of Ontario Inc.	Recovering You - Workshop Series	B		\$22,200		\$74,000	\$0
CS-H39	Safe Families Hamilton/Halton	Infant & Toddler Food Bank	B		\$25,000		\$335,000	\$0
CS-H40	Afghan Association of Hamilton	Safe Families Hamilton/Halton	D		\$35,925		\$119,750	\$0
CS-H41	Ghana Association of Hamilton	New Home - New Hope	D		\$6,300		\$21,000	\$0
CS-H42	PROGRAMME JEUNESSE ICI CORP.	Family Support Program	D		\$5,100		\$17,000	\$0
CS-H43	Colombian Refugees Association	CARING FOR OUR YOUTH PROGRAM	D		\$93,428		\$311,426	\$0
		Housing aid	D		\$12,168	\$0	\$40,560	\$0

2022 City Enrichment Fund

Community Services

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CS-H44	Hamilton Helping Hands	Community Service Program	D		\$30,000	\$0	\$100,000	\$0
CS-H45	Somali Community In Hamilton	Community Services Program	D		\$15,000	\$0	\$50,000	\$0
CS-H46	The Children's Centre For Development	Monthly Webinar	D		\$3,600	\$0	\$12,000	\$0
Emerging needs & program innovation Total				\$241,773	\$1,527,859	\$517,389		\$275,616
Total Community Services				\$1,979,683	\$3,498,881	\$2,236,687		\$257,004

2022 City Enrichment Fund

AGRICULTURE
Programs & Events

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	Program Budget	2022 Funding Approved	2019 vs 2022 Approved
AGR-A1	Ancaster Agricultural Society	Ancaster Fair	A	\$26,000	\$28,000	\$660,630	\$26,000	\$0
AGR-A2	Rockton Agricultural Society	Rockton World's Fair	A	\$27,139	\$35,000	\$645,000	\$27,139	\$0
AGR-A3	Binbrook Agricultural Society	Binbrook Fair 2022	A	\$10,200	\$30,000	\$188,250	\$10,200	\$0
AGR-A4	Golden Horseshoe Beekeepers' Association ³	GHBA Public Education Program	A	\$1,475	\$1,530	\$5,100	\$1,475	\$0
AGR-A5	Hamilton-Wentworth 4-H Association ³ Locke Street Farmers' Market (23027474 Ontario Inc)	Agriculture - 4-H Association	A	\$6,000	\$5,500	\$18,500	\$5,500	-\$500
AGR-A6		Community Programming Field of Dreams - Horses and Wellness	A	\$1,950	\$2,100	\$7,000	\$1,950	\$0
AGR-A7	The Equestrian Association for the Disabled (TEAD)	Experience	A		\$10,000	\$34,000	\$7,500	\$7,500
AGR-A8	Ancaster Farmers Market	Ancaster Farmers Market	B	\$3,000	\$5,000	\$29,921	\$3,000	\$0
AGR-A9	Hamilton Sustainable Victory Gardens Inc. Hamilton Wentworth Soil and Crop Improvement Association ³	Agriculture Improving Soil Health in Hamilton	B	\$19,741	\$11,774	\$39,247	\$11,774	-\$7,967
AGR-A10		Agriculture Southwentworth Plowmen's Association	B	\$2,923	\$4,800	\$16,000	\$2,923	\$0
AGR-A11	Southwentworth Plowmen's Association	Annual Plowing Match To beautify Ancaster and to educate on horticultural topics	B	\$1,428	\$2,100	\$7,000	\$1,428	\$0
AGR-A12	Ancaster Horticultural Society	Agriculture general - Market in the Creek in Downtown Stoney Creek	B	\$4,000	\$4,000	\$16,016	\$4,000	\$0
AGR-A13	Stoney Creek BIA		B		\$5,000	\$39,600	\$2,500	\$2,500
AGR-A14	Westdale Village BIA Wentworth District Womens Institute ¹ (Federated Women's Institute of Ontario)	Casual Market	B		\$3,000	\$21,000	\$1,500	\$1,500
AGR-A15		Scholarship and Rose Program	B		\$5,000	\$17,000	\$2,500	\$2,500
Total Agriculture				\$103,856	\$152,804	\$1,744,264	\$109,389	\$5,533

¹ Final Report Pending

³ Owes funds to the City

2022 City Enrichment Fund

ARTS

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
ARTS								
ART-A1	Supercrawl Productions	Supercrawl	A	\$135,252	\$200,000	\$135,252	\$1,757,000	\$0
ART-A2	Art Gallery of Hamilton	Annual Operations	A	\$1,000,000	\$1,500,000	\$1,000,000	\$5,624,119	\$0
ART-A3	Hamilton Literary Festival Association	gritLIT Festival 2022	A	\$13,260	\$16,000	\$13,260	\$88,000	\$0
ART-A4	Dundas Pipes and Drums	Annual Operations	A	\$3,860	\$3,042	\$3,042	\$10,140	-\$818
ART-A5	Hamilton All Star Jazz Band Inc.	Hamilton All Star Jazz Bands Inc	A	\$9,500	\$9,500	\$9,500	\$53,405	\$0
ART-A6	Hamilton Children's Choir	Hamilton Children's Choir	A	\$52,265	\$55,000	\$52,265	\$520,093	\$0
ART-A7	Centre[3] for artistic and social practice (Centre[3] for print and media arts)	Annual Operations	A	\$53,581	\$80,000	\$53,581	\$1,231,200	\$0
ART-A8	Hamilton Festival Theatre Company	Hamilton Fringe Festival/ Frost Bites/ Artist Development Programs (ALERT/SPARK)	A	\$53,053	\$60,000	\$53,053	\$641,328	\$0
ART-A9	Brott Music Festival	Annual Operations	A	\$182,800	\$225,000	\$182,800	\$1,670,400	\$0
ART-A10	Carnegie Gallery (Dundas Art and Craft Association)	Arts Operating	A	\$15,000	\$20,000	\$15,000	\$420,600	\$0
ART-A11	Hamilton Artists Inc.	Arts Operating	A	\$58,140	\$75,000	\$58,140	\$317,357	\$0
ART-A12	Hamilton Music Collective	Hamilton Music Collective - Changing Lives Through Music - Empowering Change in the Community	A	\$63,648	\$100,000	\$63,648	\$614,975	\$0
ART-A13	Hamilton Arts Council (Hamilton and Region Arts Council)	Hamilton Arts Council - Operating	A	\$42,700	\$69,219	\$42,700	\$372,551	\$0
ART-A14	Theatre Aquarius Inc.	Annual Operations	A	\$260,100	\$265,302	\$260,100	\$2,742,528	\$0
ART-A15	Culture for Kids in the Arts	Arts Operating	A	\$23,644	\$30,000	\$23,644	\$400,000	\$0
ART-A16	Bach Elgar Choir (Bach Elgar Choral Society)	Annual Operations	A	\$28,500	\$29,500	\$28,500	\$130,195	\$0
ART-A17	The Harlequin Singers of Hamilton	Harlequin Singers 2022 Concert Series	A	\$3,850	\$3,850	\$3,850	\$21,000	\$0
ART-A18	Dundas Concert Band Inc.	Dundas Concert Band	A	\$2,400	\$2,850	\$2,400	\$9,527	\$0
ART-A19	Workers Arts and Heritage Centre Inc	Annual Operations	A	\$35,700	\$36,414	\$35,700	\$404,519	\$0
ART-A20	Dundas Valley School of Art	DVSA Annual Operations	A	\$109,109	\$572,876	\$109,109	\$1,909,587	\$0
ART-A21	Factory Media Centre	Annual Operations	A	\$18,800	\$30,000	\$18,800	\$144,236	\$0
ART-A22	ICAA	Newcomer Artists Mentorship Program	A	\$19,000	\$21,000	\$19,000	\$70,000	\$0
ART-A23	Hamilton Youth Steel Orchestra	HYSO	A	\$10,098	\$15,826	\$10,098	\$52,755	\$0
ART-A24	Telling Tales	14th Annual Telling Tales Festival and Year Round Programming	A	\$16,703	\$27,500	\$16,703	\$389,095	\$0
ART-A25	Tune In Foundation	Annual Operations	A		\$23,500	\$17,625	\$78,383	\$17,625
ART-A26	Hamilton Philharmonic Orchestra	Arts Operating for Established Professional Organizations	B	\$171,666	\$275,000	\$171,666	\$1,500,000	\$0
ART-A27	Aeris Körper Contemporary Dance	Aeris Körper Contemporary Dance 2022 Operations	B		\$29,000	\$14,500	\$251,400	\$14,500
ART-A28	HCA Dance Theatre (Hamilton Conservatory for the Arts)	Arts Operating	B	\$10,000	\$40,000	\$10,000	\$216,665	\$0

2022 City Enrichment Fund

ARTS

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
ART-A29	Hamilton Academy of Performing Arts	Public Programming	B	\$20,000	\$30,000	\$20,000	\$191,935	\$0
ART-A30	Industry Performance Makers Arts Organization ¹	Annual Operations	B		\$29,925	\$14,963	\$115,845	\$14,963
ART-A31	Open Heart Arts Theatre	Open Heart Arts Theatre	B		\$27,474	\$13,737	\$91,580	\$13,737
ART-A32	The Duet Club of Hamilton	Holiday Concert, 2022	D		\$2,400	\$0	\$8,000	\$0
ART-A33	Musikay Chamber Ensemble	Musikay 2022-2023 season	D		\$7,500	\$0	\$32,300	\$0
ART-A34	Chamber Music Hamilton ²	Annual Operations - Chamber Music	C	\$5,635	\$25,500	\$5,635	\$85,000	\$0
ART-A35	Westdale Cinema Group	Hamilton - Arts Operating 2022	D		\$30,000	\$0	\$551,900	\$0
				\$2,418,264	\$3,968,178	\$2,478,271		\$60,007
FESTIVALS								
	Centre francophone Hamilton (Centre francais							
ART-B1	Hamilton Inc)	FrancoFEST 2022	A	\$26,010	\$40,000	\$26,010	\$468,300	\$0
ART-B2	Festival of Friends (Hamilton-Wentworth)	Festival of Friends	A	\$90,000	\$100,000	\$90,000	\$447,500	\$0
ART-B3	Greater Hamilton Arts And Events ¹	Arts Festivals - ArtsFest	D	\$25,000	\$25,000	\$0	\$200,000	-\$25,000
ART-B4	Greater Hamilton Arts And Events	Arts Festivals - OctoberWest	D		\$25,000	\$0	\$105,000	\$0
				\$141,010	\$190,000	\$116,010		-\$25,000
ARTS - CAPACITY BUILDING								
		Hamilton Music Collective - Changing Lives						
		Through Music - Empowering Change in the						
ART-C1	Hamilton Music Collective	Community	B		\$25,000	\$12,500	\$84,997	\$12,500
ART-C2	The Harlequin Singers of Hamilton	Harlequin Singers 2022 Concert Series	D		\$3,850	\$0	\$21,000	\$0
				\$0	\$28,850	\$12,500		\$12,500
CREATION & PRESENTATION								
ART-D1	Niv Shimshon	Creation & Presentation (Emerging)	A		\$5,000	\$3,750	\$19,502	\$3,750
ART-D2	Alex Whorms	Emerging Artist Project	A		\$5,000	\$3,750	\$25,000	\$3,750
ART-D3	Fiona Kinsella	Creation and Presentation for Arts	A		\$10,000	\$7,500	\$35,000	\$7,500
ART-D4	Karen Ancheta	Arts-Creative Presentation	A		\$10,000	\$7,500	\$37,000	\$7,500
		"Worship in a Time of Plague" Collective						
	"Worship in a Time of Plague" Collective (Capella	(Capella Intima, Wolfstone Media, Gallery						
ART-D5	Intima/Wolfstone Media/Gallery Players of Niagara)	Players of Niagara)	A		\$10,000	\$7,500	\$51,000	\$7,500
ART-D6	Sid Ryan Eilers (Lisa Emmons)	Kiss The Stormy Sky	A		\$10,000	\$7,500	\$91,975	\$7,500
ART-D7	Nancy Anne McPhee	Textile installation series	A		\$10,000	\$7,500	\$33,334	\$7,500
ART-D8	Exchange Rate Collective	Arts-Creative Presentation	A		\$10,000	\$7,500	\$40,785	\$7,500
		the pack : a full-length, site-specific dance						
ART-D9	FREEWATER (Mikaela Demers)	show	B		\$10,000	\$5,000	\$47,134	\$5,000
ART-D10	Karen Logan Art	Creation and Presentation for Artists	B		\$7,250	\$2,500	\$24,578	\$2,500
ART-D11	Azuline Duo (Sara Traficante)	Azuline Duo - A New 19th Century Parlour	B		\$4,400	\$2,200	\$14,825	\$2,200

2022 City Enrichment Fund

ARTS

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
		Arts - Creation & Presentation, Emerging						
ART-D12	CODA FACTO (Alex DeRoo)	Arts	B		\$5,000	\$2,500	\$23,000	\$2,500
ART-D13	Nea Reid	Creation and Presentation	B		\$10,000	\$5,000	\$40,000	\$5,000
		Arts - Creation and Presentation Grants for						
ART-D14	Megan English Dance (Megan English)	Arts Professionals	B		\$10,000	\$5,000	\$43,200	\$5,000
ART-D15	Radha Menon	Death Cry of a Peacock novel	B		\$10,000	\$5,000	\$34,815	\$5,000
ART-D16	Art To Go Podcast (Cornelia Peckart)	Art To Go Podcast - Seven in the Summer	B		\$10,000	\$2,910	\$19,400	\$2,910
ART-D17	Luckystickz (Dejehan Hamilton)	"Discomfort" Album	B		\$7,719	\$3,860	\$25,730	\$3,860
ART-D18	Tracey-Mae Chambers	#hopeandhealingcanada	B		\$9,000	\$2,250	\$15,000	\$2,250
ART-D19	Trip Print Press (Nicholas Kennedy) ART-	Established Artist Project	B		\$3,600	\$1,800	\$12,000	\$1,800
ART-D20	Urban Moorings Two (Nora Hutchinson)	Urban Moorings Two	B		\$10,000	\$5,000	\$78,350	\$5,000
ART-D21	Chastity	Chastity - Established Artist Project	B		\$9,000	\$4,500	\$30,000	\$4,500
ART-D22	Ellis Songs (Linnea Williams)	Established Professional Artist Project	B		\$10,000	\$5,000	\$39,219	\$5,000
ART-D23	R.G.Wunderink Art (Rachelle Wunderink)	Creation & Presentation - Arts Program	B		\$5,000	\$2,273	\$15,150	\$2,273
ART-D24	HamiltonSeen	Shining Waters	B		\$10,000	\$5,000	\$40,000	\$5,000
ART-D25	Gary Barwin	Creation & Presentation	B		\$5,000	\$2,500	\$30,000	\$2,500
ART-D26	Reverend Marco D'Andrea ¹ (Emilio Marco D'Andrea)	Creation & Presentation (Established)	D		\$6,300	\$0	\$21,000	\$0
ART-D28	Morgan Wedderspoon	Established Artist Project	Withdrew		\$5,300	\$0	\$19,200	\$0
ART-D27	Tottering Biped Theatre Inc.	Annual Summer Shakespeare Project	DNQ		\$20,000	\$0	\$70,000	\$0
TOTAL ARTS					\$0	\$237,569	\$114,793	\$114,793
					\$2,559,274	\$4,424,597	\$2,721,573	\$162,299

¹ Final Report Pending

² Addition Information Required

2022 City Enrichment Fund

Environment

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
ENV-A1	Hamilton-Wentworth Green Venture Sustainability Leadership (Sustainable	Building Greenspace Stewardship Capacity	A		\$23,000	\$17,250	\$83,400	\$17,250
ENV-A2	Hamilton)	Business Development Initiative	B	\$0	\$25,000	\$12,500	\$136,200	\$12,500
Capacity Building Total					\$0	\$48,000	\$29,750	\$29,750
ENV-C1	Hamilton-Wentworth Green Venture	NATURhoods: Helping Hamiltonians naturally adapt to urban runoff	A	\$24,545	\$25,000	\$24,545	\$155,600	\$0
ENV-C2	Hamilton-Wentworth Green Venture	Experiential Environmental Learning	A		\$25,000	\$18,750	\$85,650	\$18,750
ENV-C3	A Rocha Canada	Operation Wild	A	\$10,000	\$35,000	\$10,000	\$122,000	\$0
ENV-C4	Bay Area Restoration Council (of Hamilton and Halton Inc)	School and Volunteer Programming for Hamilton Harbour Restoration	A	\$14,382	\$35,000	\$14,382	\$175,150	\$0
ENV-C5	Sustainability Leadership (Sustainable	Water Management Framework for Business	A		\$35,000	\$26,250	\$153,530	\$26,250
ENV-C6	Hamilton)	Wonderful Woodlands	A		\$10,000	\$7,500	\$40,000	\$7,500
ENV-C7	Hamilton Naturalists Club	Environment Hamilton Climate Action Campaign	B		\$13,500	\$6,750	\$153,410	\$6,750
ENV-C8	Environment Hamilton Incorporated	Tree for Hamilton 2022 Tree Planting Events	B	\$1,400	\$2,500	\$1,400	\$22,396	\$0
Projects & Programs Total					\$50,327	\$181,000	\$109,577	\$59,250
Grand Total					\$50,327	\$229,000	\$139,327	\$89,000

2022 City Enrichment Fund

Communities, Culture & Heritage

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
Events &								
CCH-A1	Downtown Dundas BIA Downtown Hamilton Business Improvement Area	Community, Culture, Heritage Large Events	A		\$34,545	\$25,909	\$115,150	\$25,909
CCH-A2		Gore Park Summer Promenade Imagine in the Park Children's Arts Festival	A	\$9,762	\$17,929	\$9,762	\$59,762	\$0
CCH-A3	Rotary Club of Hamilton AM		A	\$5,100	\$6,000	\$5,100	\$36,300	\$0
CCH-A4	Locke Street Business Improvement Area	Sundays UnLocked	A	\$0	\$13,500	\$10,125	\$48,000	\$10,125
CCH-A5	Barton Village Business Improvement Area	Barton Village Festival	A	\$3,433	\$4,750	\$3,433	\$45,000	\$0
CCH-A6	Dundas Valley Orchestra	Activities and Concerts	A	\$5,100	\$5,000	\$5,000	\$36,030	-\$100
CCH-A7	SalsaSoul Productions	Core Programs: Salsa on the Waterfront and SalsaSoul Sundays	A	\$3,742	\$5,988	\$3,742	\$19,959	\$0
CCH-A8	Ancaster Heritage Days	Ancaster Heritage Days 2022 Summer Event	A	\$26,550	\$26,220	\$26,220	\$87,400	-\$330
CCH-A9	Hamilton Arts & Letters	CCH-Events & Est. Activities Large - HAL Presents	A	\$7,283	\$9,000	\$7,283	\$38,980	\$0
CCH-A10	Hamilton Sings! Community Choir	Hamilton Sings! Community Choir	A		\$5,000	\$3,750	\$32,420	\$3,750
CCH-A11	Dundas Museum & Archives (Dundas Historical Society Museum)	Dundas Historical Society Museum - Exhibitions Program	A	\$10,200	\$11,000	\$10,200	\$74,624	\$0
CCH-A12	Hamilton Folk Arts Heritage Council ³	Communities, Culture & Heritage Program	A	\$58,905	\$54,287	\$54,287	\$210,000	-\$4,618
CCH-A13	Bet Nahrain Assyrian Heritage Centre South Asian Heritage Association of Hamilton & Region ³	6th Annual Assyrian Festival of Nusardil	A	\$1,196	\$4,994	\$1,196	\$16,645	\$0
CCH-A14		Spring Festival of South Asia	A	\$5,250	\$9,175	\$5,250	\$37,250	\$0
CCH-A15	Lynden Canada Day Committee	Artist fees	A	\$7,500	\$7,500	\$7,500	\$40,000	\$0
CCH-A16	Waterdown's Oh Canada Ribfest (Rotary Club of Waterdown)	Waterdown's Rotary Ribfest	A	\$25,000	\$30,000	\$25,000	\$195,000	\$0
CCH-A17	Concession BIA	Concession Sidewalk Sounds 2022	A	\$4,000	\$5,000	\$4,000	\$30,990	\$0
CCH-A18	Hamilton-Halton Chinese Choir	Concert & Established Activities	A	\$1,400	\$3,100	\$1,400	\$8,600	\$0
CCH-A19	CACTUS FESTIVAL OF DUNDAS ONTARIO Comunita Racalmutese Maria SS Del Monte	DUNDAS CACTUS FESTIVAL	B	\$35,700	\$53,400	\$35,700	\$274,291	\$0
CCH-A20	Ontario Inc.	Communities, Culture & Heritage Ancaster Heritage Days 2022 Winter Event	B	\$6,000	\$12,488	\$6,000	\$99,802	\$0
CCH-A21	Ancaster Heritage Days		B	\$1,995	\$2,685	\$1,995	\$8,950	\$0

2022 City Enrichment Fund

Communities, Culture & Heritage

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
	Sinfonia Ancaster (Ancaster Society for the							
CCH-A22	Performing Arts)	Sinfonia Ancaster	B	\$8,231	\$11,081	\$8,231	\$36,935	\$0
CCH-A23	Dundas Cactus Parade Inc.	Dundas Cactus Parade	B	\$10,649	\$10,665	\$10,649	\$35,550	\$0
CCH-A24	Winona Peach Festival	Community, Culture & Heritage	B	\$86,700	\$98,000	\$86,700	\$355,600	\$0
CCH-A25	Zula Music And Arts Collective Hamilton	2022 Something Else! Festival	B	\$10,200	\$28,000	\$10,200	\$100,000	\$0
CCH-A26	Binbrook Parade Committee	Binbrook Santa Claus Parade	B	\$0	\$5,000	\$2,500	\$20,000	\$2,500
CCH-A27	Gourley Park Community Association	Winterfest/EasterEggHunt/Pumpkinfest Community, Culture, Heritage Large	B	\$2,765	\$5,355	\$2,765	\$12,600	\$0
CCH-A28	Downtown Dundas BIA	Events	B	\$28,886	\$34,153	\$28,886	\$113,842	\$0
CCH-A29	Hamilton and District Labour Council	Celebrating Labour Day 2022	B	\$10,000	\$10,000	\$10,000	\$42,120	\$0
CCH-A30	Asociacion Fraternidad Hispana (Fraternity Hispanic Association)	2022 Hispanic/Latin American Heritage Month	B	\$3,000	\$3,500	\$3,000	\$13,250	\$0
CCH-A31	Durand Neighbourhood Association Inc.	Communities, Culture and Arts	B	\$1,378	\$1,575	\$1,378	\$5,250	\$0
CCH-A32	Living Rock Ministries	Arts of August Musicata-Hamilton's Voices concert	B	\$5,000	\$5,000	\$5,000	\$35,000	\$0
CCH-A33	Musicata-Hamilton's Voices (John Laing Singers)	series, 2021-22	B	\$3,000	\$9,000	\$3,000	\$30,000	\$0
CCH-A34	Hamilton You Poets	My City, My Voice	B		\$21,738	\$0	\$72,462	\$0
CCH-A35	Concession BIA	Concession Streetfest 2022 Community Events Established -	B	\$8,000	\$10,000	\$8,000	\$46,570	\$0
CCH-A36	Stoney Creek BIA ¹	Saturdays in the Creek	B	\$3,000	\$10,000	\$3,000	\$45,000	\$0
CCH-A37	Hamilton Black Film Festival	Hamilton Black Film Festival	B		\$29,500	\$0	\$99,500	\$0
CCH-A38	Westdale Village BIA	Westdale Live!	B		\$5,000	\$0	\$28,000	\$0
CCH-A39	Dundas Museum & Archives (Dundas Historical Society Museum)	Discover Your Historical Dundas	B	\$5,100	\$5,000	\$5,000	\$20,352	-\$100
CCH-A40	Festitalia Corporation ³	Festitalia 2022	B	\$18,750	\$25,000	\$18,750	\$170,700	\$0
CCH-A41	INDIA CANADA SOCIETY	GANDHI PEACE FESTIVAL	B	\$4,140	\$4,140	\$4,140	\$15,000	\$0
CCH-A42	Stoney Creek Santa Claus Parade Children's International Learning Centre	Santa Clause Parade	B	\$3,000	\$3,500	\$3,000	\$32,100	\$0
CCH-A43	(Hamilton)	Festivals of Light Educational Programme	B	\$0	\$12,000	\$0	\$53,860	\$0
CCH-A44	Chorus Hamilton	Chorus Hamilton's 2022 season CWH - Canada's Flying Museum is 50	B	\$5,000	\$5,000	\$5,000	\$38,660	\$0
CCH-A45	Canadian Warplane Heritage	years old	D		\$146,187	\$0	\$462,300	\$0
CCH-A46	Sustainability Leadership (Sustainable Hamilton)	Signature Event Series	D		\$25,000	\$0	\$116,415	\$0

