



Hamilton

COMMITTEE OF ADJUSTMENT

City Hall, 5th floor, 71 Main Street West, Hamilton, ON L8P 4Y5
Telephone (905) 546-2424, ext. 4221, 3935 Fax (905) 546-4202

E-mail: cofa@hamilton.ca

NOTICE OF PUBLIC HEARING **Minor Variance**

You are receiving this notice because you are either:

- Assessed owner of a property located within 60 metres of the subject property
 - Applicant/agent on file, or
 - Person likely to be interested in this application
-

APPLICATION NO.: GL/A-20:225

APPLICANTS: Chris Camilleri, owner

SUBJECT PROPERTY: Municipal address **2035 Fletcher Rd., Glanbrook**

ZONING BY-LAW: Zoning By-law 05-200, as Amended by By-law 15-173

ZONING: "A1" (Agriculture) district

PROPOSAL: To permit the construction of an accessory building comprising 238 square metres located on a residential parcel of land, notwithstanding that;

1. Accessory buildings shall be permitted to be located within the front yard instead of the requirement that no accessory buildings or structures shall be located within a front yard.
2. A maximum gross floor area of 238 square metres and a maximum building height of 7.0 metres shall be permitted for all buildings accessory to the single detached dwelling instead of the requirement that all buildings accessory to a Single Detached Dwelling shall not exceed a maximum gross floor area of 200 square metres and a maximum building height of 6.0 metres.

NOTE:

1. This is a corner lot as defined in Hamilton Zoning By-law No. 05-200. Note that with reference to a corner lot, the front lot line shall mean the shorter of the lot lines abutting the streets. Therefore the lot line abutting Guyatt Road is deemed the front lot line, and the proposed accessory structure/building is located within the front yard, requiring a variance to Section 4.8.2(a) of Hamilton Zoning By-law No. 05-200.
2. The zoning by-law permits a maximum height of 6.0 metres for an accessory building/structure. No elevation drawings were included as part of this application. The applicant shall ensure that the height of the building has been measured from grade as defined in the zoning By-law; otherwise, additional variances may be required.
3. Human habitation is prohibited within the accessory building. Any proposed storage shall be incidental and associated only with the existing single detached dwelling. Any use or storage not associated with the principle use is not permitted.
4. Construction of the proposed accessory building is subject to the issuance of a building permit in the normal manner. Be advised that Ontario Building Code regulations may require specific setbacks and construction types.

This application will be heard by the Committee as shown below:

DATE: Thursday, February 18th, 2021
TIME: 2:10 p.m.
PLACE: Via video link or call in (see attached sheet for details)
To be streamed at www.hamilton.ca/committeeofadjustment
for viewing purposes only

PUBLIC INPUT

Written: If you would like to submit written comments to the Committee of Adjustment you may do so via email or hardcopy. Please see attached page for complete instructions, including deadlines for submitting to be seen by the Committee.

Orally: If you would like to speak to this item at the hearing you may do so via video link or by calling in. Please see attached page for complete instructions, including deadlines for registering to participate.

MORE INFORMATION

For more information on this matter, including access to drawings illustrating this request:

- Visit www.hamilton.ca/committeeofadjustment
- Call 905-546-CITY (2489) or 905-546-2424 extension 4221, 4130, or 3935
- Email Committee of Adjustment staff at cofa@hamilton.ca

DATED: February 2nd, 2021.

Jamila Sheffield,
Secretary-Treasurer
Committee of Adjustment

Information respecting this application is being collected under the authority of the Planning Act, R.S.O., 1990, c. P. 13. All comments and opinions submitted to the City of Hamilton on this matter, including the name, address, and contact information of persons submitting comments and/or opinions, will become part of the public record and will be made available to the Applicant and the general public.

