Authority: Item 7, Planning Committee Report 22-012 (PED22154) CM: August 12, 2022 Ward: City Wide Bill No. 197

CITY OF HAMILTON

BY-LAW NO. 22-

To Amend City of Hamilton Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Zone and Low Density Residential – Small Lot (R1a) Zone

WHEREAS Council approved Item 7 of Report 22-012 of the Planning Committee, at its meeting held on the 3rd day of August, 2022;

AND WHEREAS this By-law is in conformity with the Urban Hamilton Official Plan upon final approval of Official Plan Amendment No. 167;

NOW THEREFORE the Council of the City of Hamilton amends Zoning By-law No. 05-200 as follows:

1. That the Table of Contents be amended by adding a new section as follows:

"SECTION 15: RESIDENTIAL

| 15.1 Low Density Residential (R1) Zone | 15.1-1 |
|---|---------|
| 15.2 Low Density Residential – Small Lot (R1a) Zone | 15.2-1" |

- 2. That Section 1.12 Transitional provision be amended to include the following new subsection:
 - "c) Within the R1 and R1a Zone, a building permit may be issued to permit the erection of a building or structure in accordance with any minor variance, site specific zoning, site plan, consent, plan of subdivision or plan of condominium that has been approved or conditionally approved by the City of Hamilton or the Ontario Land Tribunal as it read on the day before By-law No. 22-197 was passed by Council, provided the Building Permit application complies with the Zoning By-law that affected the lot before By-law 22-197 came into effect. For the purposes of determine zoning conformity the following shall apply:

- i) This By-law is deemed to be modified to the extent necessary to permit a building or structure that is erected in accordance with Subsection c) above.
- ii) Once the permit or approval under Subsection c) above, has been granted, the provisions of this By-law apply in all other respects to the land in question."
- 3. That Section 2.1 Establishment and Classifications and Zones be amended to include the following new subsection:
 - "j) Residential Zones

| Low Density Residential | R1 |
|-------------------------------------|------|
| Low Density Residential – Small Lot | R1a" |

- 4. That Section 3: Definitions, be amended as follows:
 - i) By deleting and replacing the definition for Residential Care Facility as follows:

| "Residential Care Facility | wit for | all mean a group living arrangement hin a fully detached residential building supervised residents where supports d/or services are provided on site, and ay: |
|----------------------------|------------|---|
| | a) | House residents referred to the facility by a hospital, court, or government agency; and, |
| | | |

 b) Be a facility licensed, funded, and approved by a contract or agreement with the Federal, Provincial or Municipal Governments.

A residential care facility shall not include an emergency shelter, lodging house, corrections residence, or correctional facility."

ii) By adding the following definition:

"Converted Dwelling Shall mean a dwelling altered to contain a greater number of dwelling units"

- 5. That Section 4: General Provisions, be amended as follows:
 - i) By adding a new subsection to Section 4.12 Vacuum Clause as follows:
 - "j) Residential Zones
 - i) Notwithstanding any other provisions of this By-law, any lot within a Residential Zone and the location thereon of any building or structure existing on the effective date of this Bylaw, shall be deemed to comply with the regulations of this By-law and are permitted by this By-law."
 - ii) By adding a new subsection to Section 4.23 Special Setbacks as follows:

"e) Setback from the Lincoln Alexander Parkway

- i) Notwithstanding any other provisions of this By-law, all structures shall be set back a minimum 15.24 metres from the Lincoln Alexander Parkway Right-of-Way.
- Notwithstanding Section 4.23 e) i), all residential buildings shall be set back a minimum 22.86 metres from the Lincoln Alexander Parkway Right-of-Way, excluding access ramps."
- iii) By modifying Section 4.33 Additional Dwelling Unit and Additional Dwelling Unit Detached as follows:
 - i) By amending Subsection 4.33 (a) i) by adding "R1, R1a" after "TOC3" and before "Zones".
 - ii) By adding a new clause to Subsection 4.33 (a) i) as follows:
 - "(A) Notwithstanding Section 4.33 (a) i), one parking space shall be required for a Secondary Dwelling Unit - Detached if it constitutes the fourth Dwelling Unit on a lot."
 - iii) By deleting Subsection 4.33 (a) iii) in its entirety.
 - iv) By amending Subsection 4.33.1 (a) by adding "R1, R1a" after "SI" and before "Zone".
 - v) By amending Subsection 4.33.2 (a) by adding "R1, R1a" after "TOC3" and before "Zone".

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iv) By adding a new section as follows:

"4.34 CONVERTED DWELLINGS

- (a) For the purpose of Section 4.34, a Converted Dwelling shall mean a Single Detached Dwelling or Duplex Dwelling, existing as of XX, 2022, converted to contain greater than two but no more than four Dwelling Units.
- (b) A Converted Dwelling shall be permitted on a lot in an "R1" or "R1a" Zone.
- (c) All the regulations of this By-law applicable to the existing dwelling shall continue to apply unless specifically provided in Section 4.34.
- Notwithstanding any applicable regulations of this By-law, no more than four Dwelling Units shall be permitted on a lot subject to Converted Dwelling permissions as identified in Section 4.34 (b).
- (e) No parking spaces are required for Dwelling Units within a Converted Dwelling, provided the required parking spaces which existed on XX, 2022 for the existing dwelling shall continue to be provided and maintained.
 - (i) Notwithstanding Section 4.34 (e), one parking space is required for the following conditions:
 - A. For the fourth Dwelling Unit in a Converted Dwelling, and,
 - B. For the fourth Dwelling Unit on a lot.
- v) By adding a new section as follows:

"4.35 LANDSCAPE REQUIREMENTS

On lots containing a single detached dwelling, semi-detached dwelling, duplex dwelling, or street townhouse dwelling in all zones, the following shall be provided:

a) A minimum 50% landscaped area in the Front Yard, and;

- b) A minimum 50% landscaped area in the Flankage Yard."
- 6. That Section 5: Parking, be amended as follows:
 - i) By modifying Section 5.1 b) iii) by adding the words "Unless otherwise regulated in this By-law" at the beginning of the regulation so that it reads:

"Unless otherwise regulated in this By-law, a driveway shall be located so as to lead directly from a street or laneway to a required parking space, either within a garage or outside, with such driveway having a minimum width of 3.0 metres;"

- ii) By adding the following to Section 5.1 b):
 - "x) For a dwelling with an attached garage, the finished floor level of the garage shall be a minimum 0.3 metres above grade."
- iii) By modifying Section 5.1 c) ii) by adding the words "Unless otherwise regulated in this By-law" at the beginning of the regulation so that it reads:

"Unless otherwise regulated in this By-law, a driveway shall be located so as to lead directly from a street or lane to a required parking space, either within a garage or outside, and shall have a minimum width of 2.7 metres;"

- iv) By adding the following to Section 5.1 c):
 - "vi) For a dwelling with an attached garage, the finished floor level of the garage shall be a minimum 0.3 metres above grade."
- v) By modifying Section 5.2 a) to restructure and modify the regulation so that it reads as follows:
 - "a) Where a parking lot is situated on a lot which abuts a Residential Zone, Settlement Residential (S1) Zone, Downtown (D5) Zone, or Downtown (D6) Zone, the following shall be provided and maintained along that portion of the lot line that abuts a Residential Zone, Settlement Residential (S1) Zone, Downtown (D5) Zone, or Downtown (D6) Zone:
 - A minimum 1.5 metre wide landscape strip which shall contain a visual barrier in accordance with Section 4.19 of this By-law."

- vi) By modifying Section 5.2 c) to restructure the regulation and introduce a new regulation as clause iii), so that it reads:
 - "c) All required parking shall be provided as follows:
 - i) With adequate means of ingress and egress to and from the street or laneway without the necessity of moving any other motor vehicle;
 - ii) Shall be arranged so as to not interfere with normal public use of the street or laneway;
 - iii) Shall be accessed by means of an access driveway:
 - 1. Located on the lot; or,
 - 2. Located party on the lot in the case of a mutual driveway; or,
 - 3. By means of a Right-of-Way.
 - iv) Any parking lot shall provide for ingress and egress of vehicles to and front a street in a forward motion only."
- 7. That the following section be added to the new Section 15: Residential:

"15.1 LOW DENSITY RESIDENTIAL (R1) ZONE

Explanatory Note: The R1 Zone applies to low density residential areas. It represents the predominant residential lot size in the City of Hamilton. The intent of the R1 Zone is to permit a range of low density residential housing types to meet the needs of the residents of the city.

No person shall erect, or use any building in whole or in part, or use any land in whole or in part, within a Low Density Residential (R1) Zone for any purpose other than one or more of the following uses or uses accessory thereto. Such erection or use shall also comply with the prescribed regulations:

15.1.1 PERMITTED USES

Community Garden Day Nursery Duplex Dwelling Lodging House Residential Care Facility Retirement Home Semi-Detached Dwelling Single Detached Dwelling Street Townhouse Dwelling Urban Farm

15.1.1.1 RESTRICTED USES

In addition to Section 15.1.1, the following uses shall be permitted in accordance with the following restrictions:

- i) Residential Care Facility:
 - 1. Maximum capacity of six residents.
- ii) Retirement Home:
 - 1. Maximum capacity of six residents.

15.1.2 REGULATIONS

15.1.2.1 SINGLE DETACHED, DUPLEX DWELLING, AND DAY NURSERY REGULATIONS

- a) Minimum Lot Area 360.0 square metres;
- b) Minimum Lot Width 12.0 metres;
- c) Minimum Setback from the 6.0 metres; Front Lot Line
- d) Minimum Setback from a 1.2 metres; Side Lot Line
- e) Minimum Setback from a 3.0 metres; Flankage Lot Line
- f) Minimum Setback from the 7.5 metres; Rear Lot Line
- g) Maximum Building Height 10.5 metres;
- h) Parking i) In accordance with the requirements of Section 5 of this By-law.

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- ii) Notwithstanding Section 5.6 c) i., 2 parking spaces shall be required for a Single Detached Dwelling.
- i) Accessory Buildings In accordance with the requirements of Section 4.8 of this By-law.
- j) Home Business In accordance with the requirements of Section 4.21 of this By-law.

15.1.2.2 SEMI-DETACHED DWELLING REGULATIONS

- a) Minimum Lot Area for each 270.0 square metres; Dwelling Unit
 b) Minimum Lot Width for each 0.0 metros;
- b) Minimum Lot Width for each 9.0 metres; Dwelling Unit
- c) Minimum Setback from the 6.0 metres; Front Lot Line
- d) Minimum Setback from a Side Lot Line
 1.2 metres, except for the side yard related to the common wall of the semi-detached dwelling unit, in which case a minimum 0 metre side yard shall be

permitted.

- e) Minimum Setback from a 3.0 metres; Flankage Lot Line
- f) Minimum Setback from the 7.5 metres; Rear Lot Line

h) Parking

- g) Maximum Building Height 10.5 metres;
 - In accordance with the requirements of Section 5 of this By-law.
- i) Accessory Buildings In accordance with the requirements of Section 4.8 of this By-law.
- j) Home Business In accordance with the requirements of Section 4.21 of this By-law.

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15.1.2.3 STREET TOWNHOUSE DWELLING REGULATIONS

- a) Minimum Lot Area for each 180.0 square metres for each dwelling Dwelling Unit unit.
- b) Minimum Unit Width for each 6.0 metres; Dwelling Unit
- c) Minimum Setback from the 6.0 metres; Front Lot Line
- d) Minimum Setback from a Side Lot Line
 1.2 metres except for the side yard related to the common wall of the street townhouse dwelling unit, in which case a minimum 0 metre side yard shall be

permitted.

In accordance with the requirements of

Section 5 of this By-law.

Section 4.21 of this By-law.

- e) Minimum Setback from a 3.0 metres; Flankage Lot Line
- f) Minimum Setback from the 7.5 metres; Rear Lot Line
- g) Maximum Building Height 10.5 metres;
- h) Parking
- i) Accessory Buildings In accordance with the requirements of Section 4.8 of this By-law.
- j) Home Business In accordance with the requirements of
- 15.1.2.4 RESIDENTIAL CARE FACILITY AND RETIREMENT HOME REGULATIONS
 - a) Minimum Lot Area 360.0 square metres;
 - b) Minimum Lot Width 12.0 metres;

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| - | Minimum Oathaali fuana tha | |
|----------|---|--|
| c) | Minimum Setback from the Street Line | 6.0 metres; |
| d) | Minimum Setback from a Side Lot Line | 1.2 metres; |
| e) | Minimum Setback from a Flankage Lot Line | 3.0 metres; |
| f) | Minimum Setback from the Rear Lot Line | 7.5 metres; |
| g) | Maximum Building Height | 10.5 metres; |
| h) | Parking | In accordance with the requirements of Section 5 of this By-law. |
| i) | Accessory Buildings | In accordance with the requirements of Section 4.8 of this By-law. |
| 15.1.2.5 | URBAN FARM | In accordance with the requirements of Section 4.26 |
| 15.1.2.6 | COMMUNITY GARDEN REGULATIONS | In accordance with the requirements of Section 4.27 of this By-law. |
| 15.1.2.7 | ADDITIONAL DWELLING UNIT REGULATIONS | In accordance with the requirements of Section 4.33 of this By-law. |
| 15.1.2.8 | CONVERTED DWELLINGS | In accordance with the requirements of Section 4.34 of this By-law." |

8. That the following section be added to the new Section 15: Residential:

"15.2 LOW DENSITY RESIDENTIAL – SMALL LOT (R1a) ZONE

Explanatory Note: The R1a Zone applies to low density residential areas where lots are typically smaller than those in the R1 Zone. The R1a Zone is prevalent in older neighbourhoods across the Lower City. The intent of the R1a Zone is to permit a range of low density residential housing types to meet the needs of the residents of the city.

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No person shall erect, or use any building in whole or in part, or use any land in whole or in part, within a Low Density Residential – Small Lot (R1a) Zone for any purpose other than one or more of the following uses or uses accessory thereto. Such erection or use shall also comply with the prescribed regulations:

| 15.2.1 | PERMITTED USES | Community Garden Day Nursery Duplex Dwelling Lodging House Residential Care Facility Retirement Home Semi-Detached Dwelling Single Detached Dwelling Street Townhouse Dwelling Urban Farm |
|----------|-----------------|--|
| 15.2.1.1 | RESTRICTED USES | In addition to Section 15.2.1, the following uses shall be permitted in accordance with the following restrictions: |

- i) Residential Care Facility:
 - 1. Maximum capacity of six residents.
- ii) Retirement Home:
 - 1. Maximum capacity of six residents.

15.2.2 REGULATIONS

- 15.2.2.1 SINGLE DETACHED, DUPLEX DWELLING, AND DAY NURSERY REGULATIONS
 - a) Minimum Lot Area 270.0 square metres;
 - b) Minimum Lot Width 9.0 metres;
 - c) Minimum Setback from the 3.0 metres; Front Lot Line

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| d) | Minimum Setback from a Side Lot Line | 1.2 metres; | |
|----------|---|--------------|--|
| e) | Minimum Setback from a Flankage Lot Line | 3.0 metres; | |
| f) | Minimum Setback from the Rear Lot Line | 7.5 metres; | |
| g) | Maximum Building Height | 10.5 metres; | |
| h) | Parking | i) | In accordance with the requirements of Section 5 of this By-law. |
| | | ii) | Notwithstanding Section 5.6 c) i., 2 parking spaces shall be required for Single Detached Dwellings. |
| i) | Accessory Buildings | | ccordance with the requirements of tion 4.8 of this By-law. |
| j) | Home Business | | ccordance with the requirements of tion 4.21 of this By-law. |
| 15.2.2.2 | SEMI-DETACHED DWELLING REGULATIONS | | |
| a) | Minimum Lot Area for each Dwelling Unit | 225. | 0 square metres; |
| b) | Minimum Lot Width for each Dwelling Unit | 7.5 ı | netres; |
| c) | Minimum Setback from the Front Lot Line | 3.0 ı | netres; |
| | | | |

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requirements of

| e) | Minimum Setback from a Flankage Lot Line | 3.0 metres; |
|----|---|--|
| f) | Minimum Setback from the Rear Lot Line | 7.5 metres; |
| g) | Maximum Building Height | 10.5 metres; |
| h) | Parking | In accordance with the Section 5 of this By-law. |

- i) Accessory Buildings In accordance with the requirements of Section 4.8 of this By-law.
- j) Home Business In accordance with the requirements of Section 4.21 of this By-law.

15.2.2.3 STREET TOWNHOUSE DWELLING REGULATIONS

- a) Minimum Lot Area for each 180.0 square metres for each dwelling Dwelling Unit unit.
- b) Minimum Unit Width for 6.0 metres; each Dwelling Unit
- c) Minimum Setback from the 3.0 metres; Front Lot Line
- d) Minimum Setback from a Side Lot Line
 1.2 metres except for the side yard related to the common wall of the street townhouse dwelling unit, in which case a minimum 0 metre side yard shall be permitted.
- e) Minimum Setback from a 3.0 metres; Flankage Lot Line
- f) Minimum Setback from the 7.5 metres; Rear Lot Line
- g) Maximum Building Height 10.5 metres;

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| h) | Parking | In accordance with the requirements of Section 5 of this By-law. |
|----|---------------------|--|
| i) | Accessory Buildings | In accordance with the requirements of Section 4.8 of this By-law. |

j) Home Business In accordance with the requirements of Section 4.21 of this By-law.

15.2.2.4 RESIDENTIAL CARE FACILITY AND RETIREMENT HOME REGULATIONS

- a) Minimum Lot Area 270.0 square metres;
- b) Minimum Lot Width 9.0 metres;
- c) Minimum Setback from the 3.0 metres; Front Lot Line
- d) Minimum Setback from a 1.2 metres; Side Lot Line
- e) Minimum Setback from a 3.0 metres; Flankage Lot Line
- f) Minimum Setback from the 7.5 metres Rear Lot Line
- g) Maximum Building Height 10.5 metres
- h) Parking In accordance with the requirements of Section 5 of this By-law.
- i) Accessory Buildings In accordance with the requirements of Section 4.8 of this By-law.
- 15.1.2.5 URBAN FARM In accordance with the requirements of Section 4.26
 15.2.2.6 COMMUNITY GARDEN REGULATIONS In accordance with the requirements of Section 4.27 of this By-law.

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15.2.2.7 ADDITIONAL DWELLING UNIT AND ADDITIONAL DWELLING UNIT – DETACHED

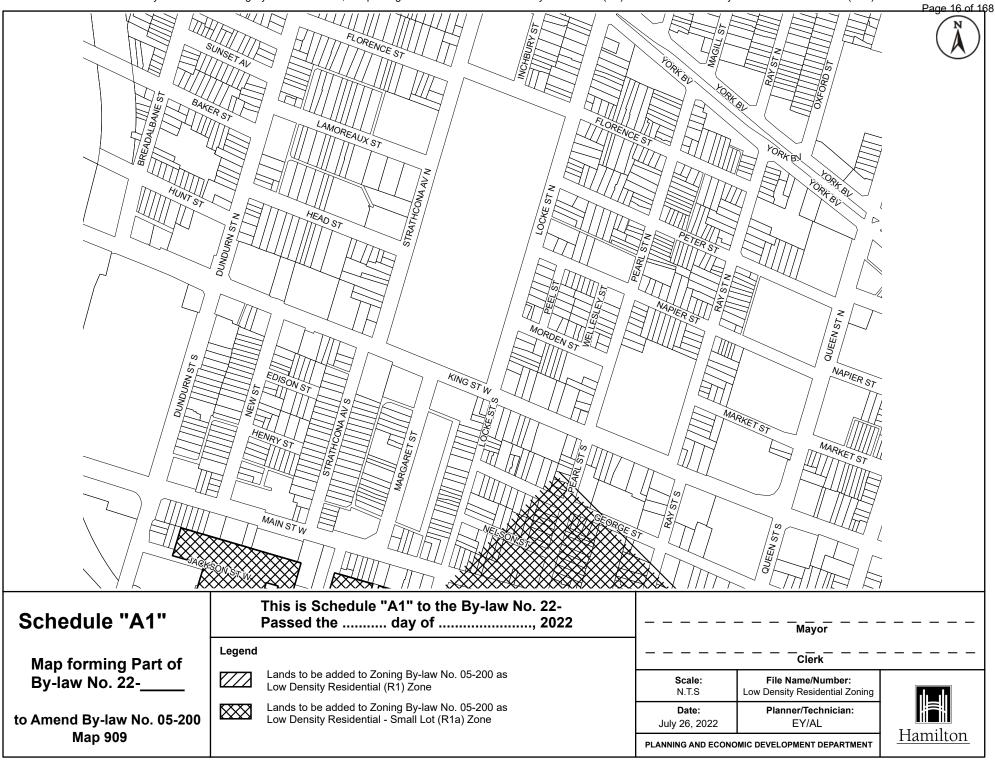
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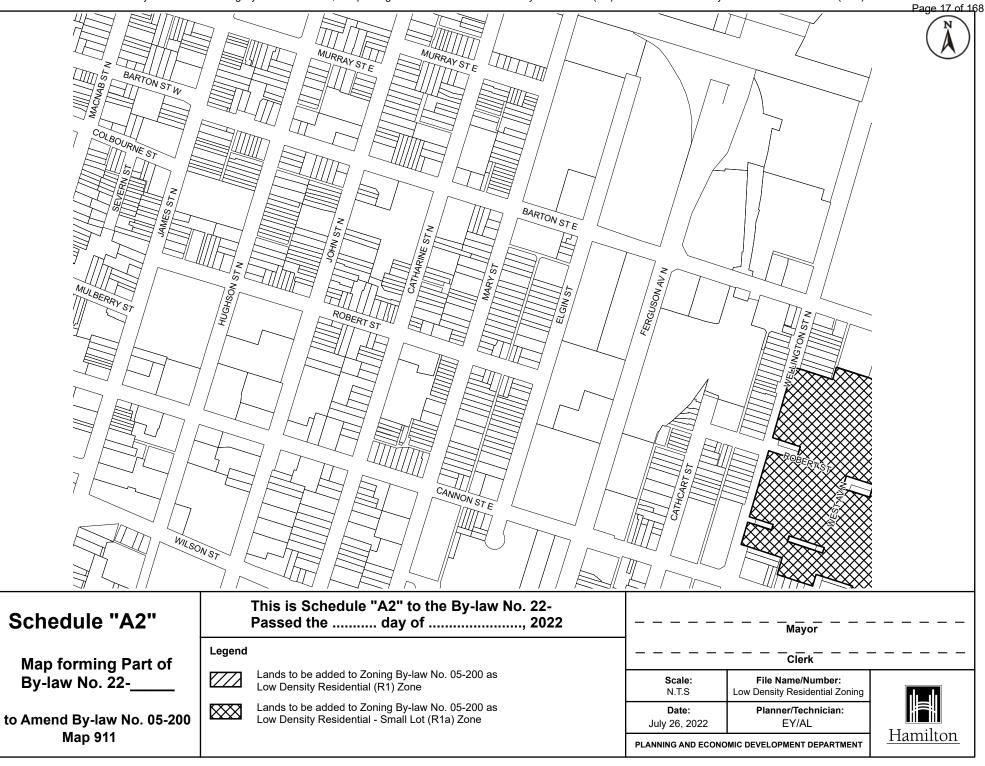
- **15.2.2.8CONVERTED**
DWELLINGSIn accordance with the requirements of
Section 4.34 of this By-law."
- That Maps 909, 911-913, 950-951, 953-958, 960-961, 992-1003, 1033-1035, 1037-1047, 1050-1051, 1079-1093, 1128-1141, 1144-1145, 1179-1193, 1195-1196, 1233-1247, 1286-1300, 1339-1348, 1350-1353, 1391-1400, 1402, 1445-1450, 1453, 1496-1499 of Schedule "A" - Zoning Maps, of Zoning By-law No. 05-200, be amended the extent and boundaries of which are shown on a plan hereto annexed as Schedule "A1 - A156" to this By-law.
- 10. That the Clerk is hereby authorized and directed to proceed with the giving of notice of the passing of this By-law in accordance with the *Planning Act*.
- 11. That this By-law shall not come into force and effect until such time as Official Plan Amendment No. 167 to the Urban Hamilton Official Plan is in full force and effect.
- 12. That for the purposes of the Ontario Building Code, this By-law or any part of it is not made until it has come into force as provided by Section 34 of the *Planning Act.*

PASSED this 12th day of August, 2022

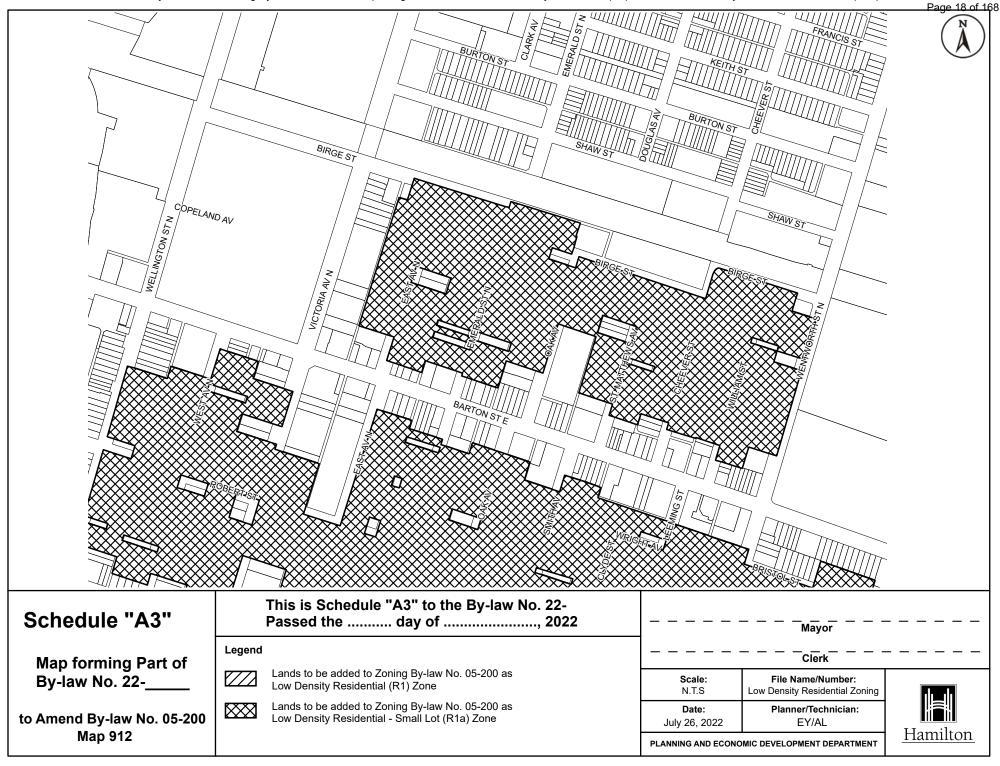
F. Eisenberger Mayor A. Holland City Clerk

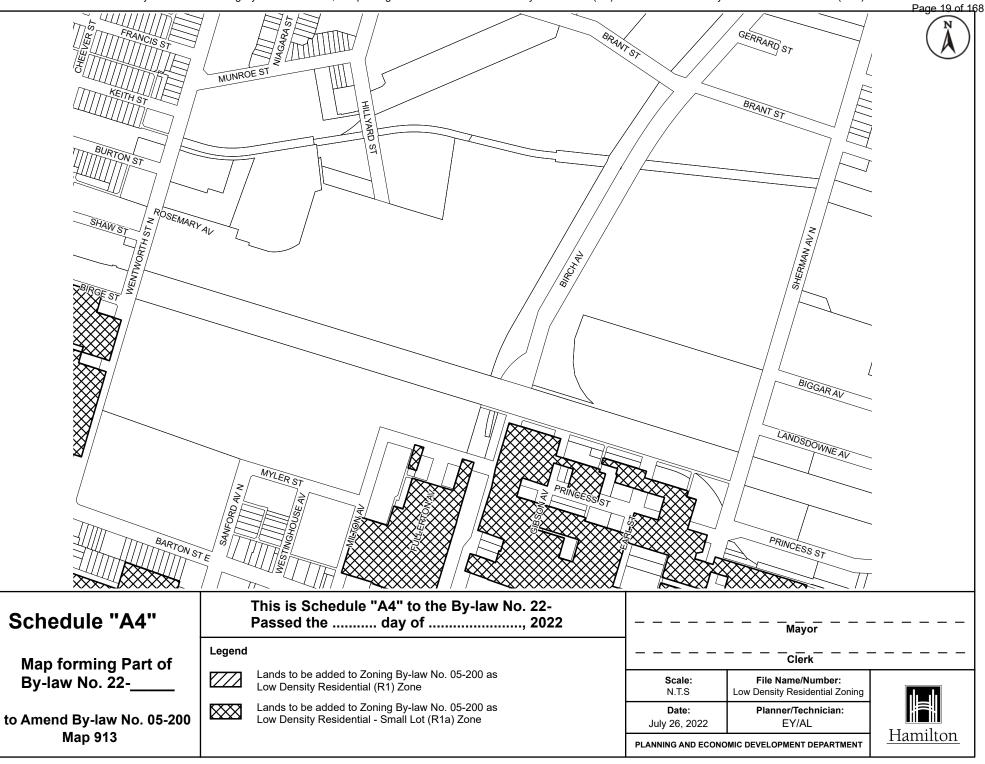
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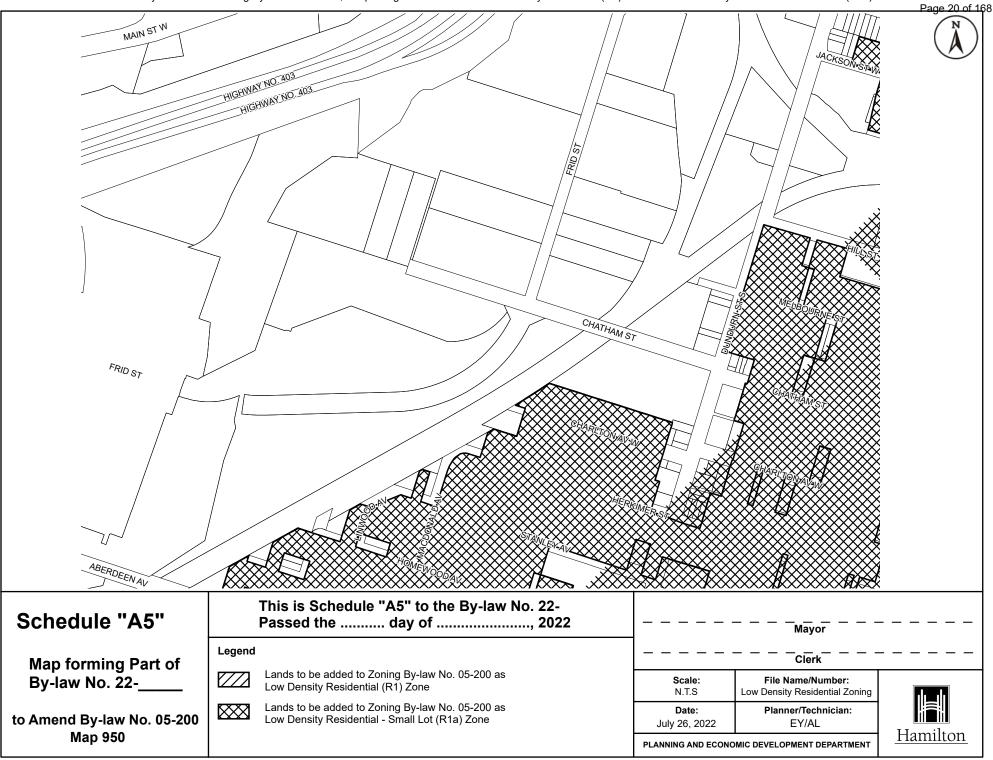