2022 City Enrichment Fund

Communities, Culture & Heritage

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CCH-A47	Ottawa Street Business Improvement Area	2022 Sidewalk Sale	D		\$3,600	\$0	\$10,779	\$0
CCH-A48	Ottawa Street Business Improvement Area	Holiday Cheer 2022	D		\$3,600	\$0	\$9,800	\$0
CCH-A49	iBalance Living Association	New links between Seniors –Juniors under the Covid-Crisis	D		\$5,000	\$0	\$9,423	\$0
CCH-A50	Flamborough Santa Claus Parade ²	FSCP	C	\$15,000	\$15,000	\$15,000	\$60,000	\$0
CCH-A51	Hamilton Waterfront Trust ²	Waterfront Tours - Hamilton Waterfront Trolley and Hamiltonian Tour Boat-Sightseeing Tours by Land & Water	C	\$5,000	\$30,000	\$5,000	\$120,000	\$0
CCH-A52	Polish Symfonia Choir ²	Christmas Carols Concert	C	\$4,038	\$4,467	\$4,038	\$14,890	\$0
CCH-A53	Hamilton Santa Claus Parade Committee ^{2,3}	Hamilton Santa Claus Parade	C	\$46,231	\$50,000	\$46,231	\$90,280	\$0
CCH-A54	African Caribbean Cultural Potpourri Inc (ACCPI)	ACCPI Scholarship Awards Program	DNQ		\$6,000		\$24,000	\$0
CCH-A55	The Gasworks (The John and Ellie Voortman Charitable Foundation)	Porch Concerts at The Gasworks	DNQ		\$1,500		\$5,000	\$0
				\$505,184	\$964,120	\$542,320		\$37,136
New Projects								
CCH-B1	Pride Hamilton ¹	Pride Hamilton 2022	A	\$7,650	\$46,050	\$26,250	\$153,500	\$18,600
CCH-B2	Red Betty Theatre	Rukmini's Gold Production	A		\$31,000	\$23,250	\$116,830	\$23,250
CCH-B3	Zula Music And Arts Collective Hamilton	2022 Watch it Burn! Series	B		\$30,000	\$0	\$100,000	\$0
CCH-B4	Canadian Society of Contemporary Iron Arts	Communities, Culture And Heritage: New Projects	B		\$2,000	\$0	\$10,000	\$0
CCH-B5	Ghana Association of Hamilton	Ghanaian Cultural Fiesta	B		\$2,165	\$0	\$6,807	\$0
CCH-B6	hamilton jewish federation	#nomoreantisemitism international conference	D		\$45,000	\$0	\$154,500	\$0
CCH-B7	Sir John A. Macdonald Society	Canada's Birthday Celebration	D		\$600	\$0	\$2,000	\$0
				\$7,650	\$156,815	\$49,500		\$41,850
Capacity Building								
CCH-C1	Zula Music And Arts Collective Hamilton	Zula Reconstruction	A		\$2,460	\$1,845	\$8,200	\$1,845
CCH-C2	Women's Art Association of Hamilton	Capacity Building for Arts Organizations	D		\$7,600	\$0	\$26,200	\$0
CCH-C3	BLK OWNED Ltd	Black Youth Entrepreneurship Hub	DNQ		\$30,000		\$156,576	\$0
CCH-C4	Jazz Connection Big Band	The Jazz Connection Big Band 2022/23 Season	DNQ		\$6,000		\$20,000	\$0

2022 City Enrichment Fund

Communities, Culture & Heritage

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
CCH-C5	Ottawa Street Business Improvement Area	Sew Hungry 2022	DNQ	\$7,892	\$14,228	\$7,892	\$47,426	\$0
TOTAL Communities, Culture & Heritage (CCH)				\$520,726	\$1,181,223	\$601,557		\$1,845
								\$80,831

¹ Final Report Pending

² Addition Information Required

³ Owes funds to the City

2022 City Enrichment Fund

Sports & Active Lifestyles

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
SAL-A1	Saltfleet Stoney Creek Soccer Club	GrassRoots Training Program - Revitalizing after the Pandemic	A	\$3,825	\$10,000	\$3,825	\$350,000	\$0
SAL-A2	Flamborough Dundas Soccer Club	FDSC followsLTPD for Grassroots Soccer	A	\$3,902	\$7,680	\$3,902	\$35,000	\$0
SAL-A3	The Hamilton and District Soccer Association ¹	Hamilton Soccer - GrassRoots Soccer - Retention of Program Initiatives	A	\$7,500	\$10,000	\$7,500	\$65,000	\$0
SAL-A4	Hamilton Ringette Association ¹	FUN 1 FUN2 FUN3	A		\$7,500	\$5,625	\$33,950	\$5,625
SAL-A5	Friends of Bishop Ryan	Wrestling Development Program	A		\$7,500	\$5,625	\$28,750	\$5,625
SAL-A6	Hamilton Aquatic Club (Hamilton-Wentworth Aquatic Club)	Swimmer Support Program	A		\$7,500	\$5,625	\$49,922	\$5,625
SAL-A7	Hamilton Aquatic Water Polo Club	Long-Term Athlete Development Planning and Implementation	B		\$1,395	\$698	\$4,650	\$698
Long Term Athlete Development Total				\$15,227	\$51,575	\$32,800		\$17,573
SAL-B1	Flamborough Dundas Soccer Club	New Club Awareness	A	\$7,491	\$5,400	\$5,400	\$18,000	-\$2,091
SAL-B2	Hamilton Olympic Club	Hamilton Olympic Club - Sport Awareness Program	A		\$7,500	\$5,625	\$64,123	\$5,625
SAL-B3	Saltfleet Stoney Creek Soccer Club	SSC - Advanced Development Program - Re-Building the Pathway	A	\$5,355	\$10,000	\$5,355	\$200,000	\$0
SAL-B4	SportHamilton	SportHamilton Community Communication strategy project	A		\$8,550	\$5,625	\$28,500	\$5,625
SAL-B5	Hamilton Aquatic Club (Hamilton-Wentworth Aquatic Club)	HAC Sport Promotion Program	A		\$5,615	\$4,211	\$18,719	\$4,211
SAL-B6	The Hamilton and District Soccer Association ¹	Hamilton Soccer - Continued Enhancement of the Match Official Mentorship	A	\$5,722	\$7,500	\$5,722	\$36,000	\$0
Sport Awareness Total				\$18,568	\$44,565	\$31,938		\$13,370
SAL-C1	The Equestrian Association for the Disabled (TEAD)	Trotting into the Future	A		\$6,500	\$4,875	\$36,350	\$4,875
SAL-C2	Flamborough Dundas Soccer Club	FDSC Builds Capacity	A	\$7,500	\$7,680	\$7,500	\$25,600	\$0
SAL-C3	Hamilton Hornets Rugby Football Club	Hornets Sustainability Program	A	\$7,500	\$7,500	\$7,500	\$25,700	\$0
SAL-C4	National Cycling Centre-Hamilton	2022 NCCH Building the Support Team for Riders/Cyclists	A		\$5,831	\$4,373	\$19,436	\$4,373
SAL-C5	Monte Cristo Track Club	Monte Cristo Track Club - Capacity Building Project	A		\$7,200	\$5,400	\$24,000	\$5,400
SAL-C6	Golden Horseshoe Track & Field Council	Golden Horseshoe Track & Field Council - Capacity Building Program	A	\$2,250	\$16,230	\$2,250	\$54,103	\$0
SAL-C7	Hamilton Aquatic Water Polo Club	Capacity building/Succession Planning/Professional Development	A		\$1,890	\$1,418	\$6,300	\$1,418
SAL-C8	Saltfleet Stoney Creek Soccer Club	SSC - Sports Rebuilding After COVID	B		\$9,000	\$4,500	\$30,000	\$4,500

2022 City Enrichment Fund

Sports & Active Lifestyles

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
Capacity Building Total				\$17,250	\$61,831	\$37,816		\$20,566
		Dragon Boat Sport Development and Wellness Program	A	\$7,500	\$7,500	\$7,500	\$26,375	\$0
SAL-D1	Knot A Breast							
SAL-D2	Wesley Urban Ministries Inc.	Youth Centre at Beasley	A	\$7,500	\$7,500	\$7,500	\$27,500	\$0
	BGC Hamilton-Halton (Hamilton East Kiwanis							
SAL-D3	Boys and Girls Club)	Midnight Basketball	A	\$7,500	\$7,500	\$7,500	\$72,500	\$0
	Binbrook Minor Baseball Association (8746974							
SAL-D4	Canada Association)	Rally Cap	B	\$5,414	\$4,950	\$4,950	\$16,500	-\$464
Sport Development Total				\$27,914	\$27,450	\$27,450		-\$464
	The Equestrian Association for the Disabled							
SAL-E1	(T.E.A.D.)	Subsidies	A	\$7,500	\$7,142	\$7,142	\$23,807	-\$358
	Royal Hamilton Yacht Club (Established 1888							
SAL-E2	Inc)	RHYC Able Sail	A	\$6,120	\$7,447	\$6,120	\$24,823	\$0
SAL-E3	Saltfleet Stoney Creek Soccer Club	SSC - Special Needs & All Abilities Program	A		\$7,500	\$5,625	\$35,000	\$5,625
SAL-E4	Flamborough Dundas Soccer Club	FDSC Special Soccer	A	\$6,900	\$8,880	\$6,900	\$29,600	\$0
	BGC Hamilton-Halton (Hamilton East Kiwanis							
SAL-E5	Boys and Girls Club)	BGC Hamilton-Halton HAVES	A	\$5,414	\$7,500	\$5,414	\$32,500	\$0
	Hamilton Aquatic Club (Hamilton-Wentworth							
SAL-E6	Aquatic Club)	Jimmy Thompson Swimmer Participation Program	B		\$11,550	\$5,775	\$38,500	\$5,775
Accessibility Total				\$25,934	\$50,019	\$36,976		\$11,042
SAL-F1	Fit Active Beautiful Foundation (FAB)	2022 FAB Girls 5K Challenge Program	A	\$7,500	\$7,500	\$7,500	\$69,041	\$0
SAL-F2	Flamborough Dundas Soccer Club	FDSC Soccer for Life	A	\$4,500	\$7,680	\$4,500	\$27,000	\$0
		New Hope Community Bikes Youth Cycling						
SAL-F3	New Hope Community Bikes	Program	A		\$7,500	\$5,625	\$25,493	\$5,625
SAL-F4	Hamilton Hornets Rugby Football Club	Rugby for Newcomers and At-Risk Youth	A	\$5,462	\$7,500	\$5,462	\$28,225	\$0
	BGC Hamilton-Halton (Hamilton East Kiwanis							
SAL-F5	Boys and Girls Club)	Let's Get Moving	A		\$7,500	\$5,625	\$407,500	\$5,625
	The Equestrian Association for the Disabled							
SAL-F6	(T.E.A.D.)	Equine-Assisted Learning Facilitator Certification	A		\$6,260	\$4,695	\$43,328	\$4,695
SAL-F7	Hamilton Chinese Sports Association	Go to Play	A	\$2,283	\$4,527	\$2,283	\$15,090	\$0
		Hamilton Olympic Club - Active for Life						
SAL-F8	Hamilton Olympic Club	Program	A		\$7,500	\$5,625	\$64,123	\$5,625
SAL-F9	Hamilton Aquatic Water Polo Club	Swim & Play Ball program	A		\$5,400	\$4,050	\$30,000	\$4,050
SAL-F10	Living Rock Ministries	Rock-in-Action	A	\$4,058	\$8,607	\$4,058	\$28,690	\$0
SAL-F11	Hamilton Bay Sailing Club	Sports and Active Lifestyle-Active for Life	B	\$7,500	\$7,500	\$7,500	\$43,482	\$0
	The Croatian Sports and Community Centre of							
SAL-F12	Hamilton	Expanding sports offerings for youth and adults	B		\$7,500	\$3,750	\$25,000	\$3,750

2022 City Enrichment Fund

Sports & Active Lifestyles

REF #	Organization Full Name:	Program Name:	RATING	2019 allocation	2022 Funding Requested	2022 Funding Approved	Program Budget	2019 vs 2022 Approved
Active for Life Total				\$31,303	\$84,974	\$60,673		\$29,370
SAL-G1	91st Highlanders Track and Field Association	Hamilton Indoor Games	Withdrew	\$23,535	\$31,500	\$0	\$105,000	-\$23,535
SAL-G2	Ontario Cycling Association	Paris to Ancaster Bicycle Race	A	\$12,546	\$20,000	\$12,546	\$225,000	\$0
Multi-Sport Total				\$36,081	\$51,500	\$12,546		-\$23,535
Grand Total				\$172,277	\$371,914	\$240,199		\$67,922

¹ Final Report Pending

COUNCIL COMMUNICATION UPDATES

June 17, 2022 to June 30, 2022

Council received the following Communication Updates during the time period listed above, the updates are also available to the public at the following link: <https://www.hamilton.ca/government-information/information-updates/information-updates-listing>, as per Section 5.18 of By-law 21-021 (A By-Law To Govern the Proceedings of Council and Committees of Council) a member of Council may refer any of the items listed below, to a Standing Committee by contacting the Clerk and it will be placed on the next available agenda of the respective Standing Committee.

Date	Department	Subject	Link
June 21, 2022	Planning and Economic Development	On-Street Patio Modifications (Wards 1, 2, 3, 4, 7, and 13)	https://www.hamilton.ca/sites/default/files/media/browser/2022-06-22/comm-update-on-street-patio-modifications.pdf
June 24, 2022	Public Works	Wild Waterworks 2022 Operating Season (ES22006) (City Wide)	https://www.hamilton.ca/sites/default/files/media/browser/2022-06-24/comms-update-pw-wild-waterworks-operating-season-jun242022.pdf
June 29, 2022	Public Health	Lessons Learned from the Vaccine Readiness Network (City Wide)	https://www.hamilton.ca/sites/default/files/media/browser/2022-06-30/comms-update-ph-lessons-learned-vaccine-readiness-network-jun292022.pdf

Authority: Item 4, Planning Committee
Report: 22-010 (PED22121)
CM: June 22, 2022
Ward: 11

Bill No. 165

CITY OF HAMILTON

BY-LAW NO. 22-

To Adopt:

**Official Plan Amendment No. 35 to the
Rural Hamilton Official Plan**

Respecting:

**5020 Tyneside Road
(Glanbrook)**

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. Amendment No. 35 to the Rural Hamilton Official Plan consisting of Schedule "1", hereto annexed and forming part of this by-law, is hereby adopted.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Rural Hamilton Official Plan Amendment No. 35

The following text, together with Appendix "A" – Volume 3: Appendix A – Site Specific Area Key Map, attached hereto, constitutes Official Plan Amendment No. "35" to the Rural Hamilton Official Plan.

1.0 Purpose and Effect:

The purpose and effect of this Amendment is to establish a Rural Site Specific Area to permit a severed lot with no frontage on a public road and a surplus farm dwelling lot that exceeds a depth of 122 metres.

2.0 Location:

The lands affected by this Amendment are known municipally as 5020 Tyneside Road, in the former Town of Flamborough.

3.0 Basis:

The basis for permitting this Amendment is to facilitate the implementation of the conditions of Committee of Adjustment applications GL/B-20:60 and GL/B-20:199 in order to permit severance of a surplus farm dwelling and associated variances for the severed property.

4.0 Actual Changes:

4.1 Volume 3 – Special Policy and Site Specific Areas

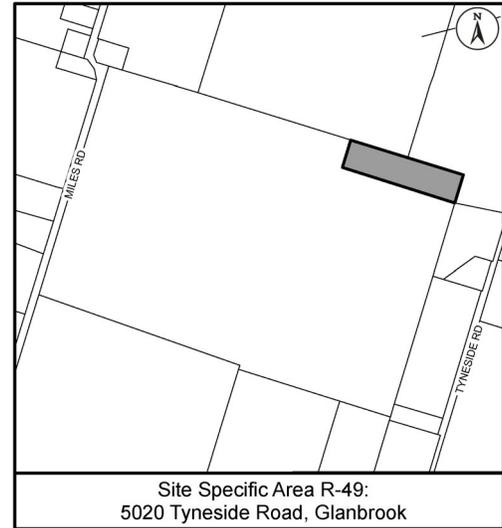
Text

4.1.1 Chapter B – Rural Site Specific Areas

- a. That Volume 3: Chapter B – Rural Site Specific Areas be amended by adding a new Site Specific Area, as follows:

“R-49 Lands located at 5020 Tyneside Road, former Township of Glanbrook

- 1.0 Notwithstanding Policies C.4.5.8.4 and F.1.14.2.1 c) vi) of Volume 1, for the property known municipally as 5020 Tyneside Road, Glanbrook, identified as Site Specific Area R-49, development or redevelopment on a severed lot with no frontage on a public road shall be permitted.
- 2.0 Notwithstanding Policy F.1.14.2.8 a) vi) 2. of Volume 1, for the property known municipally as 5020 Tyneside Road, Glanbrook, identified as Site Specific Area R-49, the property depth shall not exceed 328.0 metres.”



Schedules and Appendices

4.1.2 Appendix

- a. That Volume 3: Appendix A – Site Specific Area Key Map be amended by identifying the lands municipally known as 5020 Tyneside Road, Glanbrook as Site Specific Area R-49, as shown on Appendix “A”, attached to this Amendment.

5.0 Implementation:

An implementing Zoning By-Law Amendment will give effect to the intended uses on the subject lands.

This Official Plan Amendment is Schedule “1” to By-law No. 22-165 passed on the 8th day of July, 2022.

**The
City of Hamilton**

F. Eisenberger
Mayor

A. Holland
City Clerk

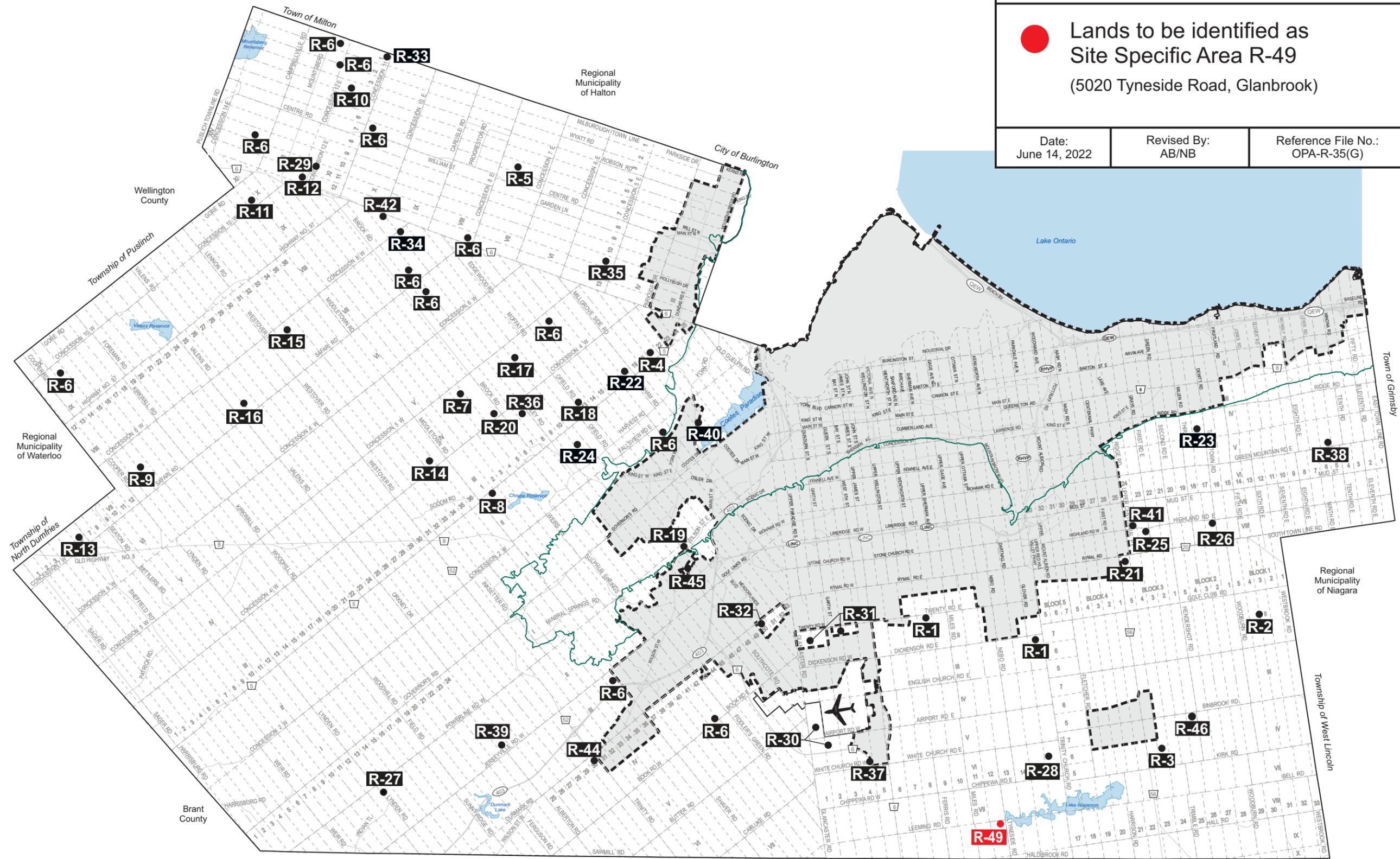
Appendix A
 APPROVED Amendment No. 35
 to the Rural Hamilton Official Plan

 Lands to be identified as
 Site Specific Area R-49
 (5020 Tyneside Road, Glanbrook)

Date:
 June 14, 2022

Revised By:
 AB/NB

Reference File No.:
 OPA-R-35(G)



- Legend**
-  Site Specific Areas (SSA)
 - R-** Refers to Rural Site Specific Area #, Volume 3, Chapter B
- Other Features**
-  Urban Area
 -  John C. Munro Hamilton International Airport
 -  Niagara Escarpment
 -  Urban Boundary
 -  Municipal Boundary

Council Adoption: September 27, 2006
 Ministerial Approval: December 24, 2008
 Effective Date: March 7, 2012

Rural Hamilton Official Plan
Volume 3: Appendix A
 Site Specific Key Map

 Not To Scale
 Date: May 2021


Authority: Item 4, Planning Committee
Report 22-010 (PED22121)
CM: June 22, 2022
Ward: 11

Bill No. 166

CITY OF HAMILTON

BY-LAW NO. 22-

To Amend Zoning By-law No. 05-200 Respecting Lands Located at 5015 and 5020 Tyneside Road, Glanbrook

WHEREAS the City of Hamilton has in force several Zoning By-laws which apply to the different areas incorporated into the City by virtue of the City of Hamilton Act, 1999 S.O. 1999, Chap. 14;

WHEREAS, THE City of Hamilton is the lawful successor of the former Municipalities identified in Section 1.7 of By-law No. 05-200;

WHEREAS, the first stage of Zoning By-law, being By-law No. 05-200 came into force and effect on the 25th day of May 2005; and

WHEREAS, the Council of the City of Hamilton, in adopting Item 4 of Report 22-010 of the Planning Committee, at its meeting held on the 22nd day of June, 2022, which recommended that Zoning By-law No. 05-200 be amended as hereinafter provided; and

AND WHEREAS this By-law conforms with the Rural Hamilton Official Plan upon adoption of Official Plan Amendment No. 35.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. That Map No. 210 and No. 211 of Schedule "A" – Zoning Maps of Zoning By-law No. 05-200, is amended by changing the zoning from Agriculture (A1) Zone, Conservation/Hazard Land-Rural (P7) Zone and Conservation/Hazard Land-Rural (P8) Zone to Agriculture (A1, 118) Zone, Conservation/Hazard Land-Rural (P7, 118) Zone, Conservation/Hazard Land-Rural (P8, 118) Zone, Agriculture (A1, 777) Zone, and Conservation/Hazard Land-Rural (P7, 777) Zone the extent and boundaries of which are shown on a plan hereto annexed as Schedule "A" to the By-law.
2. That Schedule "C": Special Exceptions is amended by adding the following new Special Exception:

- a) Adding reference to “5015 Tyneside Road” and “Map 210, 211” to the Property Address and Map Number table as follows:

Property Address	Map Numbers
5015 Tyneside Road	210, 211

- b) Adding subsection f) as follows:

The following regulations shall also apply for the property located at 5015 Tyneside Road:

- i) Notwithstanding Subsection 12.1.1, a Single Detached Dwelling and Residential Care Facility shall be prohibited on the subject lands.