- GENERAL NOTES**
1. THIS/THOSE PLANS IS/ARE NOT TO BE USED FOR CONSTRUCTION UNTIL SEALED BY THE ENGINEER AND INDICATED ISSUED FOR CONSTRUCTION ON THE DRAWING.
 2. THIS/THOSE PLANS IS/ARE NOT TO BE REPRODUCED IN WHOLE OR IN PART WITHOUT THE WRITTEN PERMISSION OF BARICH GRENKIE SURVEYING LIMITED.
 3. INFORMATION REGARDING ANY EXISTING SERVICES AND/OR UTILITIES SHOWN ON THE APPROVED SET OF CONSTRUCTION DRAWINGS ARE FURNISHED AS THE BEST AVAILABLE INFORMATION. THE CONTRACTOR SHALL INTERPRET THIS INFORMATION AS HE SEES FIT WITH THE UNDERSTANDING THAT THE OWNER AND HIS AGENTS DISCLAIM ALL RESPONSIBILITY FOR ITS ACCURACY AND /OR SUFFICIENCY. THE CONTRACTOR SHALL ASSUME LIABILITY FOR ANY DAMAGE TO EXISTING WORKS.
 4. SITE PLAN INFORMATION TAKEN FROM SURVEY BY BARICH GRENKIE SURVEYING LTD., DRAWING NO. 20-2652 DATED JUNE 15, 2020.
 5. THIS/THOSE PLANS IS/ARE TO BE USED FOR SERVING AND GRADING ONLY, FOR BUILDING LOCATION REFER TO THE SITE PLAN.
 6. MUNICIPAL APPROVAL OF THESE DRAWINGS IS FOR MATERIAL AND COMPLIANCE WITH CITY/TOWN STANDARDS AND PROVINCIAL SPECIFICATIONS AND STANDARDS ON APPROVAL AND INSPECTION OF THE WORKS BY THE CITY/TOWN STAFF DOES NOT CERTIFY THE LINE AND GRADE OF THE WORKS NOR RELIEVE THE CONTRACTOR OF CERTIFICATION OF ALL WORKS BY THE OWNER'S ENGINEER.
 7. ALTERNATE MATERIALS MAY BE ACCEPTABLE PROVIDED WRITTEN APPROVAL HAS FIRST BEEN OBTAINED FROM THE CITY OF HAMILTON AND THE ENGINEER.
 8. THE APPROVAL OF THIS PLAN DOES NOT EXEMPT THE OWNER'S BONDED CONTRACTOR FROM THE REQUIREMENTS TO OBTAIN THE VARIOUS PERMITS/APPROVALS NORMALLY REQUIRED TO COMPLETE A CONSTRUCTION PROJECT, SUCH AS, BUT NOT LIMITED TO THE FOLLOWING:
 - ROAD CUT PERMITS
 - SEWER PERMITS
 - APPROACH APPROVAL PERMITS
 - RELOCATION OF SERVICES
 - COMMITTEE OF ADJUSTMENT
 - ENCROACHMENT AGREEMENTS
 9. PRIOR TO CONSTRUCTION THE CONTRACTOR MUST:
 - i. CHECK AND VERIFY ALL DIMENSIONS AND EXISTING ELEVATIONS WHICH INCLUDE BUT ARE NOT LIMITED TO THE BENCHMARK ELEVATIONS, EXISTING SERVICE CONNECTIONS, EXISTING INVERTS AND REPORT FINDING IN WRITING TO THE ENGINEER.
 - ii. OBTAIN ALL UTILITY LOCATES AND REQUIRED PERMITS AND LICENSES.
 - iii. VERIFY ALL FINISHED FLOOR ELEVATIONS AND BASEMENT FLOOR ELEVATIONS WHICH MY APPEAR ON THESE PLANS COMPLY WITH THE FINAL ARCHITECTURAL DRAWINGS.
 - iv. CONFIRM ALL DRAWINGS USED FOR CONSTRUCTION ARE OF THE MOST RECENT REVISION.
 - v. NOTIFY THE ENGINEER OF THE PROPOSED CONSTRUCTION SCHEDULE FOR COORDINATION OF NECESSARY INSPECTIONS.
 10. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE ENGINEER 48 HOURS PRIOR TO THE COMMENCING SITE WORKS TO ARRANGE FOR INSPECTION. THE ENGINEER SHALL DETERMINE THE EXTENT OF INSPECTION AND TESTING REQUIRED FOR CERTIFICATION OF THE UNDERGROUND SERVICE INSTALLATION AS MANDATED BY THE ONTARIO BUILDING CODE DIVISION C, PART 1, SECTION 1.2.2. GENERAL REVIEW FAILURE TO MAKE SUITABLE ARRANGEMENTS FOR INSPECTION WILL LEAD TO POST CONSTRUCTION TESTING AND INSPECTION AS DETERMINED BY THE ENGINEER, THE COSTS OF WHICH INCLUDING ANY DELAYS IN CONSTRUCTION SHALL BE BORNE BY THE CONTRACTOR. FULL PAYMENT FOR UN-INSPECTED WORKS MAY BE WITHHELD UNTIL THE COMPLETION OF THE POST CONSTRUCTION INSPECTION AND TESTING TO THE SATISFACTION OF THE ENGINEER.
 11. INSPECTION BY THE OWNER'S ENGINEER IS FOR CERTIFICATION AND GENERAL COMPLIANCE PURPOSES AND DOES NOT CERTIFY LINE AND GRADE OR IMPLY AN ASSURANCE OF QUALITY CONTROL. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THE INSTALLATION OF THE WORKS TO PROPER LINE, GRADE AND QUALITY TO CURRENT INDUSTRY STANDARDS.
 12. ANY UTILITY RELOCATIONS AND RESTORATIONS DUE TO THE DEVELOPMENT TO BE UNDERTAKEN AT THE EXPENSE OF THE OWNER/DEVELOPER AND SHALL BE COORDINATED BY THE CONTRACTOR.
 13. ALL RESTORATIONS AND RECONSTRUCTIONS SHALL BE COMPLETED TO MATCH EXISTING CONDITIONS OR BETTER AND ARE TO BE PERFORMED TO THE SATISFACTION OF THE ENGINEER AND THE CITY/TOWN STANDARDS.
 14. SERVING CONTRACTOR TO MAINTAIN A "CONFINED TRENCH CONDITION" IN ALL SEWER AND WATERMAIN INSTALLATION TRENCHES.
 15. THE SITE SERVING CONTRACTOR SHALL TERMINATE ALL SERVICES 1.0m FROM THE BUILDING FACE.
 16. NO BLASTING WILL BE PERMITTED.