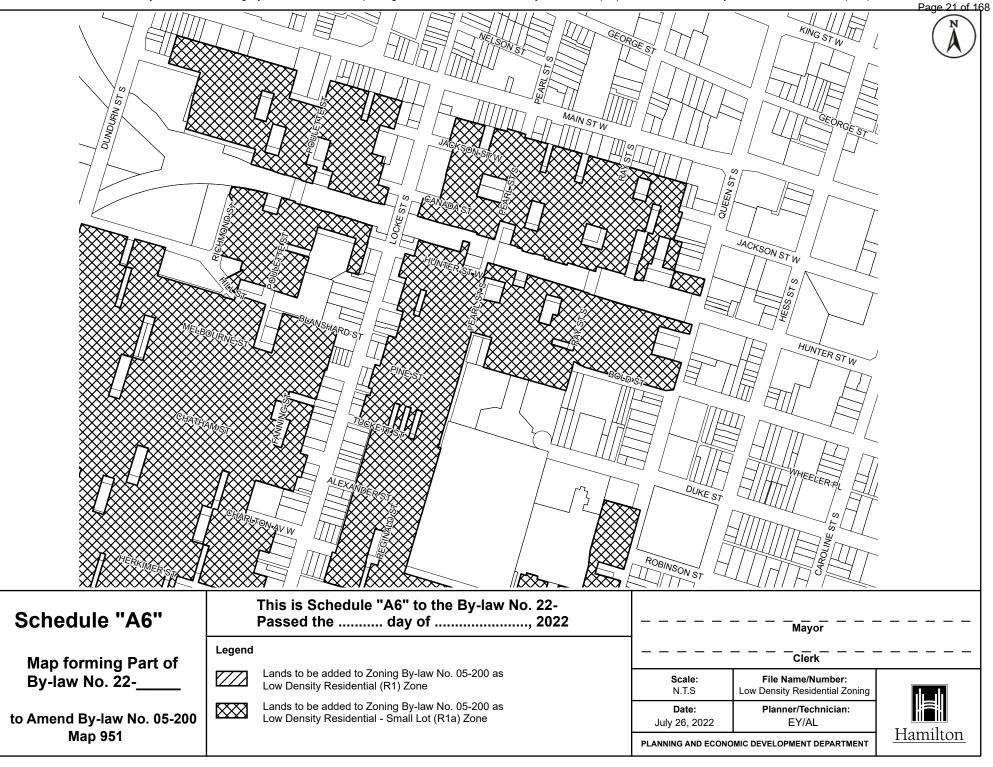


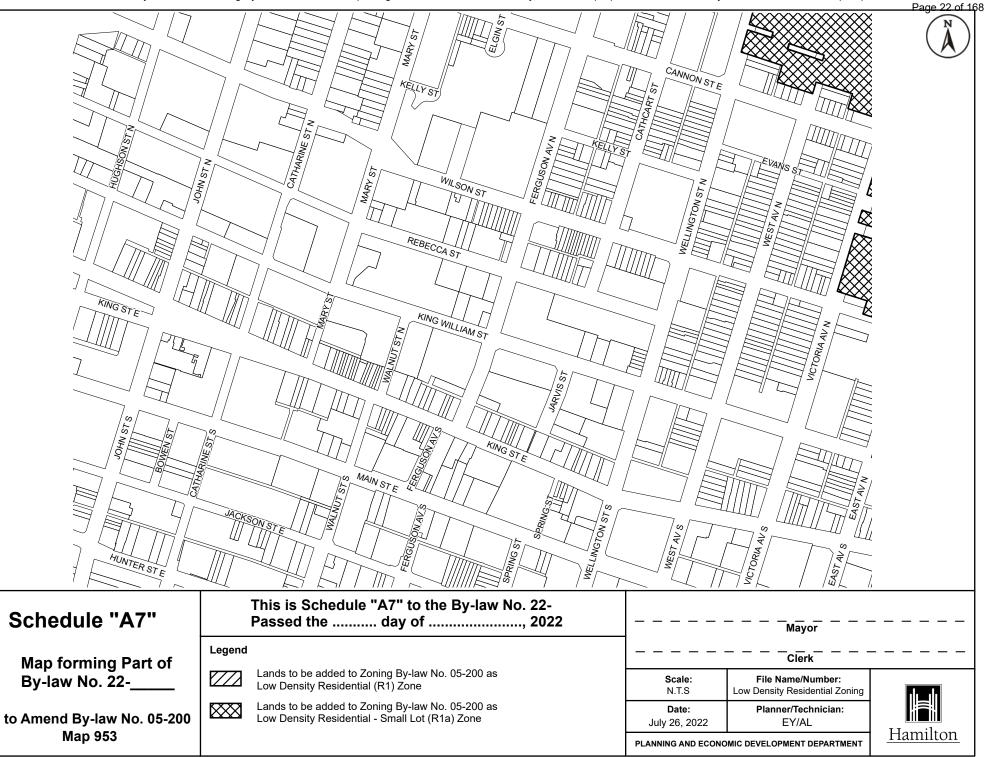


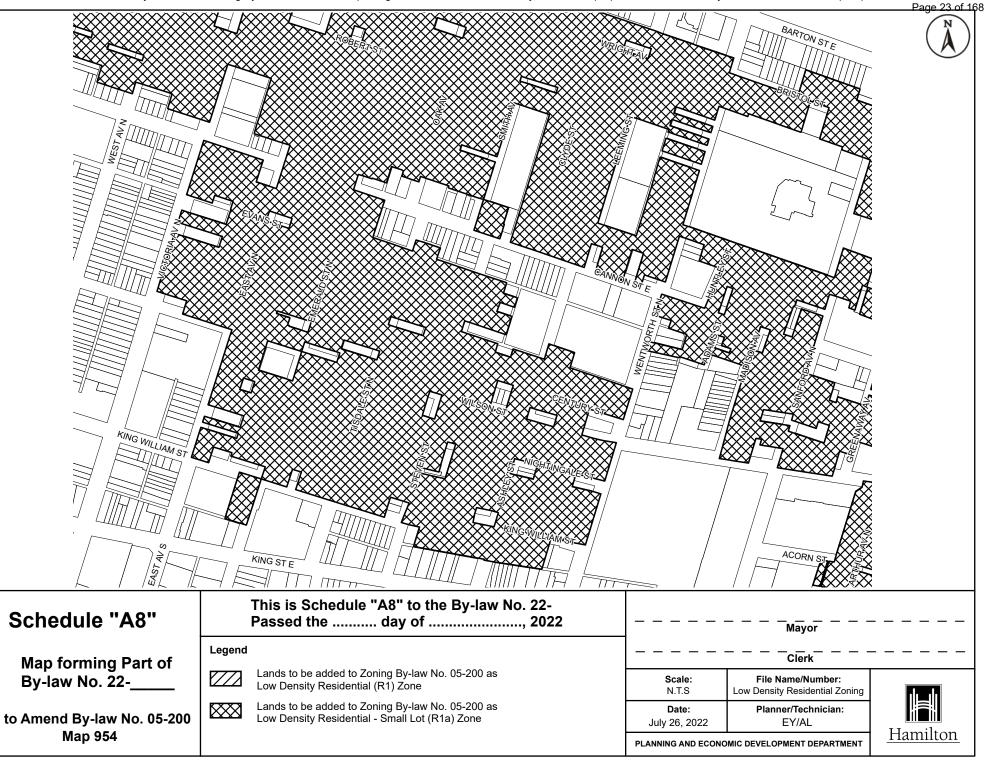


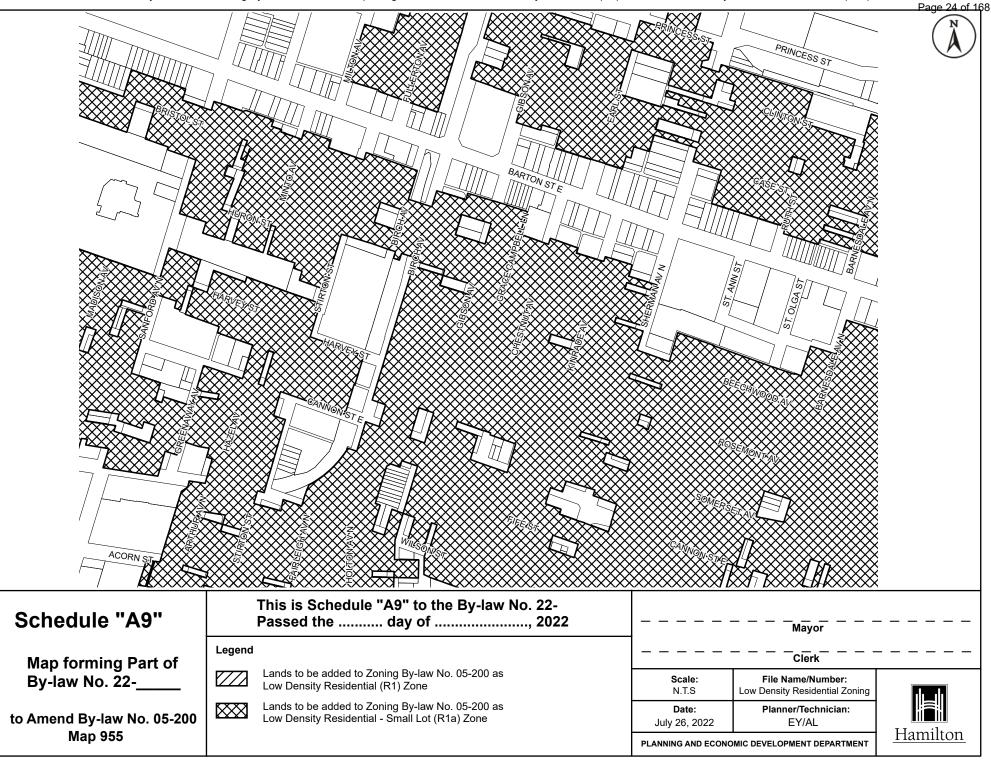


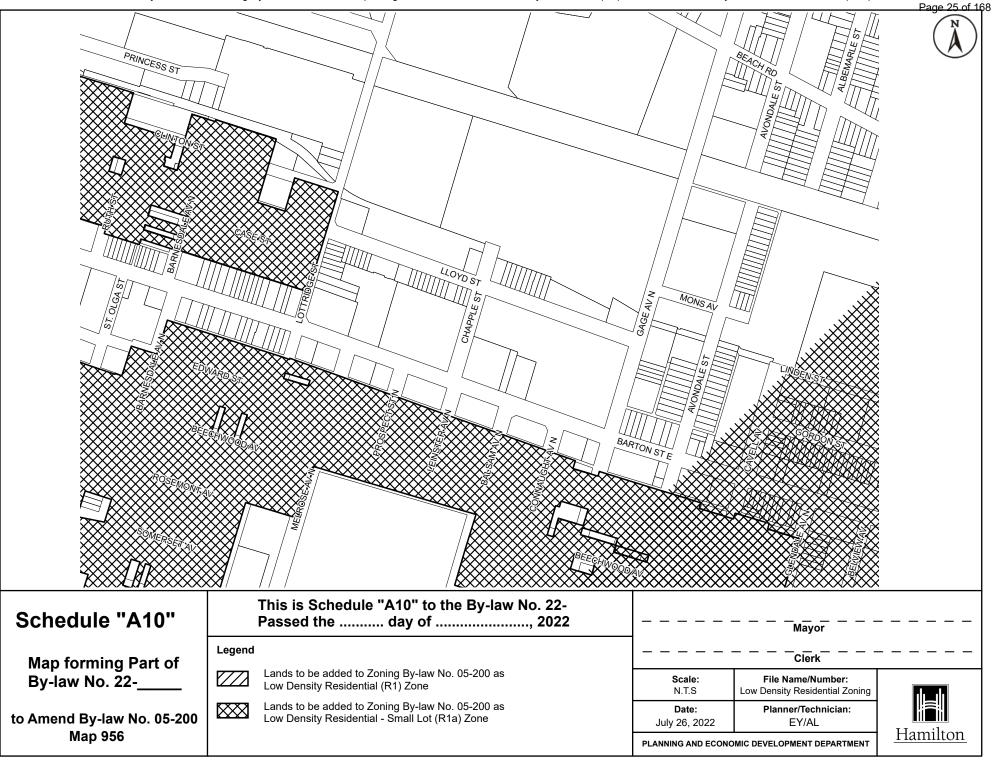


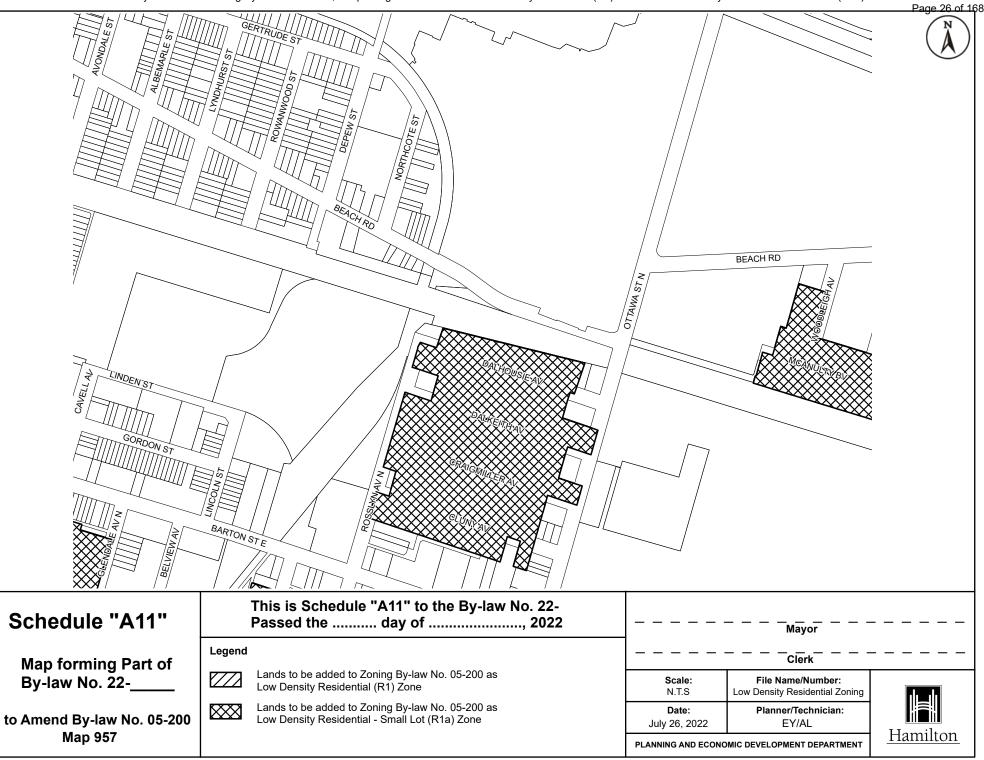




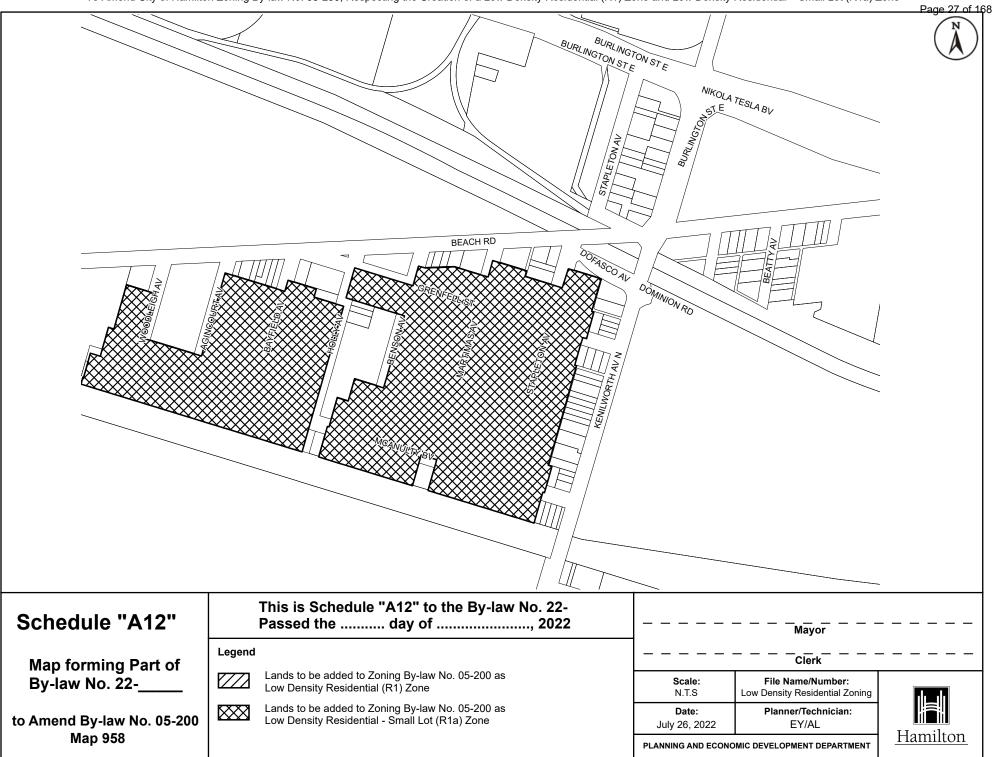


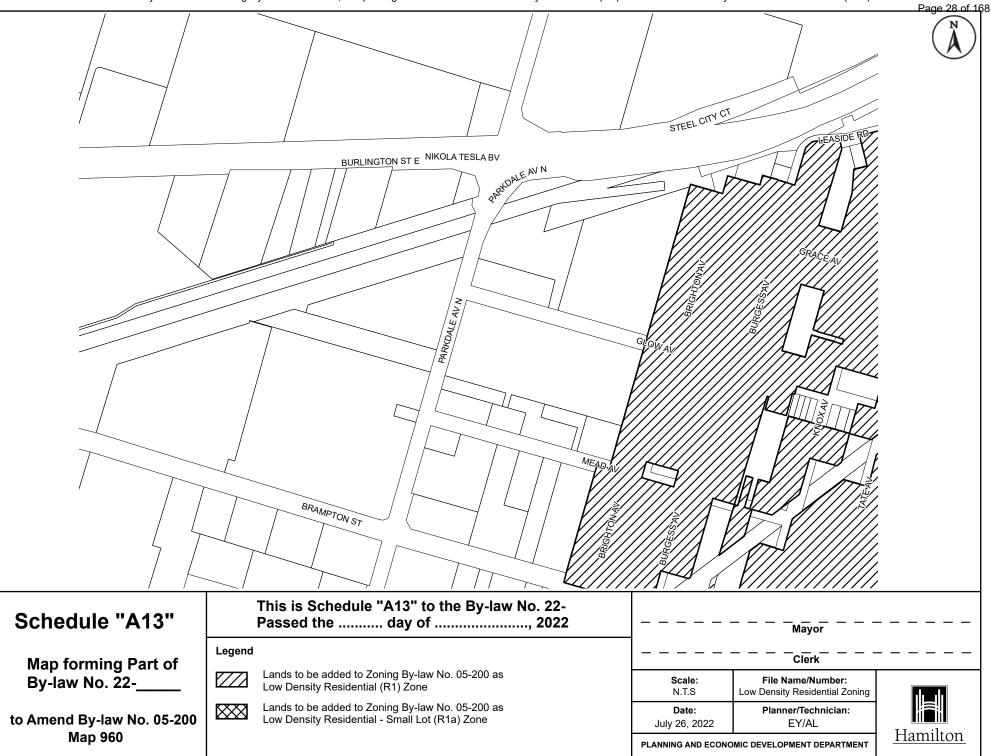


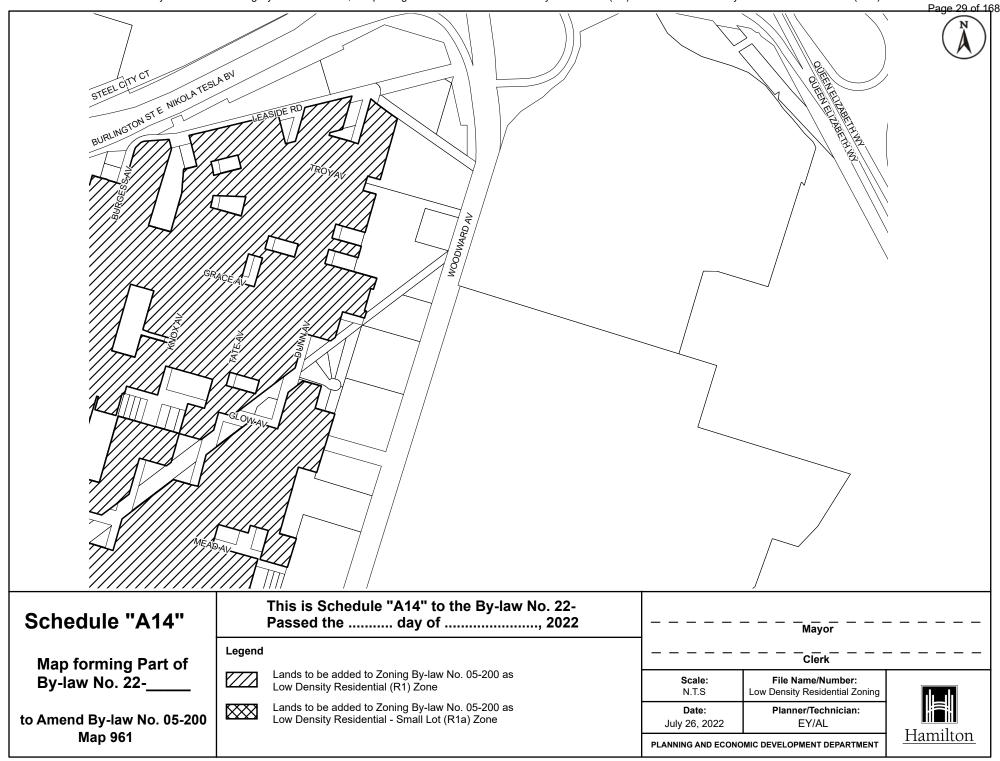


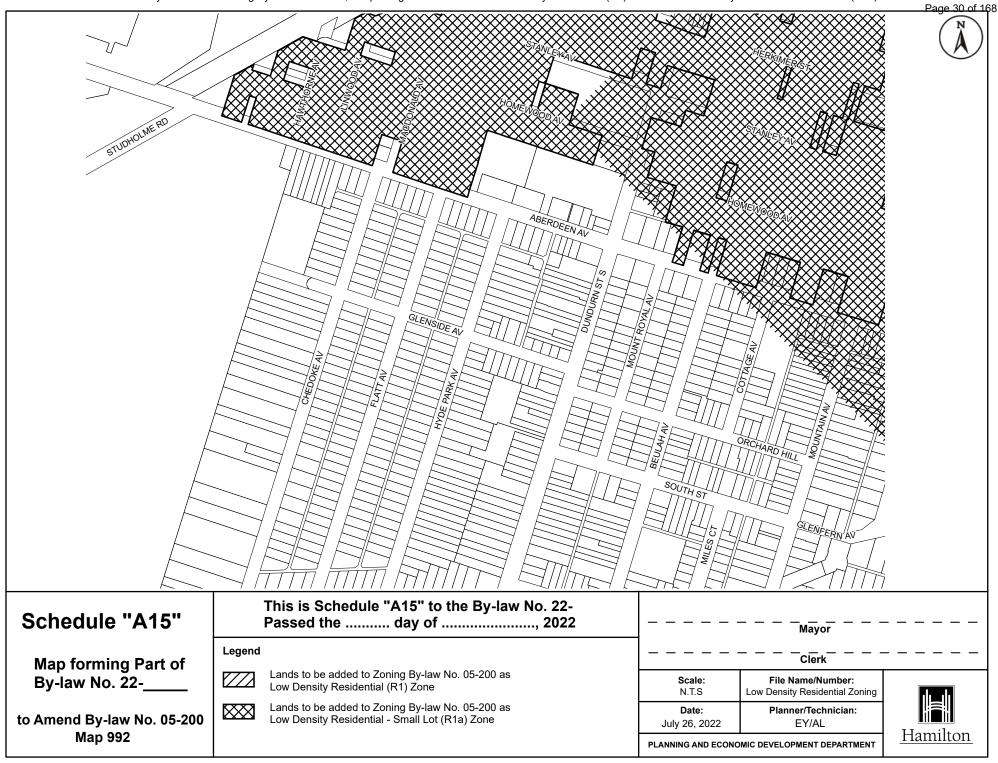


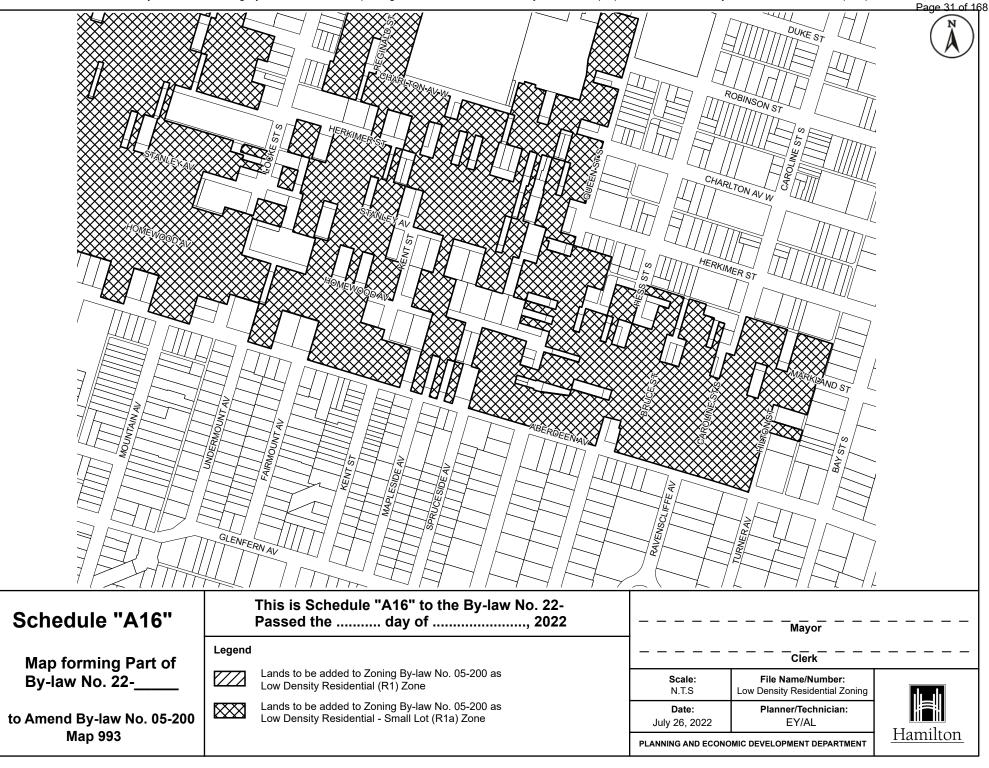


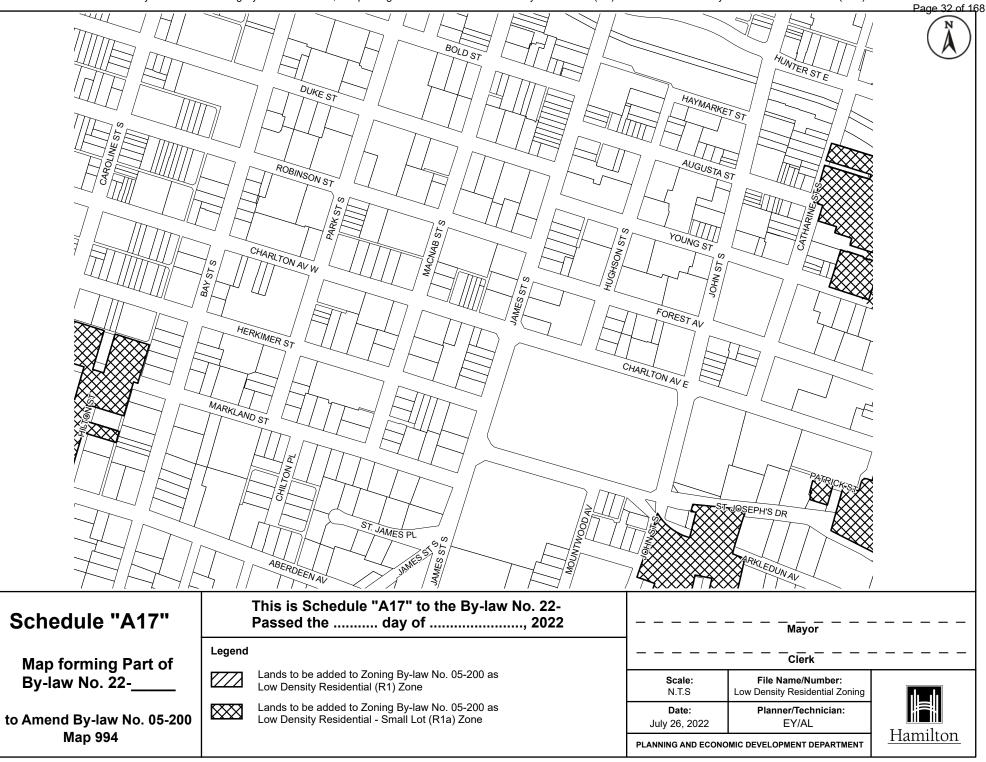


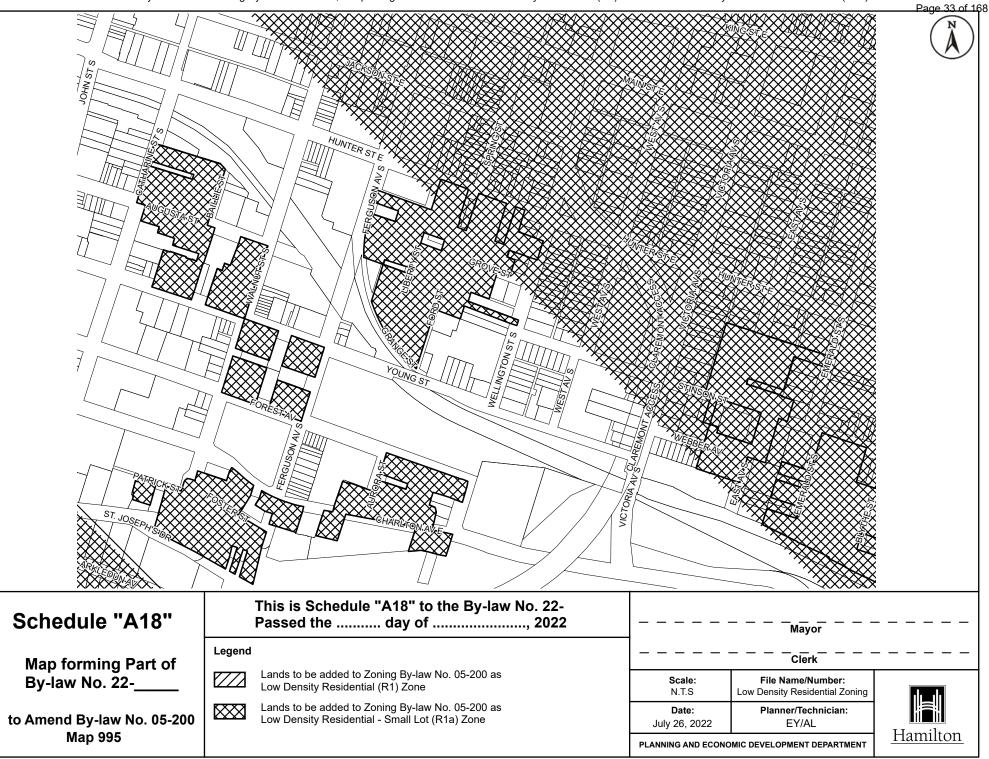


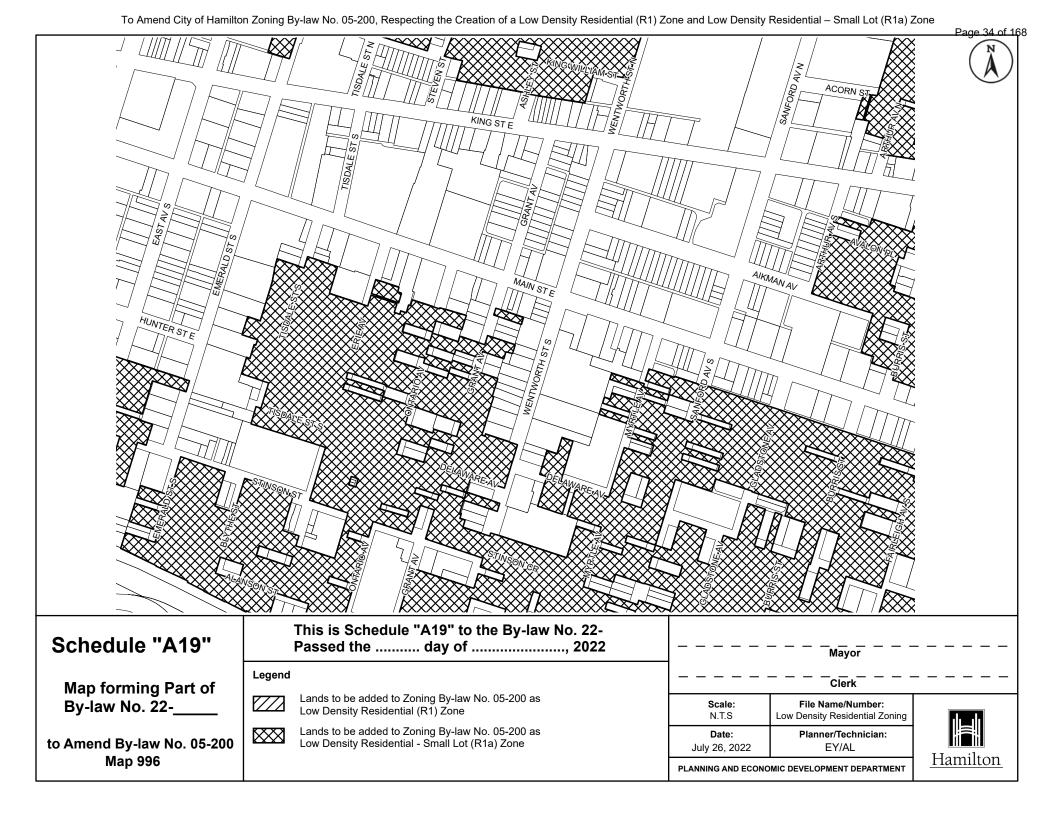




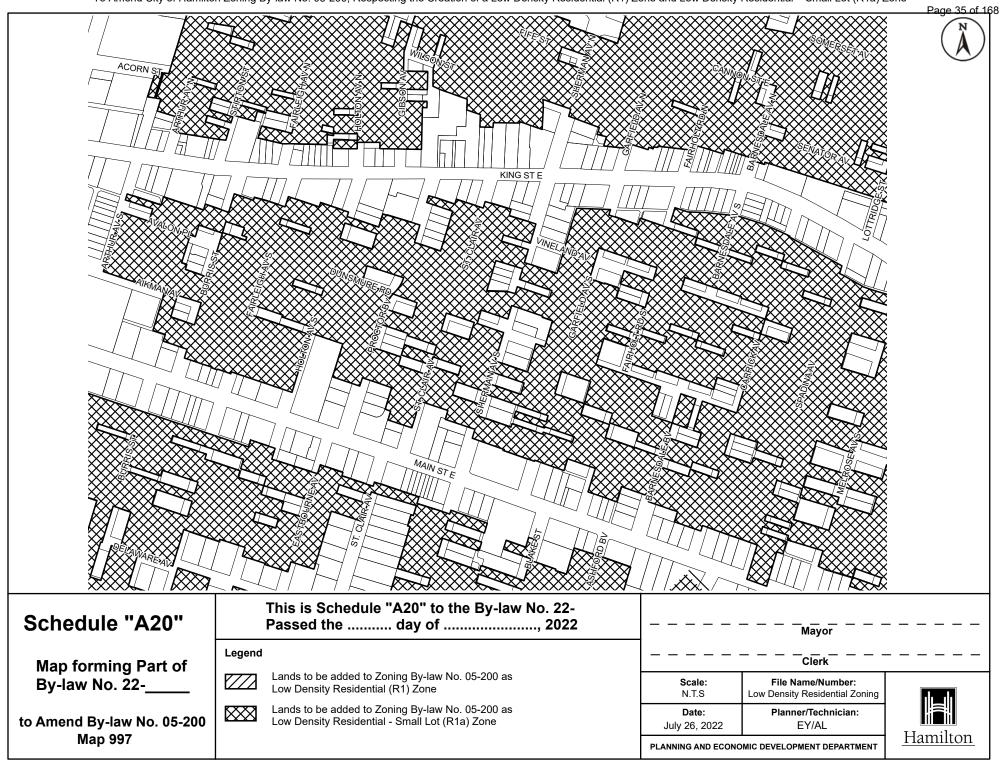


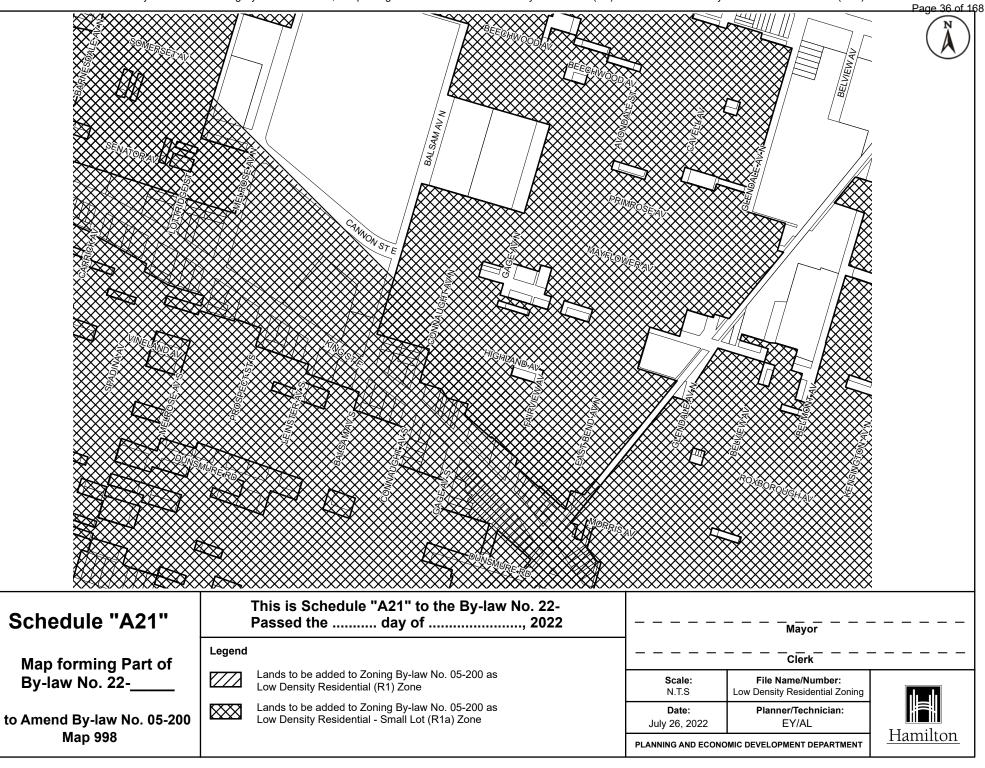


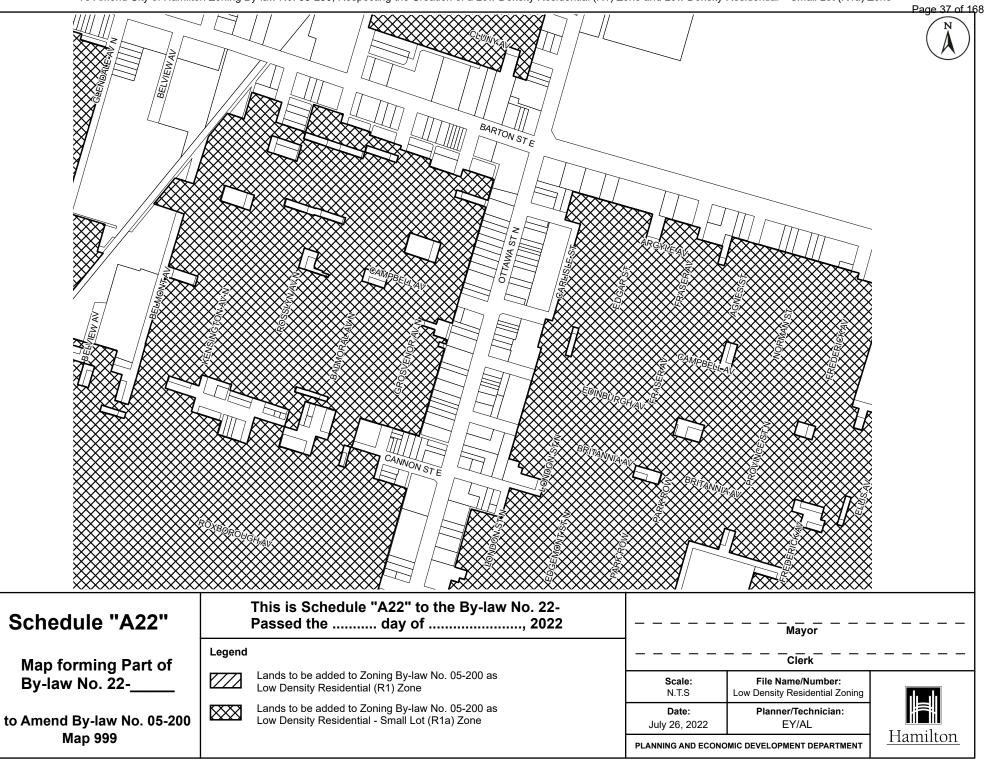


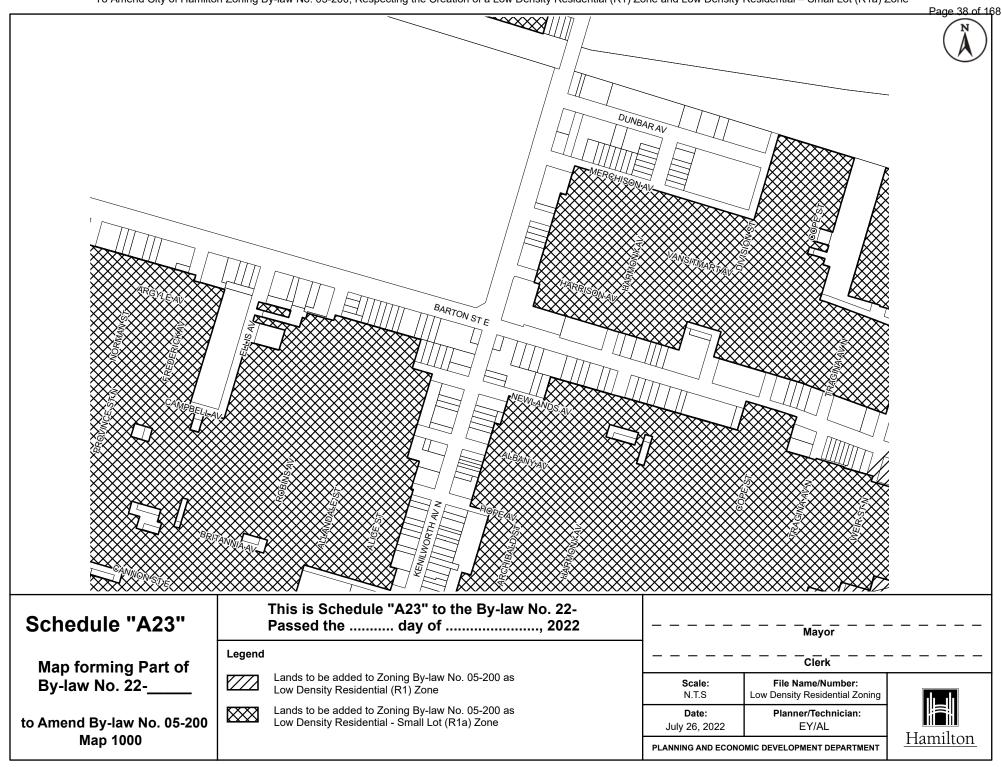