3. That Schedule “C” Special Exceptions of By-law No. 05-200 is amended by adding an additional exception, as follows:

“777. Within the lands zoned Agriculture (A1, 777) Zone and Conservation/Hazard Land (P7, 777), and identified on Map Nos. 210 and 211 of Schedule “A” Zoning Maps and described as 5020 Tyneside Road, the following special provisions shall apply:

- i) Notwithstanding Section 4.3 a), frontage on a street shall not be required;
- ii) Notwithstanding Subsection 12.1.3.3 b), the minimum lot width shall be 0.0 metres;
- iii) Notwithstanding Section 3 of this By-law, for purposes of the definition of the Interior Lot shall mean a lot with no frontage on a street excluding a corner lot;
- iv) Notwithstanding Section 3 of this By-law, for the purposes of the definition of the Front Lot Line shall mean the easterly lot line shall be considered the front lot line of the subject lands;
- v) Notwithstanding Section 7.7.2.2 a), new accessory buildings to an existing single detached dwelling shall be permitted. Sections 4.8 and 4.8.1.2 shall apply for new accessory buildings;
- vi) The maximum gross floor area for an expansion to an existing building or structure, including an existing accessory building, shall not exceed 10% of the gross floor area of the existing building or structure including an existing accessory building and may include a deck within the permitted maximum; and,

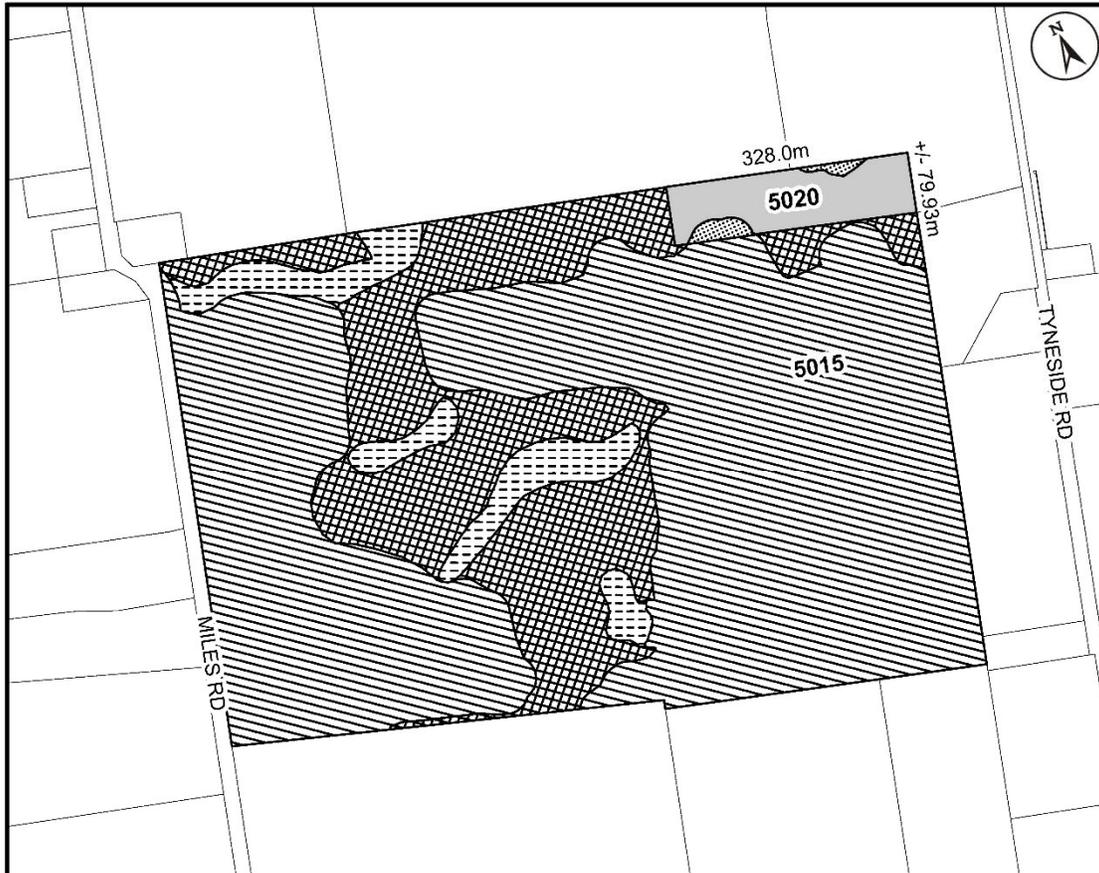
- vii) Notwithstanding Sections 7.7.2.3 i), 7.7.2.3 ii) and 7.7.2.3 iv), an existing single detached dwelling and existing accessory buildings or structures which are demolished in whole or in part may be rebuilt on a lot created subsequent to the effective date of By-law 22-166. Sections 12.1.3.3 a), c), d) e), f) shall apply for a single detached dwelling and Sections 4.8 and 4.8.1.2 shall apply for accessory buildings.

- viii) In addition to Section 7.7.2.2 b) ii), for purposes of the existing single detached dwelling, Section 12.1.3.3 c), d) e) and f) and for purposes of existing accessory buildings Sections 4.8 and 4.8.1.2 shall apply.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk



This is Schedule "A" to By-law No. 22-
 Passed the day of, 2022

 Mayor

 Clerk

Schedule "A"
 Map forming Part of
 By-law No. 22-_____
 to Amend By-law No. 05-200
 Map 210 & 211

- Subject Property**
 5015 and 5020 Tyneside Road, Glanbrook
-  Change in Zoning from Agriculture (A1) Zone to Agriculture (A1,118) Zone
 -  Change in Zoning from Conservation/Hazard Land Rural (P7) Zone to Conservation/Hazard Land Rural (P7,118) Zone
 -  Change in Zoning from Conservation/Hazard Land Rural (P8) Zone to Conservation/Hazard Land Rural (P8,118) Zone
 -  Change in Zoning from Agriculture (A1) Zone to Agriculture (A1, 777) Zone
 -  Change in Zoning from Conservation/Hazard Land Rural (P7) Zone to Conservation/Hazard Land Rural (P7, 777) Zone

Scale: N.T.S	File Name/Number: ZAC-21-032/RHOPA-21-015	 Hamilton
Date: April 1, 2022	Planner/Technician: AB/NB	
PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT		

Authority: Item 14, Committee of the Whole
 Report 01-003 (FCS01007)
 CM: February 6, 2001
 Wards: 1,2,3,4,10,12,13

Bill No. 167

**CITY OF HAMILTON
 BY-LAW NO. 22-**

**To Amend By-law No. 01-218, as amended,
 Being a By-law To Regulate On-Street Parking**

WHEREAS Section 11(1)1 of the *Municipal Act, S.O. 2001, Chapter 25*, as amended, confers upon the councils of all municipalities the power to enact by-laws for regulating parking and traffic on highways subject to the *Highway Traffic Act*;

AND WHEREAS on the 18th day of September, 2001, the Council of the City of Hamilton enacted By-law No. 01-218 to regulate on-street parking;

AND WHEREAS it is necessary to amend By-law No. 01-218, as amended.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. By-law No. 01-218, as amended, is hereby further amended by adding/deleting from the identified Schedules and Sections noted in the table below as follows:

Schedule	Section	Highway	Side	Location	Duration	Times	Days	Adding/ Deleting
6 - Time Limit	E	Pine	South	Locke to Pearl	3 hr	Anytime	Anyday	Deleting
6 - Time Limit	E	Ferguson Avenue South	West	Young Street to Forest Avenue	2 hr	Anytime	Anyday	Adding
6 - Time Limit	E	Pine Street	South	Locke Street to Pearl Street	1 hr	Anytime	Anyday	Adding

Schedule	Section	Highway	Side	Location	Times	Adding/ Deleting
8 - No Parking	E	Ferguson	West	T.H.&B. to Forest	8 am - 12 noon 2nd Thurs each month APRIL - NOV	Deleting
8 - No Parking	E	Hixon	South	200' east of Martin to 250' east of Martin	Anytime	Deleting
8 - No Parking	E	Hixon	South	200' east of Martin to 250' easterly	Anytime	Deleting

To Amend By-law No. 01-218, as amended,
Being a By-law to Regulate On-Street Parking

8 - No Parking	E	Kitty Murray Lane	East	Garner Road East to Roelfson Drive	Anytime	Deleting
8 - No Parking	D	Tanglewood Drive	East	DeGrow Crescent (northern intersection) to 37 metres south of DeGrow Crescent (southern intersection)	December 1st to March 31st	Deleting
8 - No Parking	D	Tanglewood Drive	West	37 metres south of Maggie Johnson Drive to Binbrook Road	December 1st to March 31st	Deleting
8 - No Parking	F	Memorial Avenue	South	From Kilbourn Avenue to Birchlawn Drive	8:00 am to 4:30 pm Monday to Friday Holidays Excepted	Deleting
8 - No Parking	E	Ferguson Avenue South	West	Young Street to Forest Avenue	8 am - 12 noon 2nd Thurs each month APRIL - NOV	Adding
8 - No Parking	E	Ferguson Avenue South	East	Young Street to southerly end	8 am - 12 noon 2nd Fri each month APRIL - NOV	Adding
8 - No Parking	A	Foxborough Drive	East & South	58 metres north of Liam Drive to 22 metres north and east thereof	Anytime	Adding
8 - No Parking	F	Fruitland Road	West	55 metres north of Lakeview Drive to northern end	Anytime	Adding
8 - No Parking	E	Tyrone Drive	South	West 5th Street to Caledon Avenue	December 1st to March 31st	Adding

Schedule	Section	Highway	Side	Location	Times	Adding/Deleting
12 - Permit	E	Campbell	South	from 28.8m east of Balmoral to 5m easterly	Anytime	Deleting
12 - Permit	E	Campbell Avenue	South	29 metres east of Balmoral Avenue North to 9 metres east thereof	Anytime	Adding
12 - Permit	E	Caroline Street North	West	30 metres south of Barton Street West to southerly end	Anytime	Adding

Schedule	Section	Highway	Side	Location	Times	Adding/Deleting
13 - No Stopping	F	Fruitland	Both	North Service Road to 30m northerly	Anytime	Deleting
13 - No Stopping	F	Kilbourn	East	From Memorial Avenue to a point 100m south	8:00 a.m. to 4:00 p.m. Monday to Friday	Deleting
13 - No Stopping	F	Fruitland Road	East	North Service Road to 30m northerly	Anytime	Adding
13 - No Stopping	F	Fruitland Road	West	Lakeview Drive to 55m northerly	Anytime	Adding

To Amend By-law No. 01-218, as amended,
Being a By-law to Regulate On-Street Parking

13 - No Stopping	F	Kilbourn Avenue	East	Memorial Avenue to Royce Avenue	8:00 a.m. to 4:00 p.m. Monday to Friday	Adding
13 - No Stopping	F	Kilbourn Avenue	West	Royce Avenue to 48m northerly	Anytime	Adding
13 - No Stopping	F	Kilbourn Avenue	West	48m north of Royce Avenue to 132m northerly	8:00 a.m. to 4:00 p.m. Monday to Friday	Adding
13 - No Stopping	F	Royce Avenue	North	15m west of Kilbourn Avenue to 104m westerly	8:00 a.m. to 4:00 p.m. Monday to Friday	Adding
13 - No Stopping	F	Royce Avenue	South	15m west of Kilbourn Avenue to 45m westerly	8:00 a.m. to 4:00 p.m. Monday to Friday	Adding

Schedule	Section	Highway	Side	Location	Times	Adding/ Deleting
14 - Wheelchair LZ	B	Central Park Avenue	South	54 metres east of the west intersection with Kemp Drive to 15 metres east thereof	8:00 a.m. to 5:00 p.m. Monday to Friday	Adding

Schedule	Section	Highway	Side	Location	Times	Adding/ Deleting
20 - School Bus LZ	F	Kilbourn	West	Memorial to 115m southerly	8:00 a.m. to 4:00 p.m. Monday to Friday	Deleting
20 - School Bus LZ	F	Kilbourn Avenue	West	48m north of Royce Avenue to 90m northerly	8:00 a.m. to 4:00 p.m. Monday to Friday	Adding

- Subject to the amendments made in this By-law, in all other respects, By-law No. 01-218, including all Schedules thereto, as amended, is hereby confirmed unchanged.
- This By-law shall come into force and take effect on the date of its passing and enactment.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 3, Public Works Committee
Report 22-004 (PW22013)
CM: March 30, 2022
Ward: 12

Bill No. 168

CITY OF HAMILTON

BY-LAW NO. 22-

**To Establish City of Hamilton Land
Described as Part of Lot 55, Concession 2 in the Geographic Township of
Ancaster, designated as Parts 5 and 6 on Plan 62R-7673
as Part of Scenic Drive**

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001* authorize the City of Hamilton to pass by-laws necessary or desirable for municipal purposes, and in particular by-laws with respect to highways;

AND WHEREAS section 31(2) of the *Municipal Act, 2001* provides that land may only become a highway by virtue of a by-law establishing the highway.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. The land, owned by and located in the City of Hamilton, described as **Part of Lot 55, Concession 2 in the Geographic Township of Ancaster, in the City of Hamilton, designated as Parts 5 and 6 on Plan 62R-7673**, is established as a public highway, forming part of **Scenic Drive**;
2. The General Manager of Public Works or their authorized agent is authorized to establish the said land as a public highway; and
3. This By-law comes into force on the date of its registration in the Land Registry Office (No. 62).

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 3, Public Works Committee
Report 22-004 (PW22013)
CM: March 30, 2022
Ward: 2

Bill No. 169

CITY OF HAMILTON

BY-LAW NO. 22-

**To Establish City of Hamilton Land
Described as Part of Lot A and Part of the Block Between Guise Street East,
James Street North, Hughson Street North, and Burlington Street East, on James
Hughson Survey (unregistered), as in Instrument Number AB214414
as Part of Burlington Street East**

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001* authorize the City of Hamilton to pass by-laws necessary or desirable for municipal purposes, and in particular by-laws with respect to highways;

AND WHEREAS section 31(2) of the *Municipal Act, 2001* provides that land may only become a highway by virtue of a by-law establishing the highway.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. The land, owned by and located in the City of Hamilton, **described as Part of Lot A, James Hughson Survey (unregistered) on the East Side of James Street North and North Side of Burlington Street East, and Part of the Block between Guise Street East, James Street North, Hughson Street North, and Burlington Street East, on James Hughson Survey (unregistered), in the City of Hamilton, as in Instrument Number AB214414**, is established as a public highway, forming part of **Burlington Street East**;
2. The General Manager of Public Works or their authorized agent is authorized to establish the said land as a public highway; and
3. This By-law comes into force on the date of its registration in the Land Registry Office (No. 62).

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 3, Public Works Committee
Report 22-004 (PW22013)
CM: March 30, 2022
Ward: 1

Bill No. 170

CITY OF HAMILTON

BY-LAW NO. 22-

**To Establish City of Hamilton Land
Described as Part of Lot 55, Concession 1 in the Geographic Township of
Ancaster, designated as Part 3 on Plan 62R-4056
as Part of Ewen Road**

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001* authorize the City of Hamilton to pass by-laws necessary or desirable for municipal purposes, and in particular by-laws with respect to highways;

AND WHEREAS section 31(2) of the *Municipal Act, 2001* provides that land may only become a highway by virtue of a by-law establishing the highway.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. The land, owned by and located in the City of Hamilton, described as **Part of Lot 55, Concession 1 in the Geographic Township of Ancaster, in the City of Hamilton, designated as Part 3 on Plan 62R-4056**, is established as a public highway, forming part of **Ewen Road**;
2. The General Manager of Public Works or their authorized agent is authorized to establish the said land as a public highway; and
3. This By-law comes into force on the date of its registration in the Land Registry Office (No. 62).

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 3, Public Works Committee
Report 22-004 (PW22013)
CM: March 30, 2022
Ward: 12

Bill No. 171

CITY OF HAMILTON

BY-LAW NO. 22-

**To Establish City of Hamilton Land
Described as Part of Lot 40, Concession 2 in the Geographic Township of
Ancaster, designated as Parts 12 and 13 on Plan 62R-21873
as Part of Jerseyville Road**

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001* authorize the City of Hamilton to pass by-laws necessary or desirable for municipal purposes, and in particular by-laws with respect to highways;

AND WHEREAS section 31(2) of the *Municipal Act, 2001* provides that land may only become a highway by virtue of a by-law establishing the highway.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. The land, owned by and located in the City of Hamilton, described as **Part of Lot 40, Concession 2 in the Geographic Township of Ancaster, in the City of Hamilton. designated as Parts 12 and 13 on Plan 62R-21873**, is established as a public highway, forming part of **Jerseyville Road**;
2. The General Manager of Public Works or their authorized agent is authorized to establish the said land as a public highway; and
3. This By-law comes into force on the date of its registration in the Land Registry Office (No. 62).

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 9, Public Works Committee
Report 07-016 (PW07153)
CM: December 12, 2007
Ward: City Wide
Bill No. 172

CITY OF HAMILTON

BY-LAW NO. 22-

To Amend By-law No. 01-215 Being a By-law To Regulate Traffic

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001, S.O. 2001, c. 25*, authorize the City of Hamilton to pass by-laws as necessary or desirable for the public and municipal purposes, and in particular paragraphs 4 through 8 of subsection 10(2) authorize by-laws respecting: assets of the municipality, the economic, social and environmental well-being of the municipality; health, safety and well-being of persons; the provision of any service or thing that it considers necessary or desirable for the public; and the protection of persons and property;

AND WHEREAS on the 18th day of September, 2001, the Council of the City of Hamilton enacted By-law No. 01-215 to regulate traffic; and

AND WHEREAS it is necessary to amend By-law No. 01-215.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. Schedule 2 (Speed Limits) of By-law No. 01-215, as amended, is hereby further amended by removing from Section "A" (Ancaster) thereof the following items, namely:

Filman Road (Upper)	Mohawk Road	End of travelled portion of road	40km/h
Montgomery Drive	Hendry Lane	Bailey Avenue	40km/h

And by adding to section "B" (Dundas) thereof the following items, namely;

Albert Street	Alma Street	Park Street West	30
Alma Street	Sydenham Street	Parkside Avenue	30
Dundana Avenue	Lynden Avenue	Old Ancaster Road	30
Elgin Street	Victoria Street	Park Street West	30
Melville Street	Princess Street	Sydenham Street	30
Victoria Street	Princess Street	Sydenham Street	30

And by removing from section "B" (Dundas) thereof the following items, namely;

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

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Bond Street	Park Street West	King Street West	40
Brock Street North	Melville Street	King Street West	40
Cross Street	Alma Street	King Street West	40
Dundana Avenue	Lynden Ae	Old Ancaster Rd	40km/h
Grant Boulevard	Osler Dr	McMaster Ave	40km/h
Lynden Road	South Street	Little John Road	40
Melville Street	Brock Street North	Cross Street	40
Old Guelph Rd	Hwy 6	Hwy 403 Overpass	50km/h
Old Guelph Road	Northerly limit	York Road	40
Park Street West	Westerly limit	York Road	40
Patterson Road	Valley Road	Old Guelph Rd	40km/h
Robinhood Dr.	Old Ancaster Rd.	Little John Rd.	40
South Street	Ogilvie	Osler	40
Victoria St.	Princess St.	Sydenham St.	40

And by adding to section "C" (Flamborough) thereof the following items, namely;

Howard Street	A point 50 meters North of Albert Street	A point 150 meters South of Maple Street	30
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And by adding to section "E" (Hamilton) thereof the following items, namely;

Alderson Drive	Christie Street	Upper James Street	30
Belmont Avenue	Cannon Street East	A point 100 meters North of Campbell Avenue	30
Bendamere Avenue	Upper Paradise Road	West 23rd Street	30
Berko Avenue	Bellingham Drive	Edwina Place	30
Bonaparte Way	Brigade Drive	A point 30m east of Brigade Drive (East intersection)	30
Brigade Drive	Bonaparte Way (West intersection)	A point 25m south of Essling Avenue	30
Burkholder Drive	East 25th Street	Upper Sherman Avenue	30
Christopher Drive	Christie Street	Upper James Street	30
Churchill Avenue	Prince George Avenue	Wavell Avenue	30
Crerar Drive	Resolute Drive	Westerly end	30
Cromwell Crescent	Greenford Drive	Owen Place	30
Cumberland Avenue	Norway Avenue	Balsam Avenue South	30
Currie Street	Reno Avenue	Larch Street	30
Dunsmure Road	Ottawa Street North	Balmoral Avenue North	30
East 24th Street	Halam Avenue	30 metres east of Sloan Ave.	30
East 25th Street	Halam Avenue	30 metres east of Gildea St.	30
East 27th Street	Franklin Road	Burkholder Drive	30
East 37th Street	Fennell Avenue East	Seventh Avenue	30
East 38th Street	Fennell Avenue East	A point 200 m south of	30

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

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		Fennel Avenue East	
East 5th Street	Fennell Avenue East	Brucedale Avenue East	30
Eastwood Street	Melvin Avenue	Heath Street	30
Folkestone Avenue	Berko Avenue	A point 70 meters West of Ling Street	30
Franklin Road	Upper Sherman Avenue	30 metres east of East 22nd Street	30
Glen Echo Drive	Secord Drive	Rainbow Drive	30
Hamilton Avenue	Concession Street	Mountain Park Avenue	30
Heath Street	Sumach Street	Eastwood Street	30
Hester Street	Upper James Street	Upper Wellington Street	30
Inverness Avenue East	Wavell Avenue	Skyland Drive	30
Jessica Street	Onyx Drive	Vineberg Drive	30
Karen Crescent	San Antonio Drive	San Pedro Drive	30
Lamoreaux Street	Dundurn Street North	Strathcona Avenue North	30
Leslie Avenue	Upper Paradise Road	West 23rd Street	30
Maplewood Avenue	Blake Street	Albert Street	30
Meadowvale Avenue	Centennial Parkway South	Cromwell Crescent	30
Melvin Avenue	Woodward Avenue	Talbot Street	30
Palmer Road	Reno Avenue	Behan Street	30
Prince George Avenue	Queensdale Avenue East	Churchill Avenue	30
Province Street North	Cannon Street East	A point 50 metres north of Dunsmure Road	30
Queen Victoria Drive	Rochelle Avenue	Queenslea Drive	30
Rainbow Drive	Secord Drive	Glen Echo Drive	30
Reno Avenue	Palmer Road	Currie Street	30
Ronaldshay Avenue	Hester Street	Hayden Street	30
Roxborough Avenue	Edgemont Street North	Wexford Avenue North	30
San Antonio Drive	San Remo Drive	San Pedro Drive	30
San Remo Drive	San Francisco Avenue	Lavina Crescent	30
Sanatorium Road	Rice Avenue	West 31st Street	30
Secord Drive	Sunrise Drive	Glen Echo Drive	30
Springer Avenue	Main Street East	Maplewood Avenue	30
Strathcona Avenue North	Tom Street	Head Street	30
Viewpoint Avenue	Concession Street	Mountain Park Avenue	30
West 25th Street	Elmwood Avenue	A point 100 metres north of Leslie Avenue	30
West 27th Street	Elmwood Avenue	A point 100 metres north of	30

Leslie Avenue

And by removing from section "E" (Hamilton) thereof the following items, namely;