- GRADING NOTES**
- A. GENERAL GRADING
- A. ALONG ADJOINING PROPERTIES GRADE TO MEET EXISTING OR PROPOSED ELEVATIONS WITH SLOOED SLOPES (MIN. 3% TO 1%) AND/OR RETAINING WALLS AS SPECIFIED.
- B. ALL RETAINING WALLS, DRIVEWAYS, CURBS, ETC. SHALL BE PLACED A MIN. OF 0.45m OFF THE PROPERTY LINE. ALL WALLS 1.0m OR HIGHER SHALL BE DESIGNED BY A P.E.N.G.
- C. SHOULD A RETAINING WALL BE REQUIRED, THE TOP OF WALL ELEVATIONS SHALL BE SET 150mm ABOVE THE PROPOSED SIDE YARD SWALES.
- D. RETAINING WALLS 0.6m IN HEIGHT OR GREATER REQUIRE CONSTRUCTION OF A FENCE OR GUARD RAIL AT THE TOP OF THE REAR OF THE WALL. GUARDS FOR RETAINING WALLS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF EXTERIOR GUARDS AS CONTAINED IN THE ONTARIO BUILDING CODE.
- E. TOP OF FOUNDATION WALLS FOR BUILDINGS SHALL BE 150mm (MIN) ABOVE FINISHED GRADE.
- F. DRIVEWAY SLOPES SHALL NOT BE LESS THAN 2% AND NOT MORE THAN 7.0% REVERSED SLOPED DRIVEWAYS IN NEW DEVELOPMENTS ARE NOT PERMITTED.
- G. IF GRADING IS REQUIRED ON LANDS ADJACENT TO THE DEVELOPMENT WHICH ARE NOT OWNED BY THE DEVELOPER, THEN THE DEVELOPER MUST OBTAIN WRITTEN PERMISSION FROM THE ADJACENT PROPERTY OWNER TO ALLOW THE DEVELOPER TO GRADE ON THE ADJACENT LANDS, OTHERWISE RETAINING WALLS MUST BE USED.
- H. THE WRITTEN PERMISSION REQUIRED FROM THE ADJACENT LANDOWNER SHALL BE OBTAINED PRIOR TO ENTERING THE LANDS. SHOULD PERMISSION NOT BE OBTAINED OR IS WITHDRAWN PRIOR TO COMMENCING THE WORK, THEN THE DEVELOPER SHALL LIMIT HIS ACTIVITIES TO THE LIMITS OF THE DEVELOPMENT SITE.
- I. DRIVEWAY AND DRIVEWAY APPROACHES SHALL BE LOCATED SUCH THAT HYDRO VAULTS AND OTHER STREET FURNITURE ARE A MIN. OF 1.2m FROM THE PROJECTIONS OF THE OUTSIDE GARAGE WALLS.
- J. ANY CHANGES IN GRADES AND CATCH BASINS REQUIRE THE APPROVAL OF THE CITY'S MANAGER OF DEVELOPMENT ENGINEERING.
- K. ALL DRIVEWAYS FROM PROPERTY LINES FOR THE FIRST 7.5m SHALL BE WITHIN 5% MAXIMUM GRADE. THEREAFTER, ALL DRIVEWAYS SHALL BE WITHIN 10% MAXIMUM GRADES.
- L. SLOPES OF SWALES FOR BOTH "BACK TO FRONT" AND "SPLIT" DRAINAGE SHALL BE NO LESS THAN 2.0% GRADE AND NO GREATER THAN 3.0% GRADE (3:1 SLOPE).
- M. WHEN MATCHING TO EXISTING PROPERTIES WHERE A 2.0% GRADE CANNOT BE ACHIEVED, A 1.5% GRADE IS PERMITTED, PROVIDED A 150mm SUBDRAIN IS INSTALLED BELOW THE BOTTOM OF THE SWALE AND DRAINED TO A SUITABLE OUTLET (WITH A MINIMUM 0.3m COVER OVER THE SUBDRAIN) OR OTHER MITIGATION MEASURES.
- N. MINIMUM GRADE FOR WRAP-AROUND SWALE IN BACKYARDS SHALL BE 1.0% UNLESS OTHERWISE NOTED. THE GROUND BETWEEN PROPOSED ELEVATIONS ON SIDE LOTS SHALL BE GRADED IN A STRAIGHT LINE.
- P. GARAGE FLOOR ELEVATIONS TO BE SET 0.3m HIGHER THAN BACK OF WALK, UNLESS OTHERWISE SPECIFIED.
- Q. ALL FILL PLACED ON LOTS SHALL BE COMPACTED TO A MINIMUM 95% S.P.D. (UNLESS OTHERWISE RECOMMENDED BY THE GEOTECHNICAL ENGINEER). ALL MATERIAL SHALL BE PLACED IN LIFTS NOT EXCEEDING 300mm.
2. BACKYARD GRADING
- A. DEFINITION: "REQUIRED BACK YARD" SHALL MEAN THE LESSER OF THE
- B. DISTANCE REGULATED BY THE ZONING BY-LAW OR 6.0m.
- C. THE MAXIMUM 5.0% RESTRICTION SHALL NOT APPLY TO THE SIDES OF A SWALE ALONG THE BACK OF THE LOT, PROVIDED THE TOTAL WIDTH OF THE SWALE DOES NOT EXCEED 1.0m ON EACH LOT.
- D. WHERE THE 5.0% RESTRICTION ON BACKYARD GRADES RESULTS IN ELEVATION DIFFERENCES BETWEEN DIFFERENT PROPERTIES, RETAINING WALLS SHALL BE CONSTRUCTED ALONG THE SIDES AND THE BACK OF THE LOT. 3:1 SLOPES CAN REPLACE THE WALLS WHERE THE DIFFERENCE IN ELEVATION IS LESS THAN 0.3m.
- E. GENERALLY, SLOPES SHALL BE PLACED ON THE LOWER LOT, WHEREAS RETAINING WALLS SHALL BE PLACED ON THE HIGHER LANDS.
- F. THERE IS NO CONTROL ON THE STEEPNESS OF THE SLOPES IN SIDE YARDS, FRONT YARDS, AND BACK YARDS, OUTSIDE THE AREAS DEFINED IN ITEM 1. ABOVE, PROVIDED THE SLOPES ARE STABLE FOR THE SOILS OF THE AREA (3:1 MAXIMUM).

- COMPACTION REQUIREMENTS**
- UNLESS OTHERWISE NOTED OR DIRECTED BY THE GEOTECHNICAL CONSULTANT, THE FOLLOWING SHALL APPLY:
- A. ALL BEDDING AND BACKFILL MATERIAL, ROAD SUB-GRADES AND GENERALLY ALL MATERIAL USED FOR LOT GRADING AND FILL SECTIONS, ETC., SHALL BE COMPACTED TO MIN. 98% S.P.D. ALL MATERIAL SHALL BE PLACED IN LAYERS NOT EXCEEDING 300mm LIFTS.
- B. ALL GRANULAR ROAD BASE MATERIALS SHALL BE COMPACTED TO 98% S.P.D. FOR ALL SEWERS AND WATERMAINS IN FILL SECTIONS, THE COMPACTION SHALL BE CERTIFIED BY A GEOTECHNICAL ENGINEER PRIOR TO LAYING OF PIPE.

- SILTATION AND EROSION CONTROL**
- A. SILTATION CONTROL BARRIERS SHALL BE PLACED AS DETAILLED.
- B. ALL SILTATION CONTROL MEASURES SHALL BE CLEANED AND MAINTAINED AFTER EACH RAINFALL AS DIRECTED AND TO THE SATISFACTION OF THE OF CITY/TOWN AND/OR THE CONSERVATION AUTHORITY.
- C. ADDITIONAL SILT CONTROL LOCATIONS MAY BE REQUIRED AS DETERMINED BY THE ENGINEER, THE CITY/TOWN AND/OR THE CONSERVATION AUTHORITY.

- RESTORATIONS**
- A. ALL TRENCH EXCAVATION WITHIN EXISTING ROAD ALLOWANCES SHALL BE BACKFILLED WITH GRANULAR "A", COMPACTED TO 98% S.P.D. PAVEMENT RESTORATION SHALL BE PERFORMED AS PER RD-100.01 & 100.02.
- B. ALL BOULEVARDS TO BE RESTORED WITH #1 NURSERY SOO. ON 100mm MINIMUM THICK SELECT TOPSOIL.
- C. BEDDING MATERIALS TO BE AS PER FORM 600. COMPACTION TO BE AS PER FORM 900.

- REMOVALS & ABANDONMENTS**
1. COMPLETELY REMOVE ABANDONED SEWER/WATERMAIN WHERE POSSIBLE AND/OR AS DIRECTED BY THE CITY OF HAMILTON. IF MAINS TO BE ABANDONED IN PLACE, FOLLOW OPS# 510.
2. WHERE ABANDONING A WATER SERVICE CONNECTION OR WATERMAIN CONNECTED TO A LIVE MAIN, THE PREFERRED METHOD IS TO REMOVE THE CURB STOP AND STEM, SHUT OFF MAIN, STOP AND DISCONNECT/CUT WATER SERVICE AT MAIN STOP.