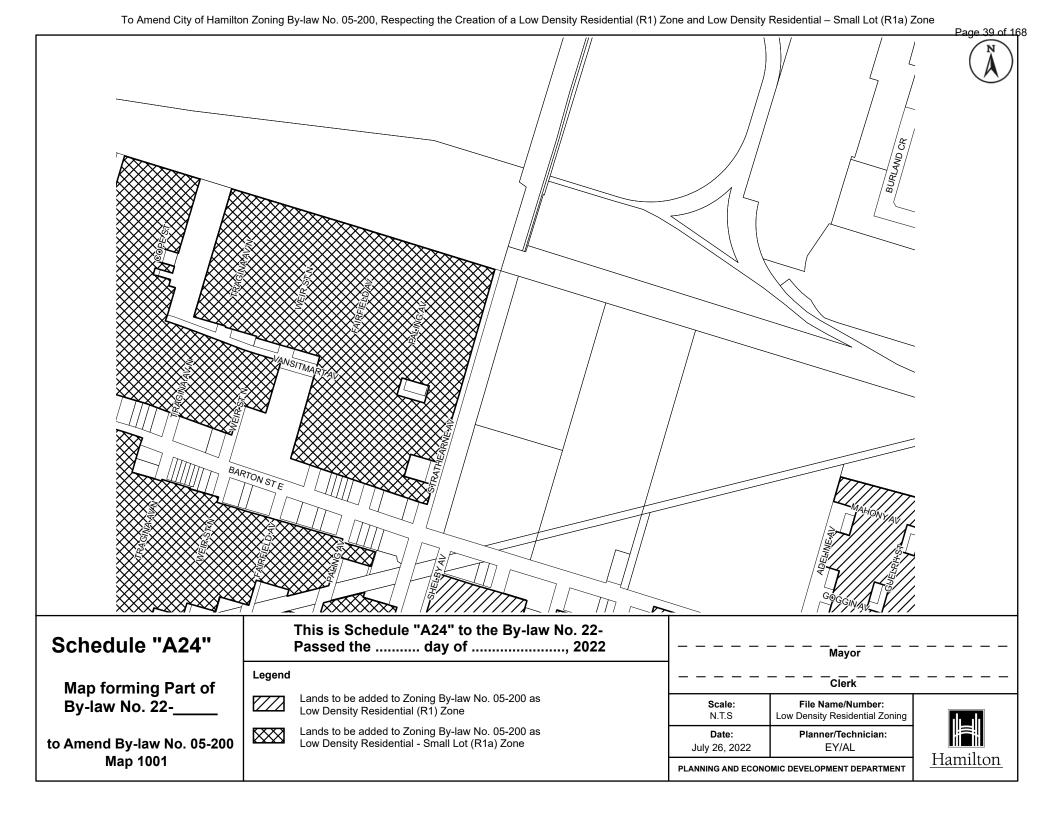


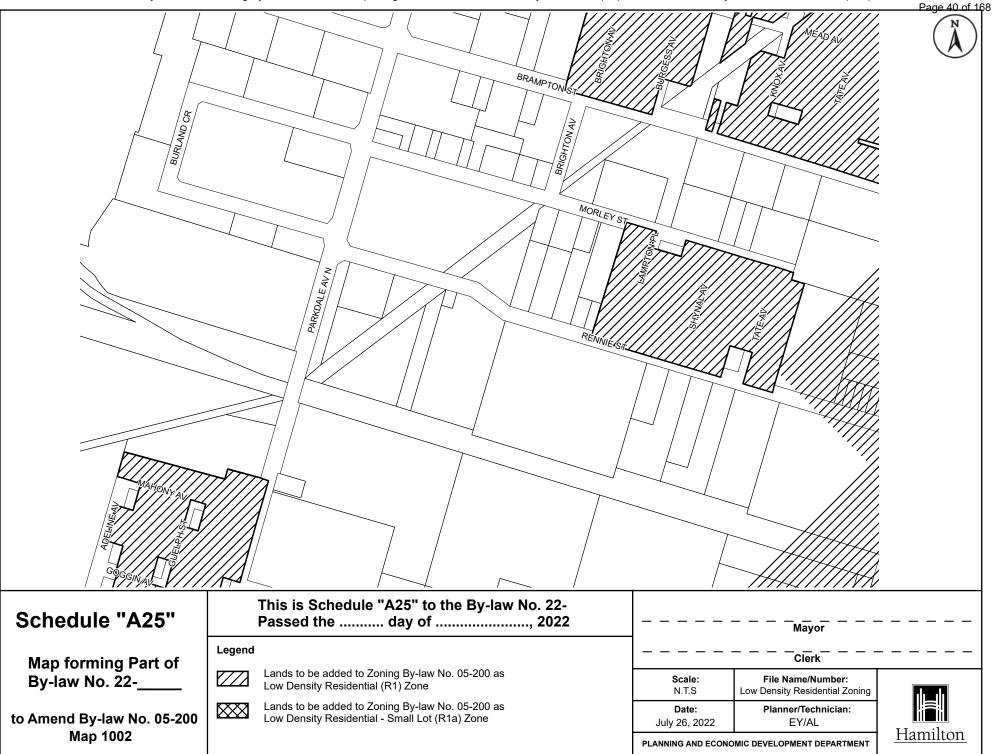


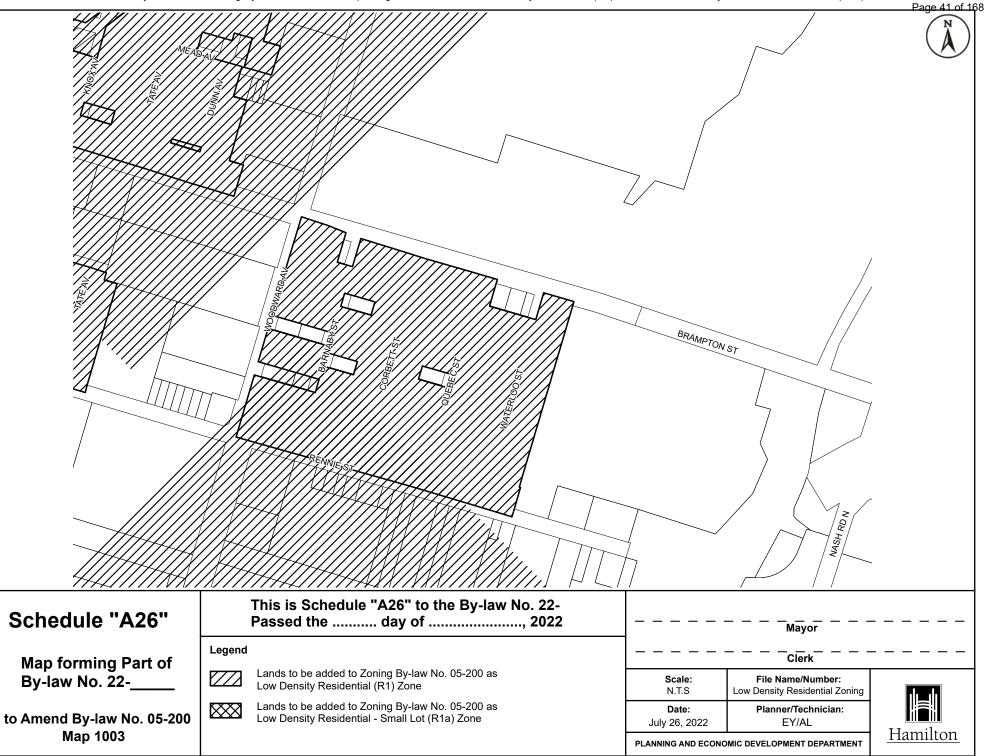


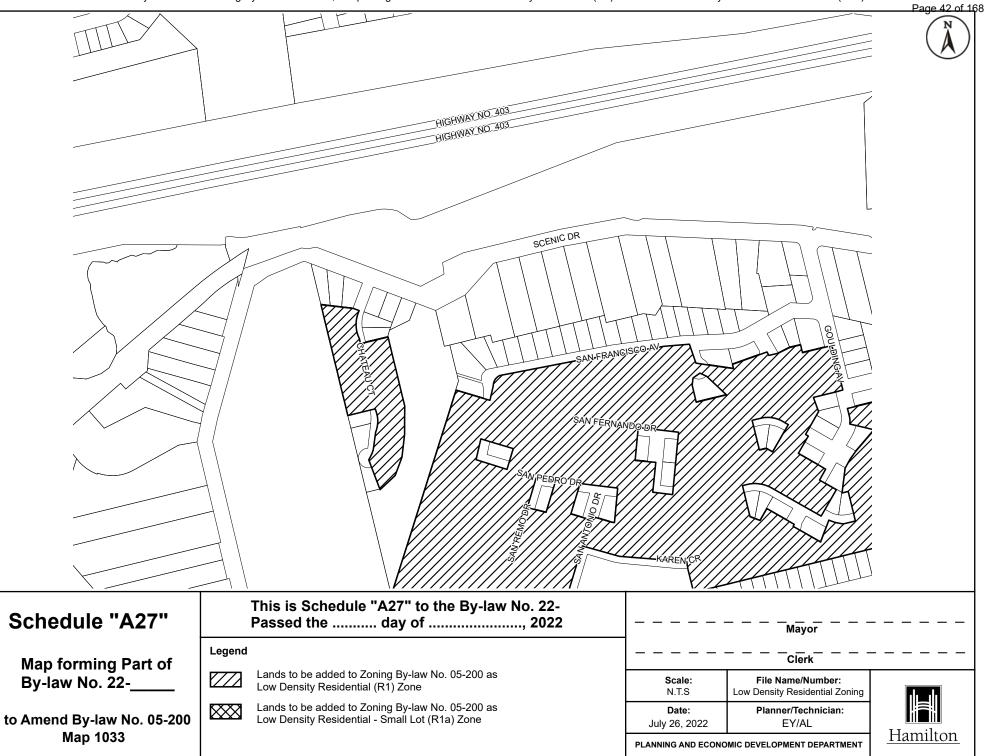


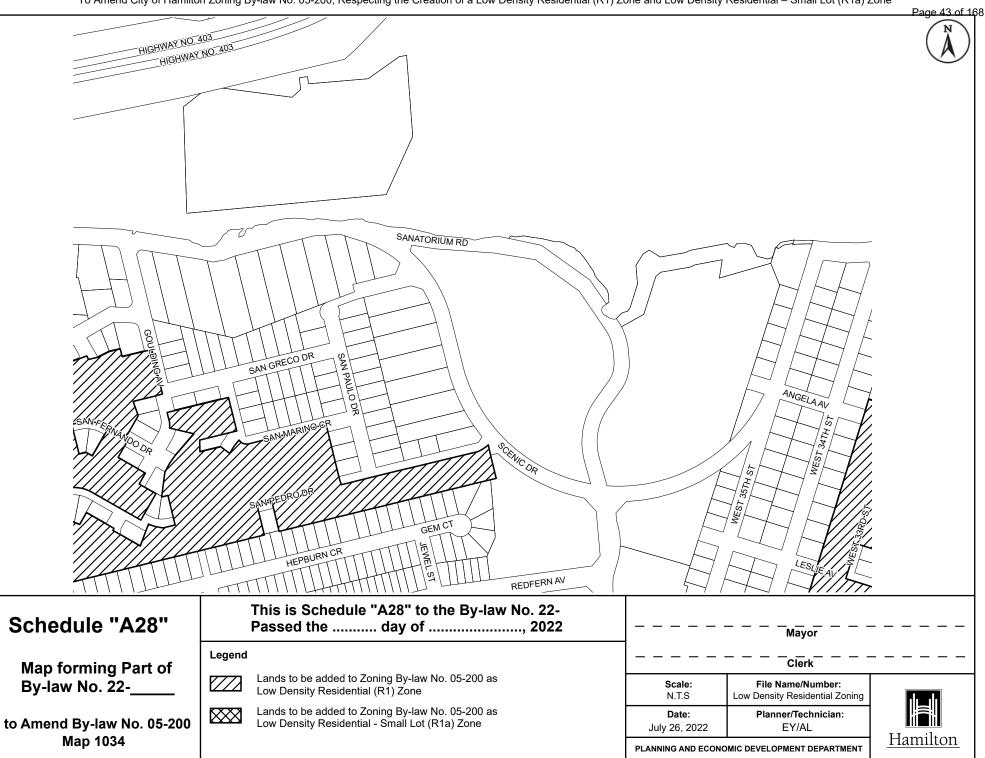


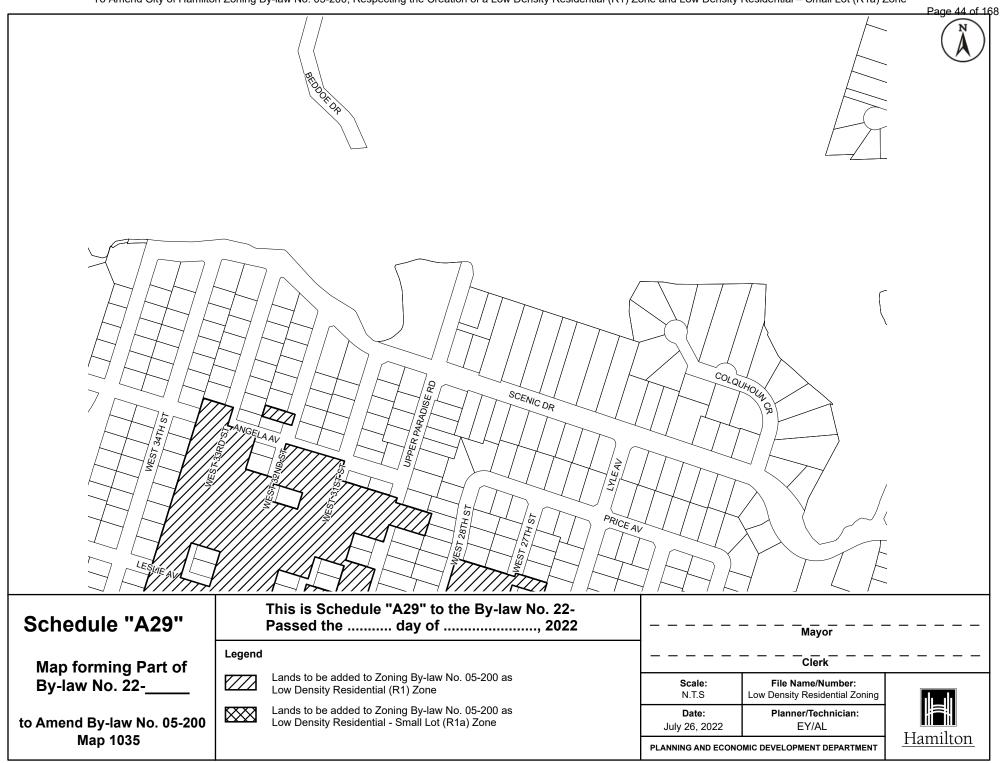


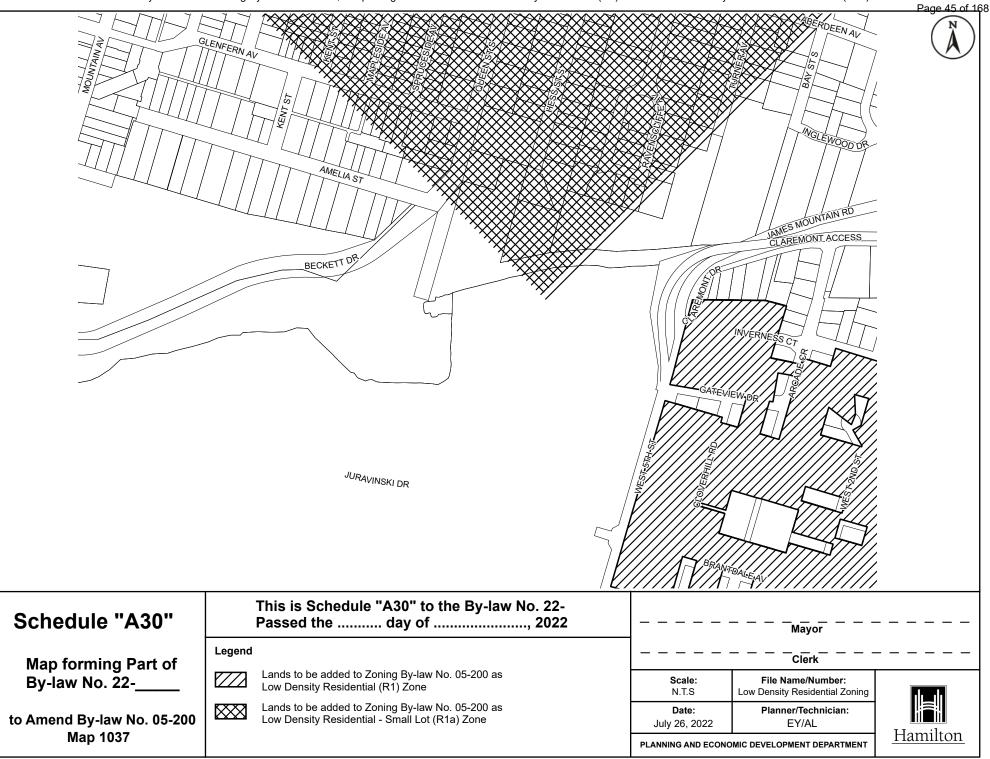


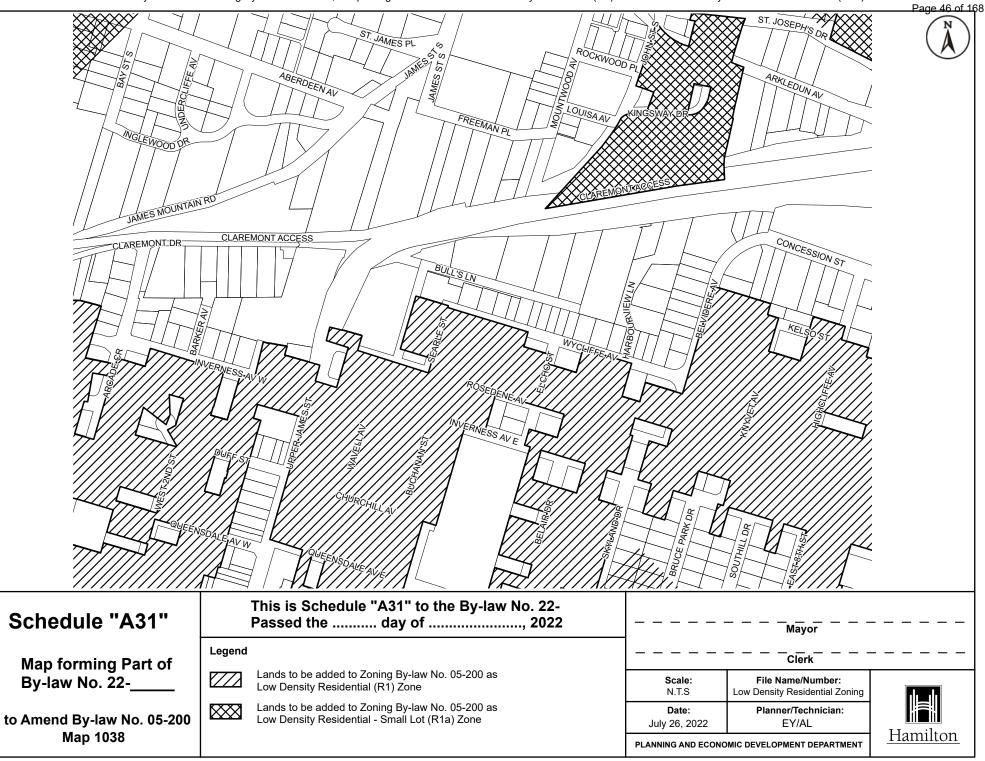


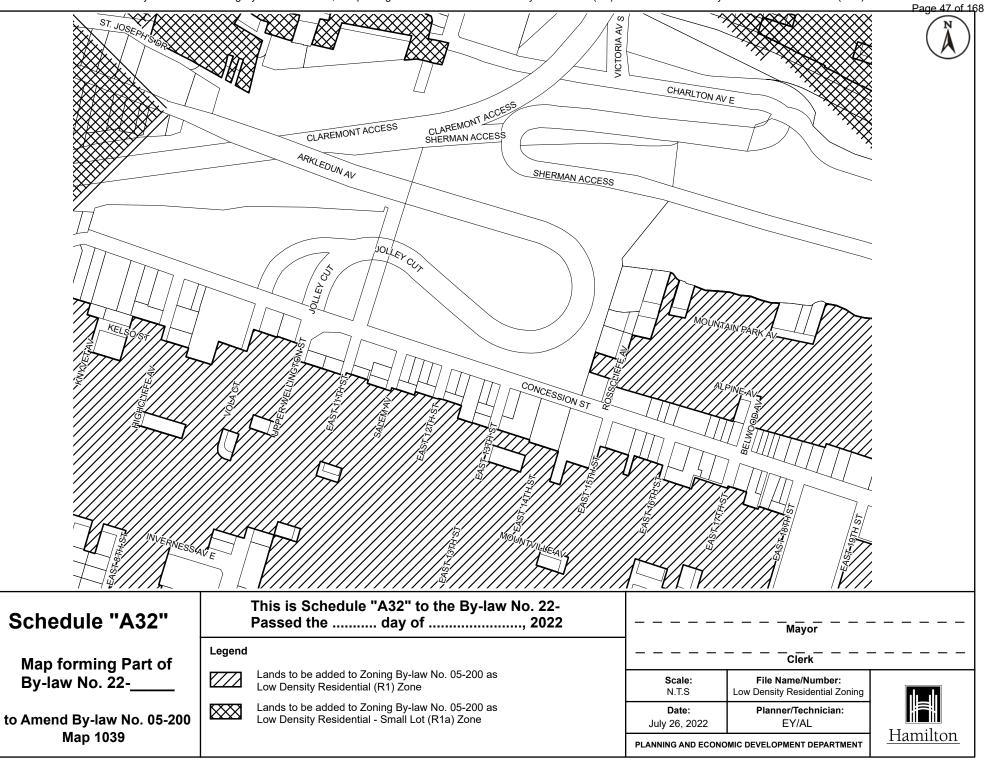


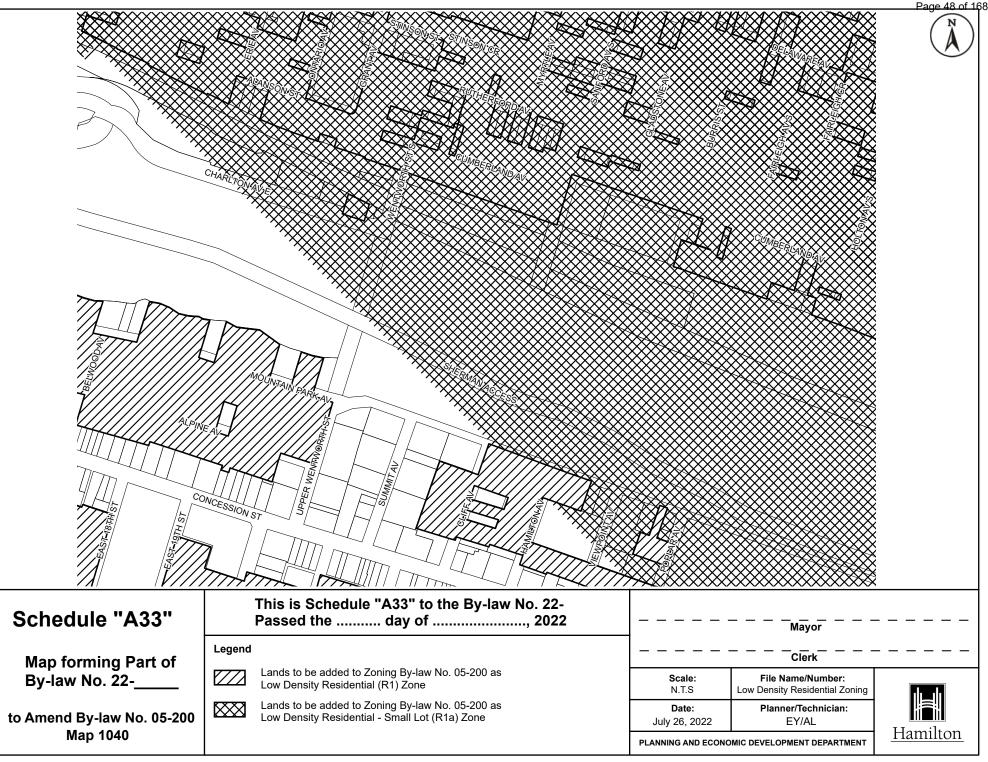


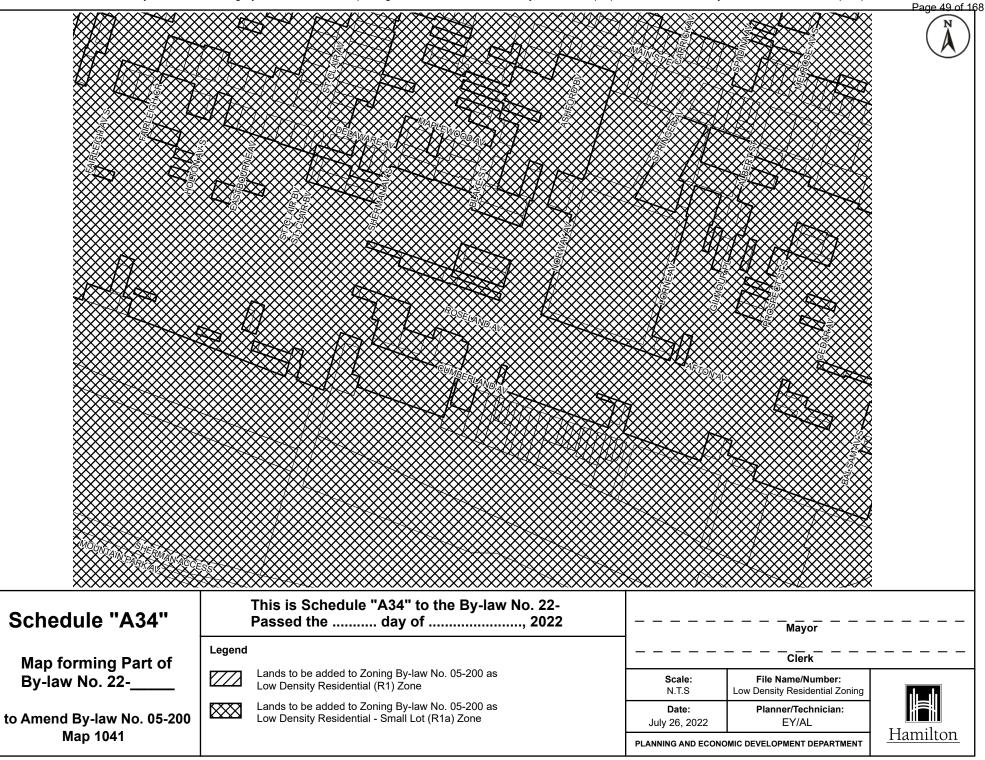


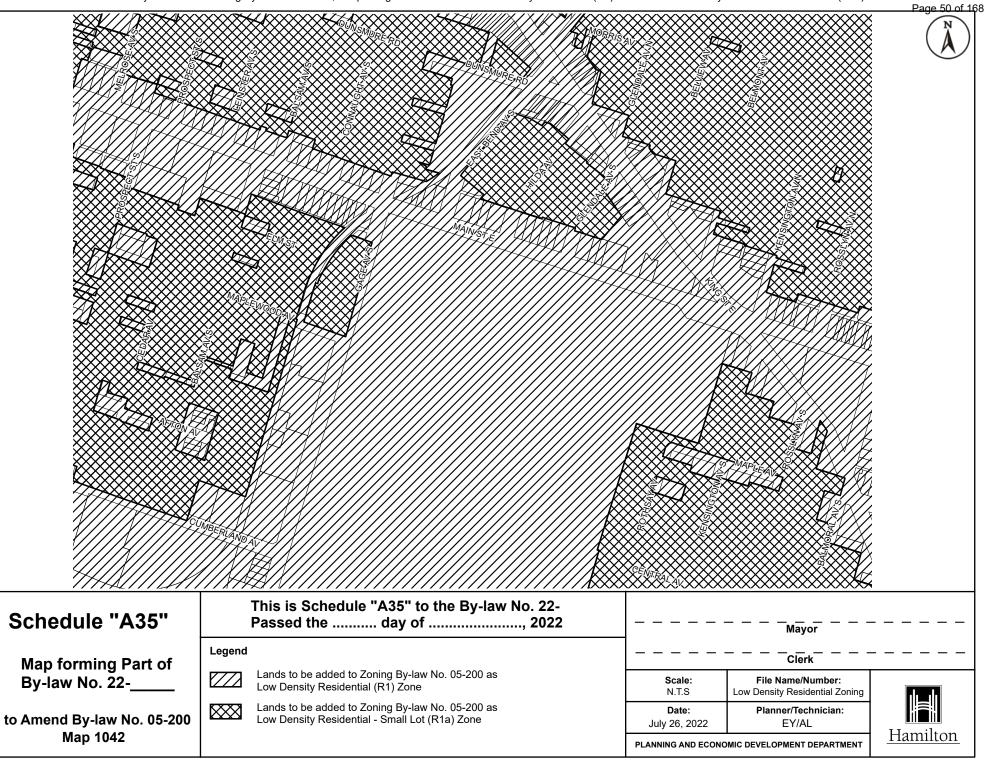


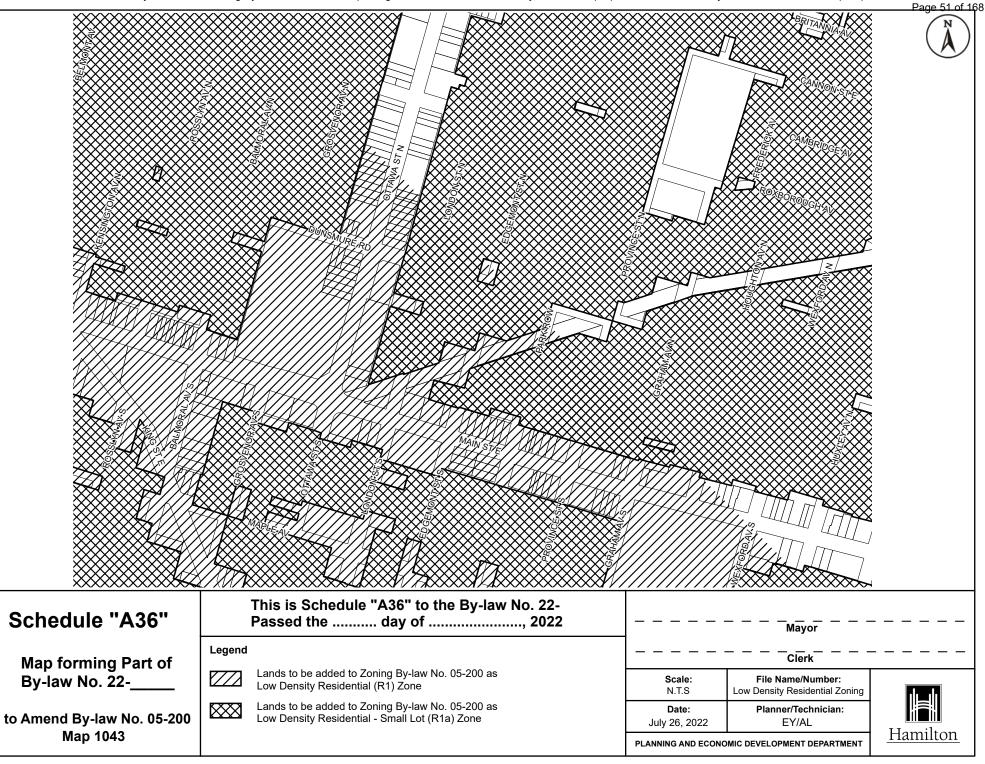


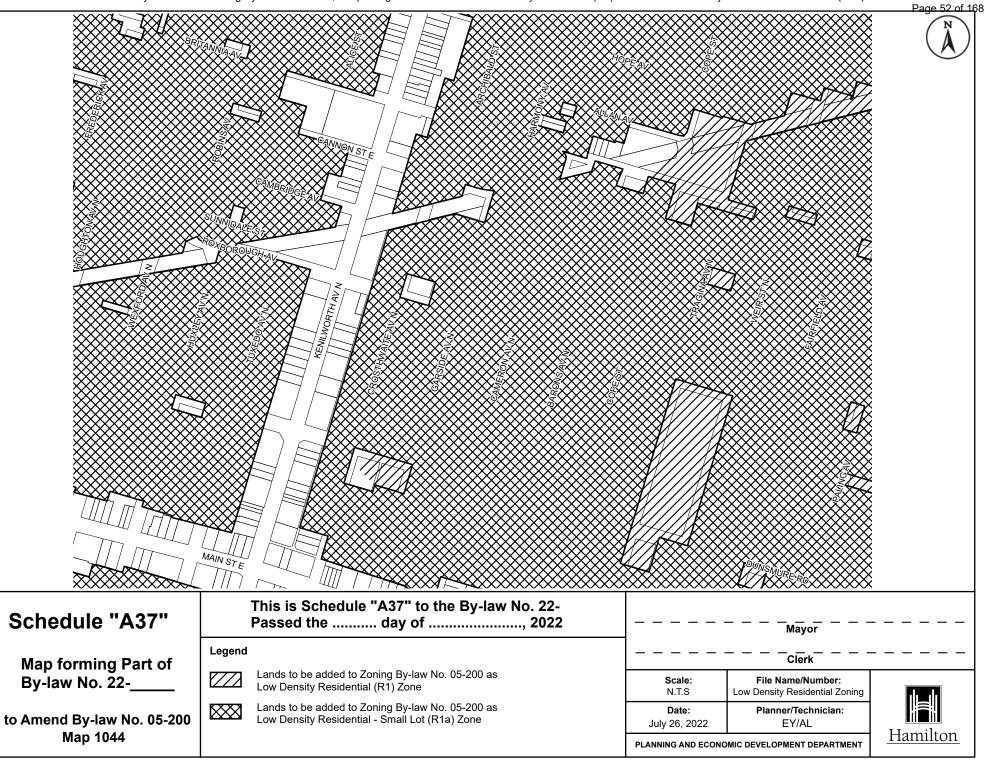


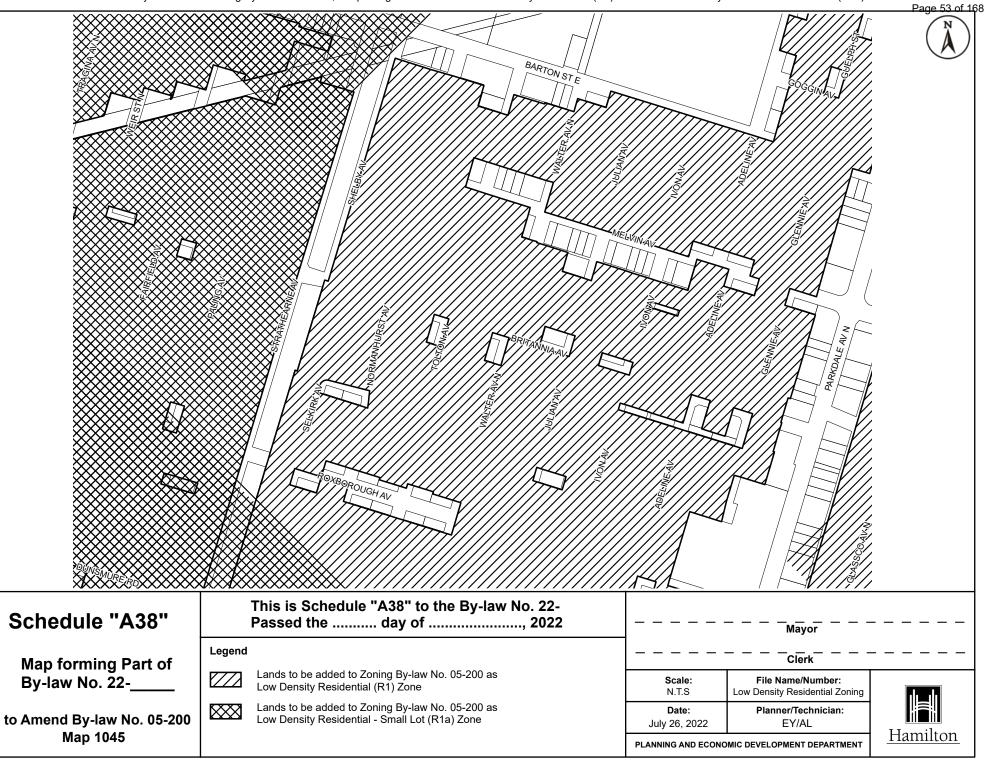


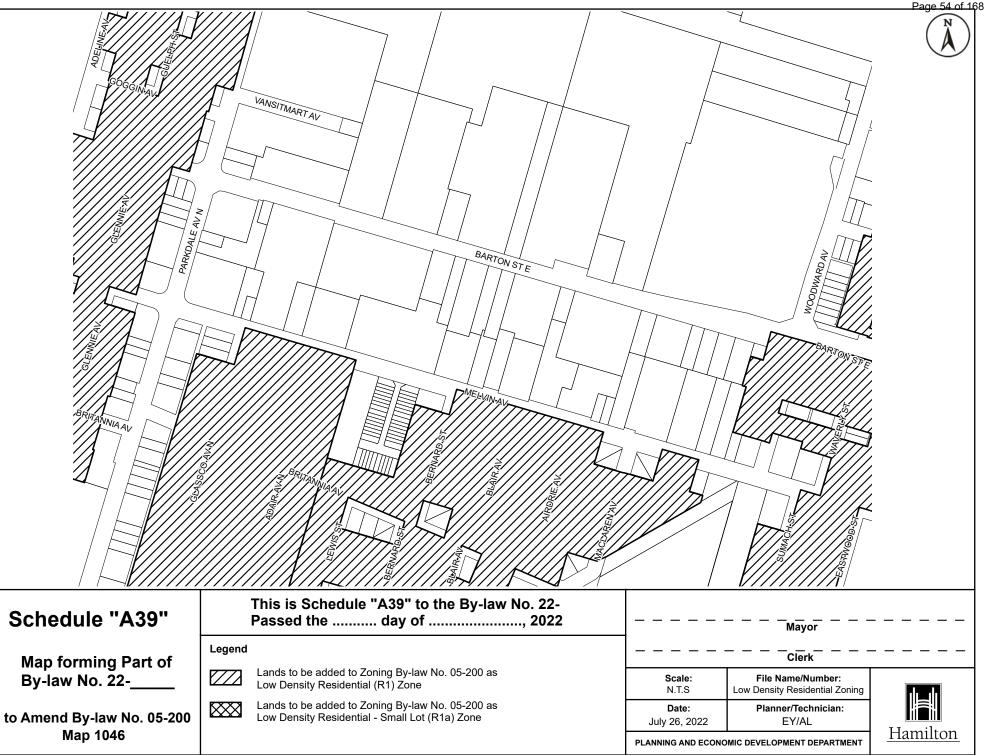


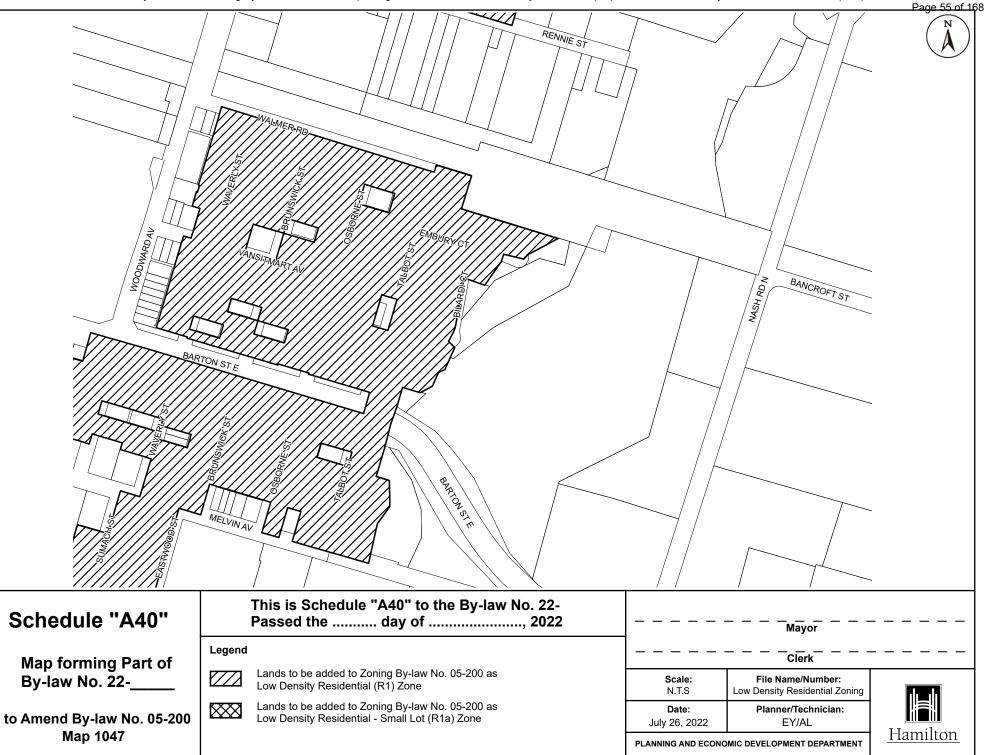






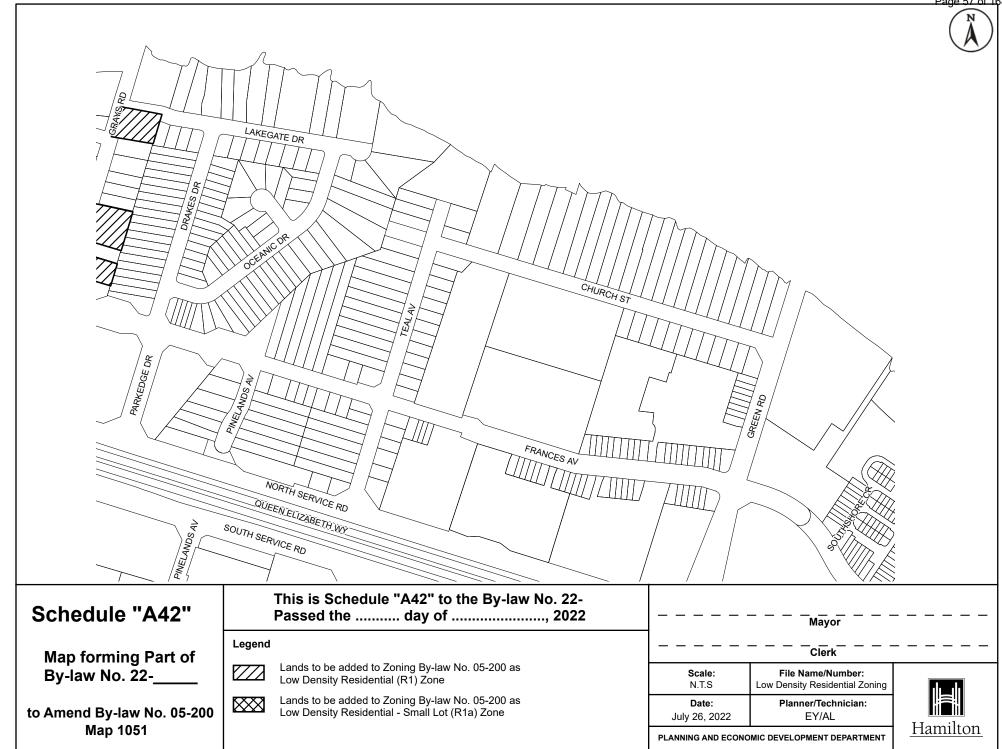


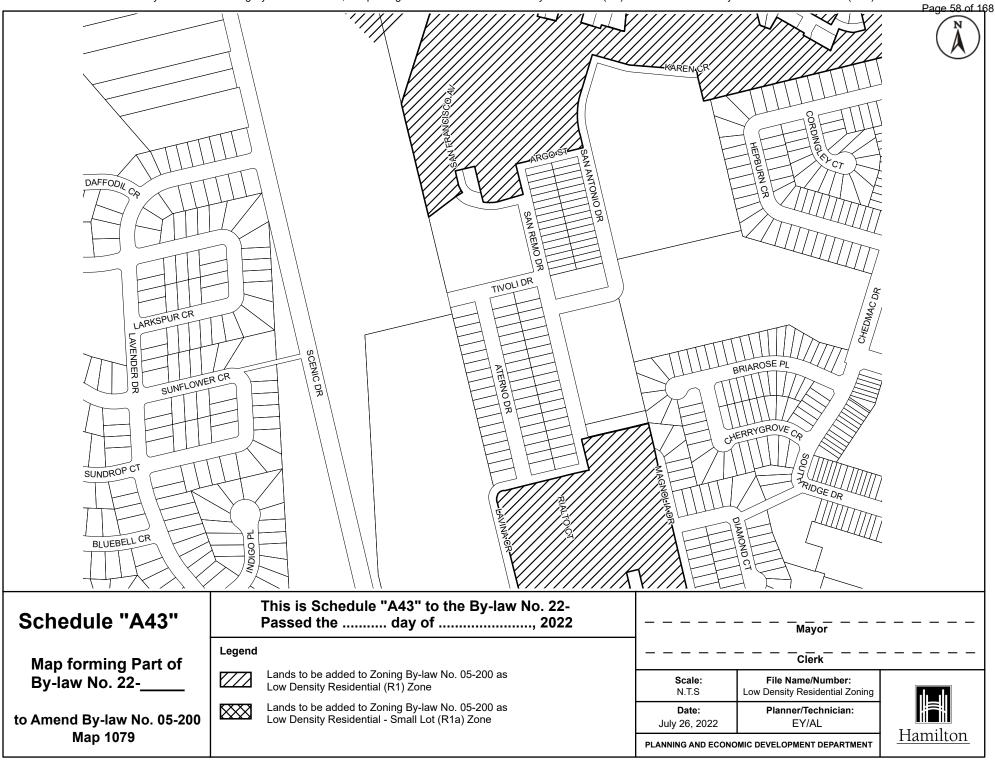