7m Avenue	East 34th Street	Upper Gage Avenue	40
Adair Avenue	Melvin Avenue	Lucerne Avenue	40
Afton Avenue	Norway Avenue	Easterly Limit	40
Agincourt Avenue	Beach Road	McAnulty Boulevard	40
Agnes Street	Barton Street	Campbell Avenue	40
Airdrie Avenue	Melvin Avenue	Britannia Avenue	40
Albemarle Street	Gertrude Street	Southerly Limit	40
Albert Street	Main Street East	Maplewood Avenue	40
Alderson Drive	Westerly limit	Upper James Street	40
Alice Street	Hope Avenue	Britannia Avenue	40
Allandale Street	Hope Avenue	Britannia Avenue	40
Argyle Avenue	Ottawa Street	Frederick Avenue	40
Armstrong Avenue	Glengrove Avenue / Oriole Crescent	Hayes Avenue	40
Ashford Boulevard	Main Street East	Maplewood Avenue	40
Assisi Street	Northerly limit	Sulmona Drive	40
Avondale Street	Gertrude Street	Primrose Avenue	40
Ayr Avenue	Reid Avenue	Bingham Road	40
Balmoral Avenue North	Barton Street East	Main Street East	40
Barnaby Street	Brampton Street	Rennie Street	40
Bayfield Avenue	Beach Road	McAnulty Boulevard	40
Beach Road	Lottridge Street	Ottawa Street North	40
Beaconsfield Drive	Currie Street	Upper Ottawa Street	40
Beddoe Drive	Studholme Road	Southerly Limit	40
Behan Street	Birchcliffe Crescent	Billington Crescent	40
Beland Avenue	Roxborough Avenue	Lucerne Avenue	40
Beland Court	Lucerne Avenue	Southerly End	40
Bellini Lane	Davinci Boulevard	Easterly Limit	40
Belmont Avenue	Barton Street East	King Street East	40
Belvidere Avenue	Inverness Avenue	Concession Street	40
Belview Avenue	Northerly limit	King Street East	40
Bendemere Ave.	West 28th St.	West 24th St.	40km/h
Benson Avenue	McAnulty Boulevard	Beach Road	40
Berko Ave.	Bellingham Dr.	Edwina Pl.	40
Berko Avenue	Upper Sherman Avenue	Edwina Place	40
Bernard Street	Melvin Avenue	Martha Street	40
Bernini Court	Giotto Lane	Easterly Limit	40
Biggar Avenue	Sherman Avenue North	Lottridge Street	40
Bilardi Court	Embury Court	Southerly Limit	40
Billington Crescent	Behan Street	Larch Street	40

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

(E. Leg)

Bingham Road	Glengrove Avenue	Hayes Avenue	40
Birchcliffe Crescent	Behan Street	Larch Street	40
Birchview Drive	North Limit	Limeridge Road	40
Birmingham Street	Burlington Street East	Beach Road	40
Blair Avenue	Melvin Avenue	Marth Street	40
Blake Street	Main Street East	Cumberland Avenue	40
Bonaparte Way	Brigade Dr.	Brigade Dr	40
Brant Street	Wentworth Street	Sherman Avenue	40
Brenlyn Court	Sanatorium Road	Southerly limit	40
Brewster Street	Limeridge Road	Birchcliffe Crescent	40
Brigade Dr.	Bonaparte Way (east end)	Bonaparte Way (west end)	40
Brighton Avenue	Northerly Limit	Morley Street	40
Brucedale Avenue	Upper James Street	Upper Wellington Street	40
Brunswick Street	Walmer Road	Melvin Avenue	40
Burgess Avenue	Leaside Road	Brampton Street	40
Burkholder Dr.	Upper Sherman Ave.	East 25th St.	40
			km/h
Cambridge Avenue	Frederick Avenue	Kenilworth Avenue	40
Campbell Avenue	Belmont Avenue	Ottawa Street North	40
Campbell Avenue	Ottawa Street	Robins Avenue	40
Campbell Avenue	Belmont Avenue	Ottawa Street North	40
Campbell Avenue	Ottawa Street	Robins Avenue	40
Carlisle Street	Argyle Avenue	Campbell Avenue	40
Cavell Avenue	Linden Street	Primrose Avenue	40
Cedar Avenue	Maplewood Avenue	Cumberland Avenue	40
Central Avenue	Parkdale Avenue	Reid Avenue	40
Christie Street	Rymal Road West	Christopher Drive	40
Christopher Drive	Malton Drive	Upper James Street	40
Churchill Avenue	Upper James Street	Prince George Avenue	40
Cluny Avenue	Rosslyn Avenue North	Ottawa Street North	40
Concession Street	Belvidere Avenue	120 metres east of East 43rd Street	40
Coopershawk Crescent	Hawkswood Trail	Falconridge Drive	40
Corbett Street	Brampton Street	Rennie Street	40
Craigmillier Avenue	Rosslyn Avenue North	Ottawa Street North	40
Cromwell Cres.	Greenford Dr.	Owen Pl.	40
Cumberland Avenue	Wentworth Street	Gage Avenue	40
Currie Street	Mohawk Road East	Larch Street	40
Dalhousie Avenue	Rosslyn Avenue North	Ottawa Street North	40
Dalkeith Avenue	Rosslyn Avenue North	Ottawa Street North	40
Dallas Avenue	West Limit	Upper Gage Avenue	40
Davinci Boulevard	Rymal Road	Medici Lane	40

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

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Delawana Drive	Kenora Avenue	Centennial Parkway	40
Delaware Avenue	Wentworth Avenue	Sherman Avenue	40
Delena Avenue	Roxborough Avenue	Central Avenue	40
Depew Street	Burlington Street East	Southerly Limit	40
Devonport Street	Tom Street	York Boulevard	40
Dickson Street	Burlington Street	Brant Street	40
Dover Dr.	Nash Rd.	Greenford Dr.	40
Dubarry Boulevard	Currie Street	Lorraine Drive	40
Dunn Avenue	Leaside Road	Brampton Street	40
Eagleleglen Way	Hawkswood Trail	Giotto Lane	40
East 22nd Street	Concession Street	Fennell Avenue	40
East 24th Street	Upper Wentworth Street	Fennell Avenue	40
East 25th St	Franklin Rd.	Gildea St.	40
East 26th Street	Crockett street	Queensdale Ave. E.	40km/h
East 27th St.	Franklin Rd.	Burkholder Dr.	40
East 27th Street	Queensdale Avenue	Burkholder Drive	40
East 28th Street	Franklin Road	Fennell Avenue	40
East 37m Street	Fennell Avenue	Mohawk Road	40
East 38m Street	Fennell Avenue	Mohawk Road	40
East 44th Street	Fennell Avenue	Ninth Avenue	40
East 45th Street	Fennell Avenue	Ninth Avenue	40
East 5th Street	Fennell Avenue	Brucedale Avenue	40
East Bend Avenue North	Cannon Street East	King Street East	40
East Bend Avenue South	King Street East	Main Street East	40
Eastbourne Avenue	Main Street	Southerly Limit	40
Eastvale Place	Armstrong Avenue	Armstrong Avenue	40
Eastwood Street	Melvin Avenue	Heath Street	40
Eastwood Street	Melvin	Heathe	40
Eaton Place	Bingham road	Bingham Road	40
Edgar Street	Argyle Avenue	Campbell Avenue	40
Edinburgh Avenue	Ottawa Street	Province Street	40
Edison Street	New Street	Easterly Limit	40
Edwina Place	Berko Avenue	Upper Gage Avenue	40
Ellis Avenue	Barton Street	Britannia Avenue	40
Elm Street	Balsam Avenue South	Easterly Limit	40
Embury Court	Talbot Street	Easterly Limit	40
Empress Avenue	Prince George Avenue	Brucedale Avenue	40
Fairleigh Crescent	Delaware Avenue	Cumberland Avenue	40
Fairview Avenue	Cannon Street East	King Street East	40
Falconridge Drive	Westerly Limit	Upper Paradise Road	40
Florence Street	Dundurn Street North	Ray Street North	40
Folkstone Ave.	Berko Ave.	Lawnhurst Dr.	40

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

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Folkstone Avenue	Berko Avenue	Lawson Street	40
Franklin Rd.	Upper Sherman Ave.	150 m. west of East 24thSt	40
Fraser Avenue	Barton Street	Edinburgh Avenue	40
Frederick Avenue	Barton Street	Roxborough Avenue	40
George Street	Locke Street	Bay Street	40
Gertrude Street	Gage Avenue North	Depew Street	40
Gilmour Place	Maplewood Avenue	Southerly Limit	40
Giotto Lane	Eagleleglen Way/Bernini Court	Michaelangelo Lane	40
Gladstone Avenue	Main Street	Cumberland Avenue	40
Glassco Avenue	Melvin Avenue	Queenston Road	40
Glen Echo	Rainbow	Nash	40
Glencarry Avenue	Northerly Limit	King Street	40
Glendale Avenue North	Barton Street East	King Street East	40
Glendale Avenue South	King Street East	Main Street East	40
Glengrove Avenue	Reid Avenue	Oriole Crescent	40
Glow Avenue	Parkdale Avenue	Woodward Avenue	40
Gordon Street	Cavell Avenue	Lincoln Street	40
Grace Avenue	Westerly Limit	Easterly Limit	40
Greenford Dr.	Owen Pl.	Queenston Rd.	40
Grenfell Street	Beach Road	Kenilworth Avenue	40
Grimsby Avenue	Britannia Avenue	Martha Street	40
Grosvenor Avenue North	Barton Street East	Dunsmure Road	40
Hamilton Avenue	Concession Street	Mountain Park Avenue	40
Hawkswood Trail	Falconridge Drive	Kittyhawk Drive	40
Hayes Avenue	Bingham Road	Armstrong Avenue	40
Head Street	Dundurn Street North	Strathcona Avenue North	40
Heath Street	Sumach Street	Eastwood Street	40
Heath Street	Sumach Street	Eastwood Street	40
Henry Street	New Street	Easterly Limit	40
Hester St.	Upper Wellington St.	Upper James St.	40
Highland Avenue	Gage Avenue North	Easterly Limit	40
Hilda Avenue	King Street East	Main Street East	40
Hillyard Street	Land Street	Monroe Street	40
Holly Avenue	McAnulty Boulevard	Beech Road	40
Imperial Street	Sherman Avenue North	Easterly Limit	40
Inchbury Street	Florence Street	York Boulevard	40
Jeremiah Court	Lawson Street	To End	40
Jessica Street	Vineberg Drive	Onyx Drive	40
Juanita Drive	Mohawk Road West	Brenlyn Court	40
Karen Crescent	San Antonio Drive	San Pedro Drive	40
			km/h
Kennedy Avenue	Westerly limit	Upper James Street	40

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Being a By-law to Regulate Traffic

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Kenora Avenue	Delawana Drive	Queenston Road	40
Kensington Avenue North	Barton Street East	Main Street East	40
Kentley Drive	Nash Road	Kenora Avenue	40
Kestrel Court	Northerly Limit	Falconridge Drive	40
Kittyhawk Drive	Hawkswood Trail	Upper Paradise Road	40
Knowles Street	Reid Avenue	Easterly Limit	40
Knox Avenue	Leaside Road	Brampton Street	40
Lamoreaux St.	Dundurn St. N.	Strathcona Ave. N.	40
Lampton Place	Morley Street	Southerly Limit	40
Land Street	Wentworth Street	Hillyard Street	40
Landsdowne Street	Sherman Avenue North	Lottridge Street	40
Lang Street	Reid Avenue	Hayes Avenue	40
Larch Street	Billington Crescent	Upper Ottawa Street	40
Lawson Street	Folkstone Avenue	Edwina Place	40
Leaside Road	Burgess Avenue	Easterly Limit	40
Leslie Ave.	West 27th St.	West 25th St.	40km/h
Lewis Street	Britannia Avenue	Martha Street	40
Lincoln Street	Linden Street	Barton Street East	40
Linden Street	Cavell Avenue	Lincoln Street	40
Locke Street North	King Street West	York Boulevard	40
Loconder Drive	Queen Victoria Drive	Upper Gage Avenue	40
Lorne Avenue	Maplewood Avenue	Cumberland Avenue	40
Lorraine Drive	Beaconsfield Drive	Reno Avenue	40
Lottridge Street	Beach Road	160m north of Clinton Street	40
Lucerne Avenue	Cochrane Road	Easterly Limit	40
Lyndhurst Street	Gertrude Street	Southerly Limit	40
MacLaren Avenue	Melvin Avenue	Britannia Avenue	40
Magill Street	Barton Street West	York Boulevard	40
Main Street East	Parkdale Avenue	Reid Avenue	40
Malton Drive	Christopher Drive	Upper James Street	40
Maplewood Avenue	Sherman Avenue South	Gage Avenue South	40
Market Street	Queen Street	Bay Street	40
Market Street	Queen Street North	Ray Street North	40
Martha Street	Lewis Street	Oriole Crescent	40
Martimas Avenue	Beach Road	McAnulty Boulevard	40
Mayflower Avenue	Gage Avenue North	Glendale Avenue North	40
McAnulty Boulevard	Westerly Limit	Kenilworth Avenue	40
McKinstry Street	Burlington Street	Brant Street	40
Mead Avenue	Parkdale Avenue	Easterly Limit	40
Medici Lane	Davinci Boulevard	Bellini Lane	40
Melvin Avenue	Parkdale Avenue	Talbot Street	40
Michelangelo Lane	Upper Paradise Road	Davinci Boulevard	40
Mona Lisa Court	Davinci Boulevard	Easterly Limit	40

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

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Mons Avenue	Gage Avenue North	Easterly Limit	40
Morden Street	Locke Street North	Pearl Street North	40
Morris Avenue	Westerly Limit	Glendale Avenue North	40
Mountain Park Avenue	Westerly Limit	Upper Sherman Avenue	40
Muir Avenue	Upper Gage Avenue	Palmer Road	40
Munroe	Wentworth Street	Hillyard Street	40
Myrtle Avenue	Main Street	Rutherford Avenue	40
Napier Street	Locke Street North	Queen Street North	40
Nelson Street	Locke Street	Pearl Street	40
Niagara Street	Land Street	Munroe Street	40
Ninth Avenue	Upper Gage Avenue	Upper Ottawa Street	40
Norman Street	Argyle Avenue	Campbell Avenue	40
Northcote Street	Depew Street	Beach Road	40
Norton Street	Northerly Limit	Beach Road	40
Norway Avenue	Maplewood Avenue	Cumberland Avenue	40
Onyx Drive	Jessica Street	Arrowhead Drive	40
Oriole Crescent	Britannia Avenue	Glengrove Avenue / Armstrong Avenue	40
Osborne Street	Walmer Road	Melvin Avenue	40
Owen Place	King	Greenford	40
Palmer Road	Mohawk Road East	Behan Street	40
Pavel Place	Northerly Limit	Eagle Glen Way	40
Pearl Street North	King Street West	York Boulevard	40
Peel Street	Morden Street	Napier Street	40
Peregrine Place	Coopershawk Crescent	Easterly Limit	40
Peter Street	Locke Street North	Queen Street North	40
Primrose Avenue	Gage Avenue North	Easterly Limit	40
Prince George Street	Bruce Dale Avenue	Churchill Avenue	40
Quaker Crescent	End	To End	40
Quebec Street	Brampton Street	Rennie Street	40
Queen Victoria Drive	End	To End	40
Queensbury Drive	Upper Ottawa Street	Queen Victoria Drive	40
Queensdale Avenue	Upper James Street	Upper Wellington Street	40
Quinn Avenue	Queen Victoria Drive	Stone Church Road East	40
Rainbow	Nash	Secord	40
Rainham Street	Queen Victoria Drive	Quaker Crescent	40
Raleigh Street	Queen Victoria Drive	Rainham Street	40
Raphael Court	Northerly Limit	Eagle Glen Way	40
Ray Street North	King Street West	York Boulevard	40
Redbury Street	Upper Ottawa Street	Queen Victoria Drive	40
Redfern Avenue	Chedmac Drive	Sanatorium Road	40
Redtail Crescent	Hawkswood Trail	Eagle Glen Way	40

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Reid Avenue	Roxborough Avenue	Lucerne Avenue	40
Rennie Street	Parkdale Avenue	Easterly Limit	40
Reno Avenue	Palmer Road	Upper Ottawa Street	40
Rianna Court	Northerly Limit	Eagleglen Way	40
Robins Avenue	Barton Street	Roxborough	40
Rochelle Avenue	Queen Victoria Drive	Stone Church Road East	40
Roseland Avenue	Blake Street	Easterly Limit	40
Rosslyn Avenue	Dalhousie Avenue	Main Street East	40
North			
Rowanwood Street	Gertrude Street	Southerly Limit	40
Rowena Court	Quaker Crescent	To westerly end	40
Roxborough	Ottawa Street	Kenilworth Avenue South	40km/h
Avenue			
Roxborough	Westerly Limit	Ottawa Street North	40
Avenue			
Rutherford Avenue	Wentworth Avenue	Sanford Avenue	40
San Antonio Drive	San Remo Drive	San Pedro Drive	40
			km/h
San Remo Dirve	San Pedro Drive	Lavina Crescent	40
			km/h
Sanatorium Rd.	Rice Ave.	West 32nd St.	40
Sanford Avenue	Myler Street	Southerly Limit	40
Secord	Sunrise	Glen Echo	40
Shadetree	Second Road West	Second Road West	40
Crescent			
Shynal Avenue	Morley Street	Rennie Street	40
Sistine Court	Davinci Boulevard	Easterly Limit	40
Springer Avenue	Main Street East	Maplewood Avenue	40
Springer Avenue	Main Street East	Maplewood Avenue	40
St Clair Boulevard	Delaware Avenue	Southerly Limit	40
Stapleton Avenue	Dofasco Raod	McAnulty Boulevard	40
Stinson Crescent	Wentworth Avenue	Easterly Limit	40
Strathcona Avenue	King Street West	York Boulevard	40
North			
Studholme Road	Aberdeen Avenue	Westerly limit	40
Sumach Street	Melvin Avenue	Heath Street	40
Sumach Street	Melvin Avenue	Heath Street	40
Sunnidale Street	Robins Avenue	Easterly limit	40
Talbot Street	Walmer Road	Melvin Avenue	40
Tate Avenue	Leaside Road	Rennie Street	40
Tenth Avenue	East 45th Street	Upper Ottawa Street	40
Thorley Drive	Upper Gage Avenue	Palmer Road	40
Tom Street	Dundurn Street North	Strathcona Avenue North	40
Troy Avenue	Tate Avenue	Easterly Limit	40
Upper Paradise	Rymal Road	Davinci Boulevard	40
Road			

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Viewpoint Avenue	Concession Street	Mountain Park Avenue	40
Vineberg Drive	Upper Wentworth Street	Jessica Street	40
Walmer Road	Woodward Avenue	Talbot Street	40
Waterloo Street	Brampton Street	Rennie Street	40
Waverly Street	Walmer Road	Melvin Avenue	40
Wellesley Street	Morden Street	Napier Street	40
West 25th St.	Leslie Ave.	Bendamere Ave.	40
			km/h
West 27th St.	Leslie Ave.	Bendamere Ave.	40
			km/h
Whitfield Avenue	Birmingham Street	Gage Avenue North	40
Woodleigh Avenue	Beach Road	McAnulty Boulevard	40
Woodward Avenue	Barton Street	Melvin Avenue	40
Woodward Avenue	Melvin Avenue	Barton Street	40

And by adding to section “F” (Stoney Creek) thereof the following items, namely;

Dupont Street	Dewitt Road	Hewitson Road	30
First Road West	Mud Street West	Green Mountain Road	60
Glenashton Drive	Dewitt Road	Harbrite Drive	30
Hewitson Road	Dupont Street	Barton Street	30

And by removing from section “F” (Stoney Creek) thereof the following items, namely;

Avery Close	Northerly Limit	McCollum Road	40
Baseline Road	Winona Road	Easterly Limit	40
Bellavista Gate	Riviera Ridge	North Service Road	40
Bridgenorth	Islandview Way (North Leg)	Islandview Way (South Leg)	40
Crescent			
Campview Road	Westerly End	Easterly End	40
Campview Road	Campview Road	North Service Road	40
Cardiff Lane	Springstead Avenue	Greenstern Crescent	40
Cardigan Place	Sandbeach Drive	Glenmeadow Crescent	40
Creanona	Northerly End	Baseline Road	40
Boulevard			
Delrosa Way	Watercliff Place	Islandview Way	40
Dewsbury Way	Glenmeadow Crescent	Northampton Boulevard	40
Dupont	Dewitt	Hewitson	40
East Street	Wendakee Drive	Baseline Road	40
Easthampton Road	Northampton Boulevard	Springstead Avenue	40
Falcon Road	Fifty Road (north leg)	Fifty Road (south leg)	40
First Road West	Mud Street	Ridgeview Drive	
Glenashton Dr.	Dewitt Rd.	West property line of	40

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Glendarling Crescent	Lockport Way	Glenashton Dr. Baseline Road	40
Glenmeadow Crescent	Islandview Way (North Leg)	Islandview Way (South Leg)	40
Goldeye Drive	Glenmeadow Crescent	Northampton Boulevard	40
Gracehill Drive	Sandbeach Drive	Islandview Way	40
Greenstern Crescent	Lockport Way (North Leg)	Lockport Way (South Leg)	40
Halifax Street	Montreal Circle	Fifty Road	40
Hartland Road	Northampton Boulevard	Springstead Avenue	40
Hewitson	Barton	Dupont	40
Islandview Way	McCollum Road	Northampton Boulevard	40
Kingspoint Circle	End	End	40
Kingston Road	Montreal Circle	Fifty Road	40
Lido Drive	Riviera Ridge	Winona Road	40
Liuna Court	Northerly Limit	Winona Road	40
Lochside Drive	McCollum Road	Northerly End	40
Lockport Way	Springstead Avenue	North Service Road	40
McCollum Road	Fifty Road	Shippee Avenue	40
Montreal Circle	End	End	40
Monza Drive	Halifax Street (West Leg)	Halifax Street (East Leg)	40
Nanaimo Crescent	Montreal Circle (North Leg)	Montreal Circle (South Leg)	40
Northampton Boulevard	Kingspoint Circle	Hartland Road	40
Oakrun St	Springstead Avenue	Westhampton Way	40
Panorama Way	Fifty Road	Westhampton Way	40
Pebble Valley Avenue	Springstead Avenue	Lockport Way	40
Portofino Place	Westerly End	Lido Drive	40
Raintree Drive	Glendarling Crescent	Baseline Road	40
Richmond Crescent	Wendakee Drive	East Street	40
Ridgegate Place	Springstead Avenue	Greenstern Crescent	40
Riviera Ridge	Shaowdale Drive	Lido Drive (East Leg)	40
Sandbeach Drive	Delrosa Way	Kingspoint Circle	40
Sedgebrook Avenue	Springstead Avenue	Lockport Way	40
Shadowdale Drive	Northerly End	Riviera Ridge	40
Shippee Avenue	northerly limit	McCollum Road	40
Springstead Avenue	Hartland Road	Lockport Way	40
Sugarberry Court	Westerly End	Riviera Ridge	40

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Vancouver Lane	Creanona Boulevard	Montreal Circle	40
Watercliff Place	Fifty Road	Islandview Way	40
Waterside Drive	Richmond Crescent	Creanona Boulevard	40
Wendakee Drive	Creanona Boulevard	East Street	40

1. Schedule 31 (Designated Areas – Reduced Speed Limit – 40 km/h Neighbourhoods) to designate specific areas, or neighbourhoods, for reduced speed limits, is hereby repealed and replaced in the form attached hereto as Appendix “A” to this by-law.
2. Subject to the amendments made in this By-law, in all other respects, By-law No. 01-215, including all Schedules thereto, as amended, is hereby confirmed unchanged.
3. This By-law shall come into force and take effect on the date of its passing and enactment.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Schedule 31 (Designated Area – Reduced Speed - 40km/h Neighbourhoods)

Neighbourhood	East/West Limits	North/South Limits	Excluded Roadways	Speed (km/h)	Area Map - - - - - Neighbourhood Limit 30km/h Roadway - . - . - Excluded Roadway
Ainslie Wood North Ward 1 Phase 1 - 2019	Hydro Corridor to west limit of Cootes Drive	Dundas Town Limit to north limit of Main Street West	None	40	
Ainslie Wood, Ainslie Wood East, Ainslie Wood West Ward 1 Phase 1 - 2019	East limit of Wilson Street/Main Street West to west limit of Longwood Road	South limit of Main Street West to Escarpment	None	40	