NOTE:

1. This property is eligible for weekly collection of garbage, organics, recyclable material, and leaf and waste through the City of Hamilton, subject to compliance with specifications indicated by the Waste Management Division and subject to compliance with the City's Solid Waste Management By-Law 09-067.
2. Construction material is prohibited from collection and arrangements shall be made with a private contractor for its collection and disposal. On collection day the collection area shall be in an accessible location, free and clear of any construction debris and vehicles.
3. Collection of garbage, green cart organics, recycling and leaf and yard waste will take place outside in front of the property.

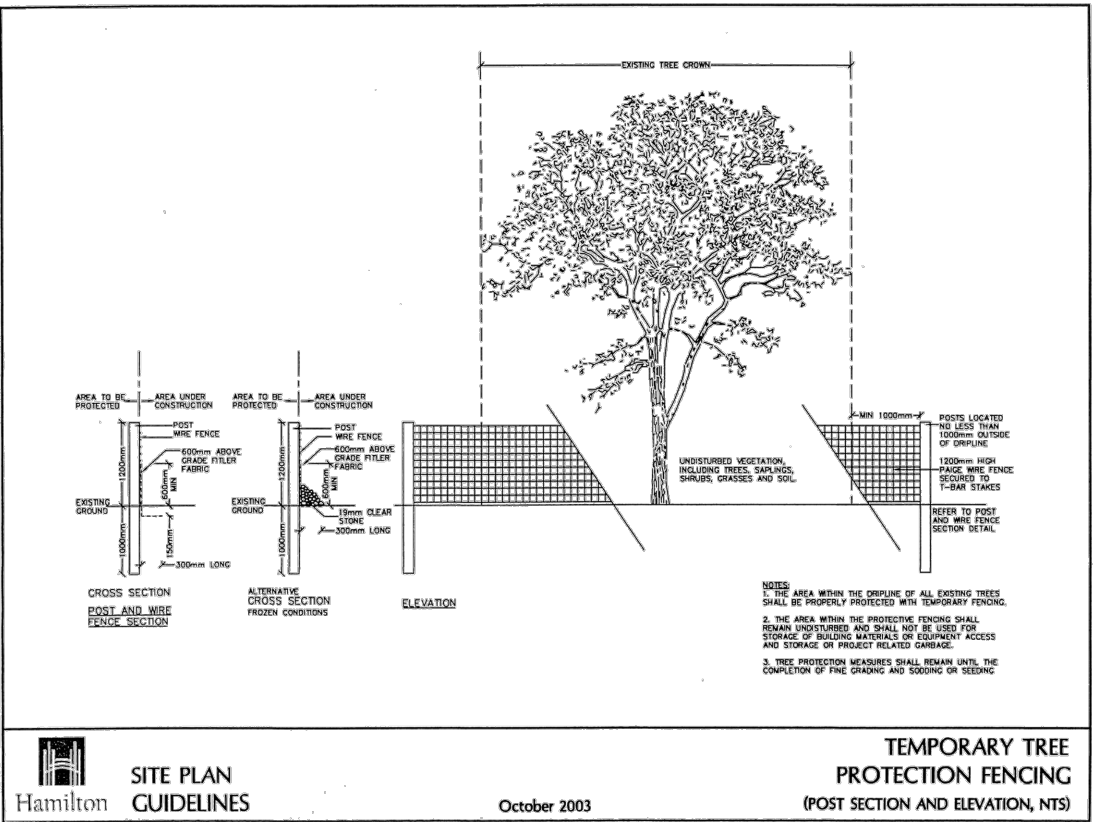
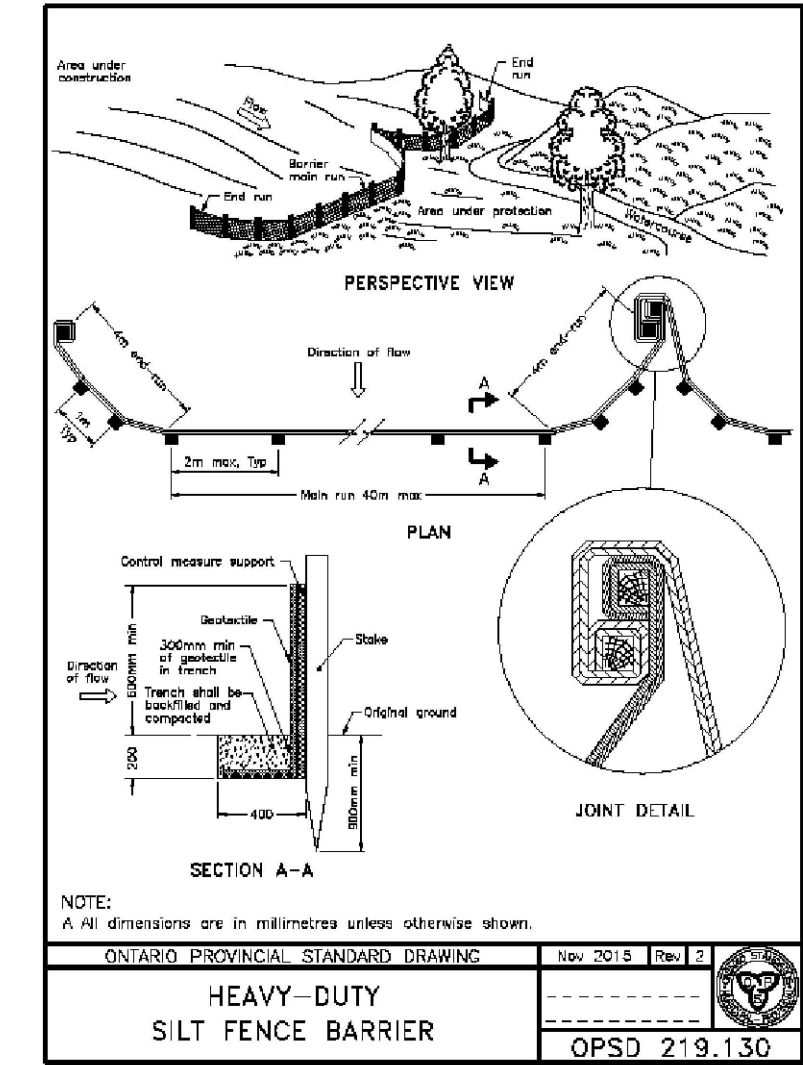