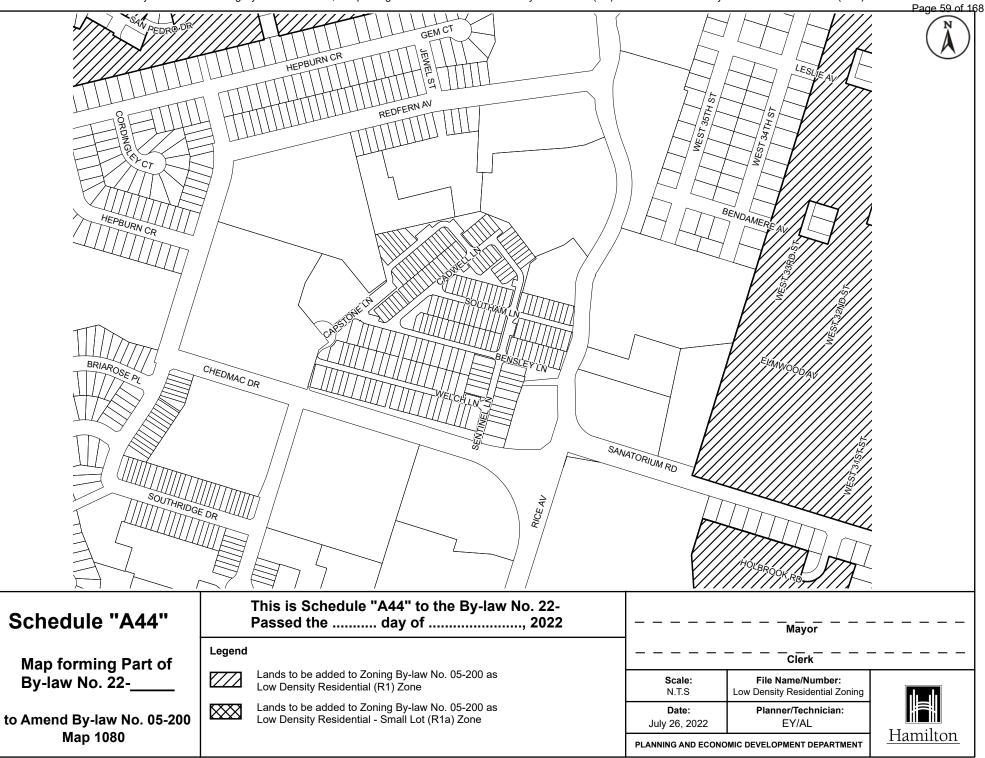


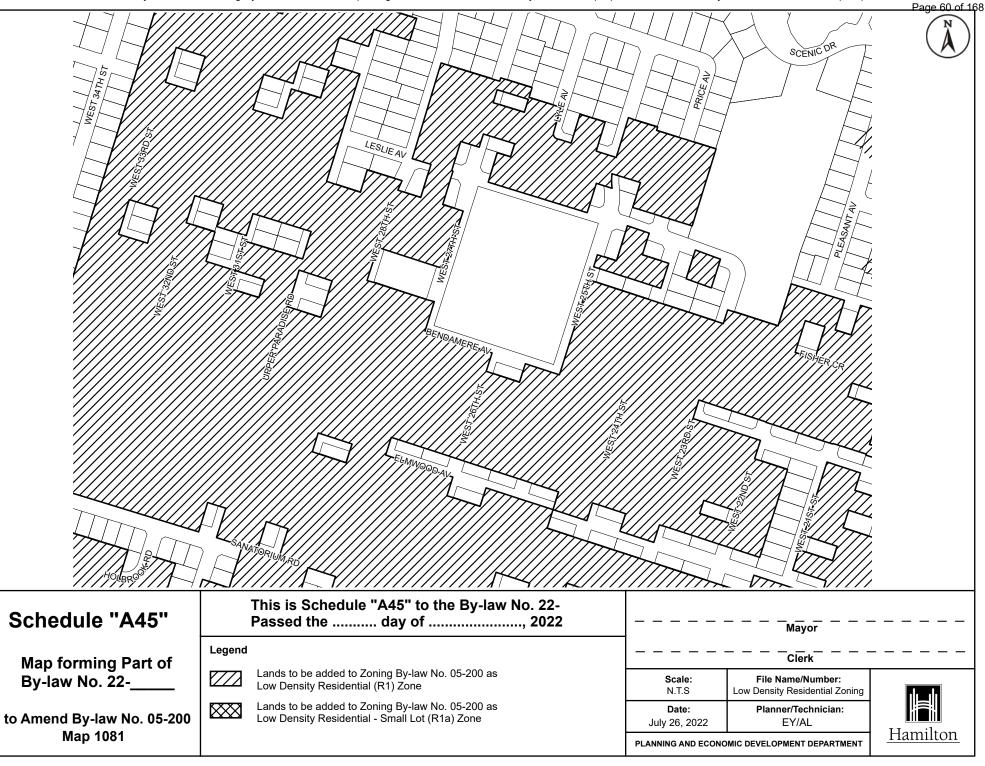
| | | Page 56 of 16 |
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| | | |
| | NORTH SERVICE RD QUEEN ELIZABETH WY SOUTH SER | CP FRANCES AV VICE RD VICE RD |
| Schedule "A41" | This is Schedule "A41" to the By-law No. 22- Passed the day of | Mayor |
| Map forming Part of By-law No. 22 | Lands to be added to Zoning By-law No. 05-200 as Low Density Residential (R1) Zone | Clerk Scale: File Name/Number: |
| to Amend By-law No. 05-200 Map 1050 | Lands to be added to Zoning By-law No. 05-200 as Low Density Residential - Small Lot (R1a) Zone | N.T.S Low Density Residential Zoning Date: Planner/Technician: July 26, 2022 EY/AL PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT Hamilton |