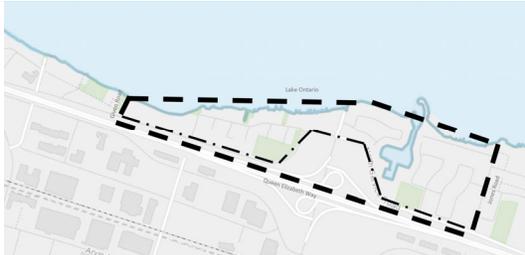
<p>Albion Ward 9 Phase 2 - 2020</p>	<p>East limit of Red Hill Valley Parkway to east limit of Echovalley Drive</p>	<p>Escarpment to north limit of Mud Street West</p>	<p>None</p>	<p>40</p>	
<p>Albion Falls Ward 6 Phase 3 - 2022</p>	<p>East limit of Albion Falls Boulevard to west limit of Red Hill Valley Parkway</p>	<p>South limit of Limeridge Road East to north limit of Stone Church Road East</p>	<p>Pritchard Road</p>	<p>40</p>	
<p>Alderlea Ward 11 Phase 2 - 2020</p>	<p>East limit of Upper James Street to Turner Park</p>	<p>Hydro Corridor to north limit of Twenty Road East</p>	<p>None</p>	<p>40</p>	

<p>Allison Ward 8 Phase 2 - 2020</p>	<p>East limit of Upper James Street to Mount Hamilton Cemetery</p>	<p>South limit of Rymal Road East to Hydro Corridor</p>	<p>None</p>	<p>40</p>	
<p>Balfour Ward 8 Phase 1 - 2019</p>	<p>East limit of Upper James Street to west limit of Upper Wellington Street</p>	<p>South limit of Fennell Avenue East to north limit of Mohawk Road East</p>	<p>None</p>	<p>40</p>	

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<p>Barnstown Ward 7 Phase 3 - 2022</p>	<p>East limit of Upper Wellington Street to west limit of Upper Wentworth Street</p>	<p>South limit of Stone Church Road East to north limit of Rymal Road East</p>	<p>None</p>	<p>40</p>	 <p>A map of the Barnstown area showing a dashed black boundary. The boundary is roughly rectangular, with the top side following Stone Church Road East, the right side following Upper Wentworth Street, and the bottom side following Rymal Road East. A blue dotted area is located in the center of the boundary, near Upper Wellington Street.</p>
<p>Bartonville Ward 4 Phase 1 - 2019</p>	<p>East limit of Kenilworth Avenue South to east limit of Cochrane Road</p>	<p>South limit of Main Street East to south limit of Lawrence Road</p>	<p>King Street East</p>	<p>40</p>	 <p>A map of the Bartonville area showing a dashed black boundary. The boundary is roughly rectangular, with the top side following Main Street East, the right side following Cochrane Road, the bottom side following Lawrence Road, and the left side following Kenilworth Avenue South. The boundary also follows King Street East on the east side.</p>

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<p>Battlefield Ward 5 Phase 2 - 2020</p>	<p>East limit of Centennial Parkway to west limit of Lake Avenue Drive</p>	<p>South limit of Queenston Road to Escarpment</p>	<p>King Street West</p>	<p>40</p>	
<p>Bayview Ward 10 Phase 3 - 2022</p>	<p>Lake Ontario to north limit of Queen Elizabeth Way</p>	<p>East limit of Given Road to west limit of Jones Road</p>	<p>North Service Road</p>	<p>40</p>	

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<p>Beasley Ward 2 Phase 1 - 2019</p>	<p>East limit of James Street North/James Street North to West limit of Wellington Street North/Wellington Street South</p>	<p>Rail Corridor (280m north of Barton Street East) to North limit of Main Street East</p>	<p>John Street North, Barton Street East, Cannon Street East, Wilson Street, King Street, King Street East</p>	<p>40</p>	
<p>Berrisfield Ward 6 Phase 3 - 2022</p>	<p>East limit of Upper Gage Avenue to west limit of Upper Ottawa Street</p>	<p>South limit of Mohawk Road East to north limit of Lincoln M. Alexander Parkway</p>	<p>None</p>	<p>40</p>	

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<p>Binbrook Ward 11 Phase 1 - 2019</p>	<p>East limit of Fletcher Road to west limit of Hendershot Road</p>	<p>South limit of Guyatt Road to north limit of Kirk Road</p>	<p>Binbrook Road, Cemetery Road, Regional Road 56</p>	<p>40</p>	
<p>Blakeley Ward 3 Phase 3 - 2022</p>	<p>East limit of Sherman Avenue South to west limit of Gage Avenue South</p>	<p>South limit of Main Street East to Escarpment</p>	<p>None</p>	<p>40</p>	

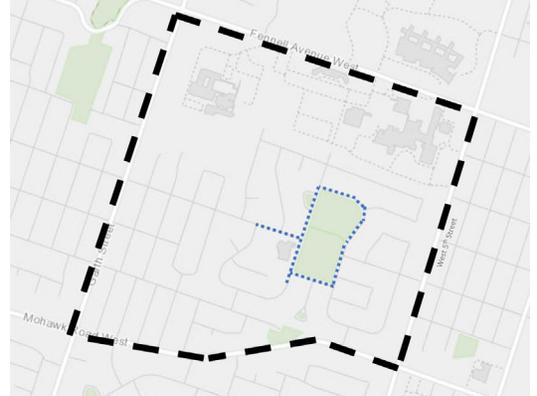
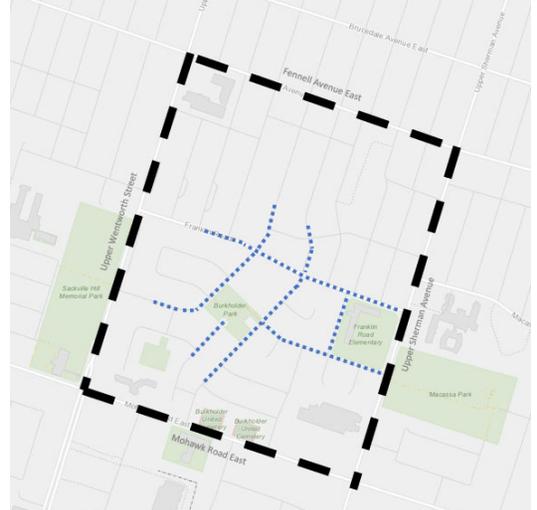
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<p>Bonnington Ward 8 Phase 3 - 2022</p>	<p>East limit of West 5th Street to west limit of Upper James Street</p>	<p>South limit of Fennell Avenue West to North limit of Mohawk Road West</p>	<p>None</p>	<p>40</p>	
<p>Broughton East Ward 6 Phase 4 - 2022</p>	<p>West Limit of Upper Gage Avenue to East Limit of approximately 140.0m East of Grayrocks Avenue</p>	<p>North Limit of Rymal Road East to East Limit of Hydro Corridor</p>	<p>None</p>	<p>40</p>	

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<p>Broughton West Ward 6/7 Phase 4 - 2022</p>	<p>West limit of Upper Sherman Avenue to East Limit of Upper Gage Avenue</p>	<p>North Limit of Rymal Road East to South Limit of Hydro Corridor</p>	<p>Miles Road</p>	<p>40</p>	
<p>Bruleville Ward 7 Phase 1 - 2019</p>	<p>East limit of Upper Wellington Street to west limit of Upper Wentworth Street</p>	<p>South limit of Mohawk Road to north limit of the LINC</p>	<p>None</p>	<p>40</p>	

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<p>Buchanan Ward 8 Phase 2 - 2020</p>	<p>East limit of Garth Street to west limit of West 5th Street</p>	<p>South limit of Mohawk College Property Boundary to north limit of Mohawk Road West</p>	<p>None</p>	<p>40</p>	 <p>A map of Buchanan Ward 8 showing a dashed black boundary and a blue dotted boundary. The dashed boundary is roughly rectangular, bounded by Fennell Avenue West to the north, West 5th Street to the east, and Mohawk Road West to the south. The blue dotted boundary is located within the dashed boundary, following a path through a residential area.</p>
<p>Burkholme Ward 7 Phase 4 - 2022</p>	<p>Upper Wentworth Street to Upper Sherman Avenue</p>	<p>Fennell Avenue East to Mohawk Road East</p>	<p>None</p>	<p>40</p>	 <p>A map of Burkholme Ward 7 showing a dashed black boundary and a blue dotted boundary. The dashed boundary is roughly rectangular, bounded by Fennell Avenue East to the north, Mohawk Road East to the south, Upper Wentworth Street to the west, and Upper Sherman Avenue to the east. The blue dotted boundary follows a path through the ward, including areas around Siskiwit Hill Memorial Park, Burkholme Park, Franklin Road Elementary, and Macrossa Park.</p>

<p>Butler Ward 7 Phase 1 - 2019</p>	<p>East limit of Upper Wentworth Street to west limit of Upper Sherman Avenue</p>	<p>South limit of Stone Church Road to north limit of Rymal Road</p>	<p>None</p>	<p>40</p>	
<p>Carlisle Northeast Ward 15 Phase 2 - 2020</p>	<p>West limit of Centre Road to west limit of Milbrough Line</p>	<p>South limit of Concession 10 East to north limit of Carlisle Road</p>	<p>None</p>	<p>40</p>	

<p>Carlisle Northwest Ward 15 Phase 2 - 2020</p>	<p>East limit of Highway 6 to west limit of Centre Road</p>	<p>South limit of Concession 10 East to north limit of Carlisle Road</p>	<p>None</p>	<p>40</p>	
<p>Carlisle Southeast Ward 15 Phase 2 - 2020</p>	<p>West limit of Centre Road to west limit of Milborough Line</p>	<p>South limit of Carlisle Road to north limit of Concession 8 East</p>	<p>Green Spring Road</p>	<p>40</p>	

<p>Carlisle Southwest Ward 15 Phase 2 - 2020</p>	<p>East limit of Highway 6 to west limit of Centre Road</p>	<p>South limit of Carlisle Road to north limit of Concession 8 East</p>	<p>None</p>	<p>40</p>	
<p>Carpenter Ward 14 Phase 4 - 2022</p>	<p>East limit of Garth Street to West limit of Hydro Corridor</p>	<p>North limit of Rymal Road to South limit of Hydro Corridor</p>		<p>40</p>	

<p>Central Ward 2 Phase 2 - 2020</p>	<p>East limit of Queen Street North/Queen Street South to west limit of James Street North/James Street South</p>	<p>Rail Corridor (280m north of Barton Street East) to north limit of Main Street West</p>	<p>Barton Street West, Bay Street North, Cannon Street West, York Boulevard, King Street West</p>	<p>40</p>	
<p>Central Business District Ward 13 Phase 3 - 2022</p>	<p>East limit of Market Street to west limit of Olympic Drive</p>	<p>South limit of Park Street to north limit of Osler Drive</p>	<p>King Street, Cootes Drive, Governor's Road, Dundas Street, Main Street</p>	<p>40</p>	

<p>Centremount Ward 8 Phase 3 - 2022</p>	<p>East limit of Upper James Street to west limit of Upper Wellington Street</p>	<p>Escarpment to north limit of Fennell Avenue East</p>	<p>None</p>	<p>40</p>	
<p>Chappel East Ward 7 Phase 4 - 2022</p>	<p>West limit of Upper Wentworth Street to East limit of Upper Sherman Avenue</p>	<p>North Limit of Rymal Road East to south limit of Hydro Corridor</p>	<p>None</p>	<p>40</p>	

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<p>Chappel West Ward 7 Phase 4 – 2022</p>	<p>West limit of Upper Wellington Street to East Limit of Upper Wentworth Street</p>	<p>North limit of Rymal Road East to South Limit of Hydro Corridor</p>	<p>None</p>	<p>40</p>	
<p>Chedoke Park Ward 1 Phase 3 - 2022</p>	<p>East limit of Highway 403 to west limit of Chedoke Avenue</p>	<p>South limit of Aberdeen Avenue to Escarpment</p>	<p>None</p>	<p>40</p>	

<p>Clearview/Old Mill Ward 12 Phase 2 - 2020</p>	<p>Robert E. Wade Ancaster Community Park to west limit of Wilson Street East</p>	<p>South limit of Mineral Springs Road to north limit of Jerseyville Road West</p>	<p>None</p>	<p>40</p>	
<p>Colborne Ward 13 Phase 3 - 2022</p>	<p>Rail Corridor to west limit of Market Street North</p>	<p>Escarpment to north limit of King Street West</p>	<p>None</p>	<p>40</p>	

<p>Corktown Ward 2 Phase 1 - 2019</p>	<p>East limit of James Street South to West limit of Wellington Street South</p>	<p>South limit of Main Street East to north limit of Claremont Access</p>	<p>John Street South, Arkledun Avenue, St. Joseph's Drive (Between James Street South and John Street South)</p>	<p>40</p>	
<p>Corman Ward 5 Phase 3 - 2022</p>	<p>East limit of Red Hill Valley Parkway to west limit of Nash Road South</p>	<p>South limit of Queenston Road to north limit of King Street East</p>	<p>None</p>	<p>40</p>	

<p>Corman/Cherry Heights Ward 10 Phase 2 - 2020</p>	<p>East limit of Gray Road to west limit of Green Road</p>	<p>South limit of Highway 8 to Escarpment</p>	<p>King Street East</p>	<p>40</p>	
<p>Crerar Ward 7 Phase 3 - 2022</p>	<p>East limit of Upper Wellington Street to west limit of Upper Wentworth Street</p>	<p>South limit of Lincoln M. Alexander Parkway to north limit of Stone Church Road East</p>	<p>None</p>	<p>40</p>	

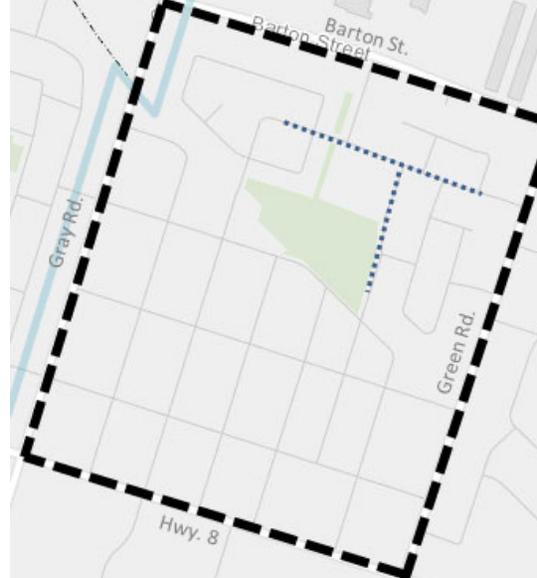
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<p>Crown Point East Ward 4 Phase 3 - 2022</p>	<p>East limit of Ottawa Street North to west limit of Kenilworth Avenue North</p>	<p>Rail Corridor to north limit of Main Street East</p>	<p>Barton Street, Cannon Street</p>	<p>40</p>	
<p>Crown Point West Ward 3 Phase 3 - 2022</p>	<p>East limit of Gage Avenue North to west limit of Ottawa Street North</p>	<p>Rail Corridor to north limit of Main Street East</p>	<p>Barton Street, Cannon Street, King Street East</p>	<p>40</p>	

<p>Delta East Ward 4 Phase 1 - 2019</p>	<p>East limit of Ottawa Street South to West limit of Kenilworth Street South</p>	<p>South limit of Main Street East to Escarpment</p>	<p>King Street East</p>	<p>40</p>	<p>A map of the Delta East area in Ottawa. A dashed black line outlines a specific zone. The zone is bounded to the east by King Street East, to the south by the Escarpment, and to the west by Ottawa Street South. Within this zone, a network of blue dotted lines indicates a proposed traffic regulation scheme. Other streets shown include Main Street East and Kenilworth Avenue South.</p>
<p>Delta West Ward 3 Phase 1 - 2019</p>	<p>East limit of Gage Avenue South to West limit of Ottawa Street South</p>	<p>South limit of Main Street East to Escarpment</p>	<p>King Street East, Lawrence Road</p>	<p>40</p>	<p>A map of the Delta West area in Ottawa. A dashed black line outlines a specific zone. The zone is bounded to the east by King Street East, to the south by the Escarpment, and to the west by Gage Avenue South. Within this zone, a network of blue dotted lines indicates a proposed traffic regulation scheme. Other streets shown include Main Street East, Ottawa Street South, Lawrence Road, and Kenilworth Access.</p>

<p>Desjardin's Ward 13 Phase 3 - 2022</p>	<p>East limit of York Road to west limit of Old Guelph Road</p>	<p>South limit of York Road to Rail Corridor</p>	<p>None</p>	<p>40</p>	
<p>Dewitt Ward 10 Phase 3 - 2022</p>	<p>East limit of Dewitt Road to west limit of Fruitland Road</p>	<p>South limit of Highway 8 to Escarpment</p>	<p>None</p>	<p>40</p>	

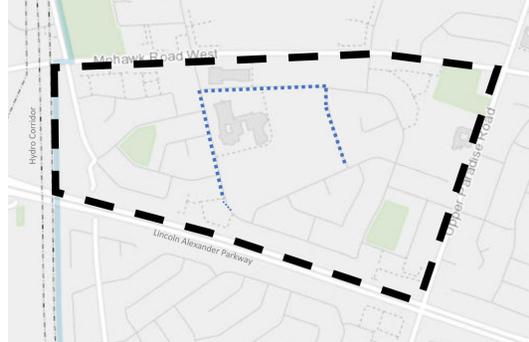
<p>Dundana East & Dundana West Ward 13 Phase 3 – 2022</p>	<p>East limit of Old Ancaster Road to west limit of Osler Drive</p>	<p>South limit of Governor’s Road to Escarpment</p>	<p>None</p>	<p>40</p>	
<p>Durand Ward 2 Phase 1 - 2019</p>	<p>East limit of Queen Street South to West limit of James Street South</p>	<p>South limit of Main Street West to Escarpment</p>	<p>Bay Street South (Between Hunter West Street and Main Street West)</p>	<p>40</p>	

<p>Eastdale Ward 10 Phase 1 - 2019</p>	<p>East limit of Gray Road to west limit of Green Road</p>	<p>South limit of Barton Street to north limit of Highway 8</p>	<p>None</p>	<p>40</p>	
<p>Eastmount Ward 7 Phase 3 - 2022</p>	<p>East limit of Upper Wentworth Street to west limit of Upper Sherman Avenue</p>	<p>Escarpment to north limit of Fennell Avenue East</p>	<p>None</p>	<p>40</p>	

<p>Eleanor Ward 6 Phase 1 - 2019</p>	<p>East limit of Upper Sherman Avenue to West limit of Upper Gage Avenue</p>	<p>South limit of Stone Church Road to north limit of Rymal Road</p>	<p>None</p>	<p>40</p>	
<p>Falkirk East Ward 14 Phase 1 - 2019</p>	<p>East limit of Upper Paradise Road to west limit of Garth Street</p>	<p>South limit of Stone Church Road to north limit of Rymal Road</p>	<p>None</p>	<p>40</p>	

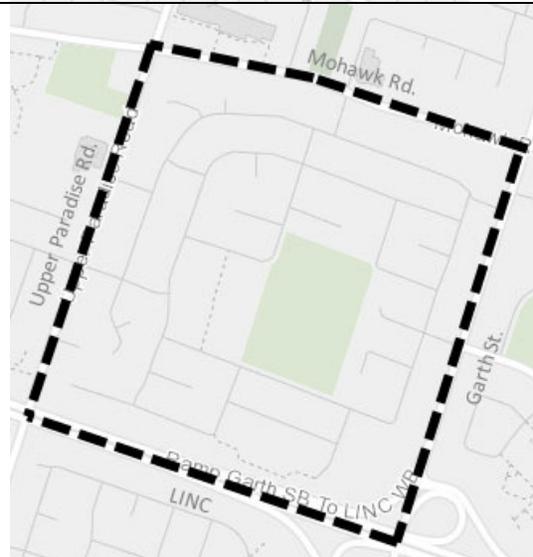
<p>Falkirk West Ward 14 Phase 2 - 2020</p>	<p>Hydro Corridor to west limit of Upper Paradise Road</p>	<p>South limit of Stone Church Road West to north limit of Rymal Road West</p>	<p>None</p>	<p>40</p>	
<p>Felker Ward 9 Phase 2 - 2020</p>	<p>Maplewood Park to West limit of Upper Centennial Parkway</p>	<p>Mud Street West to Highland Road West</p>	<p>None</p>	<p>40</p>	

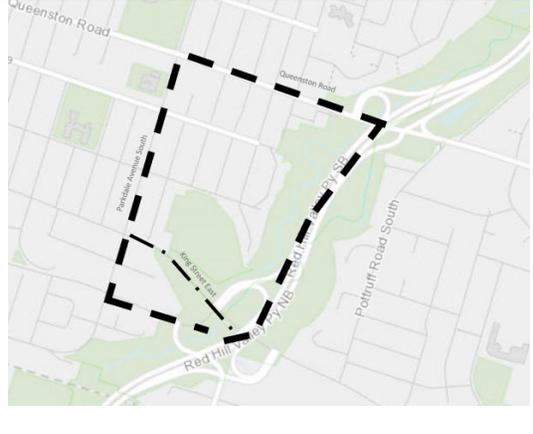
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<p>Fessenden Ward 14 Phase 2 - 2020</p>	<p>Hydro Corridor to west limit of Upper Paradise Road</p>	<p>South limit of Mohawk Road West to north limit of the LINC</p>	<p>None</p>	<p>40</p>	
<p>Fifty Point Ward 10 Phase 3 - 2022</p>	<p>East limit of Creanona Boulevard to City Limits</p>	<p>Lake Ontario to north limit of Queen Elizabeth Way</p>	<p>North Service Road</p>	<p>40</p>	
<p>Freelton Ward 15 Phase 3 - 2022</p>	<p>East limit of Karendale Crescent to west limit of Wildan Drive</p>	<p>South limit of Freelton Road to south limit of Peebles Drive</p>	<p>Brock Road, Regional Road 97, Freelton Road, Concession 11 East, Highway 6</p>	<p>40</p>	

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<p>Garner Ward 12 Phase 2 - 2020</p>	<p>East limit of Fiddlers Green Road to west limit of Southcote Road</p>	<p>South limit of Highway 403 to north limit of Garner Road East</p>	<p>Highway 6</p>	<p>40</p>	
<p>Gersholme Ward 5 Phase 2 - 2020</p>	<p>East limit of Vincent/Gershome Open Space to west limit of Centennial Parkway</p>	<p>South limit of King Street East to Escarpment</p>	<p>Greenhill Avenue</p>	<p>40</p>	

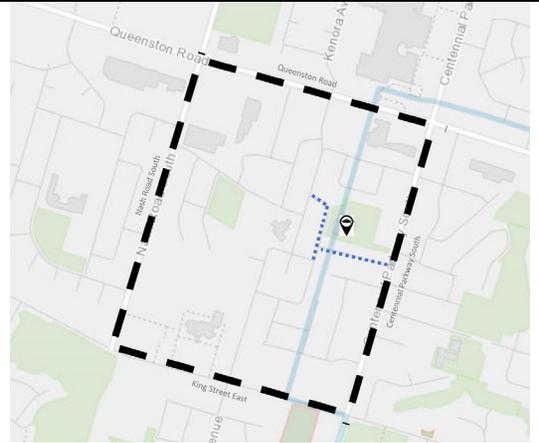
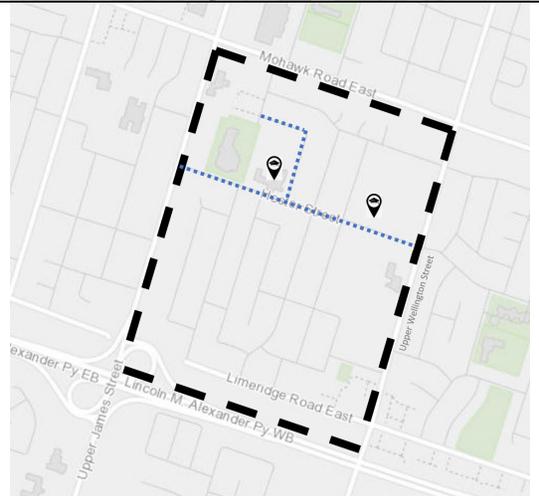
<p>Gibson Ward 3 Phase 1 - 2019</p>	<p>East limit of Wentworth Street North/Wentworth Street South to West limit of Sherman Avenue North/Sherman Avenue South</p>	<p>Rail Corridor (315m north of Barton Street East) to north limit of Main Street East</p>	<p>Sanford Avenue North, Sanford Avenue South, Birch Avenue, Barton Street East, Cannon Street East, Wilson Street, King Street East</p>	<p>40</p>	
<p>Gilbert Ward 14 Phase 1 - 2019</p>	<p>East limit of Upper Paradise Road to west limit of Garth Street</p>	<p>South limit of Mohawk Road to north limit of the LINC</p>	<p>None</p>	<p>40</p>	