Table 1 - Tree Protection Zones		
Trunk Diameter (DBH) ²	Tree Protection Zone (TPZ) Minimum Protection Distances Required	Critical Root Zone (CRZ) Distances Required ^{3 & 4}
<10 cm	1.8 m	1.8 m
11-40 cm	2.4 m	4.0 m
41-50 cm	3.0 m	5.0 m
51-60 cm	3.6 m	6.0 m
61-70 cm	4.2 m	7.0 m
71-80 cm	4.8 m	8.0 m
81-90 cm	5.4 m	9.0 m
91-100+ cm	6.0 m	10.0 m

1. Tree protection barriers for trees situated on the City road allowance where visibility must be maintained can be 1.2m high and consist of orange plastic web snow fencing on a wood frame of 2" x 4" s, supported on metal "T" bars, 2.0m c/c max. Where orange plastic web snow fencing creates a restriction to sightlines, page wire fencing shall be used.
2. Where some excavate or fill has to be temporarily located near a tree protection barrier plywood must be used to ensure no material enters the Tree Protection Zone.
3. All supports and bracing should be outside the Tree Protection Barrier.
4. No construction activity, grade changes, surface treatment or excavations of any kind is permitted within the Tree Protection Zone.

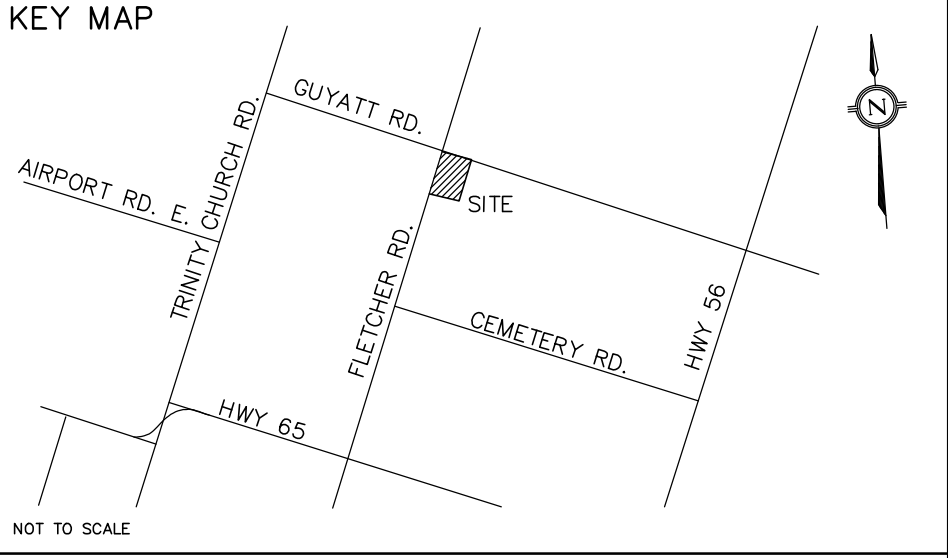
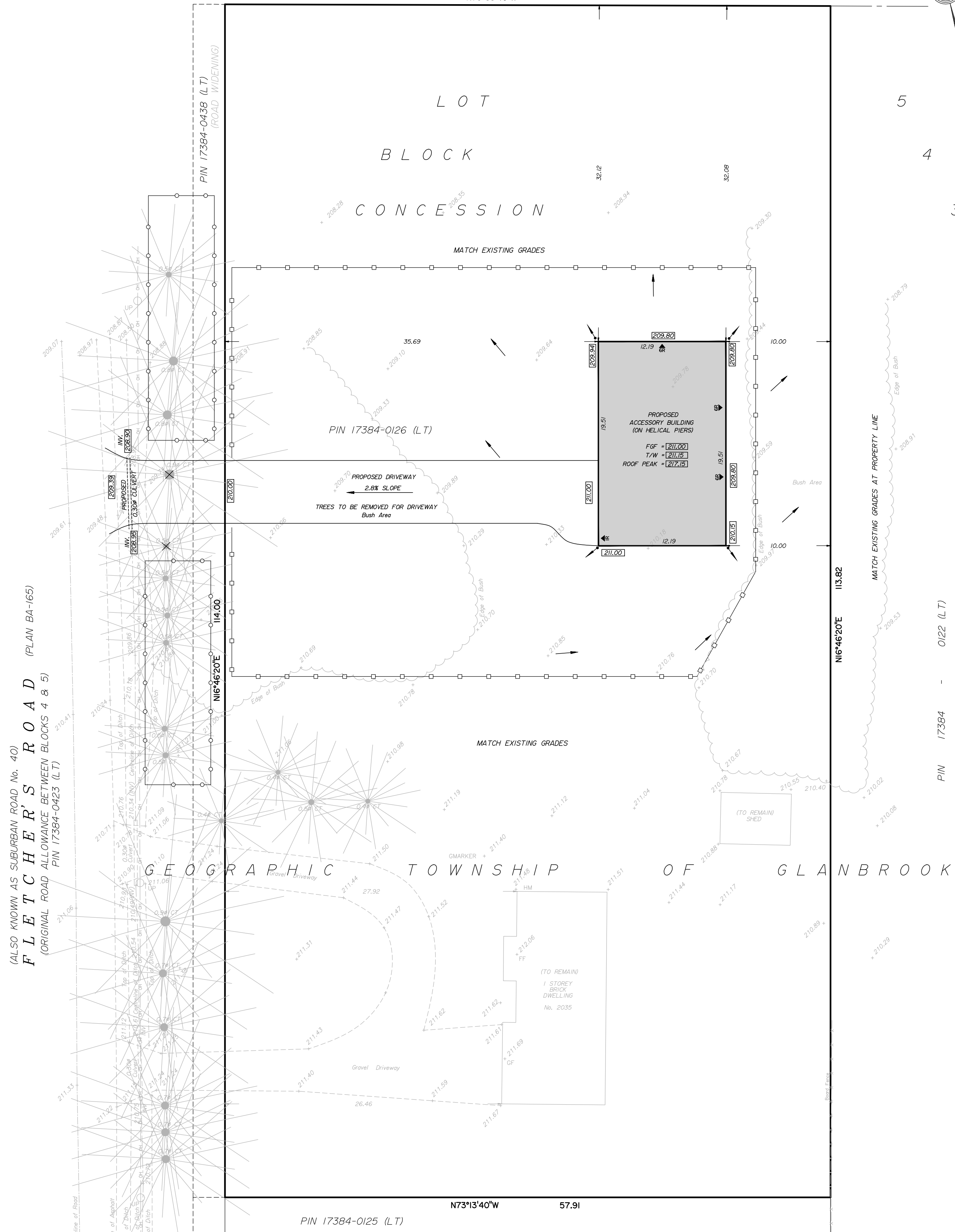
MUNICIPAL ADDRESS 2035 FLETCHER ROAD, BINBROOK		
LEGAL DESCRIPTION PART OF LOT 5, BLOCK 4, CONCESSION 2 GEOGRAPHIC TOWNSHIP OF GLANBROOK CITY OF HAMILTON		
ZONING A1		
ACCESSORY BUILDING		
SITE STATISTICS	BYLAW	PROPOSED
LOT AREA	0.4 ha	0.66 ha (EXISTING)
FRONT YARD SETBACK	10.0 m	35.69 m
REAR YARD SETBACK	1.0 m	10.00 m
SIDE YARD SETBACK	1.0 m	32.08 m
HEIGHT (PEAK OF ROOF)	6.0 m	6.94 m**
LOT WIDTH	30.0 m	114.00 m (EXISTING)
LOT COVERAGE	5.0% (MAX. 200m ²)	3.6% (237.83m ²)**