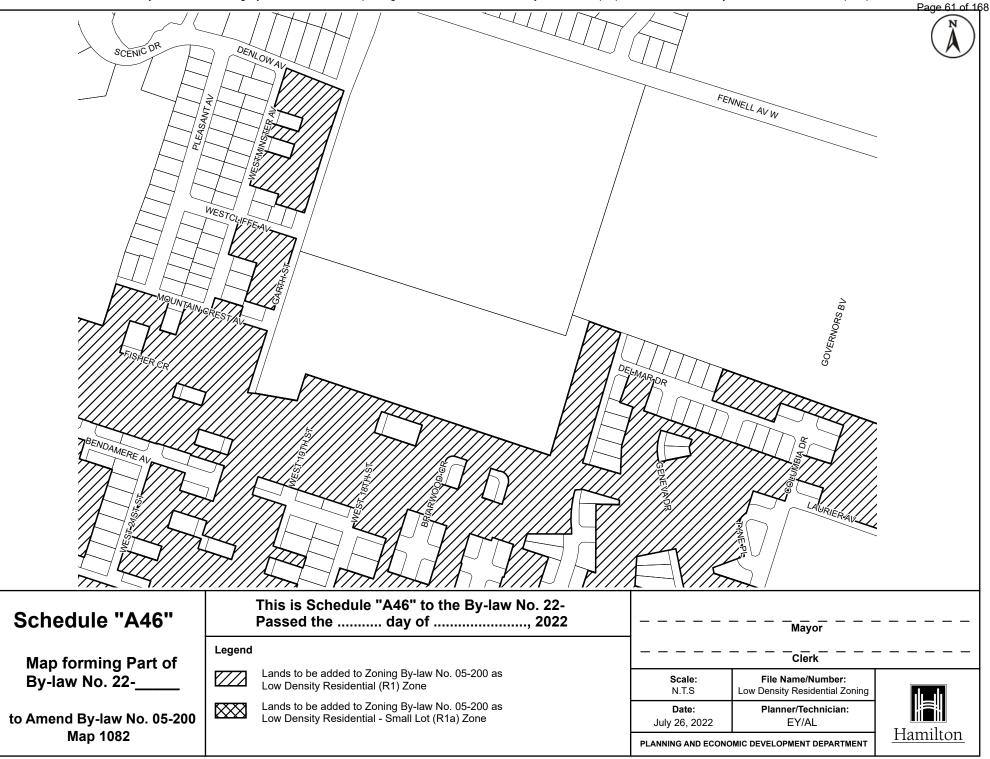


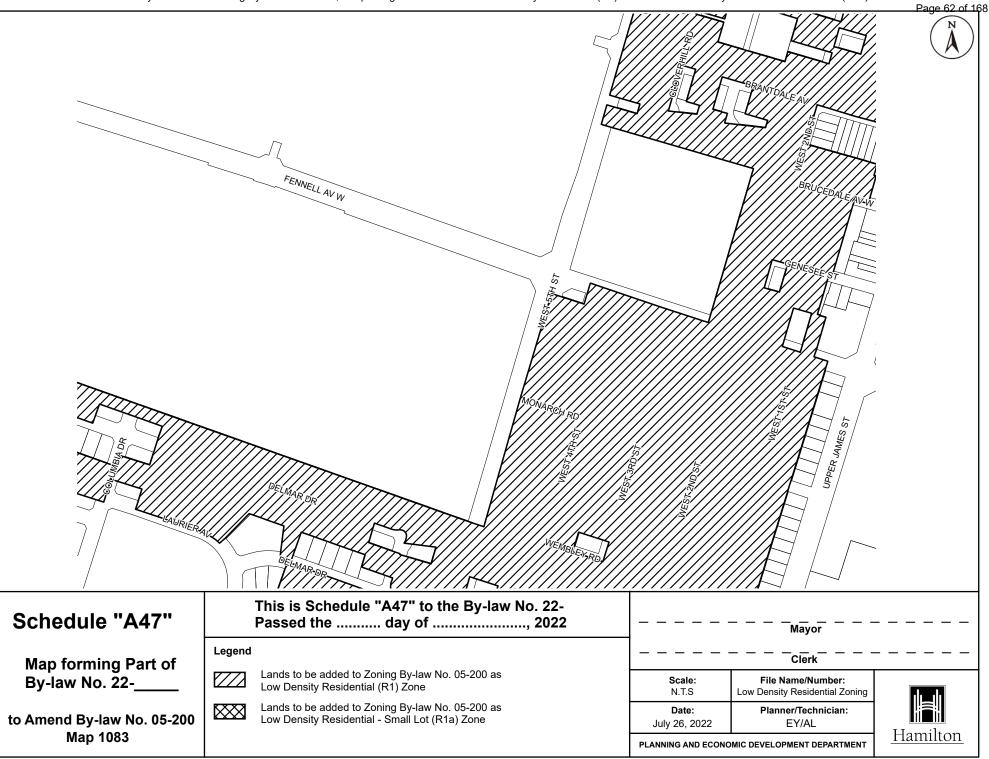


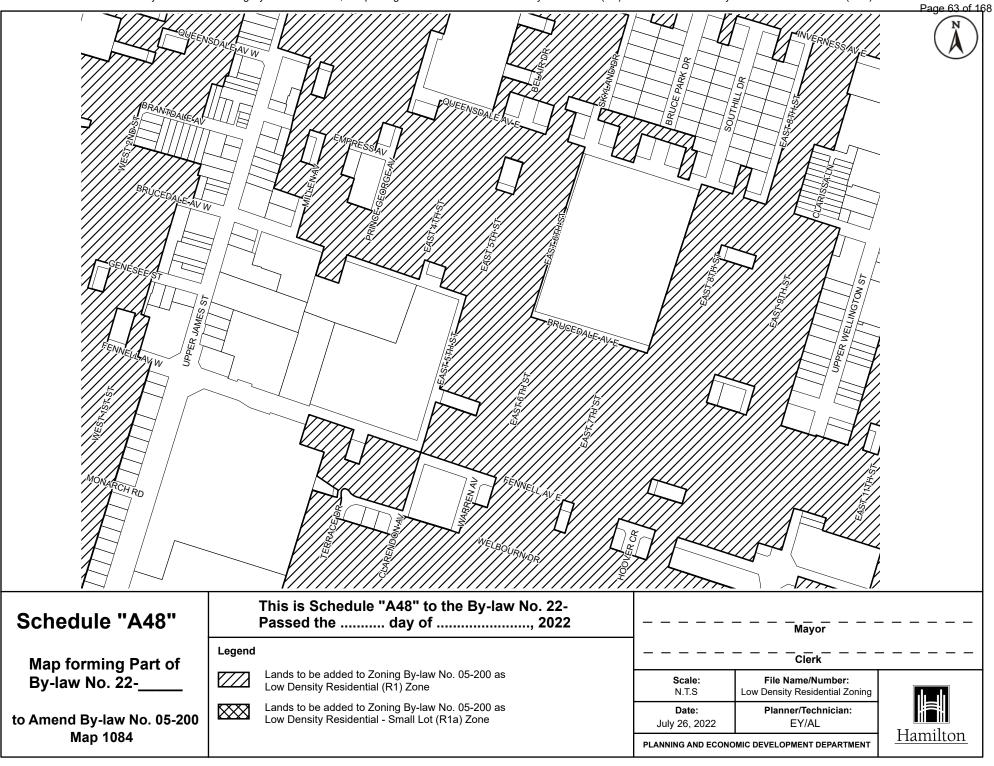


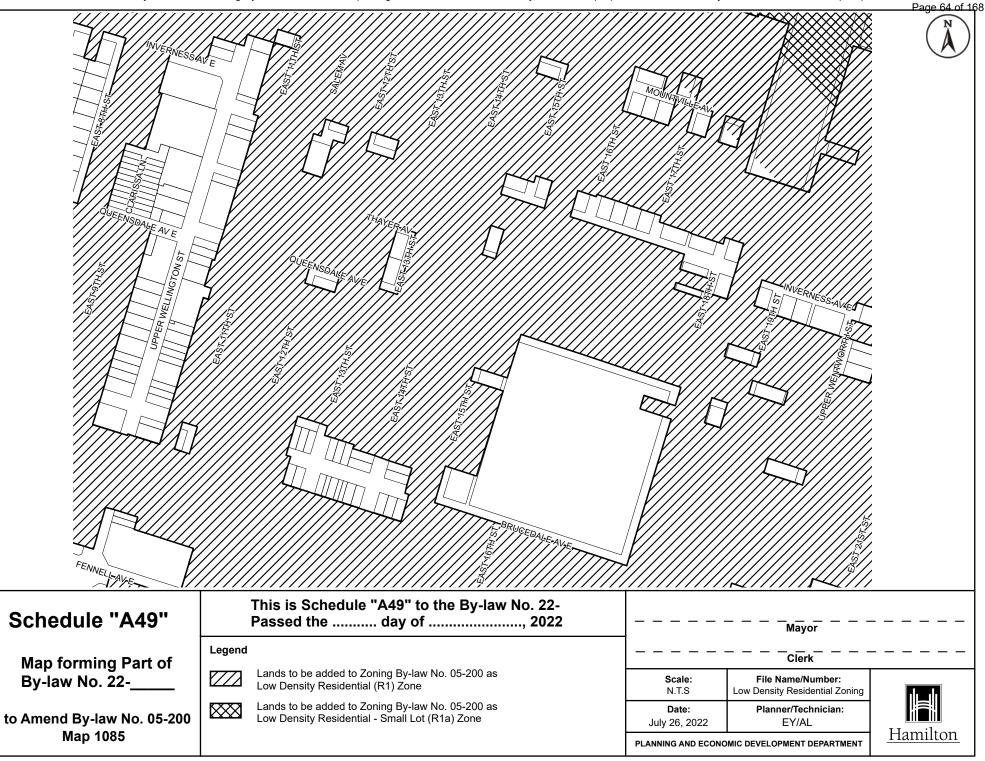


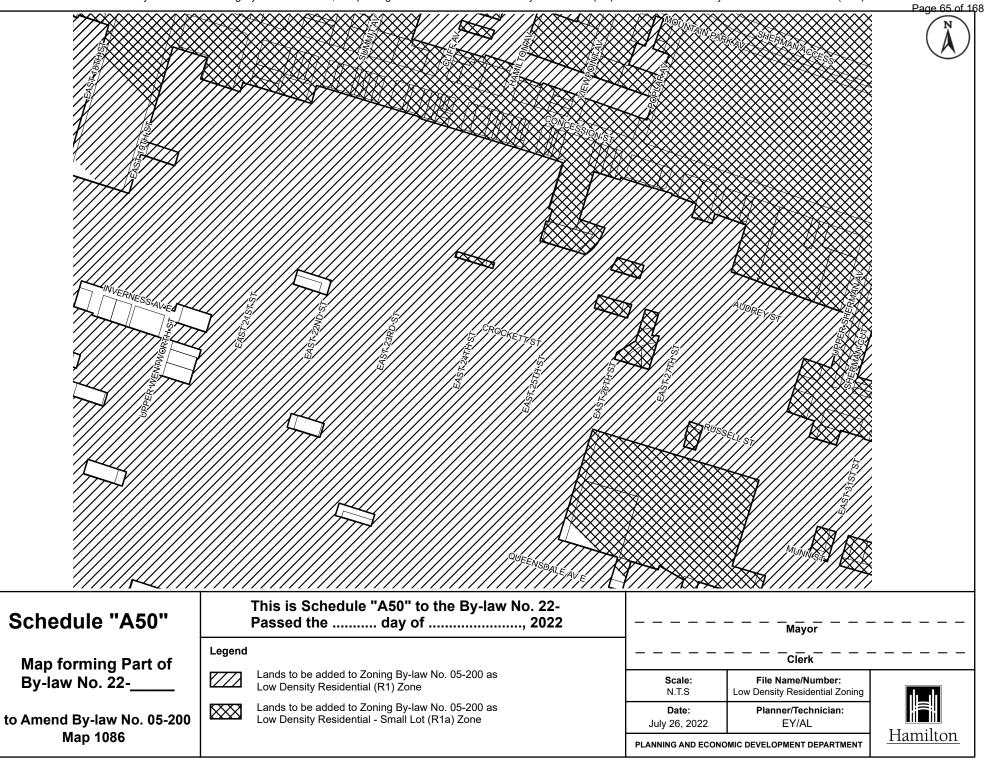


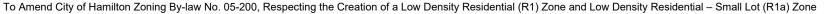


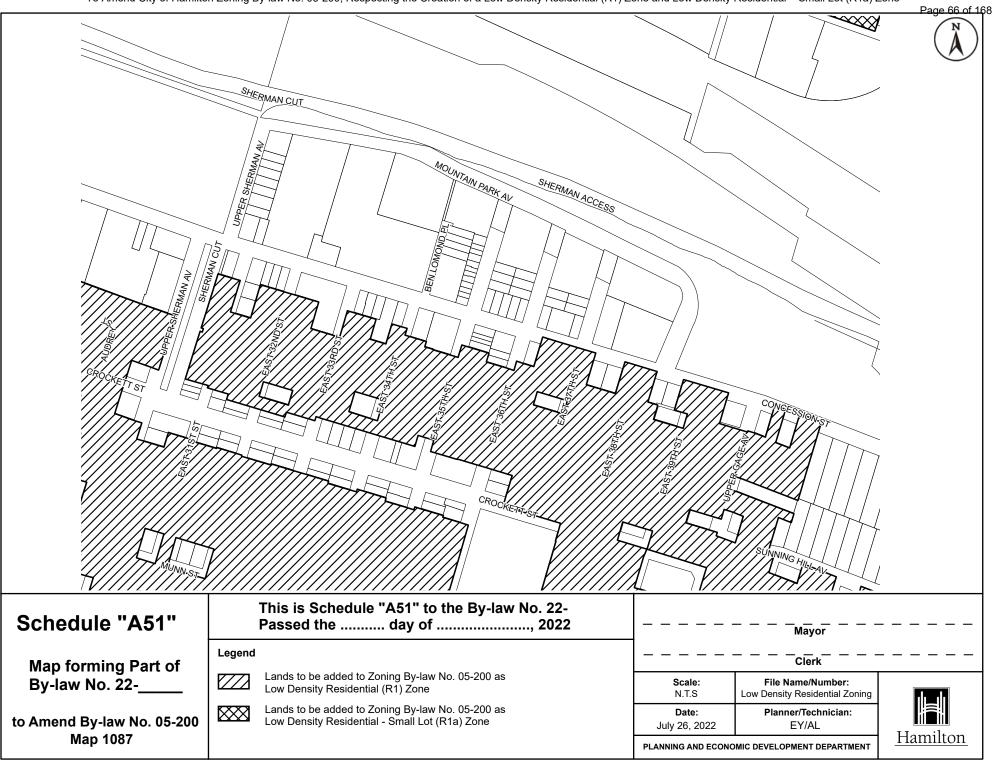


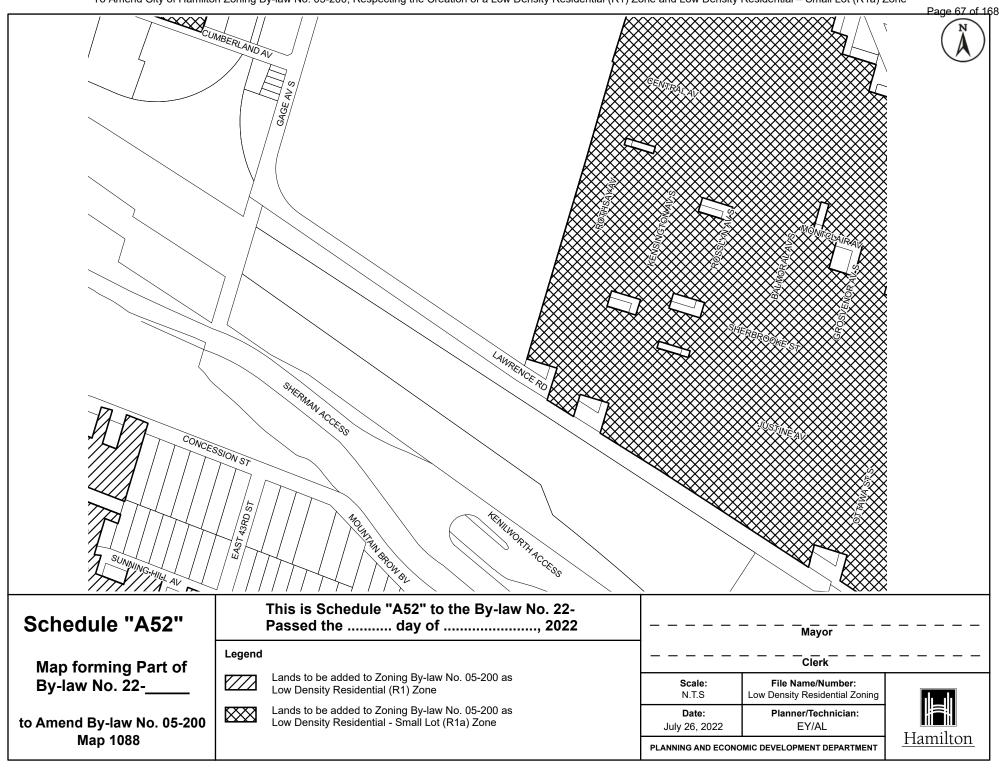


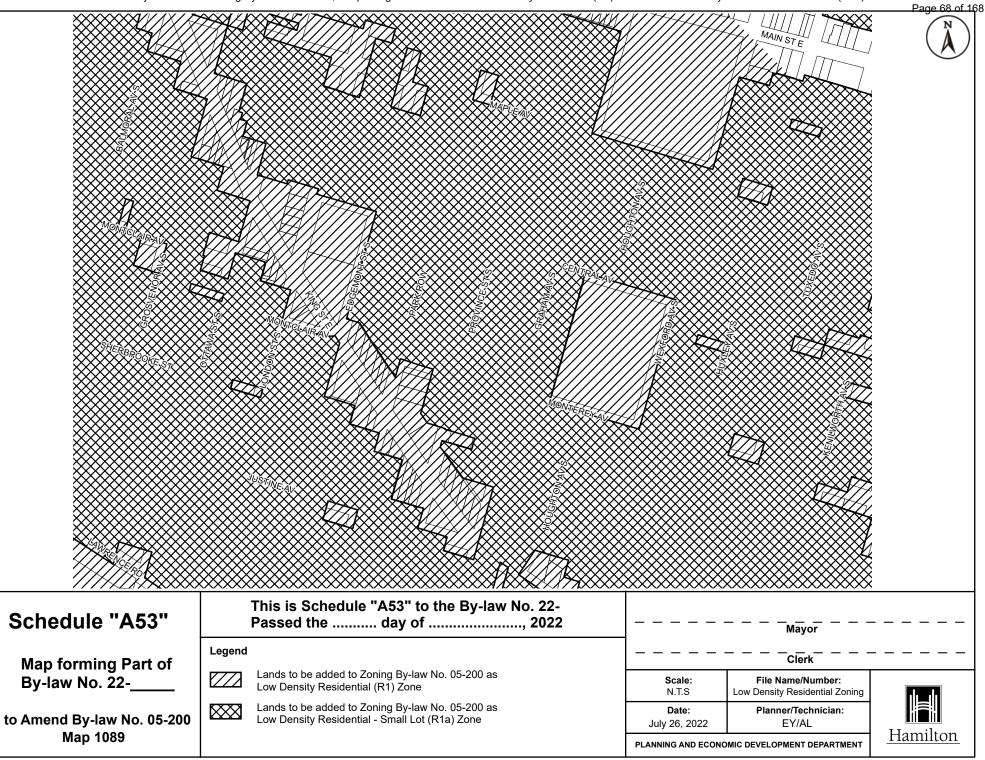


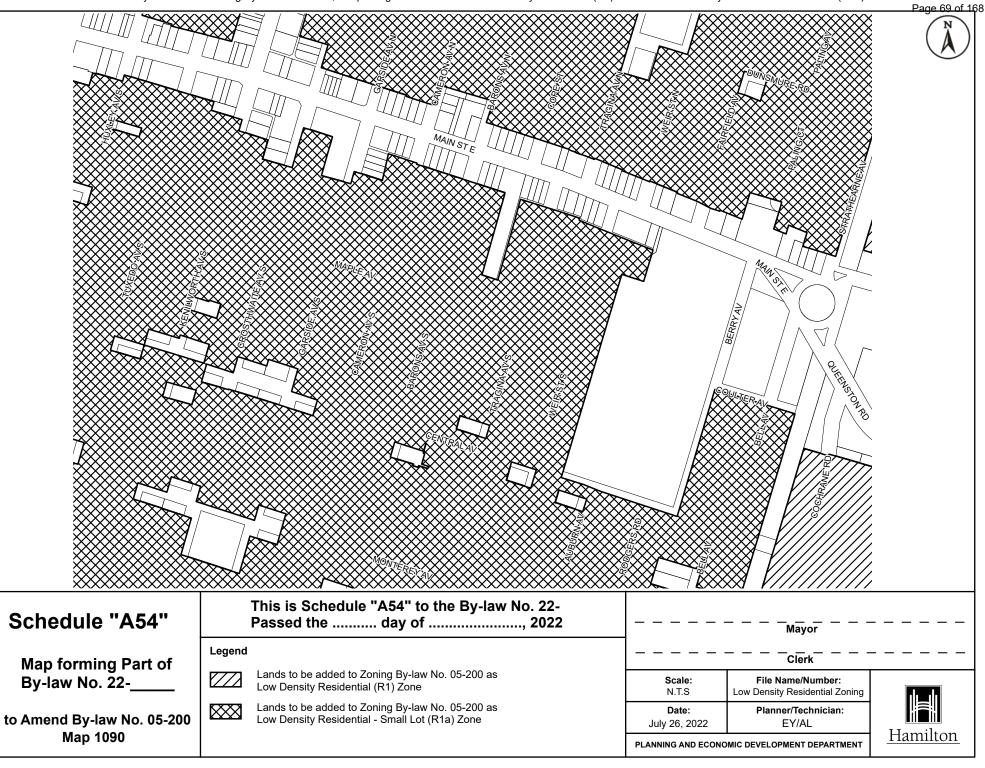


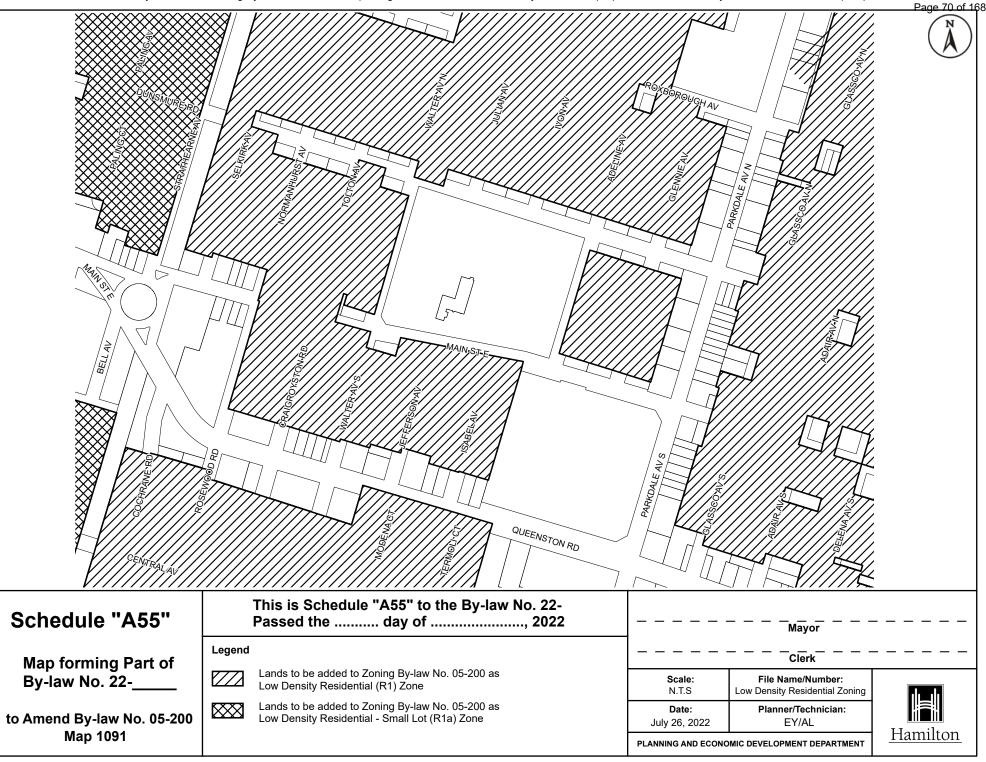


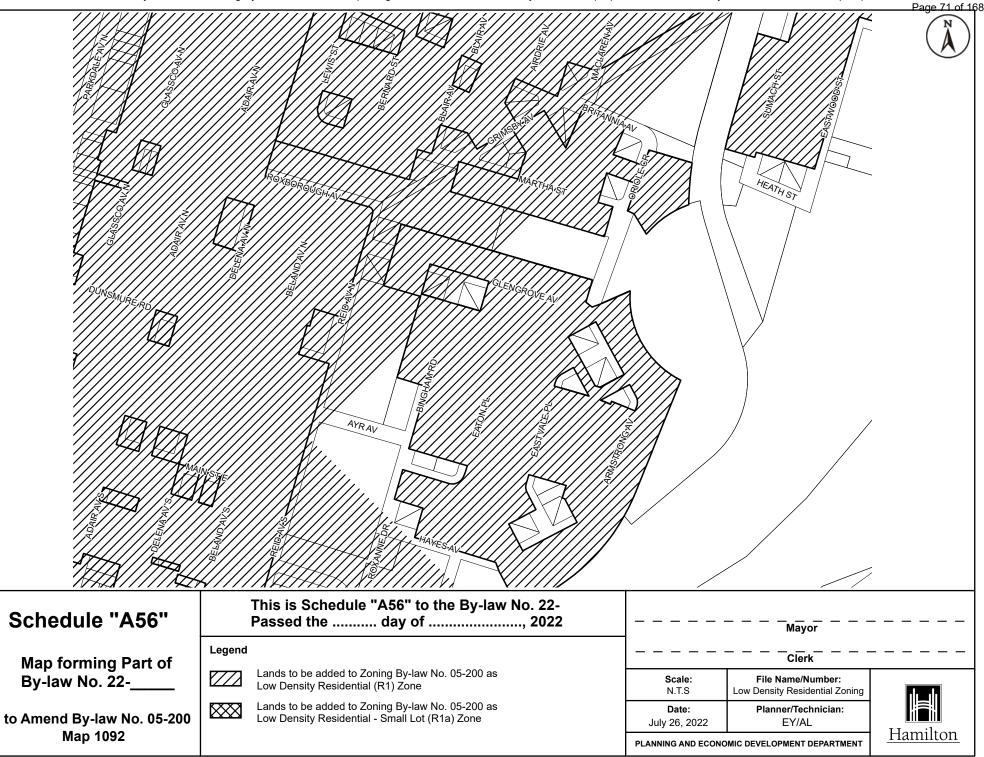


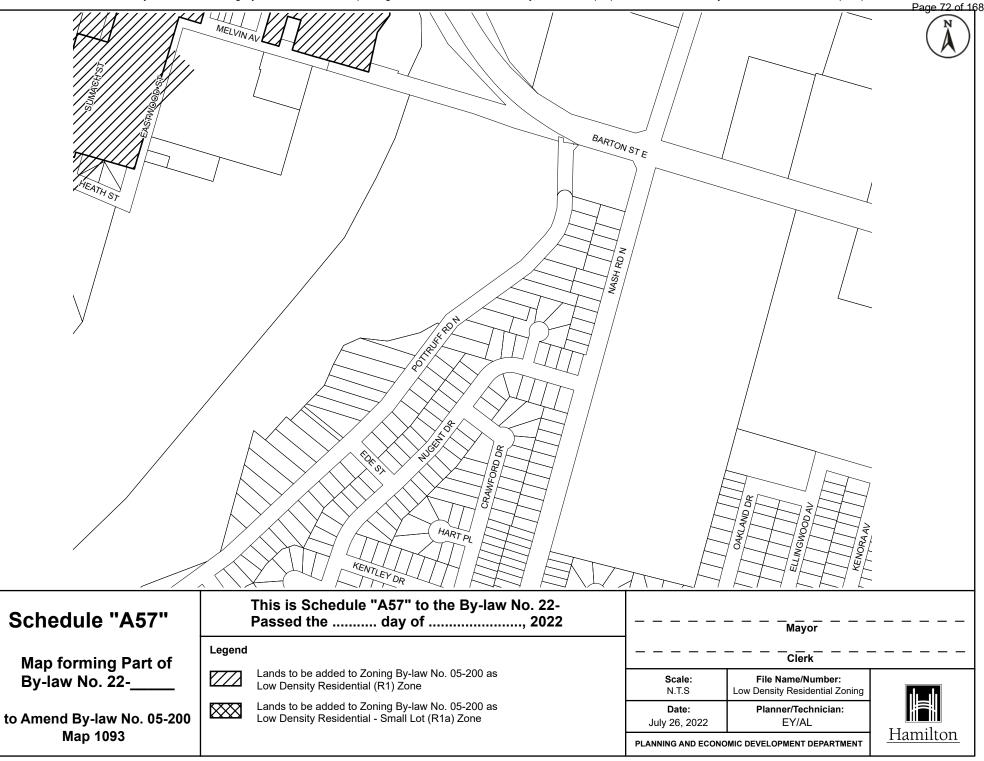