<p>Gilksion Ward 14 Phase 1 - 2019</p>	<p>East limit of Upper Paradise Road to west limit of Garth Street</p>	<p>South limit of the LINC to north limit of Stone Church Road</p>	<p>None</p>	<p>40</p>	 <p>A map of the Gilksion area showing a dashed black boundary line. The boundary starts at the east limit of Upper Paradise Rd., goes south to the west limit of Garth Street, then west along Garth Street to the south limit of the LINC, then north along the LINC to the north limit of Stone Church Road, and finally east along Stone Church Road back to Upper Paradise Rd. A blue dotted line indicates a specific route within the boundary.</p>
<p>Glenview East Ward 4 Phase 3 - 2022</p>	<p>East limit of Parkdale Avenue South to west limit of Red Hill Valley Parkway</p>	<p>South limit of Queenston Road to north limit of Lawrence Road</p>	<p>King Street East</p>	<p>40</p>	 <p>A map of the Glenview East area showing a dashed black boundary line. The boundary starts at the east limit of Parkdale Avenue South, goes south to the west limit of Red Hill Valley Parkway, then east along Red Hill Valley Parkway to the south limit of Queenston Road, then north along Queenston Road to the north limit of Lawrence Road, and finally west along Lawrence Road back to Parkdale Avenue South. The map also shows other roads like Pottruff Road South and Red Hill.</p>

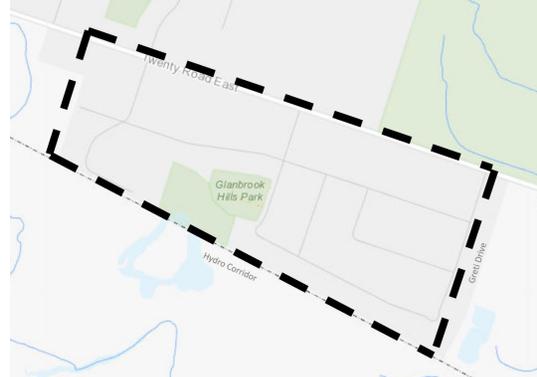
Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Glenview West Ward 4 Phase 2 - 2020</p>	<p>West limit of Cochrane Road to west limit of Parkdale Avenue</p>	<p>South limit of Queenston Road to north limit of Lawrence Road</p>	<p>King Street East</p>	<p>40</p>	
<p>Gourley Ward 8 Phase 2 - 2020</p>	<p>East limit of Garth Street to west limit of West 5th Street</p>	<p>South limit of the LINC to north limit of Stone Church Road</p>	<p>None</p>	<p>40</p>	

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<p>Greenford Ward 5 Phase 3 - 2022</p>	<p>East limit of Nash Road South to west limit of Centennial Parkway South</p>	<p>South limit of Queenston Road to north limit of King Street East</p>	<p>None</p>	<p>40</p>	
<p>Greeningdon Ward 8 Phase 3 - 2022</p>	<p>East limit of Upper James Street to west limit of Upper Wellington Street</p>	<p>South limit of Mohawk Road East to north limit of Lincoln M. Alexander Parkway</p>	<p>None</p>	<p>40</p>	

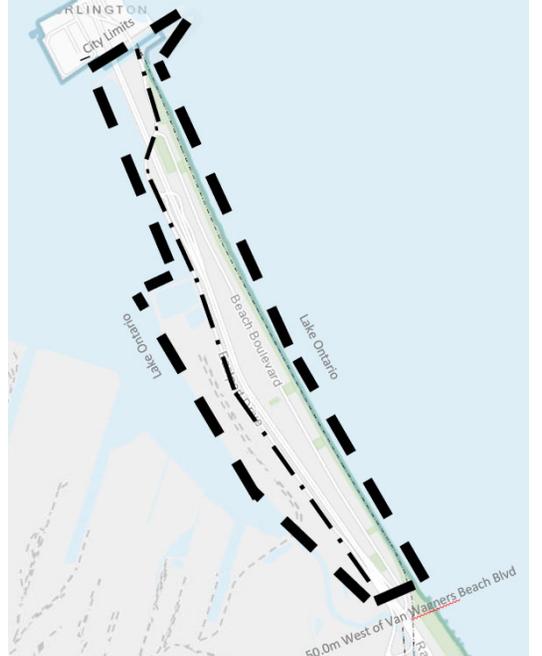
Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Greenville Ward 13 Phase 1 - 2019</p>	<p>South limit of Highway 5 to Escarpment</p>	<p>East limit of Middletown Road to west limit of Ofield Road South</p>	<p>Brock Road, Highway 8</p>	<p>40</p>	
<p>Greti Ward 11 Phase 2 - 2020</p>	<p>East limit of Upper James Street to east limit of Greti Drive</p>	<p>South limit of Twenty Road East to Hydro Corridor (370m south of Twenty Road East)</p>	<p>None</p>	<p>40</p>	

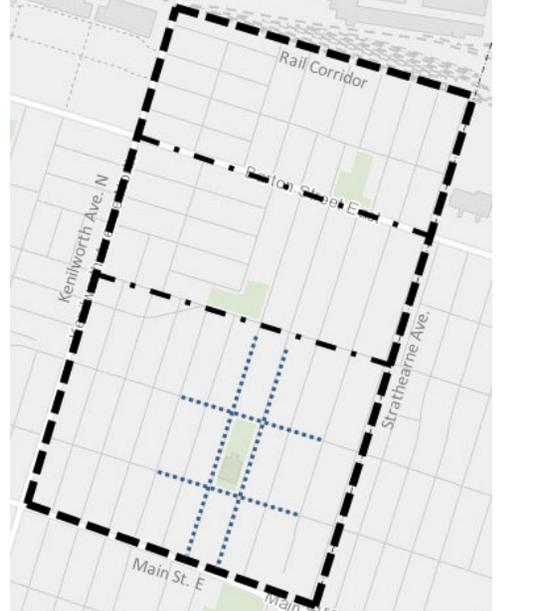
<p>Guelph Ward 13 Phase 3 - 2022</p>	<p>East limit of Old Guelph Road to west limit of Highway 6</p>	<p>South limit of York Road to Dundas Town Limit</p>	<p>None</p>	<p>40</p>	 <p>A map of Guelph showing a specific area outlined with a thick dashed black line. The area is bounded to the east by Highway 6, to the south by Dundas Town Limit, and to the west by Old Guelph Road. Other roads shown include York Road, Plains Road West, and York Blvd. The map also shows a park area and a water body.</p>
<p>Guernsey Ward 10 Phase 1 - 2019</p>	<p>East limit of Millen Road to west limit of Dewitt Road</p>	<p>South limit of Barton Street to north limit of Highway 8</p>	<p>None</p>	<p>40</p>	 <p>A map of Guernsey showing a specific area outlined with a thick dashed black line. The area is bounded to the east by Barton St., to the south by Hwy. 8, and to the west by Millen Rd. Dewitt Rd. is also shown. The map includes labels for STONEY CREEK and other local streets.</p>

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To Regulate Traffic

<p>Gurnett Ward 14 Phase 2 - 2020</p>	<p>Hydro Corridor to west limit of Upper Paradise Road</p>	<p>South limit of the LINC to north limit of Stone Church Road West</p>	<p>None</p>	<p>40</p>	<p>The map displays a residential area with a hydro corridor highlighted in light blue. A thick dashed black line outlines a specific area within the hydro corridor. The area is bounded on the north by Lincoln Alexander Parkway, on the east by Upper Paradise Road, and on the south by Stone Church Road West. A blue dotted line indicates a path or boundary within the hydro corridor area.</p>
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<p>Hamilton Beach Ward 5 Phase 4 - 2022</p>	<p>North limit of Lake Ontario to south limit of Lake Ontario</p>	<p>North Limit of City Boundary to approximately West Limit of 50.0m west of Van Wagners Beach Blvd</p>	<p>Eastport Drive</p>	<p>40</p>	
<p>Hamilton Golf and Country Club Ward 12 Phase 3 - 2022</p>	<p>East limit of Fiddler's Green Road to west limit of Highway 403</p>	<p>South limit of Golf Links Road to north limit of Highway 403</p>	<p>None</p>	<p>40</p>	

<p>Hampton Heights Ward 6 Phase 4 - 2022</p>	<p>West limit of Upper Gage Avenue to East Limit of Upper Ottawa Street</p>	<p>North Limit of Fennell Avenue East to South Limit of Mohawk Road East</p>	<p>None</p>	<p>40</p>	
<p>Highway Valley Ward 10 Phase 3 - 2022</p>	<p>East limit of Millen Road to west limit of Dewitt Road</p>	<p>South limit of Highway 8 to Escarpment</p>	<p>None</p>	<p>40</p>	

<p>Hill Park Ward 7 Phase 2 - 2020</p>	<p>East limit of Upper Wellington Street to west limit of Upper Wentworth Street</p>	<p>South limit of Fennell Avenue East to north limit of Mohawk Road East</p>	<p>None</p>	<p>40</p>	
<p>Homeside Ward 4 Phase 1 - 2019</p>	<p>East limit of Kenilworth Avenue North to east limit of Strathearne Avenue</p>	<p>Rail Corridor (380m north of Barton Street East) to north limit of Main Street East</p>	<p>Barton Street East, Cannon Street East</p>	<p>40</p>	

<p>Huntington Ward 6 Phase 2 - 2020</p>	<p>East limit of Upper Ottawa Street to east limit of Mountain Brow Boulevard</p>	<p>South limit of Fennell Avenue East to north limit of Mohawk Road East</p>	<p>Mountain Brow Boulevard</p>	<p>40</p>	
<p>Inch Park Ward 7 Phase 2 - 2020</p>	<p>East limit of Upper Wellington Street to west limit of Upper Wentworth Street</p>	<p>Escarpment to north limit of Fennell Avenue East</p>	<p>Jolly Cut, Concession Street</p>	<p>40</p>	

<p>Industrial Sector A & Keith Ward 3 Phase 2 - 2020</p>	<p>East limit of Wellington Street North to west limit of Wentworth Street North</p>	<p>South limit of Burlington Street East to Rail Corridor (280m north of Barton Street East)</p>	<p>Victoria Avenue North</p>	<p>40</p>	
<p>Industrial Sector B & Keith Ward 3 Phase 3 – 2022</p>	<p>East limit of Wentworth Street North to west limit of Sherman Avenue North</p>	<p>Lake Ontario to Rail Corridor</p>	<p>Burlington Street East</p>	<p>40</p>	

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To Regulate Traffic

<p>Industrial Sector C Ward 3 Phase 3 – 2022</p>	<p>East limit of Sherman Avenue North to west limit of Gage Avenue North</p>	<p>South limit of Burlington Street East to Rail Corridor</p>	<p>None</p>	<p>40</p>	
<p>Industrial Sector D Ward 3 Phase 3 – 2022</p>	<p>East limit of Gage Avenue North to west limit of Ottawa Street North</p>	<p>South limit of Burlington Street East to Rail Corridor</p>	<p>None</p>	<p>40</p>	

<p>Industrial Sector E Ward 4 Phase 3 - 2022</p>	<p>East limit of Ottawa Street North to west limit of Kenilworth Avenue North</p>	<p>South limit of Nikola Tesla Boulevard to Rail Corridor</p>	<p>None</p>	<p>40</p>	
<p>Jerome Ward 8 Phase 4 - 2022</p>	<p>West Limit of Upper James Street to East Limit of Upper Wellington Street</p>	<p>North Limit of Lincoln Alexander Parkway to East limit of Stone Church Road East</p>	<p>None</p>	<p>40</p>	

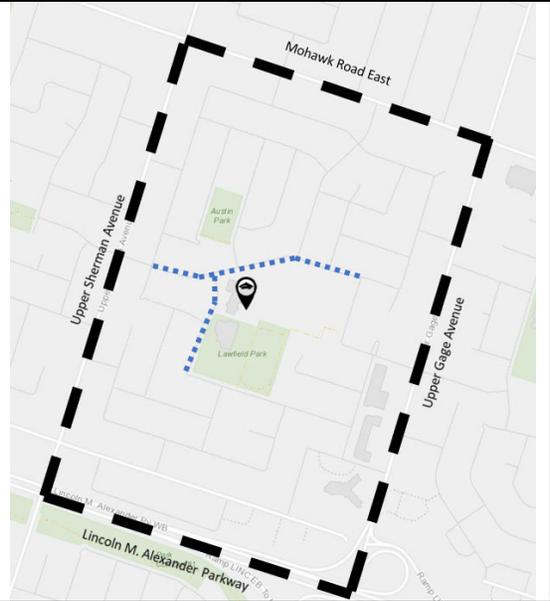
Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Kennedy East Ward 8 Phase 4 - 2022</p>	<p>West limit of Approximately 40.0m West of Westlawn Drive to East Limit of Upper James Street</p>	<p>North Limit of Rymal Road West to South Limit of Hydro Corridor</p>	<p>None</p>	<p>40</p>	
<p>Kentley Ward 5 Phase 3 - 2022</p>	<p>East limit of Red Hill Valley Parkway to west limit of Centennial Parkway North</p>	<p>South limit of Barton Street East to north limit of Queenston Road</p>	<p>Nash Road North</p>	<p>40</p>	

<p>Kernighan Ward 8 Phase 4 - 2022</p>	<p>East Limit of West 5th Street to West Limit of Upper James Street</p>	<p>North Limit of Lincoln Alexander Parkway to South Limit of Stone Church Road West</p>	<p>None</p>	<p>40</p>	
<p>Kirkendall North Ward 1 Phase 2 - 2020</p>	<p>East limit of Highway 403 to west limit of Queen Street South</p>	<p>South limit of Main Street West to north limit of Aberdeen Avenue</p>	<p>Dundurn Street South</p>	<p>40</p>	

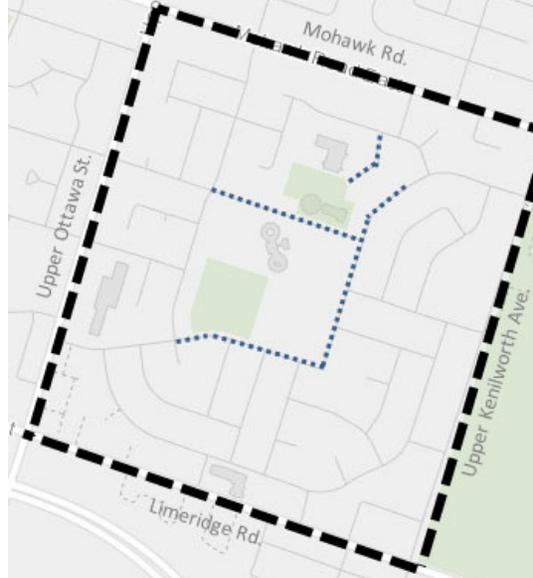
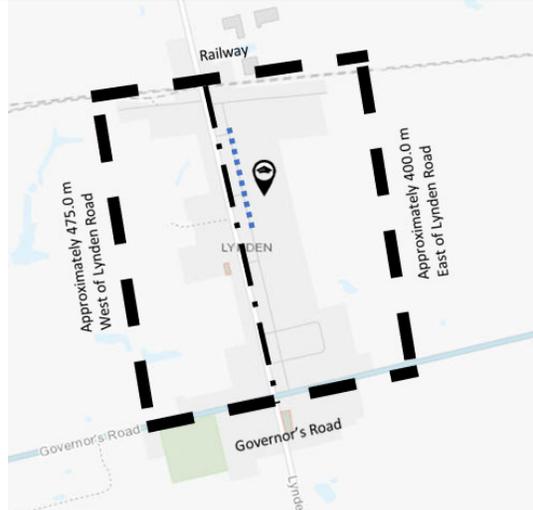
<p>Kirkendall South Ward 1 Phase 1 - 2019</p>	<p>West limit of Chedoke Avenue to west limit of Queen Street</p>	<p>South limit of Aberdeen Avenue to Escarpment</p>	<p>None</p>	<p>40</p>	
<p>Kopperfield Park Ward 11 Phase 1 - 2019</p>	<p>East limit of Glancaster Road to west limit of Garth Street</p>	<p>Hydro Corridor to north limit of Twenty Road</p>	<p>None</p>	<p>40</p>	
<p>Lakeshore Ward 10 Phase 3 - 2022</p>	<p>East limit of Grays Road to west limit of Given Road</p>	<p>Lake Ontario to north limit of North Service Road</p>	<p>None</p>	<p>40</p>	

<p>Landsdale Ward 3 Phase 1 - 2019</p>	<p>East limit of Wellington Street North/Wellington Street South to West limit of Wentworth Avenue North/Wentworth Avenue South</p>	<p>Rail Corridor (280m north of Barton Street East) to north limit Main Street East</p>	<p>Victoria Avenue North, Victoria Avenue South, Barton Street East, Cannon Street East, Wilson Street, King Street East</p>	<p>40</p>	
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<p>Lawfield Ward 7 Phase 4 - 2022</p>	<p>East limit of Upper Gage Avenue to West limit of Upper Sherman Avenue</p>	<p>North Limit of Mohawk Road E to South limit of Lincoln M. Alexander Parkway</p>		<p>40</p>	
<p>Leckie Park Ward 9 Phase 1 - 2019</p>	<p>West limit of Second Road to west limit of Upper Centennial Parkway</p>	<p>South limit of Highland Road to north limit of Rymal Road</p>	<p>None</p>	<p>40</p>	

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To Regulate Traffic

<p>Leeming Ward 12 Phase 1 - 2019</p>	<p>East limit of Hamilton Drive to west limit of Fiddler's Green Road</p>	<p>South limit of Wilson Street to north limit of Highway 403</p>	<p>None</p>	<p>40</p>	
<p>Lime Kiln Ward 12 Phase 3 - 2022</p>	<p>East limit of Wilson Street East to west limit of Highway 403</p>	<p>South limit of Wilson Street East to north limit of Mohawk Road</p>	<p>None</p>	<p>40</p>	

<p>Lisgar Ward 6 Phase 1 - 2019</p>	<p>East limit of Upper Ottawa Street to west limit of Upper Kenilworth Avenue</p>	<p>South limit of Mohawk Road East to south limit of Limeridge Road</p>	<p>None</p>	<p>40</p>	
<p>Lynden Ward 12 Phase 4 - 2022</p>	<p>East limit of approximately 400.0m east of Lynden Road to West limit of approximately 475.0m west of Lynden Road</p>	<p>North limit of the Railway to South limit of Governor's Road</p>	<p>Lynden Road</p>	<p>40</p>	

<p>Macassa Ward 7 Phase 4 - 2022</p>	<p>West Limit of Upper Sherman Avenue to East Limit of Upper Gage Avenue</p>	<p>North Limit of Fennell Avenue East to East Limit of Mohawk Road East</p>	<p>None</p>	<p>40</p>	
<p>McQueston East Ward 4 Phase 3 – 2022</p>	<p>East limit of Woodward Avenue to west limit of Red Hill Valley Parkway</p>	<p>Rail Corridor to north limit of Heath Street</p>	<p>Barton Street East</p>	<p>40</p>	

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<p>McQueston West Ward 4 Phase 3 - 2022</p>	<p>East limit of Parkdale Avenue North to west limit of Red Hill Valley Parkway</p>	<p>Rail Corridor to north limit of Queenston Road</p>	<p>Barton Street East</p>	<p>40</p>	
<p>Meadowlands Ward 12 Phase 1 - 2019</p>	<p>East limit of Southcote Road to Hydro Corridor</p>	<p>South limit of the LINC/Hwy 403 to north limit of Garner Road</p>	<p>Golf Links Road, Stone Church Road West</p>	<p>40</p>	

<p>Mewburn Ward 8 Phase 4 - 2022</p>	<p>West Limit of West 5th Street to East Limit of Upper James Street</p>	<p>North Limit of Stone Church Road West to East Limit of Rymal Road West</p>	<p>None</p>	<p>40</p>	
<p>Millgrove Ward 15 Phase 3 - 2022</p>	<p>East limit of Cumminsville Drive to west limit of Highway 6</p>	<p>South limit of Highway 6 to north limit of Sheltons Lane</p>	<p>Millgrove Side Road, Concession 5 West</p>	<p>40</p>	

<p>Mohawk Ward 8 Phase 3 - 2022</p>	<p>East limit of Garth Street to west limit of West 5th Street</p>	<p>Escarpment to north limit of Delmar Drive</p>	<p>Fennell Avenue West</p>	<p>40</p>	
<p>Morden/Sobel/Creighton East/Creighton West Ward 13 Phase 1 - 2019</p>	<p>East limit of Dundas Conservation Lands to west limit of Main Street</p>	<p>South limit of Escarpment/ King Street West to north limit of Governors Road</p>	<p>None</p>	<p>40</p>	

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To Regulate Traffic

<p>Mount Hope Ward 11 Phase 1 - 2019</p>	<p>East limit of Glancaster Road to West limit of Upper James Street</p>	<p>South limit of Dickenson Road West to north limit of White Church Road</p>	<p>Airport Road West, Highway 6</p>	<p>40</p>	
<p>Mountview Ward 14 Phase 3 - 2022</p>	<p>Ancaster Town Limits to west limit of Sanatorium Road</p>	<p>Escarpment to north limit of Mohawk Road</p>	<p>None</p>	<p>40</p>	

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To Regulate Traffic

<p>Nash North Phase 4 – 2022 Ward 4</p>	<p>East limit of Upper Centennial and West Limit of Escarpment</p>	<p>North Limit of Escarpment and South Limit of Green Mountain Road</p>	<p>First Road from Bedrock Dr to Green Mountain Road West</p>	<p>40</p>	
<p>Normanhurst Ward 4 Phase 2 - 2020</p>	<p>West limit of Strathearn Avenue to west limit of Parkdale Avenue North</p>	<p>Rail Corridor (280m north of Barton Street East) to north limit of Queenston Road</p>	<p>Barton Street East</p>	<p>40</p>	

<p>Parkview Avenue East Phase 4 Ward 4</p>	<p>East Limit of Red Hill Valley Park and West Limit of Woodward Avenue</p>	<p>North Limit of QEW and South Limit of Rail Corridor</p>	<p>None</p>	<p>40</p>	
<p>Parkview Avenue East Phase 4 - 2022 Ward 4</p>	<p>East Limit of Parkview Ave North and West Limit of Woodward Avenue</p>	<p>North Limit of Nikola Tesla Boulevard and South Limit Rail Corridor</p>	<p>None</p>	<p>40</p>	

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To Regulate Traffic

<p>Patterson Ward 13 Phase 3 - 2022</p>	<p>East limit of Valley Road to west limit of Wesley Avenue</p>	<p>Dundas Town Limits to north limit of York Road</p>	<p>None</p>	<p>40</p>	
<p>Pinehill Ward 9 Phase 1 - 2019</p>	<p>East limit of Trinity Church Road to west limit of Regional Road 56</p>	<p>South limit of Rymal Road to north limit of Golf Club Road</p>	<p>Fletcher Road</p>	<p>40</p>	

<p>Pleasant View Ward 13 Phase 3 - 2022</p>	<p>East limit of Wesley Avenue to west limit of Highway 6</p>	<p>Dundas Town Limits to north limit of York Road</p>	<p>None</p>	<p>40</p>	
<p>Poplar Park Ward 10 Phase 2 - 2020</p>	<p>East limit of Green Road to West limit of Millen Road</p>	<p>South limit of Barton Street to north limit of Highway 8</p>	<p>None</p>	<p>40</p>	

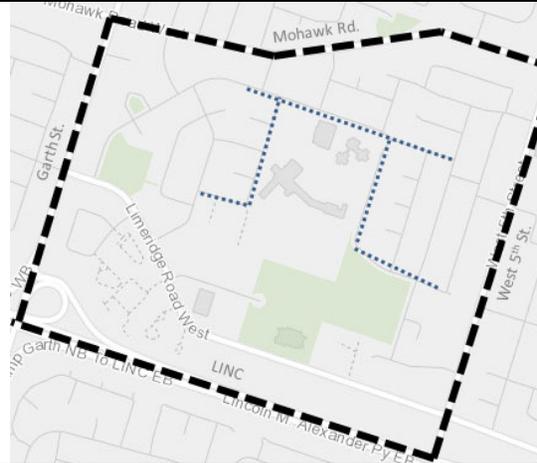
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To Regulate Traffic