** MINOR VARIANCE TO BE APPLIED FOR.

AVERAGE GRADE = 211.00+211.00+209.94+209.80+209.80+209.80+210.15 =210.21

AVERAGE GRADE AS PER CITY OF HAMILTON ZONING BY-LAW(HEIGHT DEFINITION)

(ALSO KNOWN AS SUBURBAN ROAD No. 40)
FLETCHERS ROAD (PLAN BA-165)
ORIGINAL ROAD ALLOWANCE BETWEEN BLOCKS 4 & 5)
PIN 17384-0423 (LT)



ADDRESS: 2035 FLETCHER ROAD, BINBROOK

SITE AND GRADING PLAN OF
**PART OF LOT 5, BLOCK 4
CONCESSION 3**
(GEOGRAPHIC TOWNSHIP OF GLANBROOK)
CITY OF HAMILTON
BARICH GRENKIE SURVEYING LTD.
A DIVISION OF GEOMAPLE
© COPYRIGHT 2020

METRIC
DISTANCES AND ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

LEGEND	
DT	DENOTES DECIDUOUS TREE
CT	DENOTES CONIFEROUS TREE
---	DENOTES SUBJECT LANDS BOUNDARY
---	DENOTES DEED LINE
---	DENOTES LOT LINE
---	DENOTES FENCE LINE
X	DENOTES TREE TO BE REMOVED
N-E-S-W	DENOTES NORTH-EAST-SOUTH-WEST
100.00	DENOTES EXISTING ELEVATION
100.00	DENOTES PROPOSED ELEVATION
100.00	DENOTES PROPOSED TREE PROTECTION
100.00	DENOTES PROPOSED SILT FENCE
100.00	DENOTES DOWN SPOUT WITH SPLASH PAD

ELEVATION NOTE
ELEVATIONS ARE REFERRED TO THE CANADIAN GEODETIC VERTICAL DATUM (CGVD-1928/1978) AND ARE DERIVED FROM CITY OF HAMILTON BENCHMARK No. 07720030067 HAVING AN ELEVATION OF 208.231 m.

1	11/20/2020	JMH	AS PER CLIENT REQUEST		
0	09/17/2020	JMH	ISSUED FOR REVIEW		
NO.	DATE	BY	REVISIONS		
DESIGN	JMH	CHK'D	EWA	DATE	
DRAWN	JMH	CHK'D	EWA	NOVEMBER 20, 2020	

Scale 1"=250'

A horizontal graphic scale bar with alternating black and white segments. Below the bar are numerical markers for 0, 5, 10, and 15 METRES.

Scale 1:250

APPROVALS

STAMP	
LICENSED PROFESSIONAL ENGINEER M. FATHI 100086843 NOV. 20, 2020 PROVINCE OF ONTARIO	

Barich Grenkie Surveying Ltd. 297 HWY No. 8 (UNIT 101) - STONEY CREEK, ON (905) 664-6777	
DWN BY: JMH	CHK BY: GRCH
A DIVISION OF GEOMAPLE	
JOB No. 20-2652	

CUSTOMER
REMONDA & CHRIS CAMILLERI

PROJECT NAME

**PROPOSED ACCESSORY BUILDING
2035 FLETCHER ROAD, BINBROOK**

TITLE
SITE AND GRADING PLAN

PROJECT No.	20-2652	DRAWING No.	20-2652
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GENERAL FLOOR PLAN SPECIFICATIONS:

WINDOWS & EXT. DOORS NOTE:

BUILDER/HOMEOCNER TO VERIFY ALL WINDOW & EXTERIOR DOOR STYLES/SIZES PRIOR TO ORDERING. FRAMING CONTRACTOR TO BE SUPPLIED WINDOW SUPPLIER RSO (ROUGH STUD OPENINGS) PRIOR TO FRAMING.

UNLESS OTHERWISE NOTED, TOP OF ALL WINDOWS TO BE FRAMED AT 6'-10" MAX.