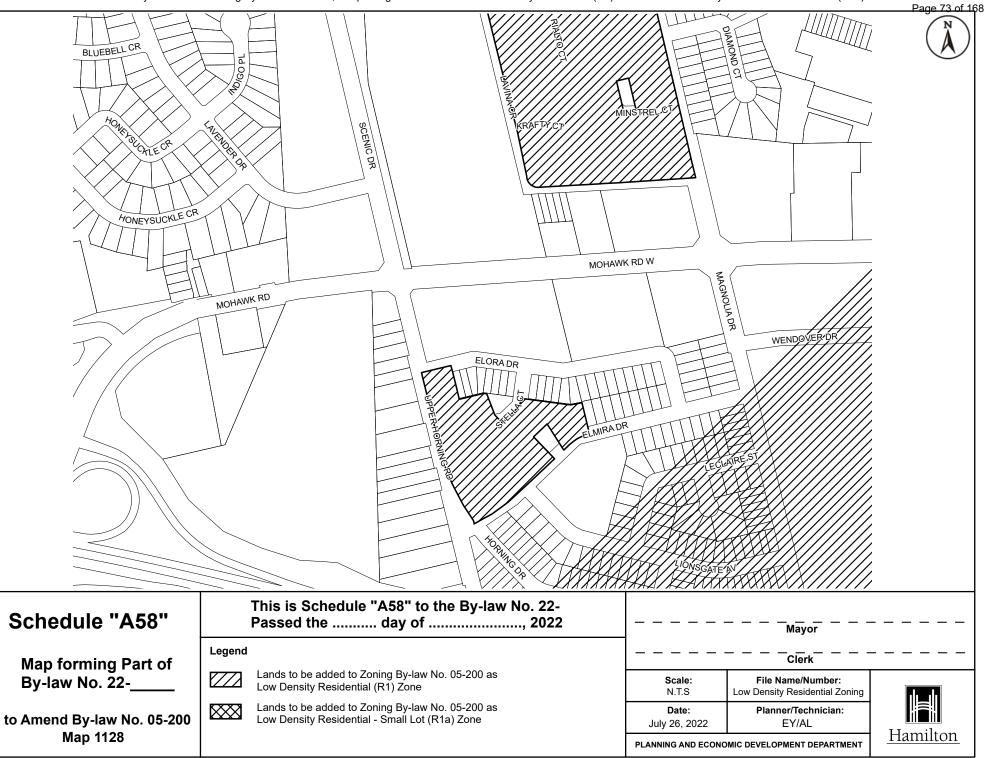


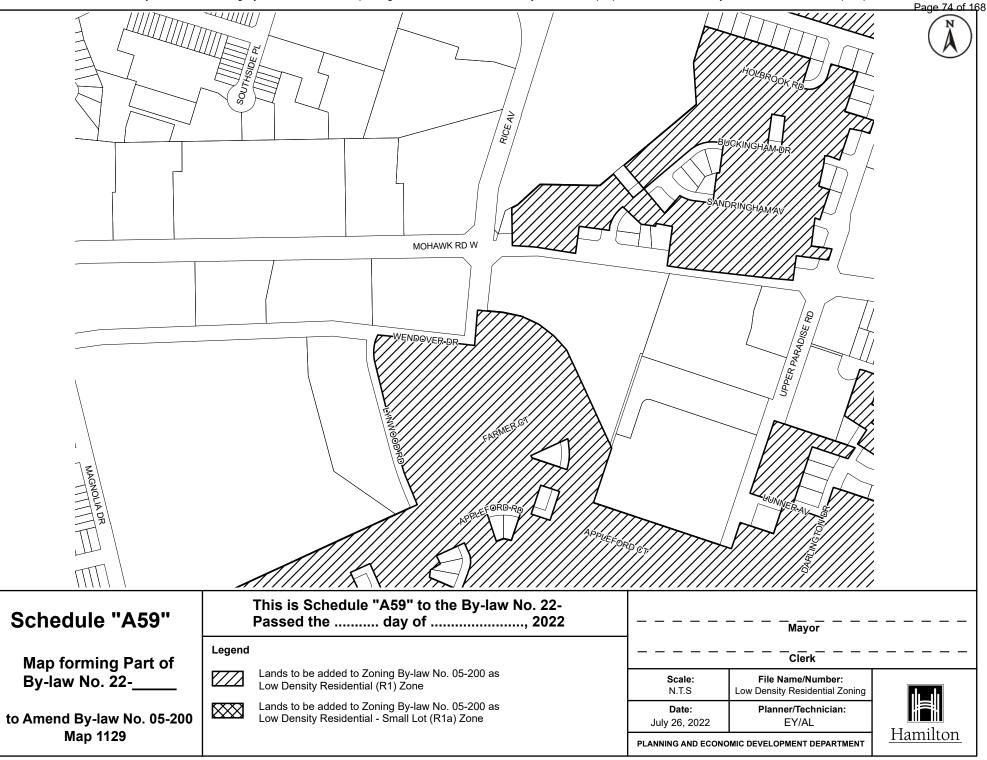


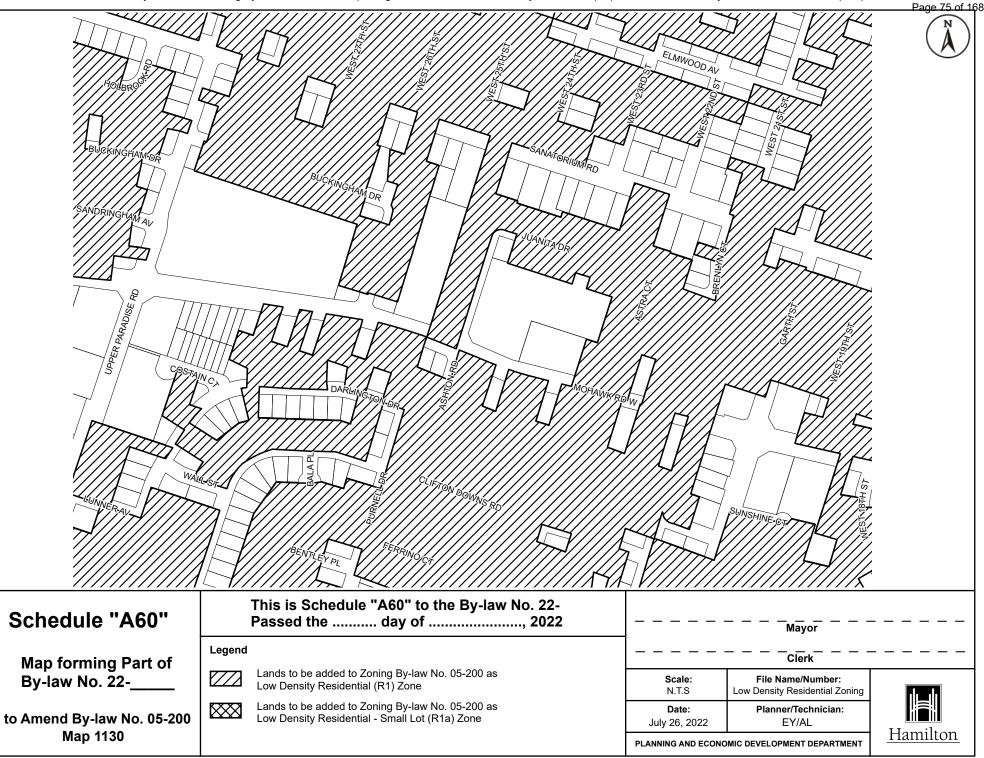


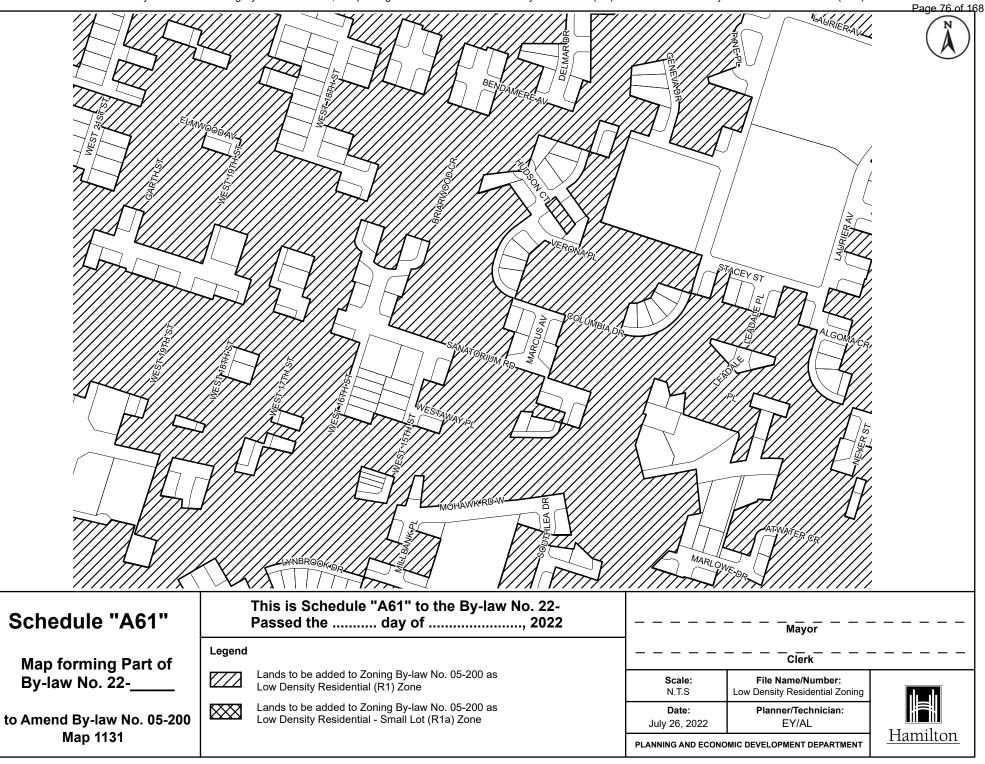


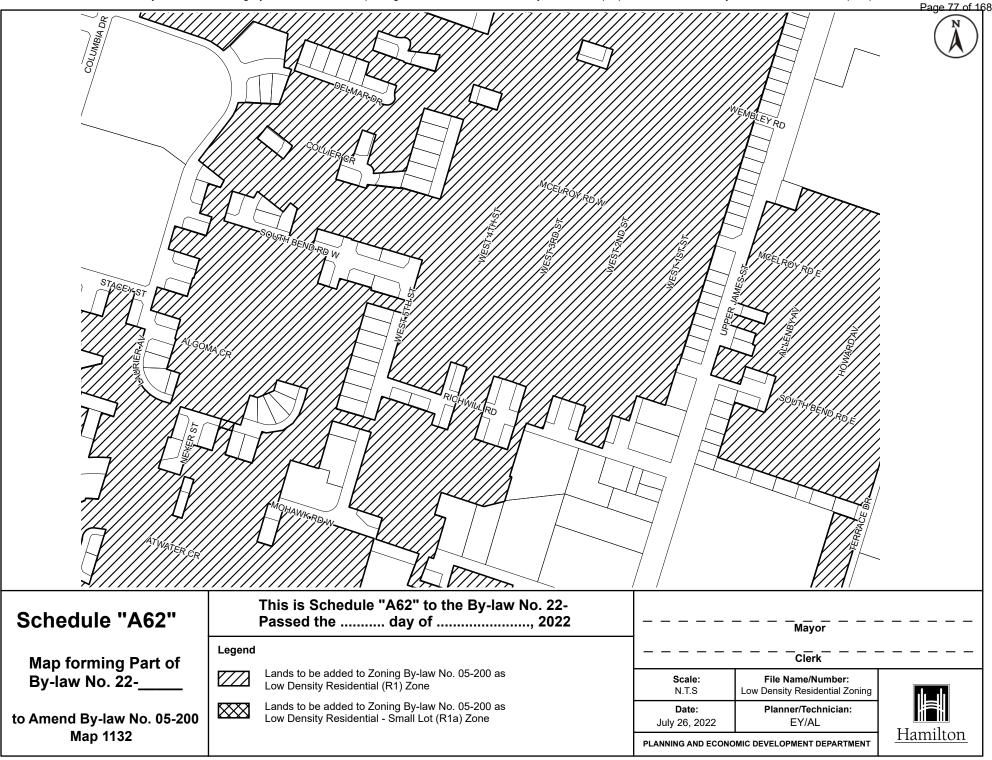


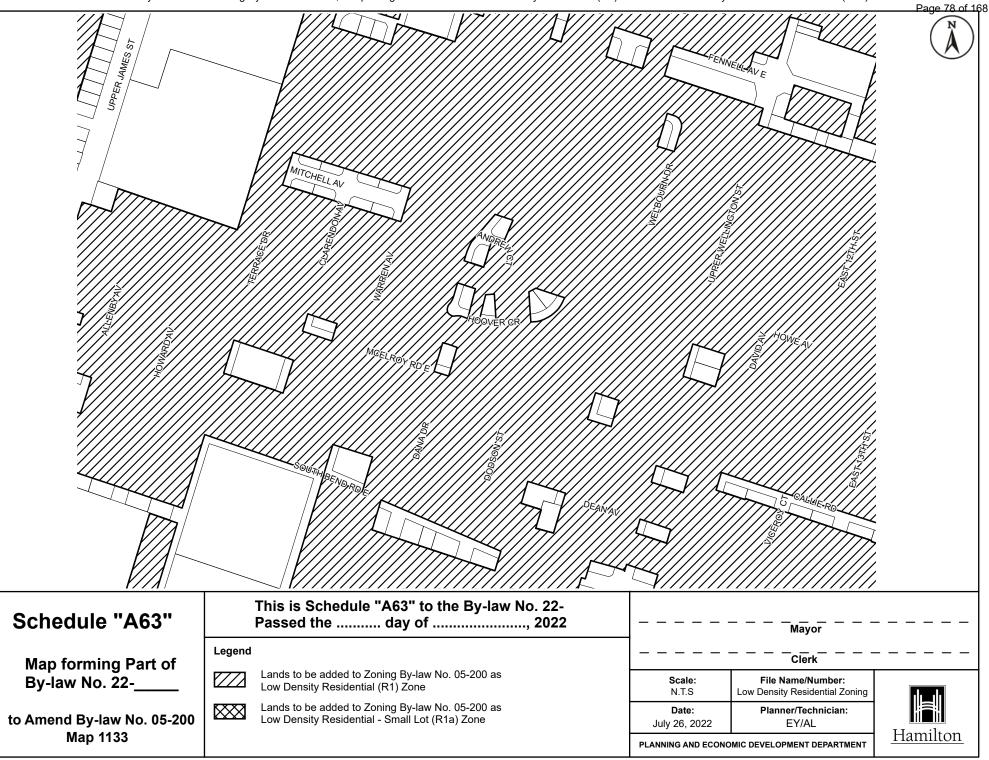


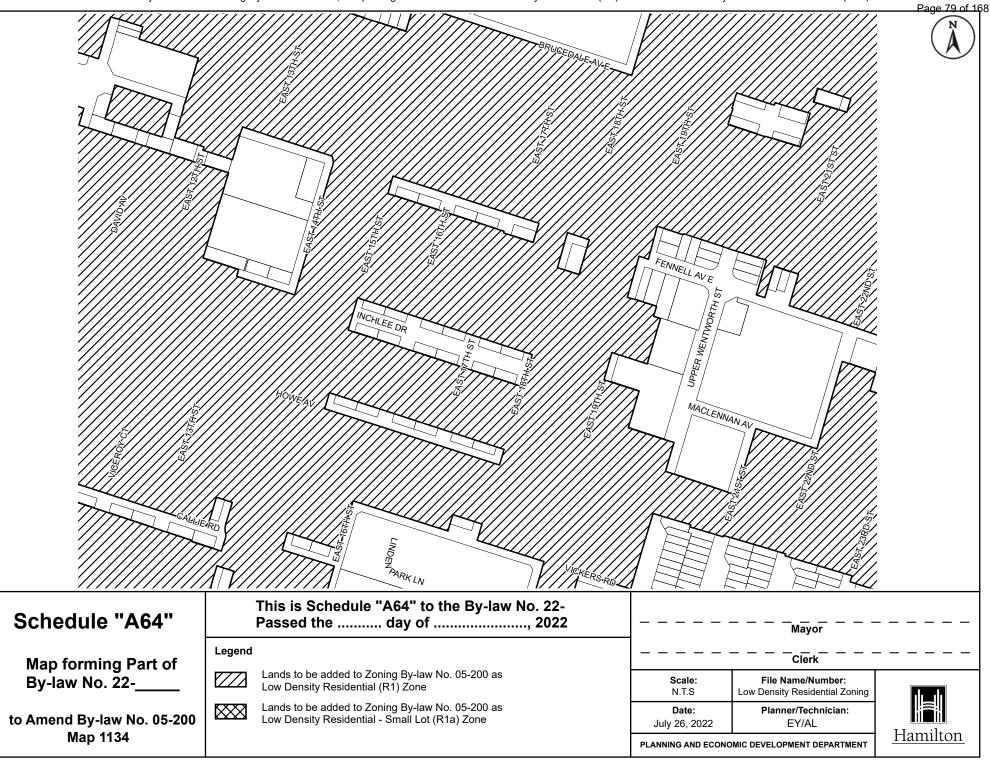


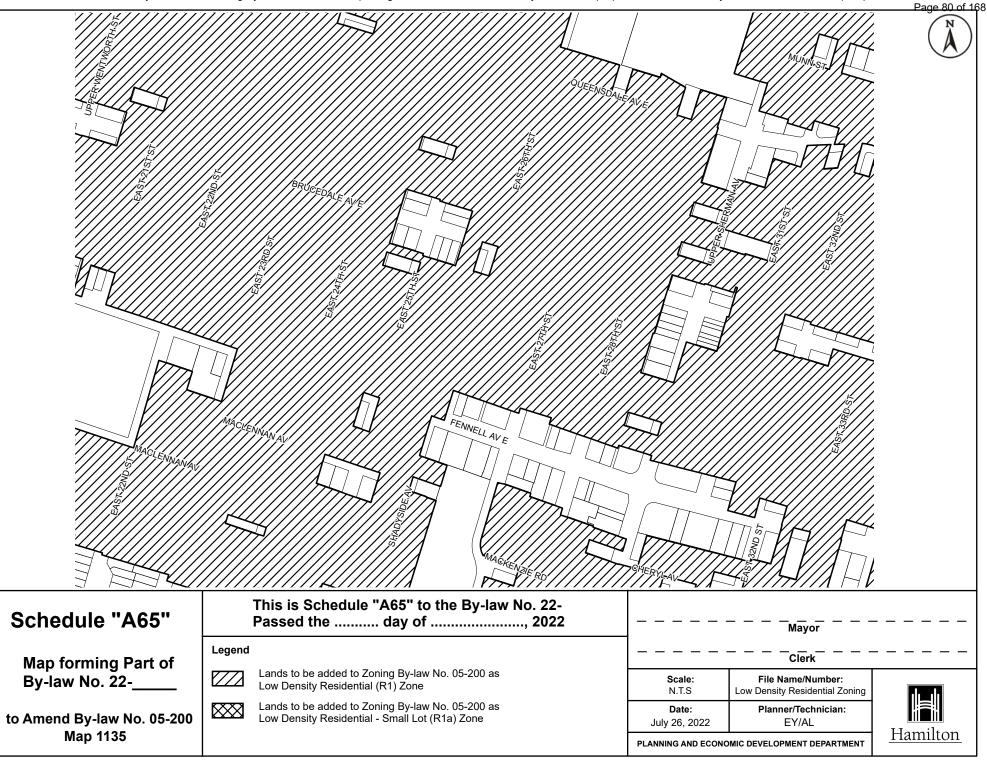


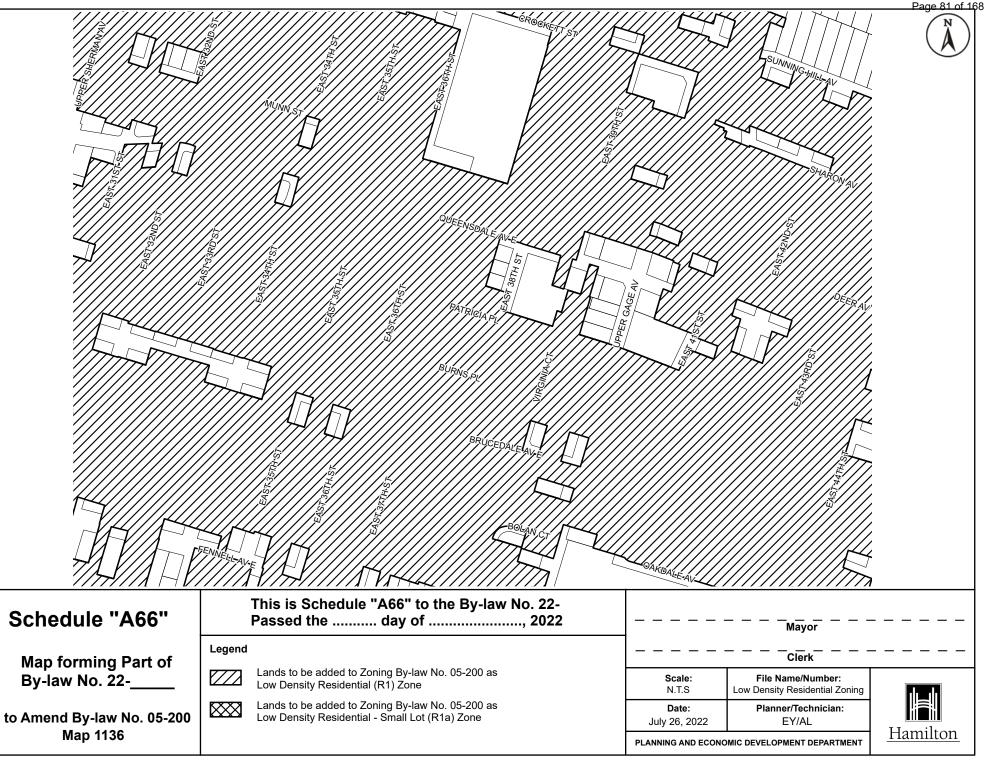


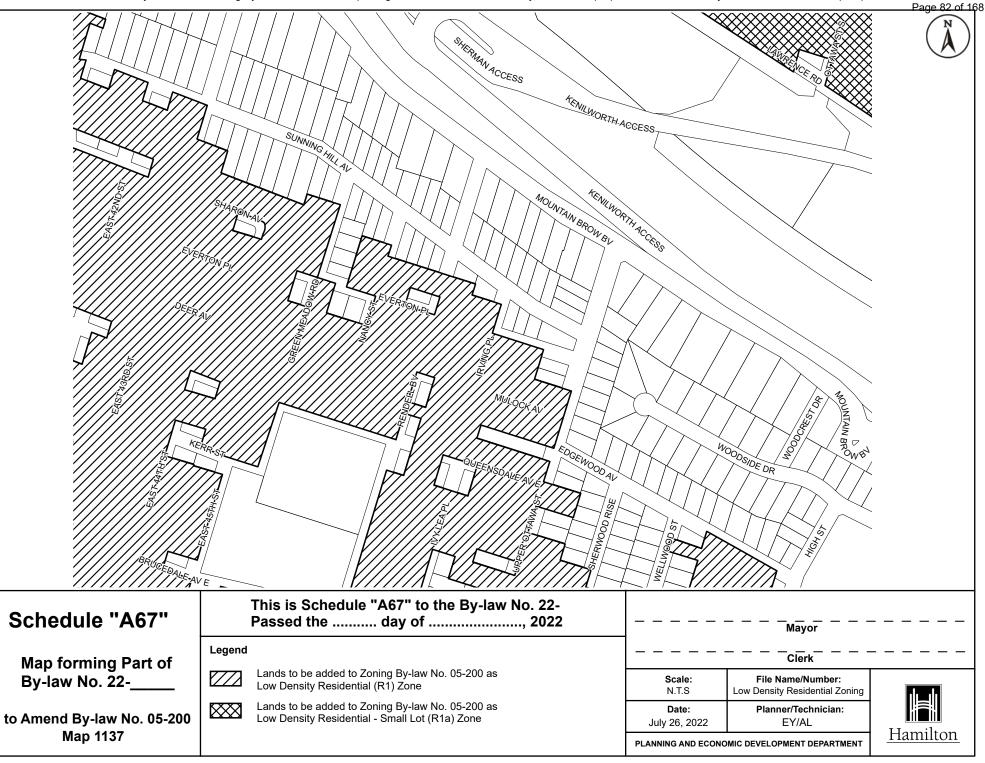


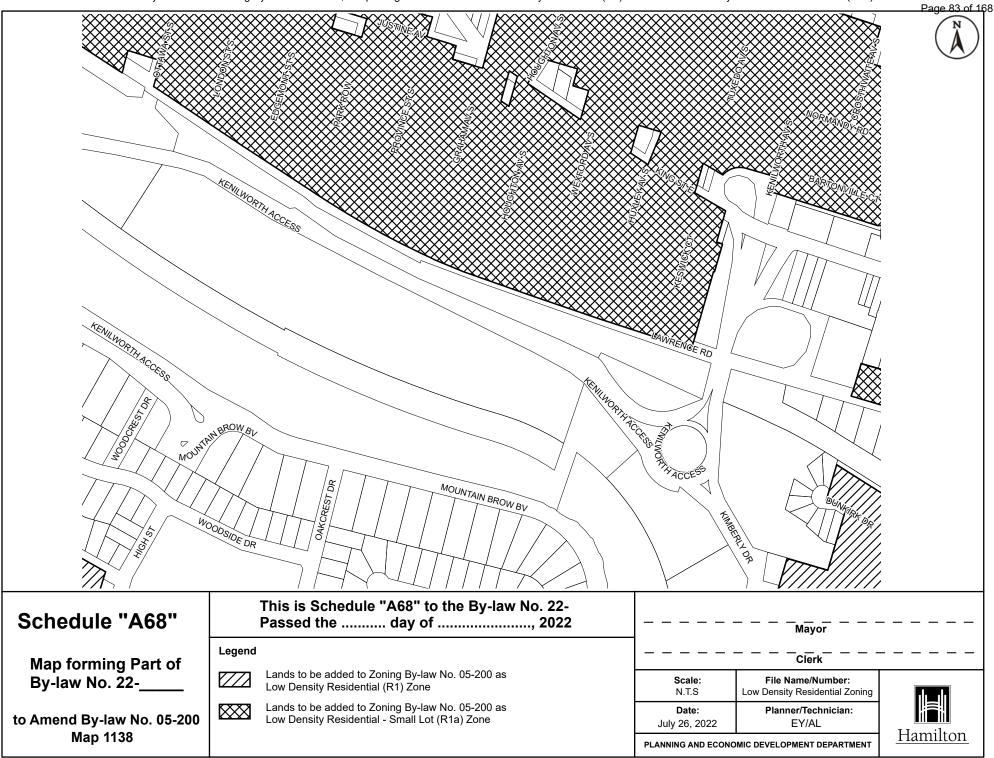


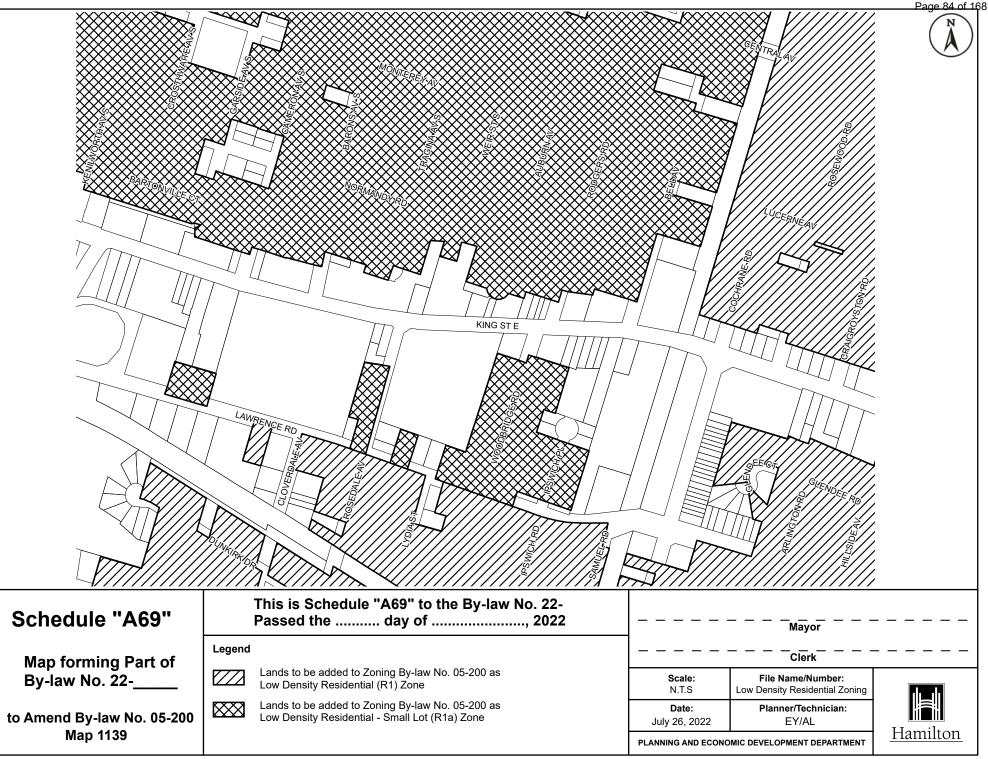


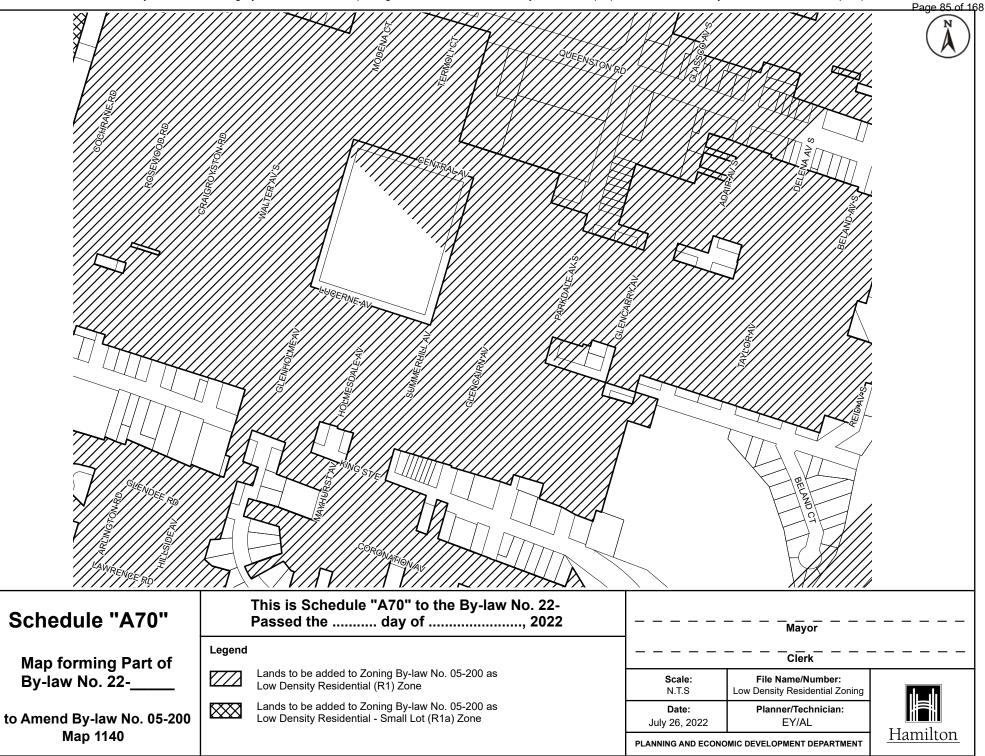


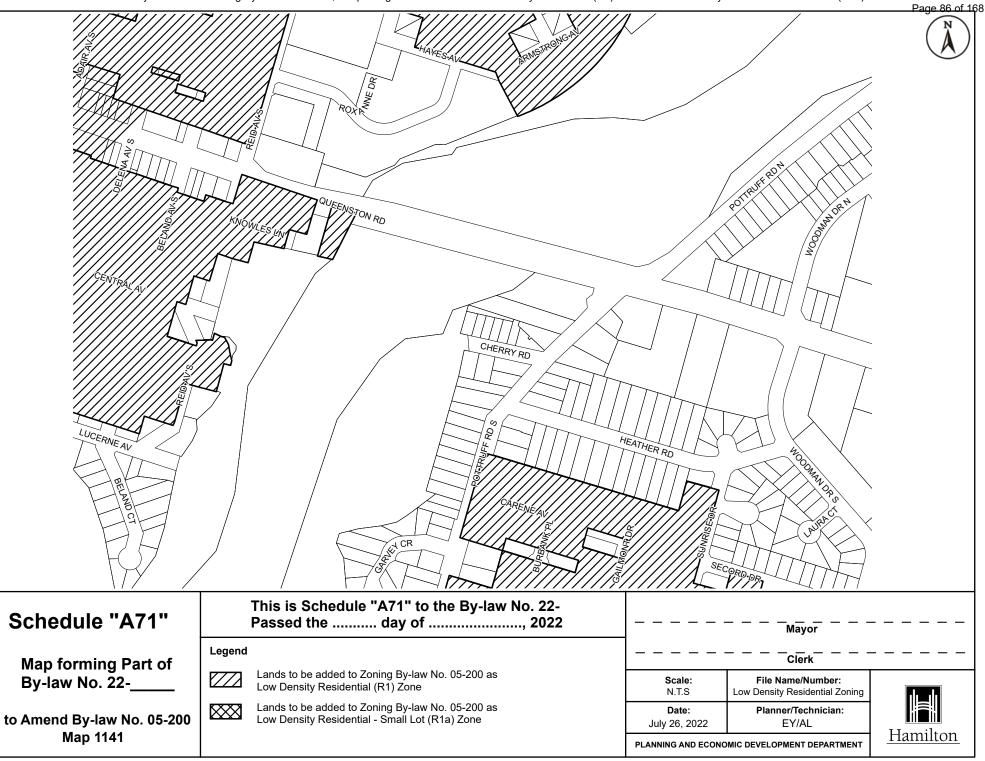