<p>Quinndale Ward 6 Phase 3 - 2022</p>	<p>East limit of Upper Gage Avenue to west limit of Upper Ottawa Street</p>	<p>South limit of Lincoln M. Alexander Parkway to north limit of Stone Church Road East</p>	<p>None</p>	<p>40</p>	
<p>Raleigh Ward 7 Phase 1 - 2019</p>	<p>East limit of Upper Sherman Avenue to west limit of Upper Gage Avenue</p>	<p>Escarpment to north limit of Fennell Avenue</p>	<p>Concession Street</p>	<p>40</p>	

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<p>Randall Ward 6 Phase 2 - 2020</p>	<p>East limit of Upper Sherman Avenue to west limit of Upper Gage Avenue</p>	<p>South limit of the LINC to north limit of Stone Church Road East</p>	<p>None</p>	<p>40</p>	
<p>Redhill Ward 5 Phase 1 - 2019</p>	<p>East limit of Red Hill Valley Parkway to east limit of Montmorency Drive/Mount Albion Road</p>	<p>South limit of King Street East to Escarpment</p>	<p>None</p>	<p>40</p>	

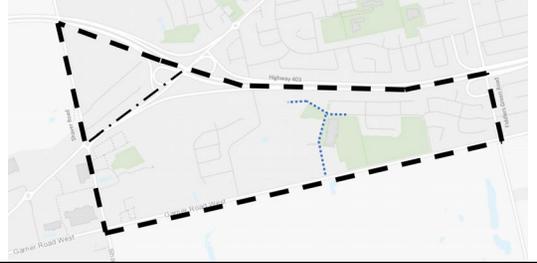
<p>Riverdale East Ward 5 Phase 1 - 2019</p>	<p>East limit of Lake Avenue to west limit of Gray Road</p>	<p>South limit of Barton Street East to north limit of Queenston Road</p>	<p>None</p>	<p>40</p>	
<p>Riverdale West Ward 5 Phase 1 - 2019</p>	<p>East limit of Centennial Parkway North to west limit of Lake Avenue North</p>	<p>South limit of Barton Street East to north limit of Queenston Road</p>	<p>None</p>	<p>40</p>	

<p>Rolston Ward 8 Phase 1 - 2019</p>	<p>East limit of Garth Street to west limit of West 5th Street</p>	<p>South limit of Mohawk Road to north limit of the LINC</p>	<p>None</p>	<p>40</p>	
<p>Rosedale Ward 4 Phase 1 - 2019</p>	<p>Escarpment to west limit of Red Hill Valley Parkway</p>	<p>North limit of Lawrence Road to south limit of Greenhill Avenue</p>	<p>None</p>	<p>40</p>	

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<p>Rushdale Ward 7 Phase 2 - 2020</p>	<p>East limit of Upper Wentworth Street to west limit of Upper Sherman Avenue</p>	<p>South limit of the LINC to north limit of Stone Church Road East</p>	<p>None</p>	<p>40</p>	
<p>Ryckmans Ward 8 Phase 1 - 2019</p>	<p>East limit of Upper James to west limit of Upper Wellington</p>	<p>South limit of Stone Church Road to north limit of Rymal Road</p>	<p>None</p>	<p>40</p>	

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<p>Scenic Woods Ward 14 Phase 3 - 2022</p>	<p>East limit of Highway 403 to Ancaster Town Limits</p>	<p>Escarpment to north limit of Lincoln M. Alexander Parkway</p>	<p>None</p>	<p>40</p>	
<p>Shaver Ward 12 Phase 2 - 2020</p>	<p>East limit of Shaver Road to west limit of Fiddlers Green Road</p>	<p>South limit of Highway 403 to north limit of Garner Road West</p>	<p>Wilson Street West</p>	<p>40</p>	
<p>Sheldon Ward 8 Phase 4 - 2022</p>	<p>East limit of West 5th Street to West limit of Garth Street</p>	<p>North limit of Stone Church Road W to South limit of Rymal Road W</p>	<p>None</p>	<p>40</p>	

<p>Sherwood Ward 6 Phase 2 - 2020</p>	<p>East limit of Upper Ottawa Street to east limit of Mountain Brow Boulevard</p>	<p>Escarpment to north limit of Fennell Avenue East</p>	<p>Mountain Brow Boulevard, Kenilworth Access</p>	<p>40</p>	
<p>South Meadow Ward 10 Phase 1 - 2019</p>	<p>East limit of Green Road to west limit of Millen Road</p>	<p>South limit of Highway 8 to Escarpment</p>	<p>King Street East</p>	<p>40</p>	

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To Regulate Traffic

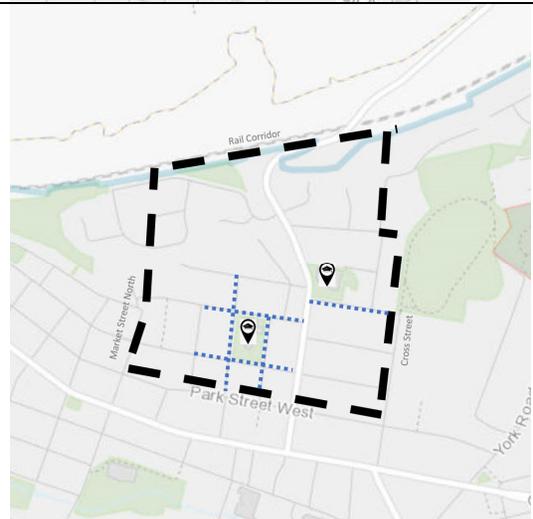
<p>Southam Ward 8 Phase 3 - 2022</p>	<p>East limit of West 5th Street to west limit of Upper James Street</p>	<p>Escarpment to Fennell Avenue West</p>	<p>None</p>	<p>40</p>	
<p>Spencer Creek Ward 13 Phase 3 - 2022</p>	<p>East limit of Highway 8 to west limit of Market Street South</p>	<p>South limit of King Street West to north limit of Mill Street</p>	<p>None</p>	<p>40</p>	

<p>Spring Valley Ward 12 Phase 1 - 2019</p>	<p>East limit of Shaver Road to west limit of Fiddler's Green Road</p>	<p>South limit of Jerseyville Road to North limit of Wilson Street</p>	<p>None</p>	<p>40</p>	
<p>St. Clair Ward 3 Phase 3 - 2022</p>	<p>East limit of Wentworth Street South to west limit of Sherman Avenue South</p>	<p>South limit of Main Street East to Escarpment</p>	<p>None</p>	<p>40</p>	
<p>St. John's Ward 12 Phase 3 - 2022</p>	<p>East limit of Wilson Street East to west limit of Highway 403</p>	<p>South limit of Wilson Street East to north limit of Golf Links Road</p>	<p>None</p>	<p>40</p>	

<p>Stinson Ward 3 Phase 2 - 2020</p>	<p>East limit of Wellington Street South to west limit of Wentworth Street North</p>	<p>South limit of Main Street East to Escarpment</p>	<p>Victoria Avenue North, Claremont Access, Sherman Access, Charlton Avenue</p>	<p>40</p>	
<p>Stipley Ward 3 Phase 1 - 2019</p>	<p>East limit of Sherman Avenue North to West limit of Gage Avenue North</p>	<p>Rail Corridor (350m north of Barton Street) to North limit of Main Street East</p>	<p>Barton Street East, Cannon Street East, King Street East</p>	<p>40</p>	

Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Stoney Creek Ward 5 Phase 2 - 2020</p>	<p>East limit of Lake Avenue Drive to west limit of Gray Road</p>	<p>South limit of Queenston Road to Escarpment</p>	<p>King Street West</p>	<p>40</p>	
<p>Strathcona Ward 1 Phase 3 - 2022</p>	<p>East limit of Highway 403 to west limit of Queen Street North</p>	<p>Lake Ontario to north limit of Main Street West</p>	<p>King Street West, York Boulevard, Barton Street West</p>	<p>40</p>	

<p>Sunninghill Ward 6 Phase 1 - 2019</p>	<p>East limit of Upper Gage Avenue to West limit of Upper Ottawa Street</p>	<p>Escarpment to north limit of Fennell Avenue</p>	<p>Concession Street, Mountain Brow Boulevard</p>	<p>40</p>	
<p>Sydenham Ward 13 Phase 3 - 2022</p>	<p>East limit of Market Street North to west limit of Cross Street</p>	<p>Rail Corridor to north limit of Park Street West</p>	<p>None</p>	<p>40</p>	

Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Thorner Ward 7 Phase 4 - 2022</p>	<p>East Limit of Upper Wentworth Street to West Limit Upper Sherman Avenue</p>	<p>North Limit of Mohawk Road East to South Limit of Lincoln Alexander Parkway</p>	<p>Mall Road, Limeridge Road East</p>	<p>40</p>	
<p>Trenholme Ward 6 Phase 3 - 2022</p>	<p>East limit of Upper Ottawa Street to west limit of Trenholme Crescent</p>	<p>South limit of Limeridge Road East to north limit of Stone Church Road East</p>	<p>None</p>	<p>40</p>	

<p>Trillium Ward 10 Phase 2 - 2020</p>	<p>East limit of Fruitland Road to west limit of Winona Road</p>	<p>Lake Ontario to north limit of North Service Road</p>	<p>None</p>	<p>40</p>	
<p>Trinity Ward 9 Phase 3 - 2022</p>	<p>East limit of Upper Red Hill Valley Parkway to west limit of Second Road West</p>	<p>South limit of Highland Road West to north limit of Rymal Road East</p>	<p>None</p>	<p>40</p>	
<p>Turnball/Highland Hills West/ Highland Hills East/ Pleasant Valley West/ Pleasant Valley East Ward 13 Phase 2 - 2020</p>	<p>Dundas town limits to Spencer Creek</p>	<p>Rail Corridor to Escarpment</p>	<p>Governors Road</p>	<p>40</p>	

Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>University Gardens Ward 13 Phase 4 - 2022</p>	<p>West Limit of Osler Drive to East Limit of approximately 60.0m east of Grant Boulevard</p>	<p>North Limit of Cootes Drive to South Limit of approximately 70.0m south of Desjardins Avenue</p>	<p>None</p>	<p>40</p>	
<p>Valley Park Ward 9 Phase 2 - 2020</p>	<p>East limit of Upper Red Hill Valley Parkway to Maplewood Park</p>	<p>Mud Street West to Highland Road West</p>	<p>Winterberry Drive, Paramount Drive</p>	<p>40</p>	

<p>Vincent Ward 5 Phase 2 - 2020</p>	<p>East limit of Montmorency Drive/Mount Albion Road to west limit of Vincent/Gershome Open Space</p>	<p>South limit of King Street East to Escarpment</p>	<p>Greenhill Avenue, Quigley Road</p>	<p>40</p>	
<p>Waterdown East Ward 15 Phase 2 - 2020</p>	<p>East limit Hamilton Street North to City Limits</p>	<p>South limit of Parkside Drive to north limit of Dundas Street East</p>	<p>Avonsyde Boulevard</p>	<p>40</p>	

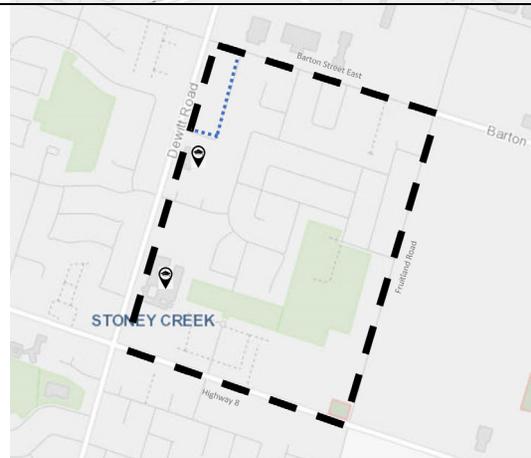
<p>Waterdown Northeast Ward 15 Phase 2 - 2020</p>	<p>East limit of Centre Road to west limit of Milborough Line</p>	<p>South limit of Concession 5 Road East to north limit of Parkside Drive</p>	<p>Robson Road</p>	<p>40</p>	
<p>Waterdown Northwest Ward 15 Phase 1 - 2019</p>	<p>East limit of Highway 6 to west limit of Centre Road/Hamilton Street North</p>	<p>South limit of Concession 5 Road East to north limit of Parkside Drive</p>	<p>None</p>	<p>40</p>	

<p>Waterdown Southeast Ward 15 Phase 2 - 2020</p>	<p>East limit of Mill Street South to City Limits</p>	<p>South limit of Dundas Street East to Escarpment</p>	<p>None</p>	<p>40</p>	
<p>Waterdown Southwest Ward 15 Phase 1 - 2019</p>	<p>East limit of Highway 6 to west limit of Mill Street South</p>	<p>South limit of Dundas Street to Escarpment</p>	<p>None</p>	<p>40</p>	

<p>Waterdown West Ward 15 Phase 1 - 2019</p>	<p>East limit of Highway 6 to west limit of Hamilton Street North</p>	<p>South limit of Parkside Drive to north limit of Dundas Street</p>	<p>None</p>	<p>40</p>	
<p>Westcliffe East Ward 14 Phase 3 – 2022</p>	<p>East limit of Upper Paradise Road to west limit of Garth Street</p>	<p>Escarpment to north limit of Mohawk Road West</p>	<p>None</p>	<p>40</p>	

Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Westcliffe West Ward 14 Phase 3 - 2022</p>	<p>East limit of Sanatorium Road to west limit of Upper Paradise Road</p>	<p>Escarpment to north limit of Mohawk Road</p>	<p>None</p>	<p>40</p>	
<p>Westdale North Ward 1 Phase 2 - 2020</p>	<p>Churchill Park to west limit of Highway 403</p>	<p>Westdale North Open Space to north limit of King Street West</p>	<p>None</p>	<p>40</p>	

<p>Westdale South Ward 1 Phase 2 - 2020</p>	<p>West limit of Forsyth Avenue North/Forsyth Avenue South to west limit of Highway 403</p>	<p>Churchill Park/North limit of King Street West to north limit of Main Street West</p>	<p>Paradise Road South, Main Street West</p>	<p>40</p>	
<p>Westmeria Ward 10 Phase 3 - 2022</p>	<p>East limit of Dewitt Road to west limit of Fruitland Road</p>	<p>South limit of Barton Street to north limit of Highway 8</p>	<p>None</p>	<p>40</p>	
<p>Winona North Ward 10 Phase 3 – 2022</p>	<p>East limit of Lewis Road to west limit of Creanona Boulevard</p>	<p>Lake Ontario to north limit of North Service Road</p>	<p>None</p>	<p>40</p>	

Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>Winona South Ward 10 Phase 3 - 2022</p>	<p>East limit of Lewis Road to west limit of Fifty Road</p>	<p>Rail Corridor to Escarpment</p>	<p>Winona Road, Barton Street, Highway 8</p>	<p>40</p>	
<p>Yeoville Ward 8 Phase 2 - 2020</p>	<p>East limit of West 5th Street to west limit of Upper James Street</p>	<p>South limit of Mohawk Road West to north limit of the LINC</p>	<p>None</p>	<p>40</p>	

Appendix A to By-law No. 22-172 – To Amend By-law No. 01-215, Being a By-law
To Regulate Traffic

<p>York Heights/Hunter Ward 13 Phase 2 - 2020</p>	<p>East limit of Sydenham Street to west limit of Olympic Drive/York Road</p>	<p>Rail Corridor to north limit of King Street East</p>	<p>None</p>	<p>40</p>	
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Authority: Item 9, Public Works Committee
Report 07-016 (PW07153)
CM: December 12, 2007
Ward: 1, 2, 3, 13
Bill No. 173

CITY OF HAMILTON

BY-LAW NO. 22-

To Amend By-law No. 01-215 Being a By-law To Regulate Traffic

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001, S.O. 2001, c. 25*, authorize the City of Hamilton to pass by-laws as necessary or desirable for the public and municipal purposes, and in particular paragraphs 4 through 8 of subsection 10(2) authorize by-laws respecting: assets of the municipality, the economic, social and environmental well-being of the municipality; health, safety and well-being of persons; the provision of any service or thing that it considers necessary or desirable for the public; and the protection of persons and property;

AND WHEREAS on the 18th day of September, 2001, the Council of the City of Hamilton enacted By-law No. 01-215 to regulate traffic; and

AND WHEREAS it is necessary to amend By-law No. 01-215.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. Schedule 2 (Speed Limits) of By-law No. 01-215, as amended, is hereby further amended by adding to Section "B" (Dundas) thereof the following items, namely:

Old Ancaster Road	Ogilvie Street	Old Dundas Road	40
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2. Schedule 9 (No Right Turn on Red) of By-law No. 01-215, as amended, is hereby further amended by adding to Section "E" (Hamilton) thereof the following item, namely;

Main Street East	Eastbound	Gage Avenue South	Anytime
Gage Avenue South	Northbound	Main Street East	Anytime
Main Street East	Eastbound	Springer Avenue	Anytime
Springer Avenue	Northbound	Main Street East	Anytime
Main Street East	Eastbound	Sherman Avenue South	Anytime
Sherman Avenue South	Northbound	Main Street East	Anytime
Main Street East	Eastbound	Fairleigh Avenue South	Anytime
Fairleigh Avenue South	Northbound	Main Street East	Anytime
Sanford Avenue South	Northbound	Main Street East	Anytime
Main Street East	Eastbound	Wentworth Street South	Anytime
Wentworth Street South	Northbound	Main Street East	Anytime

To Amend By-law No. 01-215
Being a By-law to Regulate Traffic

Page 2 of 2

Main Street East	Eastbound	Emerald Street South	Anytime
Emerald Street South	Northbound	Main Street East	Anytime
Main Street East	Eastbound	East Avenue South	Anytime
East Avenue South	Northbound	Main Street East	Anytime
Victoria Avenue South	Northbound	Main Street East	Anytime
Main Street East	Eastbound	Walnut Street South	Anytime
Walnut Street South	Northbound	Main Street East	Anytime
Main Street East	Eastbound	Catherine Street South	Anytime
Main Street East	Eastbound	Hughson Street South	Anytime
Main Street West	Eastbound	MacNab Street South	Anytime
MacNab Street South	Northbound	Main Street West	Anytime
Bay Street South	Northbound	Main Street West	Anytime
Main Street West	Eastbound	Caroline Street South	Anytime
Main Street West	Eastbound	Queen Street South	Anytime
Main Street West	Eastbound	Pearl Street South	Anytime
Main Street West	Eastbound	Locke Street South	Anytime
Locke Street South	Northbound	Main Street West	Anytime
Dundurn Street South	Northbound	Main Street West	Anytime

3. Subject to the amendments made in this By-law, in all other respects, By-law No. 01-215, including all Schedules thereto, as amended, is hereby confirmed unchanged;
4. This By-law shall come into force and take effect on the date of its passing and enactment.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 3, Public Works Committee
Report 22-004 (PW22013)
CM: March 30, 2022
Ward: 10

Bill No. 174

CITY OF HAMILTON

BY-LAW NO. 22-

**To Establish City of Hamilton Land
Described as Part of Block 111 on Plan 62M-1073, designated as Parts 1 and 2 on
Plan 62R-21881 as Part of Watershore Drive**

WHEREAS sections 8, 9 and 10 of the *Municipal Act, 2001* authorize the City of Hamilton to pass by-laws necessary or desirable for municipal purposes, and in particular by-laws with respect to highways; and

WHEREAS section 31(2) of the *Municipal Act, 2001* provides that land may only become a highway by virtue of a by-law establishing the highway.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. The land, owned by and located in the City of Hamilton, described as **Part of Block 111 on Plan 62M-1073, designated as Parts 1 and 2 on Plan 62R-21881**, is established as a public highway, forming part of **Watershore Drive**;
2. The General Manager of Public Works or their authorized agent is authorized to establish the said land as a public highway; and
3. This By-law comes into force on the date of its registration in the Land Registry Office (No. 62).

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Authority: Item 5, Planning Committee
Report 22-011 (PED22149)
CM: July 8, 2022
Ward: 7

Bill No. 175

CITY OF HAMILTON

BY-LAW NO. 22-

To Amend Zoning By-law No. 6593 (Hamilton) Respecting Lands Located at 1540 Upper Wentworth Street (Hamilton)

WHEREAS the City of Hamilton Act, 1999, Statutes of Ontario, 1999 Chap. 14, Schedule C. did incorporate, as of January 1, 2001, the municipality “City of Hamilton”;

AND WHEREAS the City of Hamilton is the successor to certain area municipalities, including the former municipality known as the “The Corporation of the City of Hamilton” and is the successor to the former regional municipality, namely, “The Regional Municipality of Hamilton-Wentworth”;

AND WHEREAS the City of Hamilton Act, 1999 provides that the Zoning By-laws and Official Plans of the former area municipalities and the Official Plan of the former regional municipality continue in full force in the City of Hamilton until subsequently amended or repealed by the Council of the City of Hamilton;

AND WHEREAS the Council of The Corporation of the City of Hamilton passed Zoning By-law No. 6593 (Hamilton) on the 25th day of July 1950, which by-law was approved by the Ontario Municipal Board by Order dated the 7th day of December 1951 (File No. P.F.C. 3821);

AND WHEREAS the Council of the City of Hamilton, in adopting Item 5 of Report 22-011 of the Planning Committee, at its meeting held on the 8th day of July 2022, recommended that Zoning By-law No. 6593 (Hamilton), be amended as hereinafter provided; and,

WHEREAS, this By-law is in conformity with the Urban Hamilton Official Plan.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. That Map No. E18d of the District Maps, appended to and forming part of Zoning By-law No. 6593 (Hamilton), is amended by changing the zoning from the "RT-20" (Townhouse - Maisonette) District to "RT-20/S-1823" (Townhouse - Maisonette) District (Block 1) and “E-2/S-1823-H” (Multiple Dwellings) District, Modified, Holding (Block 2) the extent and boundaries of which are shown on a plan hereto annexed as Schedule “A”.

2. For the purpose of the regulations contained in Hamilton Zoning By-law No. 6593, the external boundary of the lot shown on Schedule "A" shall be deemed to be the lot lines for this purpose and the regulations of the "RT-20/S-1823" (Townhouse - Maisonette) District (Block 1) and "E-2/S-1823-H" (Multiple Dwellings) District, Modified, Holding including but not limited to the lot area, lot frontage, lot depth, lot coverage and building setbacks, shall be from the external boundaries of these lots shown on Schedule "A" and not the individual property boundaries of any lots created by registration of a Condominium Plan, Part Lot Control, or Consent.
3. That the "E-2" (Multiple Dwellings) District provisions, as contained in Section 11B of Zoning By-law No. 6593, applicable to a portion of the subject lands (Block 2), be modified to include the following special requirements:
 - a) Notwithstanding Sections 2(2)J.(xiii) and 2(2)J.(xv), for the purposes of this By-law:
 - i. "Lot-Line, Front" shall mean the boundary lot line along Upper Wentworth Street;
 - ii. "Lot-Line, Exterior Side" shall mean the boundary lot line along Emperor Avenue; and,
 - iii. "Lot-Line, Interior Side" shall mean any lot line except a front lot line, rear lot line or exterior side lot line.
 - b) Notwithstanding Section 11B(2)(ii), no building or structure for any other use shall exceed eight storeys or 27.0 metres in height.
 - c) Notwithstanding Section 11B(3)(i)(b), for all other buildings or structures, a minimum front yard depth of 5.0 metres.
 - d) Notwithstanding Section 11B(3)(ii)(b), for all other buildings or structures, a minimum side yard width of 3.0 metres.
 - e) Notwithstanding Section 11B(5), a maximum floor area ratio shall not apply.
 - f) Notwithstanding Section 18(3)(vi)(b)(iii), a canopy may project into a required side yard not more than 2.0 metres.
 - g) Notwithstanding Section 18(3)(vi)(e), a terrace, uncovered porch, platform or ornamental feature which does not extend more than 1.0 metre above the floor level of the first storey, may project into a required yard, if distant at least 1.0 metre from the nearest side lot line and at least 1.0 metre from the nearest street line.
 - h) Notwithstanding Section 18(8)(c)(1), each multiple dwelling shall be distant from every other building as follows:
 - i. By at least 6.0 metres; except,

- ii. By at least 4.5 metres from the southerly exterior wall of the multiple dwelling.
 - i) Notwithstanding Section 18A(1)(a), 18A(1)(b), 18A Table 1, and 18A Table 2, multiple dwellings shall require:
 - i. 0.59 parking spaces per Class A dwelling unit greater than 50.0 square metres in gross floor area, except;
 - ii. 0.29 parking spaces per Class A dwelling unit less than 50.0 square metres in gross floor area; and,
 - iii. 8.5% of total required parking shall be dedicated visitor parking.
 - j) Notwithstanding Section 18A(7), a parking space shall have dimensions not less:
 - i) Than 2.8 metres by 5.8 metres; except,
 - ii) Where 10 or more parking spaces are required on a lot, the minimum parking space size of not more than 8.5% of such required parking spaces shall be a width of 2.6 metres and a length of 5.5 metres, provided that any such parking space is clearly identified as being reserved for the parking of small cars only.
 - k) Notwithstanding Section 18A(1)(c), and 18A Table 3, one loading space shall be required, having a minimum size of that is 9.0 metres in length, 3.7 metres in width, and 4.3 metres in height.
3. That the 'H' symbol applicable to the lands referred to in Section 1 of this By-law, shall be removed conditional upon:
- a) The Holding Provision "E-2/S-1823-H" (Multiple Dwellings) District Modified, Holding be removed conditional upon:
 - i) An acceptable tenant relocation and assistance plan addressing the right to return to occupy the replacement housing at similar rents, the provision of alternative accommodation at similar rents, and other assistance to lessen the hardship, is provided to the satisfaction of the Director of Housing Services and Director of Planning and Chief Planner.
4. That no building or structure shall be erected, altered, extended or enlarged, nor shall any building or structure or part thereof be used, nor shall any land be used, except in accordance with the "E-2" (Multiple Dwellings) District provisions, subject to the special requirements referred to in Section 2 of this By-law.
5. That By-law No. 6593 is amended by adding this By-law to Section 19B as Schedule S-1823.