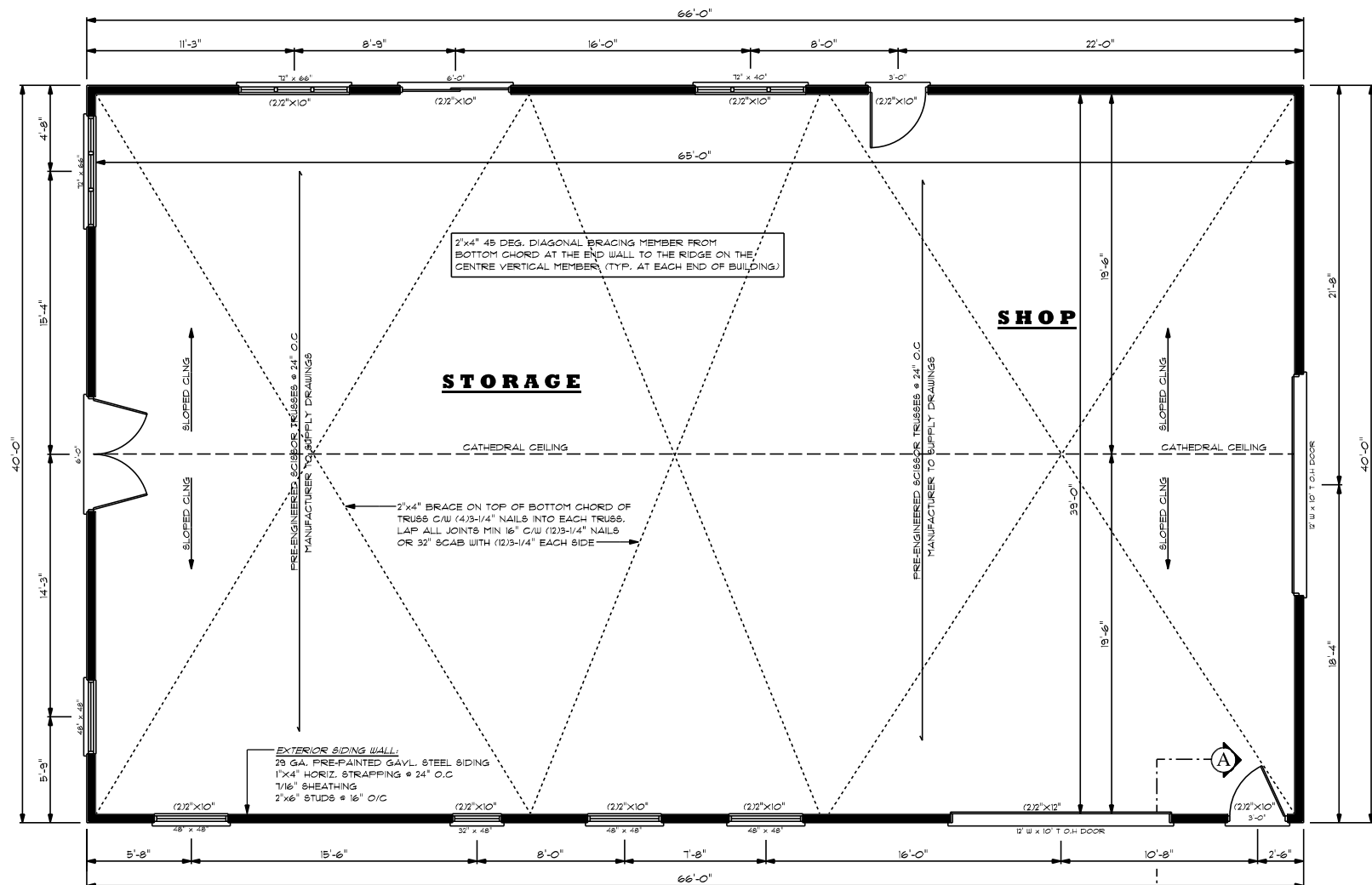
PRE-ENGINEERED TRUSSES:

TRUSS LAYOUT TO BE DESIGNED/ENGINEERED BY TRUSS MANUFACTURER. MANUFACTURER TO SUBMIT SEALED STAMPED ENGINEERED SHOP DRAWINGS TO CONTRACTOR FOR APPROVAL BY BUILDING DEPARTMENT PRIOR TO CONSTRUCTION. IF GIRDER TRUSS LOCATIONS ARE SHOWN ON THESE DRAWINGS, THEY ARE BASED ON ANTICIPATED PLACEMENT DESIGNED BY TRUSS MANUFACTURER.

END BEARINGS:

ALL WOOD & STEEL BEAMS SHALL HAVE EVEN & LEVEL BEARING OF NOT LESS THAN 3-1/2" OF AT END OF SUPPORTS AS PER (S.23.8.1.)

ALL WOOD LINTELS WITH SPANS LESS THAN 9'-8" REQUIRE MIN. 1/2" BEARINGS AT EACH END, EXCEPT WHERE SPANS ARE GREATER THEN 9'-10" MIN. BEARING SHALL BE 3" (S.23.12.3)