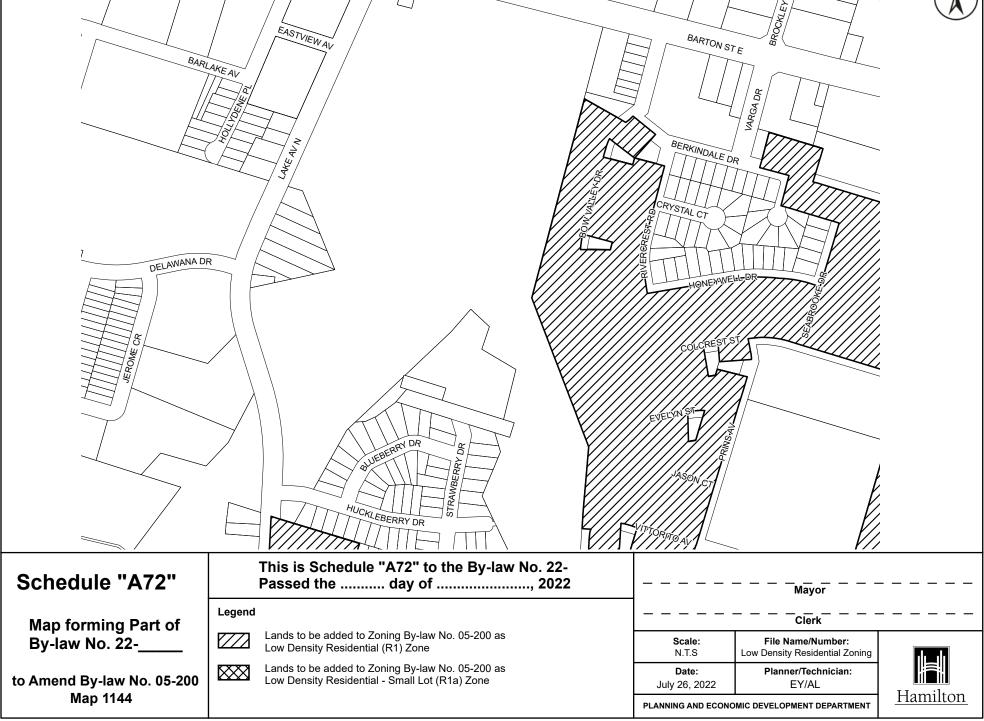


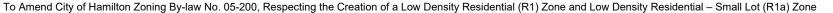


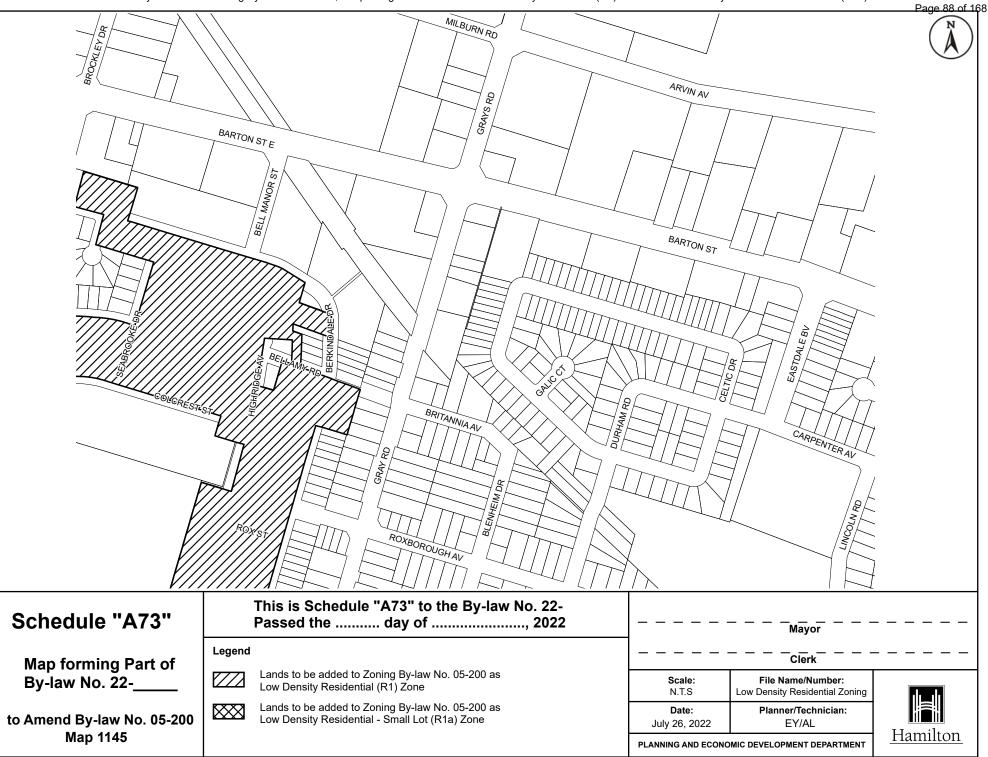


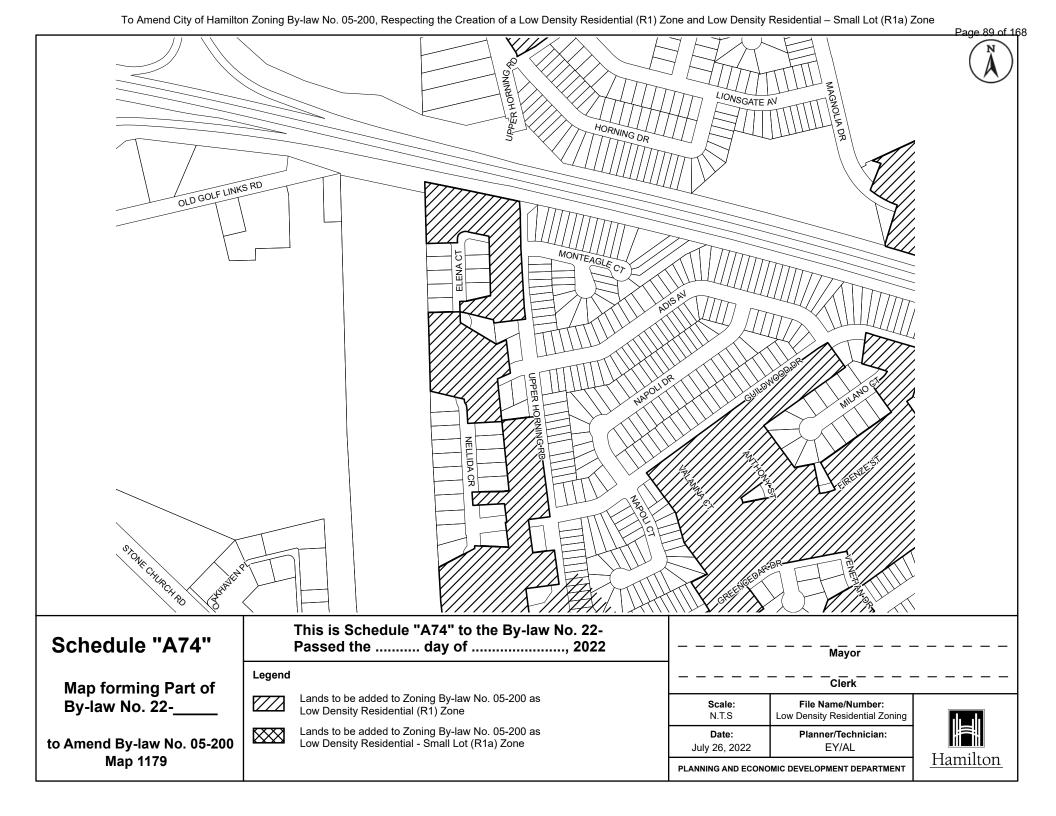


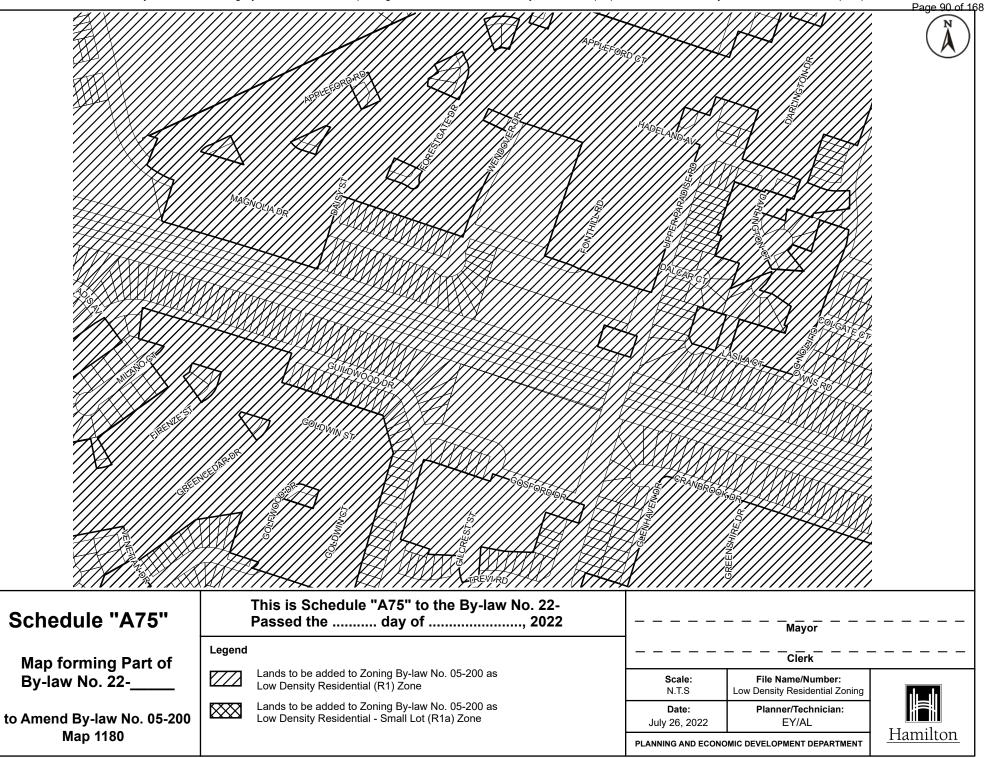


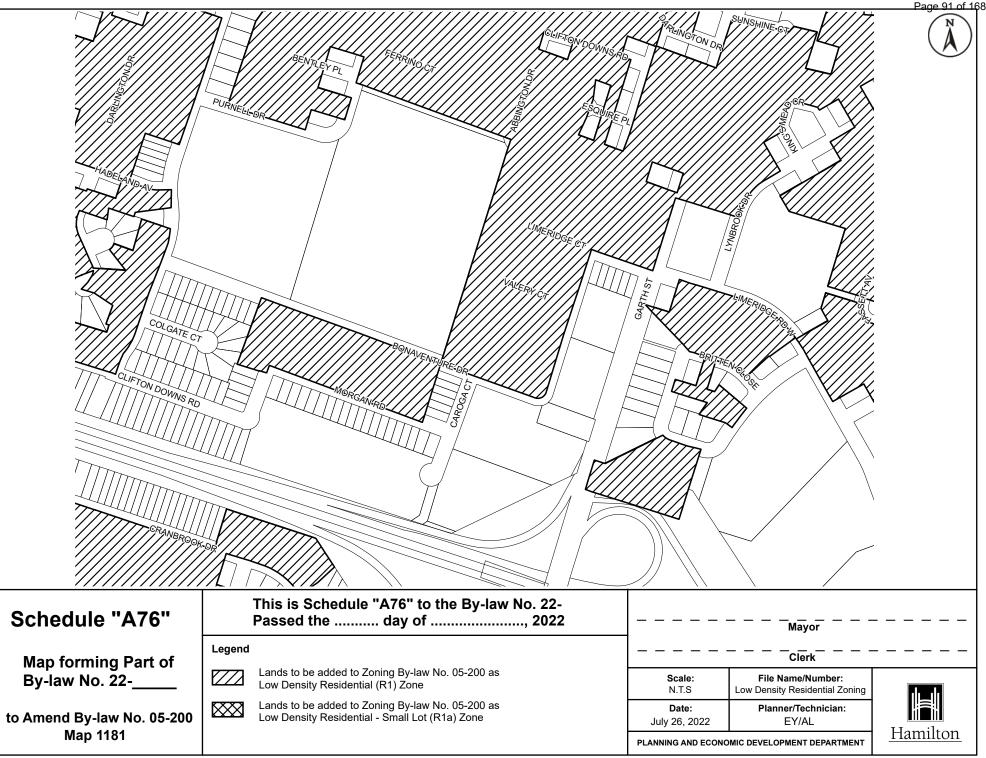


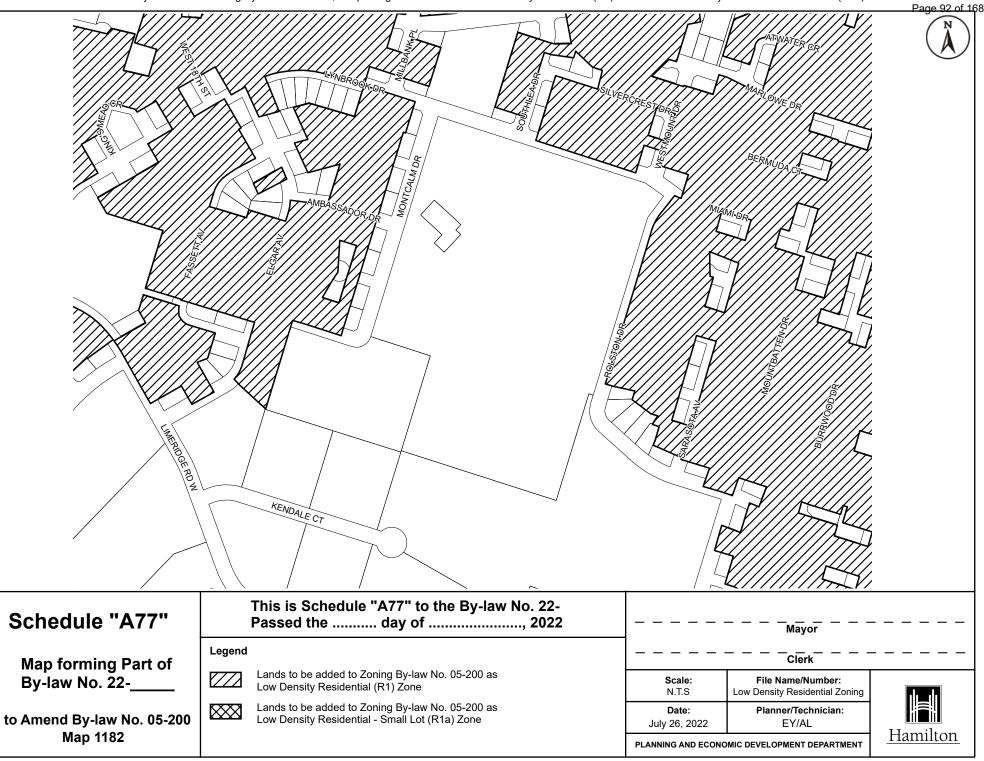


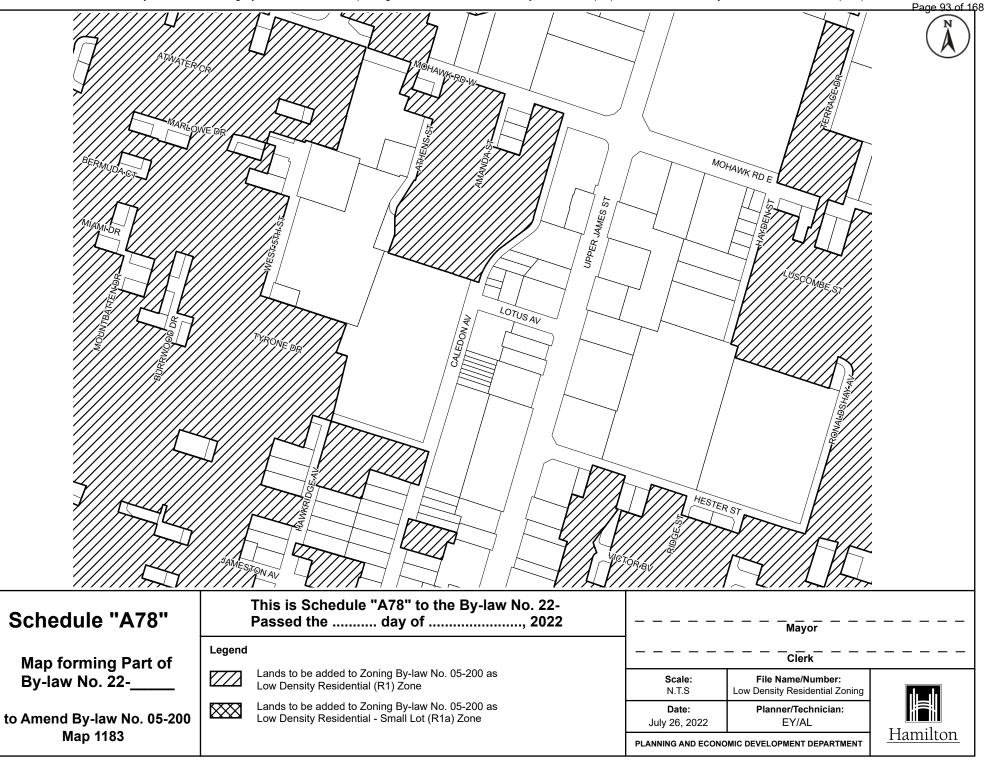


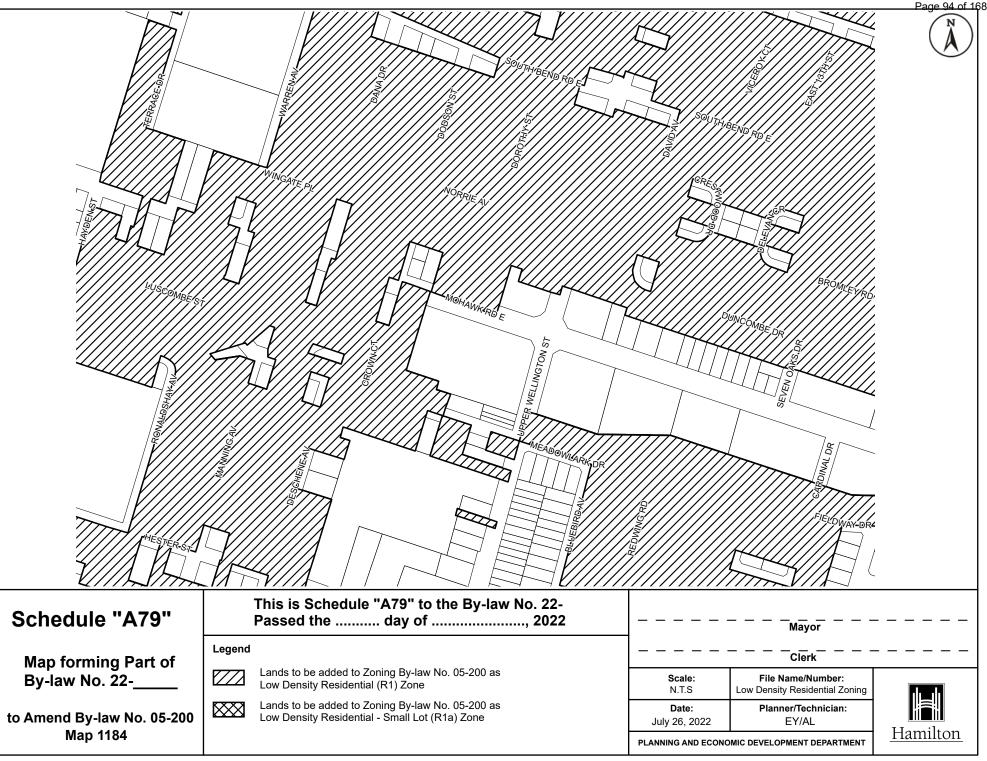


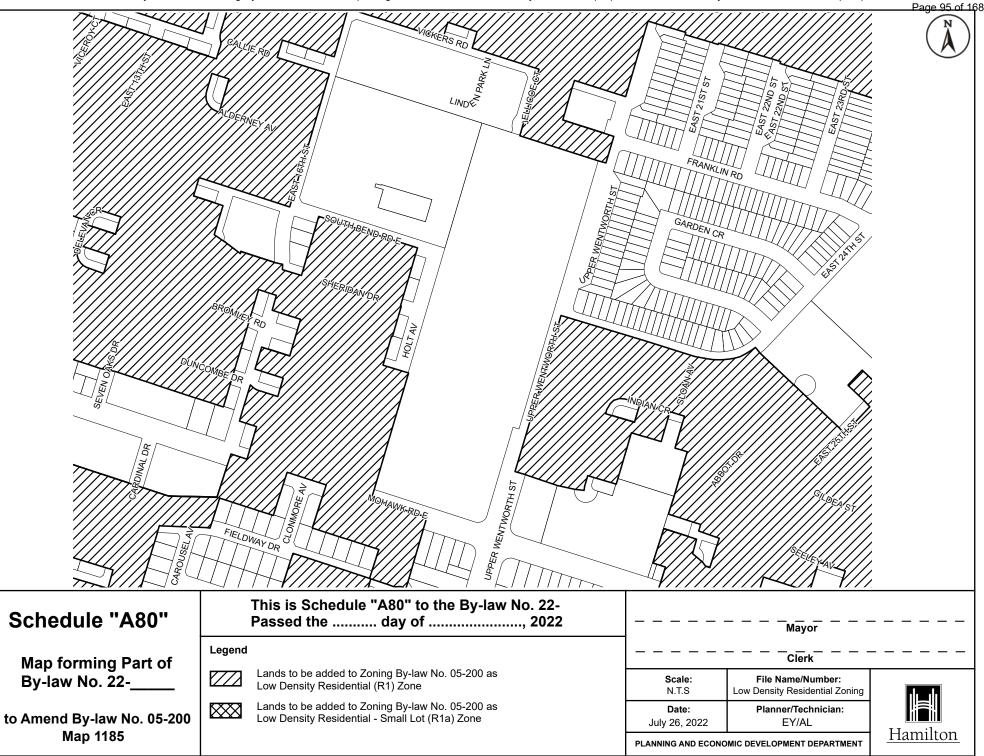


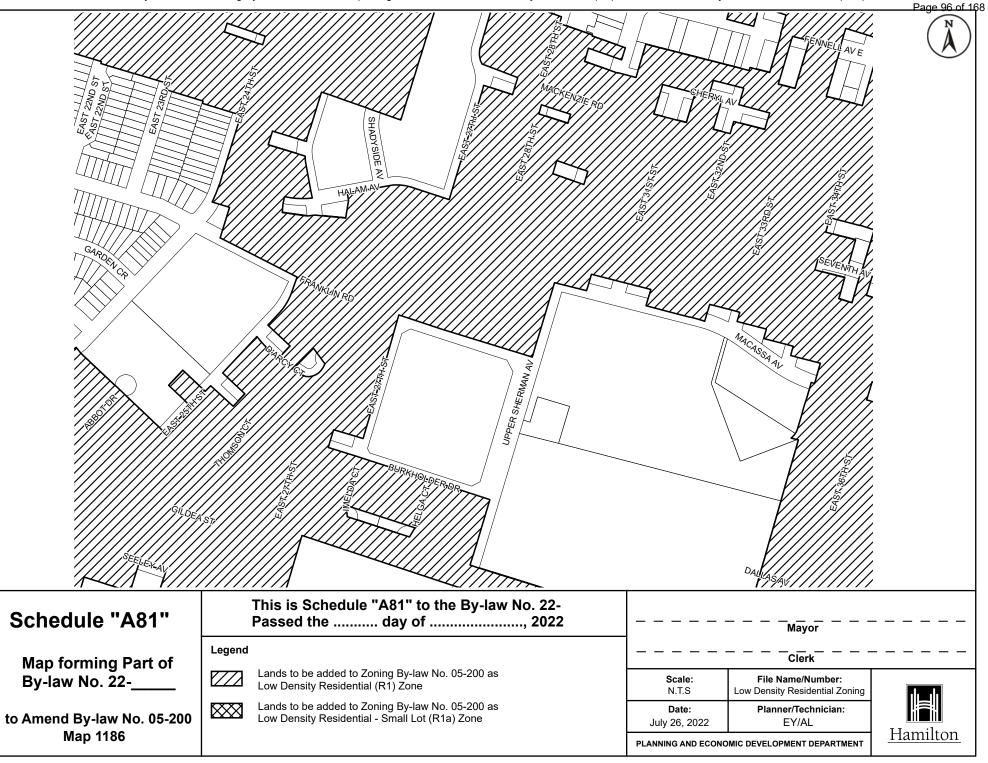


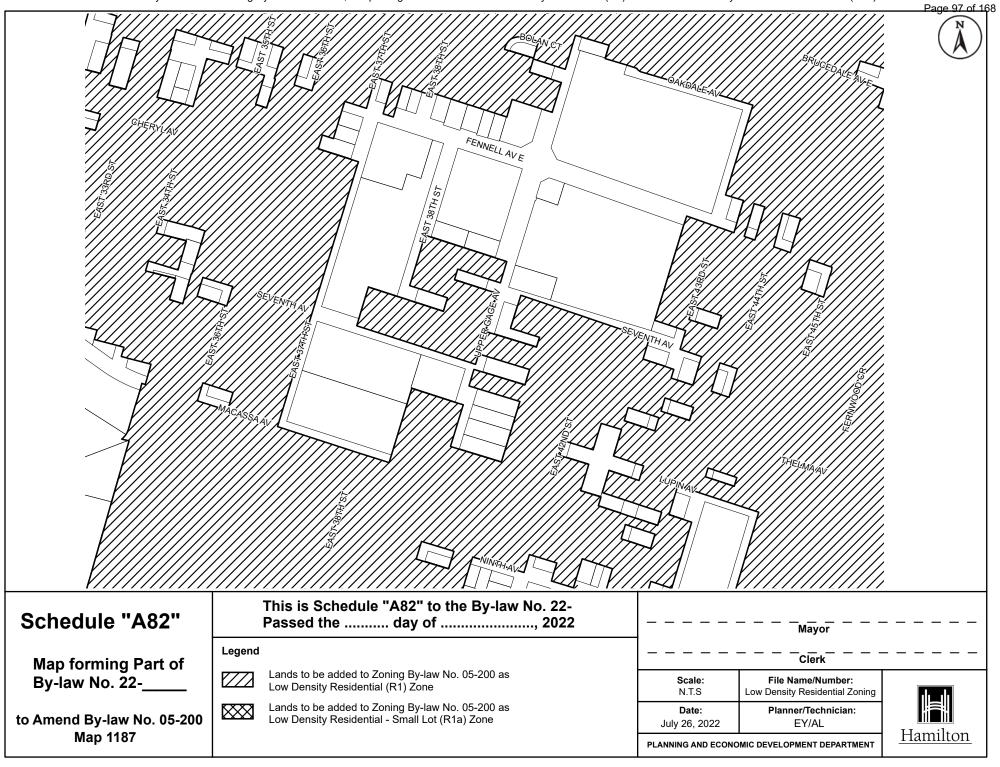


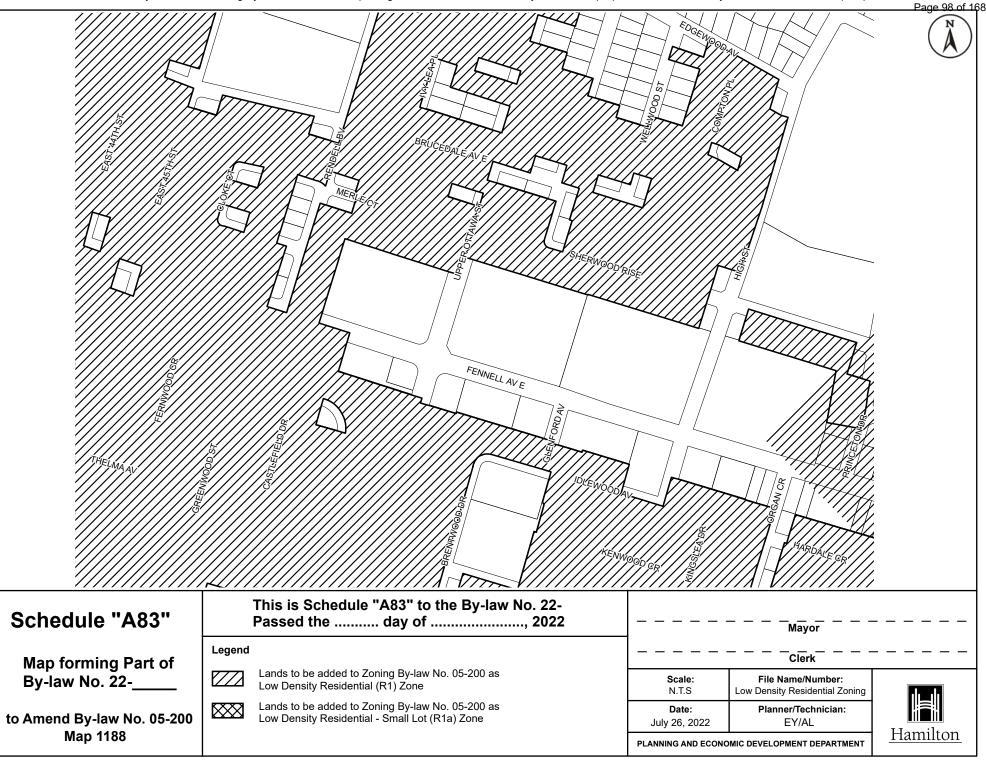