6. That the Clerk is hereby authorized and directed to proceed with the giving of notice of the passing of this By-law in accordance with the *Planning Act*.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

ZAC-21-023



<p>This is Schedule "A" to By-law No. 22-</p> <p>Passed the day of, 2022</p>		<p>-----</p> <p>Mayor</p> <p>-----</p> <p>Clerk</p>
<p>Schedule "A"</p> <p>Map forming Part of</p> <p>By-law No. 22-_____</p> <p>to Amend By-law No. 6593</p>		<p>Subject Property</p> <p>1540 Upper Wentworth Street</p> <p> Block 1 - Change in zoning from "RT-20" (Townhouse - Maisonette) District to "RT-20/S-1823" (Townhouse - Maisonette) District, Modified</p> <p> Block 2 - Change in zoning from "RT-20" (Townhouse - Maisonette) District to "E-2/S-1823-H" (Multiple Dwellings) District, Modified, Holding</p>
<p>Scale: N.T.S</p>	<p>File Name/Number: ZAC-21-023</p>	<p>Hamilton</p>
<p>Date: June 1, 2022</p>	<p>Planner/Technician: MF/AL</p>	
<p>PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT</p>		

Authority: Item 7, Planning Committee
Report: 22-011 (PED22143)
CM: July 8, 2022
Ward: 8

Bill No. 176

CITY OF HAMILTON

BY-LAW NO. 22-

To Amend Zoning By-law No. 05-200 with Respect to Lands Located at 60 Caledon Avenue, Hamilton

WHEREAS Council approved Item 7 of Report 22-011 of the Planning Committee, at its meeting held on July 8, 2022;

AND WHEREAS this By-law conforms to the Urban Hamilton Official Plan;

NOW THEREFORE Council amends Zoning By-law No. 05-200 as follows:

1. That Map 1183 of Schedule “A” – Zoning Maps is amended by changing the zoning from the Community Institutional (I2) Zone to the Community Institutional (I2, 810, H127) Zone for the lands attached as Schedule “A” to this By-law.
2. That Schedule “C” - Special Exceptions is amended by adding the following new Special Exception:
 - “810. Within the lands zoned Community Institutional (I2) Zone, identified on Map No. 1183 of Schedule “A” – Zoning Maps and described as 60 Caledon Avenue, the following special provisions shall apply:
 - a) In addition to the uses permitted in Section 8.2.1, a multiple dwelling shall be permitted in accordance with Section 8.2.3.1.
 - b) In addition to Section 8.2.3.1 and notwithstanding Sections 8.2.3.1 c), 8.2.3.1 e), 8.2.3.1 f), the following special provisions shall apply:
 - i) Minimum Side Yard and Flankage Yard A flankage yard of 5.5 metres and a minimum westerly side yard of 3 metres.
 - ii) Maximum Building Height 22 metres
 - iii) Minimum Landscaped Area 45% of the lot area
 - iv) Rooftop mechanical equipment shall be located and/or screened from view of any abutting street.

- v) A walkway shall be permitted in a required planting strip.
- vi) Minimum Amenity Area for Multiple Dwellings
 - 1. An area of 4.0 square metres for each dwelling unit less than or equal to 50 square metres of gross floor area.
 - 2. An area of 6.0 square metres for each dwelling unit greater than 50 square metres of gross floor area.
 - 3. In addition to the definition of Amenity Area in Section 3: Definitions of By-law 05-200, an Amenity Area located outdoors shall be unobstructed and shall be at or above the surface and exposed to light and air.
- c) That Section 5.1 a), v) shall not apply.
- d) For the purposes of Special Exception No. 810, the following special provisions shall apply to Area A and Area B as shown on Figure 25 of Schedule “F” – Special Figures:
 - i) Maximum Building Height 22 metres
Area A
 - ii) Maximum Building Height 11.5 metres
Area B
- 3. That Schedule “D” – Holding Provisions, of By-law No. 05-200, be amended by adding the additional Holding Provision as follows:
 - H127. Notwithstanding Section 8.2 of this By-law, within lands zoned Community Institutional (I2, 810) Zone on Map No. 1183 on Schedule “A” – Zoning Maps, and described as 60 Caledon Avenue, Hamilton, no development shall be permitted until such time as:
 - 1. That the Owner submits and receives approval of an updated Functional Servicing Report and Stormwater Management Report and related drawings to demonstrate that adequate capacity exists in the existing municipal sanitary sewer downstream of the site to

support the proposed development, to the satisfaction of the Director of Growth Management.

4. That the Clerk is hereby authorized and directed to proceed with the giving of notice of the passing of this By-law in accordance with the *Planning Act*.
5. That this By-law No. 22-176 shall come into force and be deemed to come into force in accordance with Sub-section 34(21) of the *Planning Act*, either upon the date of passage of this by-law or as otherwise provided by the said Sub-section.

PASSED this 8th day of July, 2022

F. Eisenberger
Mayor

A. Holland
City Clerk

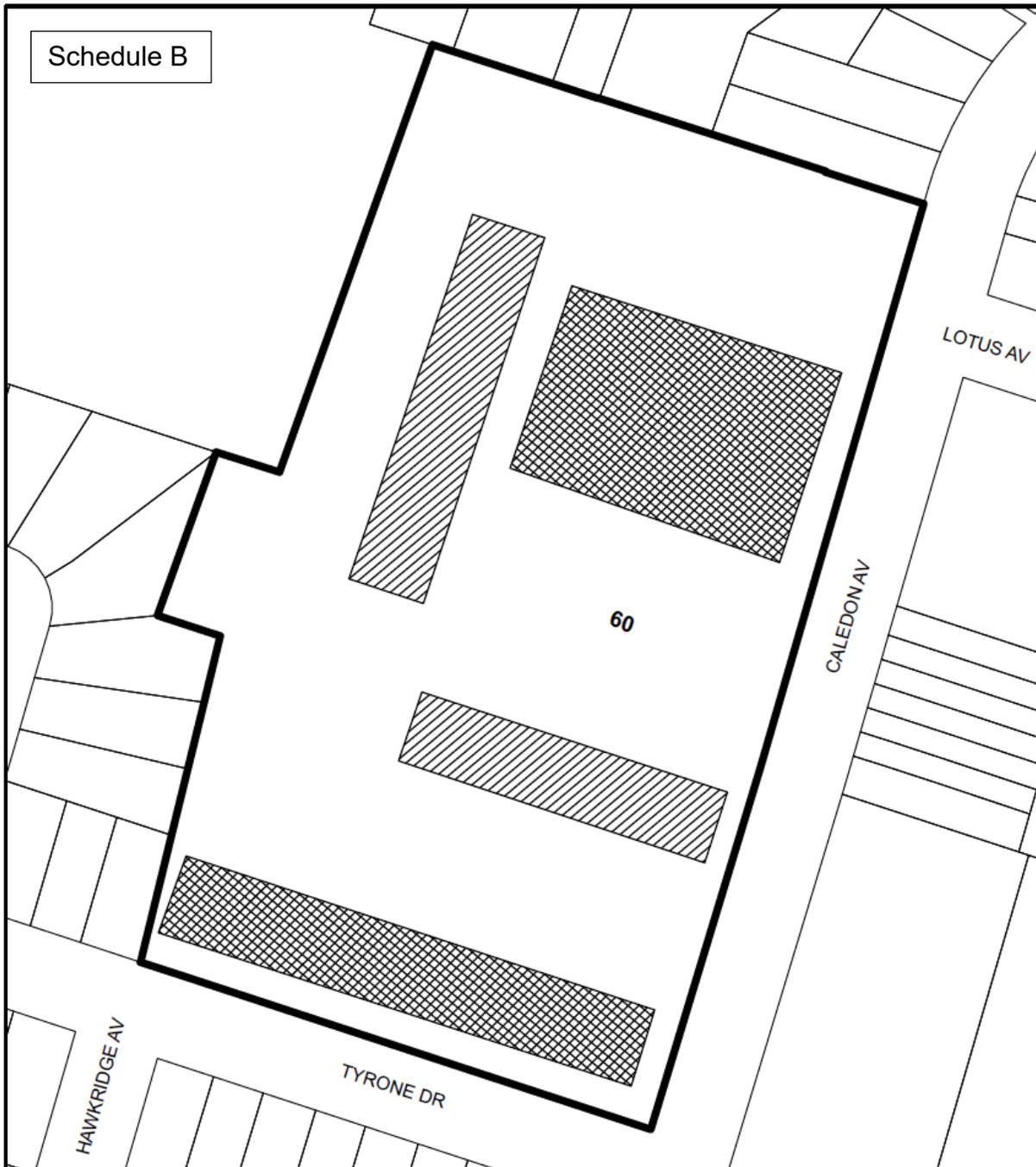
ZAC-21-025



<p>This is Schedule "A" to By-law No. 22-</p> <p>Passed the day of, 2022</p>	<p>-----</p> <p style="text-align: center;">Mayor</p> <p>-----</p> <p style="text-align: center;">Clerk</p>
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<h2 style="margin: 0;">Schedule "A"</h2> <p style="margin: 5px 0;">Map forming Part of By-law No. 22-_____</p> <p style="margin: 10px 0;">to Amend By-law No. 05-200 Map 1183</p>	<p>Subject Property</p> <p>60 Caledon Avenue, Hamilton (Ward 8)</p> <p> Change in Zoning from the Community Institutional (I2) Zone to the Community Institutional (I2, 810, H127) Zone</p>
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<p>Scale: N.T.S</p>	<p>File Name/Number: ZAC-21-025/UHOPA-21-011</p>	
<p>Date: May 12, 2022</p>	<p>Planner/Technician: JVR/NB</p>	
<p>PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT</p>		



Special Figure 28: Maximum Build Heights for 60 Caledon Avenue

Date:
June 1, 2022

Legend

-  Area A - 22 metres
-  Area B - 11.5 metres



Authority: Item 8, Planning Committee
Report 22-011 (PED22144)
CM: July 8, 2022
Ward: 2

Bill No. 177

CITY OF HAMILTON

BY-LAW NO. 22-

To Repeal By-law No. 87-167 and To Amend Zoning By-law No. 6593 Respecting Lands Located at 221-223 Charlton Avenue East and 200 Forest Avenue, Hamilton

WHEREAS the *City of Hamilton Act, 1999*, Statutes of Ontario, 1999 Chap. 14, Schedule C. did incorporate, as of January 1, 2001, the municipality “City of Hamilton”;

AND WHEREAS the City of Hamilton is the successor to certain area municipalities, including the former municipality known as the “The Corporation of the City of Hamilton” and is the successor to the former regional municipality, namely, “The Regional Municipality of Hamilton-Wentworth”;

AND WHEREAS the City of Hamilton Act, 1999 provides that the Zoning By-laws of the former area municipalities continue in force in the City of Hamilton until subsequently amended or repealed by the Council of the City of Hamilton;

AND WHEREAS Zoning By-law No. 6593 (Hamilton) was enacted on the 25th day of July 1950, which was approved by the Ontario Municipal Board by Order dated the 7th date of December 1951, (File. No. O.F.C. 3821);

AND WHEREAS the Council of the City of Hamilton, in adopting Item 8 of Report 22-011 of the Planning Committee, at its meeting held on the 8th day of July, 2022, recommended that Zoning By-law No. 6593 (Hamilton), be amended as hereinafter provided;

AND WHEREAS this By-law is in conformity with the Urban Hamilton Official Plan.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. That By-law No. 87-167 is hereby repealed in its entirety.
2. That Sheet No. E5 of the District Maps appended to and forming part of Zoning By-law No. 6593 (Hamilton), is amended by changing the zoning from the “DE-3/S-970” (Multiple Dwellings) District, Modified (Block 1) and the “D” (Urban Protected Residential – One and Two Family Dwellings, Etc.) District (Block 2) to the “DE-3/S-1819” (Multiple Dwellings) District, Modified; the extent and boundaries of which are shown on a plan hereto annexed as Schedule “A”.
3. That the “DE-3” (Multiple Dwellings) District provisions, as contained in Section 10C of Zoning By-law No. 6593 applicable to the subject lands, be modified to include the following special requirements:

- a) The lands zoned “DE-3/S-970” (Multiple Dwellings) District, Modified shall be deemed to be one lot for the purposes of applying the provisions of the By-law. Zoning provisions shall apply only to the external lot lines of the overall lands, not to internal lot lines resulting from any future severance.
- b) Notwithstanding Sections 2(2)J.(xiii), 2(2)J.(xiv), and 2(2)J.(xv), for the purposes of this By-law:
 - i) “Lot-Line, Front” shall mean the boundary line along Charlton Avenue East;
 - ii) “Lot-Line, Rear” shall mean the boundary line along Forest Avenue;
 - iii) “Lot-Line, Exterior Side” shall mean the boundary lot line along Aurora Street; and,
 - iv) “Lot-Line, Interior Side” shall mean any lot line except a front lot line, rear lot line or exterior side lot line.
- c) Notwithstanding Section 10C(2), no building shall exceed three storeys or 11.5 metres.
- d) In addition to Section 3.c) above, the four storey building height of the multiple dwelling existing on the date of the passing of this By-law shall be permitted.
- e) Notwithstanding Section 10C(3)(i)(b), a minimum front yard depth of 1.0 metres.
- f) Notwithstanding Section 10C(3)(ii)(b), the following side yards shall be provided:
 - i) a minimum interior side yard width of 6.3 metres, except 1.5 metres where no window of a habitable room overlooks the side yard; and,
 - ii) a minimum exterior side yard width of 6.8 metres.
- g) Notwithstanding Section 10C(3)(iii)(b), a minimum rear yard of 6.0 metres.
- h) That Section 10C(5) shall not apply.
- i) That Section 18(3)(vi)(b) shall not apply.
- j) That Section 18(3)(vi)(d) shall not apply.
- k) Notwithstanding Section 18A.(1)(a), not less than 0.55 parking spaces per Class A dwelling unit shall be provided.
- l) That Section 18A.(1)(b) shall not apply.
- m) Notwithstanding Section 18A.(1)(c), one loading space shall be provided with the following dimensions:

Length – 9.0 metres;
Width – 3.7 metres; and,
Height – 4.3 metres.

- n) That in addition to Section 18A(7), not more than 10% of the required parking spaces may have dimensions not less than 2.6 metres wide and 5.5 metres long, provided that any such parking space is clearly identified as being reserved for the parking of small cars only.
 - o) Notwithstanding Section 18(25), where a townhouse dwelling, maisonette dwelling or multiple dwelling is adjacent to a residential district that does not permit such uses, every access driveway to the townhouse dwelling, maisonette dwelling or multiple dwelling shall be located not less than 1.5 metres from the common boundary between the district in which the townhouse dwelling, maisonette dwelling or multiple dwelling is located and the district that does not permit such uses.
4. That no building or structure shall be erected, altered, extended or enlarged, nor shall any building or structure or part thereof be used, nor shall any land be used, except in accordance with the “DE-3” (Multiple Dwellings) District provisions, subject to the special requirements referred to in Section 3 of this By-law.
5. That By-law No. 6593 is amended by adding this By-law to Section 19B as Schedule S-1819.
6. That the Clerk is hereby authorized and directed to proceed with the giving of notice of the passing of this By-law in accordance with the *Planning Act*.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

ZAC-21-020



<p>This is Schedule "A" to By-law No. 22-</p> <p>Passed the day of, 2022</p>		<p>-----</p> <p>Mayor</p> <p>-----</p> <p>Clerk</p>
<p>Schedule "A"</p> <p>Map forming Part of</p> <p>By-law No. 22-_____</p> <p>to Amend By-law No. 6593</p>		<p>Subject Property</p> <p>221 - 223 Charlton Avenue East & 200 Forest Avenue</p> <p> Block 1 - Change in zoning from the "DE-2/S-970" (Multiple Dwellings) District, Modified to the "DE-3/S-1819" (Multiple Dwellings) District, Modified</p> <p> Block 2 - Change in zoning from the "D" (Urban Protected Residential - One and Two Family Dwellings, Etc.) District, to the "DE-3/S-1819" (Multiple Dwellings) District, Modified</p>
<p>Scale: N.T.S</p>	<p>File Name/Number: ZAC-21-020</p>	 Hamilton
<p>Date: May 18, 2022</p>	<p>Planner/Technician: MK/AL</p>	
<p>PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT</p>		

Authority: Item 9, Planning Committee
Report: 22-011 (PED22153)
CM: July 8, 2022
Ward: 4

Bill No. 178

CITY OF HAMILTON

BY-LAW NO. 22-

To Adopt:

**Official Plan Amendment No. 169 to the
Urban Hamilton Official Plan**

Respecting:

**20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2,
4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue
(Hamilton)**

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. Amendment No. 169 to the Urban Hamilton Official Plan consisting of Schedule "1", hereto annexed and forming part of this by-law, is hereby adopted.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

Urban Hamilton Official Plan Amendment No. 169

The following text attached hereto, constitutes Official Plan Amendment No. 169 to the Urban Hamilton Official Plan.

1.0 Purpose and Effect:

The purpose and effect of this Amendment is to further modify Urban Site Specific Policy UHN-25 to delete the maximum permitted net residential density of 165 units per hectare, and to require that increases in height and density be implemented through a Zoning By-law Amendment or Minor Variance in accordance with applicable servicing and design criteria.

2.0 Location:

The lands affected by this Amendment are known municipally as 20 Reid Avenue North, 11-17 and 41 Reid Avenue South, 22-116 Lang Street and 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22 and 24 Hayes Avenue in the former City of Hamilton.

3.0 Basis:

The basis for permitting this Amendment is:

- The Amendment implements Phase 1 of the City's Municipal Comprehensive Review / GRIDS 2 and the direction to remove maximum permitted residential densities for individual developments from the “Neighbourhoods” designation in the Urban Hamilton Official Plan;
- The Amendment allows for greater flexibility to implement residential intensification without further amendments to the Official Plan, provided applicable servicing and design criteria are met; and,
- The Amendment is consistent with the Provincial Policy Statement, 2020 and conforms to the Growth Plan for the Greater Golden Horseshoe, 2019, as amended.

4.0 Actual Changes:

4.1 Volume 3 – Special Policy Areas, Area Specific Policies, and Site Specific Policies

Text

4.1.1 Chapter C – Urban Site Specific Policies

a. That Volume 3: Chapter C – Urban Site Specific Policies be amended by further modifying Site Specific Policy UHN-25, as follows:

i. Deleting Policy UHN-25 1.0 b) and replacing it with the following:

“b) That Policies E.3.4.4, E.3.5.7 and E.3.6.6 b) of Volume 1 shall not apply.”

ii. Adding Policies UHN-25 1.0 c), d), e) and f) as follows:

“c) In addition to Section C.5.0 – Infrastructure of Volume 1, the approval of *development* resulting in a *net residential density* greater than 165 units per hectare shall be contingent on the availability of water, wastewater and storm water capacity.

d) Any *multiple dwelling* with a building height greater than 12 storeys shall demonstrate compliance with the design criteria in Policy E.3.6.7 of Volume 1 applicable to high profile *multiple dwellings*.

e) An implementing Zoning By-law Amendment shall be required to facilitate any *development* with a building height greater than 12 storeys and / or that results in a *net residential density* greater than 165 units per hectare.

f) Notwithstanding Policy e) above, a modification to the Zoning By-law shall be considered minor and implementable through a Minor Variance for any *development* with a building height greater than 12 storeys but not more than 18 storeys, and / or that results in a *net residential density* greater than 165 units per hectare, but not more than 254 units per hectare.”

5.0 Implementation:

An implementing Zoning By-Law Amendment or Minor Variance and Site Plan will give effect to the intended uses on the subject lands.

This Official Plan Amendment is Schedule "1" to By-law No. 22-178 passed on the 8th day of July, 2022.

**The
City of Hamilton**

F. Eisenberger
Mayor

A. Holland
City Clerk

**CITY OF HAMILTON
BY-LAW NO. 22-**

**Respecting Removal of Part Lot Control
Lot 4, Registered Plan No. 62M-1229, Municipally Known As 73 Aeropark Boulevard,
Hamilton (Ward 11)**

WHEREAS the sub-section 50(5) of the *Planning Act*, (R.S.O. 1990, Chapter P.13, as amended, establishes part-lot control on land within registered plans of subdivision;

AND WHEREAS sub-section 50(7) of the *Planning Act*, provides as follows:

“(7) **Designation of lands not subject to part lot control.** -- Despite subsection (5), the council of a local municipality may by by-law provide that subsection (5) does not apply to land that is within such registered plan or plans of subdivision or parts of them as are designated in the by-law.”

AND WHEREAS the Council of the City of Hamilton is desirous of enacting such a by-law with respect to the lands hereinafter described;

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. Sub-section 5 of Section 50 of the *Planning Act*, for the purpose of creating three parts to facilitate the phased development of two commercial lots (Parts 1 and 3) and a hydro box easement (Part 2), as shown on Deposited Reference Plan 62R-21918, shall not apply to the portion of the registered plan of subdivision that is designated as follows, namely:

Lot 4, Registered Plan No. 62M-1229, in the City of Hamilton.
2. This by-law shall be registered on title to the said designated land and shall come into force and effect on the date of such registration.
3. This by-law shall expire and cease to be of any force or effect on the 8th day of July, 2024.

PASSED this 8th day of July 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk

CITY OF HAMILTON

BY-LAW NO. 22-

To Confirm the Proceedings of City Council at its meeting held on July 8, 2022.

**THE COUNCIL OF THE
CITY OF HAMILTON
ENACTS AS FOLLOWS:**

1. The Action of City Council at its meeting held on the 8th day of July 2022, in respect of each recommendation contained in

General Issues Committee Report 22-013 – June 23, 2022,
General Issues Committee Report 22-014 – July 4, 2022,
Planning Committee Report 22-011 – July 5, 2022,
Board of Health Report 22-007 – July 6, 2022,
Public Works Committee Report 22-011 – July 6, 2022,
Audit, Finance & Administration Committee Report 22-013 – July 7, 2022,
and
Emergency & Community Services Committee Report 22-011 – July 7, 2022,

considered by City of Hamilton Council at the said meeting, and in respect of each motion, resolution and other action passed and taken by the City Council at its said meeting is hereby adopted, ratified and confirmed.

2. The Mayor of the City of Hamilton and the proper officials of the City of Hamilton are hereby authorized and directed to do all things necessary to give effect to the said action or to obtain approvals where required, and except where otherwise provided, the Mayor and the City Clerk are hereby directed to execute all documents necessary in that behalf, and the City Clerk is hereby authorized and directed to affix the Corporate Seal of the Corporation to all such documents.

PASSED this 8th day of July, 2022.

F. Eisenberger
Mayor

A. Holland
City Clerk