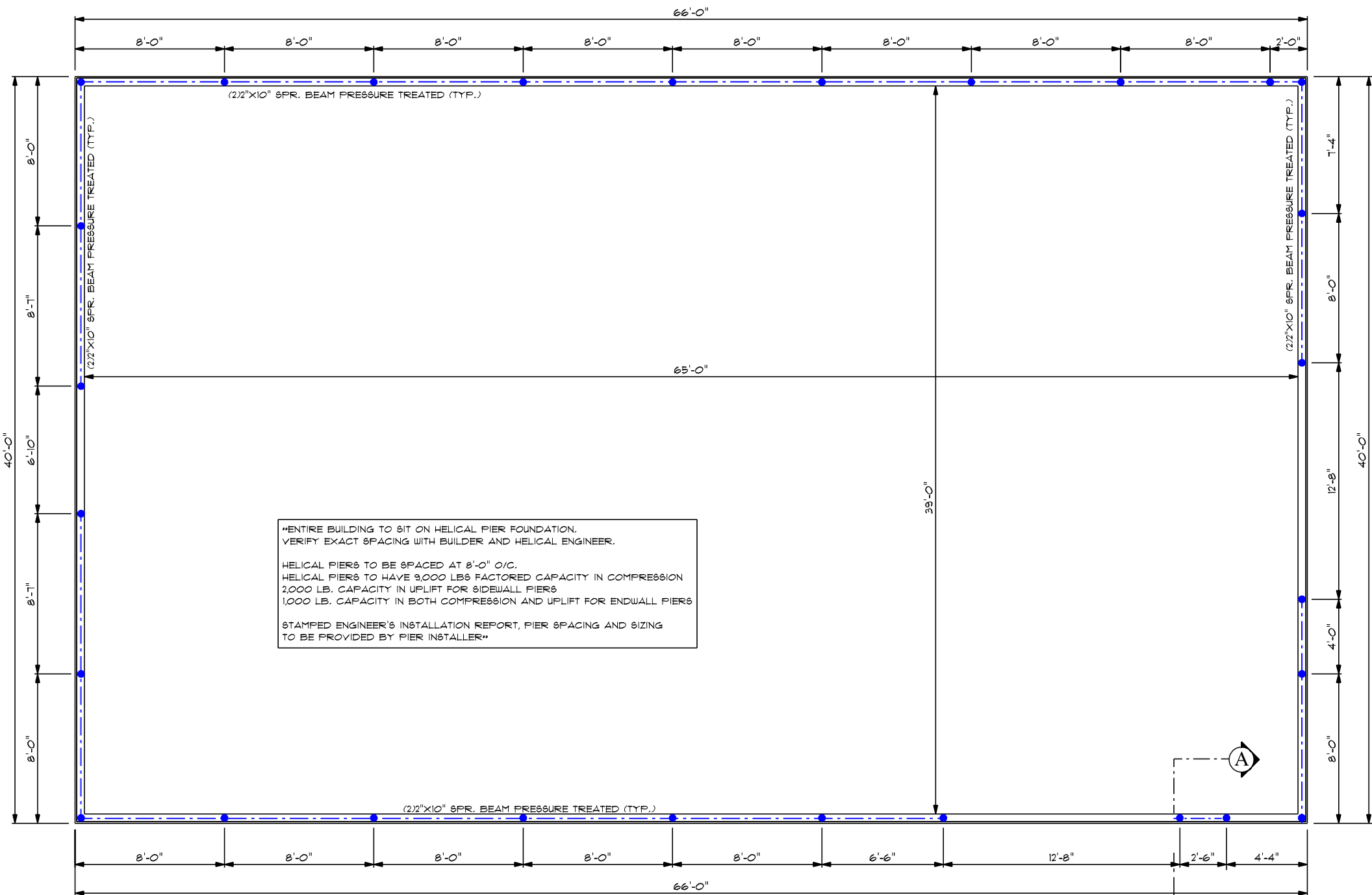


MAIN FLOOR PLAN

SCALE: 1/4" = 1'0"



ENTIRE BUILDING TO SIT ON HELICAL PIER FOUNDATION. HELICAL PIERS TO BE SPACED AT 8'-0" O.C. STAMPED ENGINEER'S INSTALLATION REPORT AND SIZING TO BE PROVIDED BY PIER INSTALLER.



PIER PLAN

SCALE: 1/4" = 1'0"





Hamilton

Planning and Economic Development Department
Planning Division

Committee of Adjustment
City Hall
5th floor 71 Main Street West
Hamilton, Ontario L8P 4Y5

Phone (905) 546-2424 ext.4221
Fax (905) 546-4202

**PLEASE FILL OUT THE FOLLOWING PAGES AND
RETURN TO THE CITY OF HAMILTON PLANNING
DEPARTMENT.**

FOR OFFICE USE ONLY.

APPLICATION NO. _____ DATE APPLICATION RECEIVED _____

PAID _____ DATE APPLICATION DEEMED COMPLETE _____

SECRETARY'S
SIGNATURE _____

**CITY OF HAMILTON
COMMITTEE OF ADJUSTMENT
HAMILTON, ONTARIO**

The Planning Act

Application for Minor Variance or for Permission

The undersigned hereby applies to the Committee of Adjustment for the City of Hamilton under Section 45 of the *Planning Act*, R.S.O. 1990, Chapter P.13 for relief, as described in this application, from the Zoning By-law.

1. Name of Owner Chris Camilleri Telephone No. _____
FAX NO. _____ E-mail address. _____
2. Address _____

Postal Code _____
3. Name of Agent _____ Telephone No. _____
FAX NO. _____ E-mail address. _____
4. Address _____

Postal Code _____

Note: Unless otherwise requested all communications will be sent to the agent, if any.

5. Names and addresses of any mortgagees, holders of charges or other encumbrances:

Postal Code _____

Postal Code _____

6. Nature and extent of relief applied for:

POLE BARN

7. Why it is not possible to comply with the provisions of the By-law?

OVER THE LIMIT ALLOWED

8. Legal description of subject lands (registered plan number and lot number or other legal description and where applicable, street and street number):

9. PREVIOUS USE OF PROPERTY

Residential ☒ Industrial ☐ Commercial ☐

Agricultural ☐ Vacant ☐

Other ☐

- 9.1 If Industrial or Commercial, specify use

- 9.2 Has the grading of the subject land been changed by adding earth or other material, i.e. has filling occurred?

Yes ☐ No ☐ Unknown ☐

- 9.3 Has a gas station been located on the subject land or adjacent lands at any time?

Yes ☐ No ☐ Unknown ☐

- 9.4 Has there been petroleum or other fuel stored on the subject land or adjacent lands?

Yes ☐ No ☐ Unknown ☐

- 9.5 Are there or have there ever been underground storage tanks or buried waste on the subject land or adjacent lands?

Yes ☐ No ☐ Unknown ☐

- 9.6 Have the lands or adjacent lands ever been used as an agricultural operation where cyanide products may have been used as pesticides and/or sewage sludge was applied to the lands?

Yes ☐ No ☐ Unknown ☐

- 9.7 Have the lands or adjacent lands ever been used as a weapon firing range?

Yes ☐ No ☐ Unknown ☐

- 9.8 Is the nearest boundary line of the application within 500 metres (1,640 feet) of the fill area of an operational/non-operational landfill or dump?

Yes ☐ No ☐ Unknown ☐

- 9.9 If there are existing or previously existing buildings, are there any building materials remaining on site which are potentially hazardous to public health (eg. asbestos, PCB's)?

Yes ☐ No ☐ Unknown ☐

- ### ACKNOWLEDGEMENT CLAUSE

Date Oct 1/2020

Signature Property Owner

Chris Lammert
Print Name of Owner

- Page 3

13. Date of acquisition of subject lands:

14. Date of construction of all buildings and structures on subject lands:

AS SOON AS PERMIT?

15. Existing uses of the subject property:

OPEN LAND

16. Existing uses of abutting properties:

17. Length of time the existing uses of the subject property have continued:

18. Municipal services available: (check the appropriate space or spaces)

Water _____ Connected _____

Sanitary Sewer _____ Connected _____

Storm Sewers _____

19. Present Official Plan/Secondary Plan provisions applying to the land:

20. Present Restricted Area By-law (Zoning By-law) provisions applying to the land:

21. Has the owner previously applied for relief in respect of the subject property?

Yes

No

If the answer is yes, describe briefly.

22. Is the subject property the subject of a current application for consent under Section 53 of the *Planning Act*?

Yes

No

23. The applicant shall attach to each copy of this application a plan showing the dimensions of the subject lands and of all abutting lands and showing the location, size and type of all buildings and structures on the subject and abutting lands, and where required by the Committee of Adjustment such plan shall be signed by an Ontario Land Surveyor.

NOTE: It is required that two copies of this application be filed with the secretary-treasurer of the Committee of Adjustment together with the maps referred to in Section 5 and be accompanied by the appropriate fee in cash or by cheque made payable to the City of Hamilton.