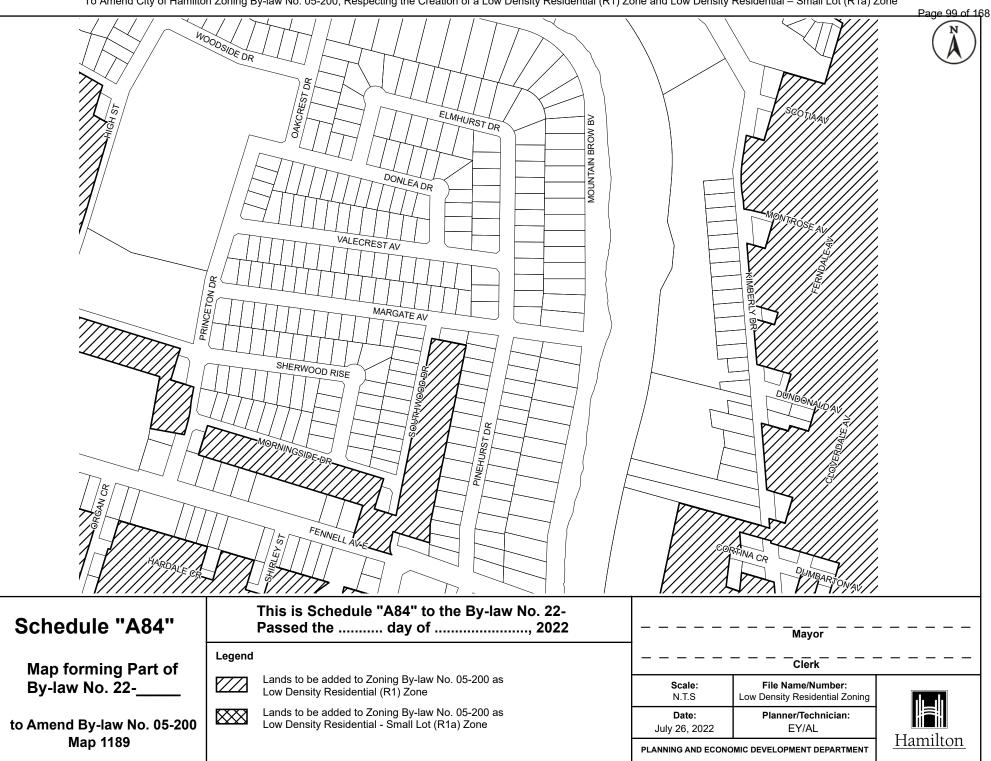




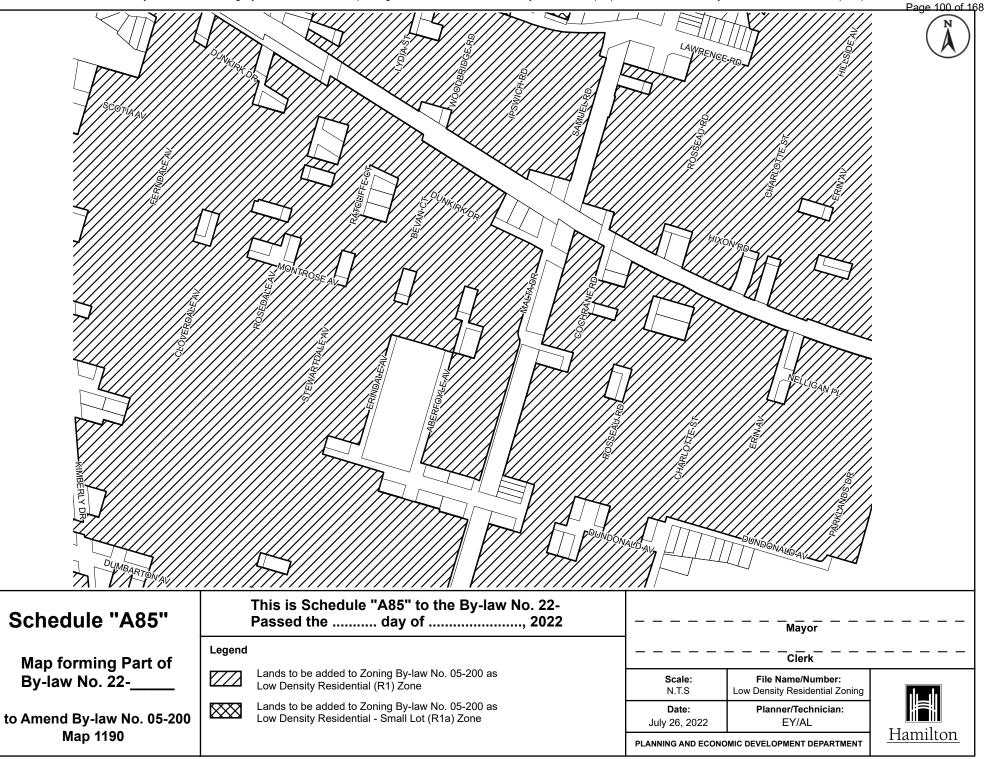


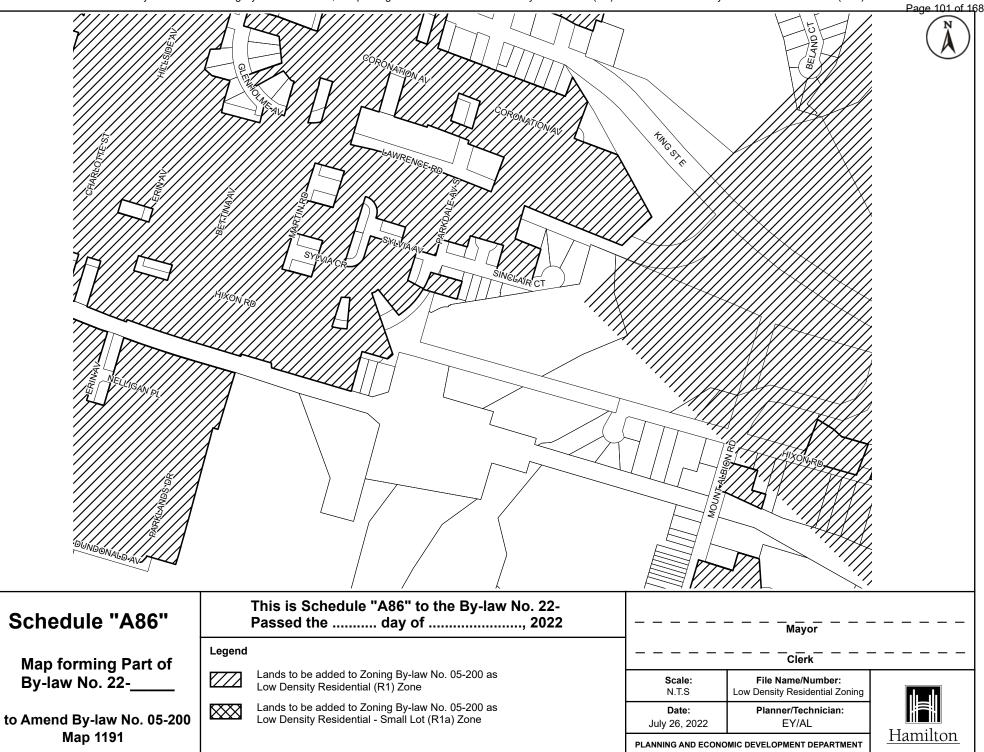


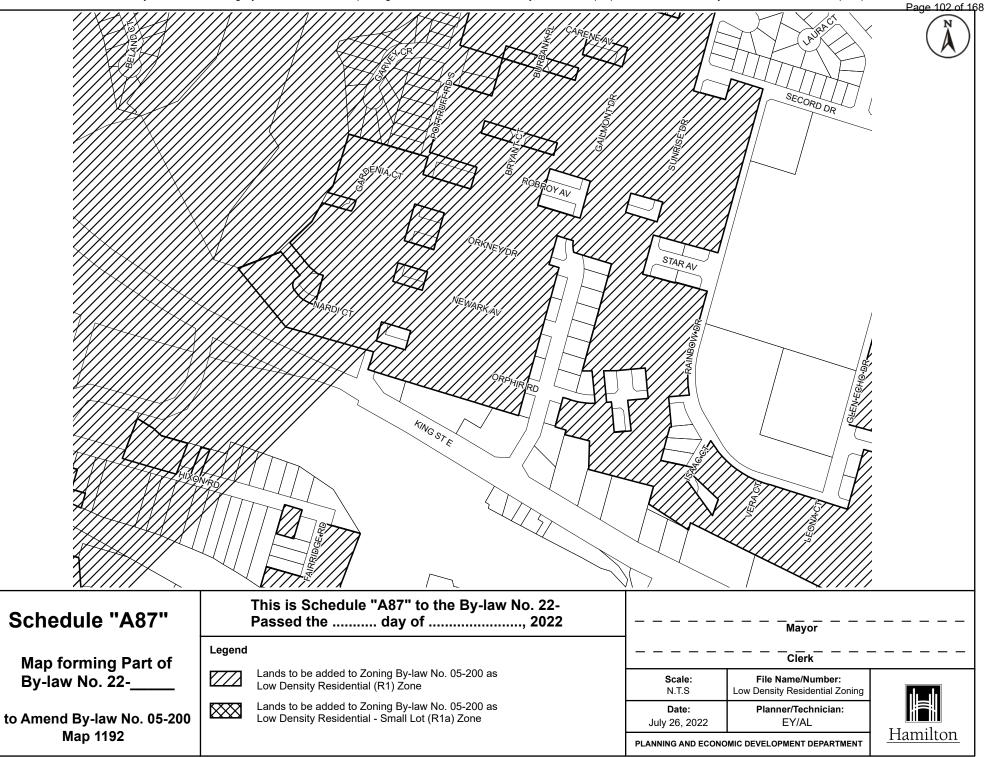


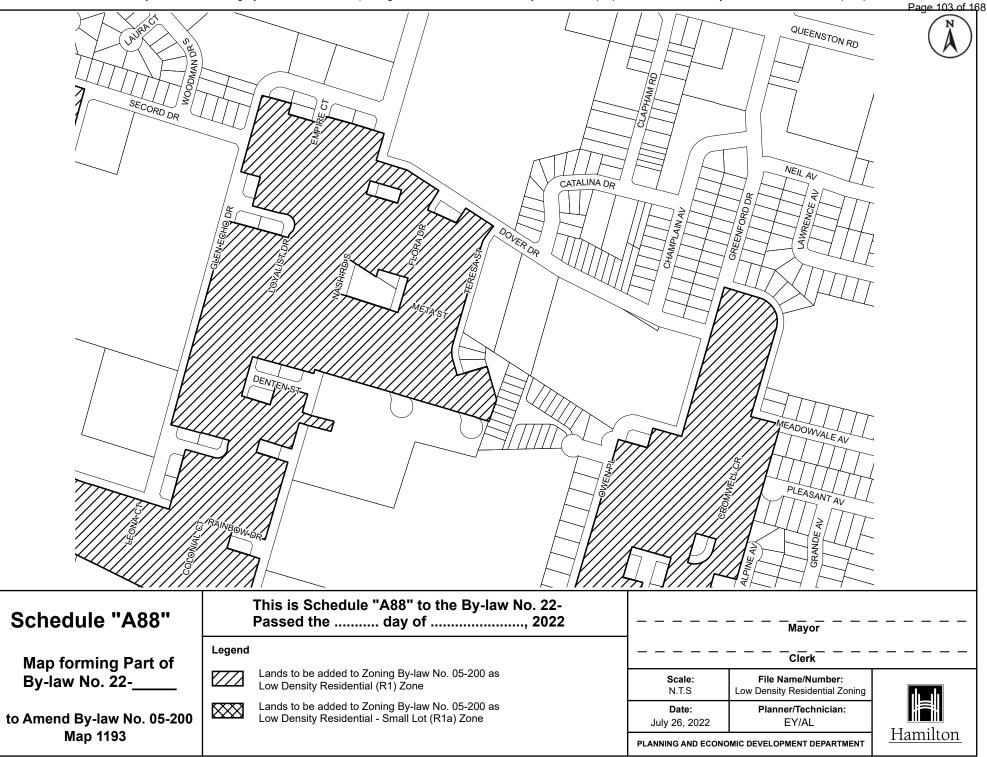


To Amend City of Hamilton Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Zone and Low Density Residential – Small Lot (R1a) Zone

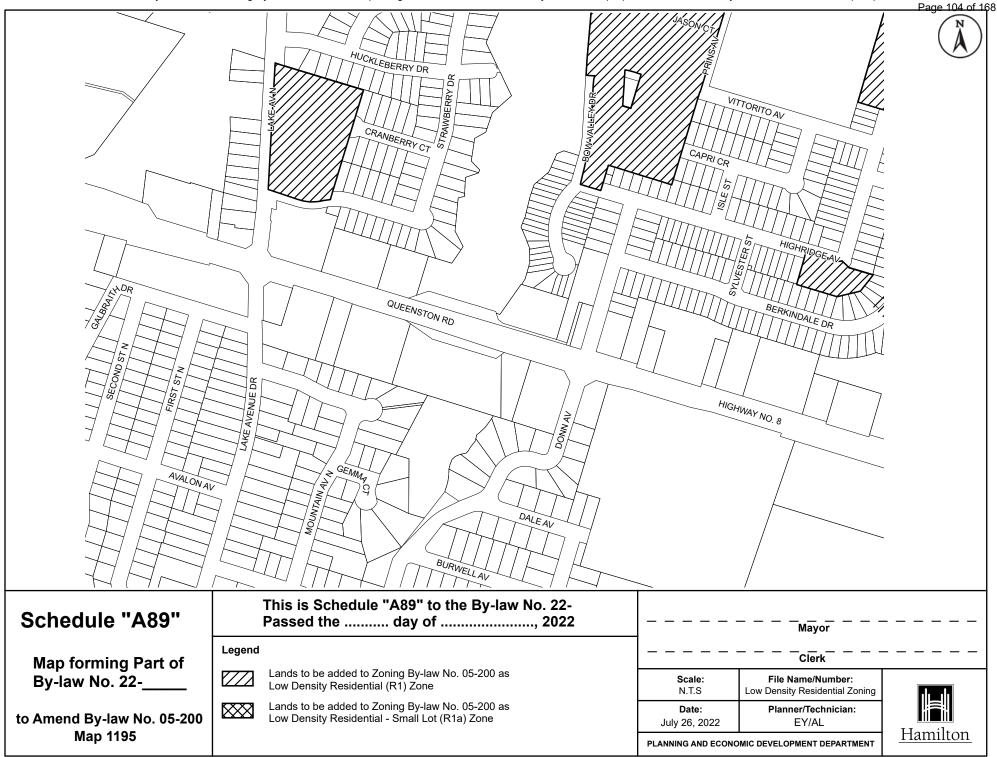


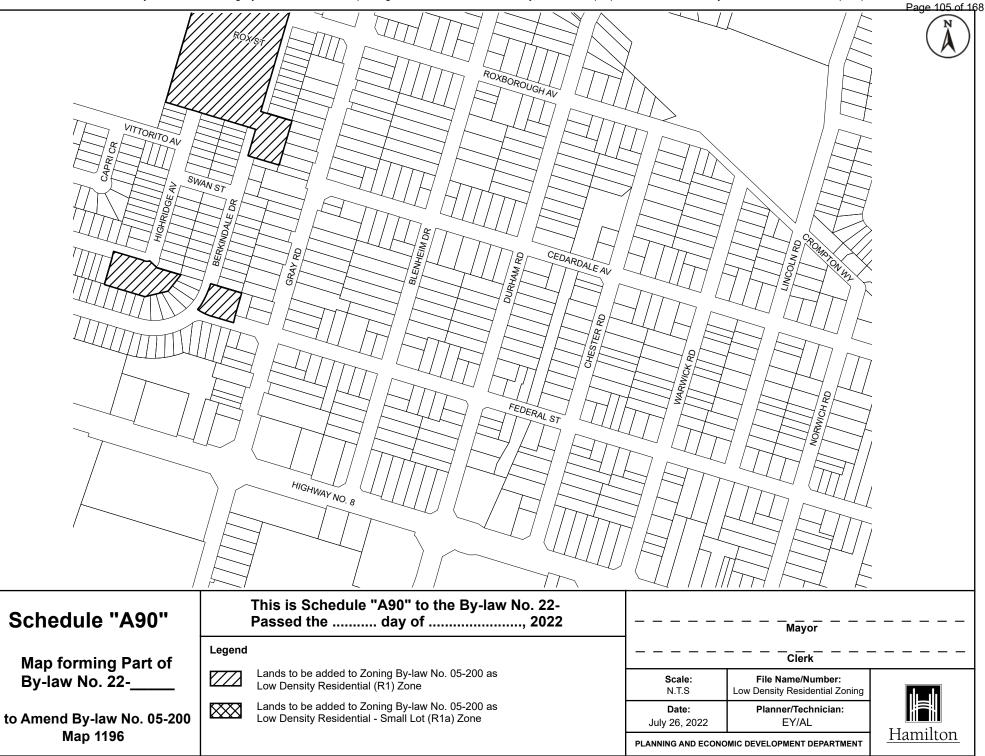


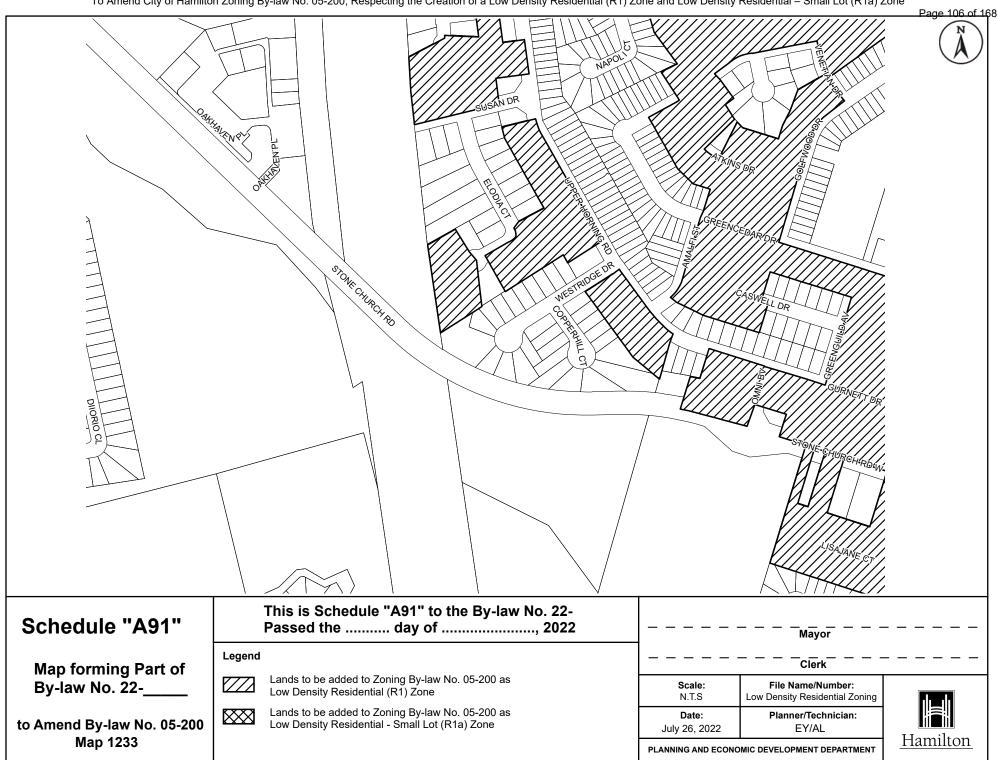




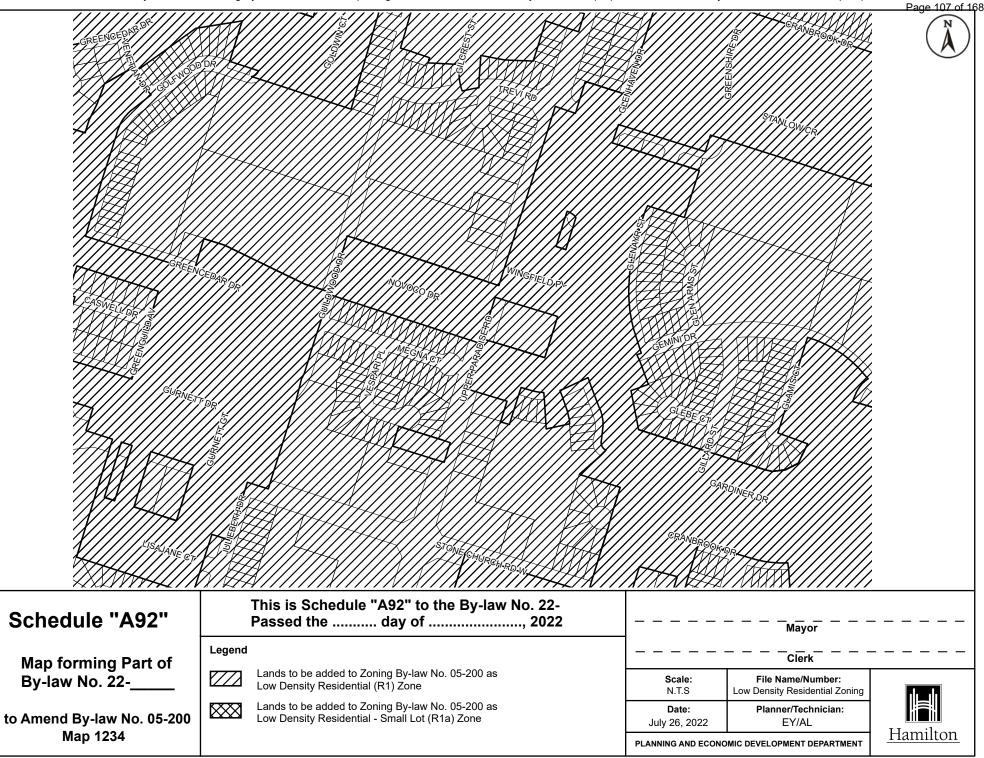


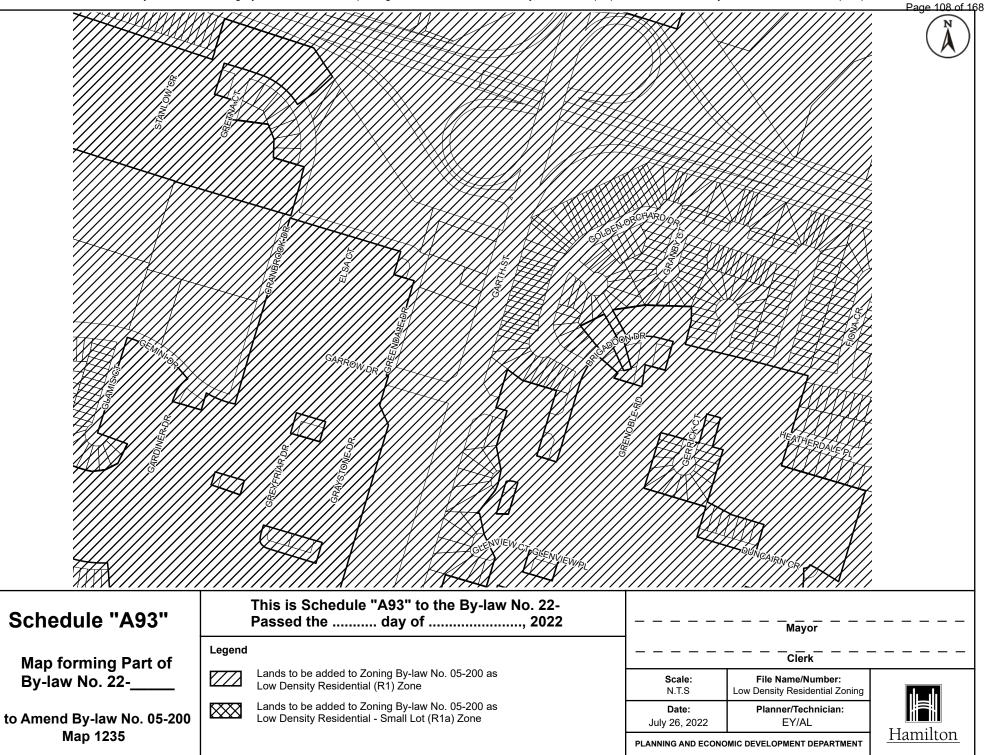


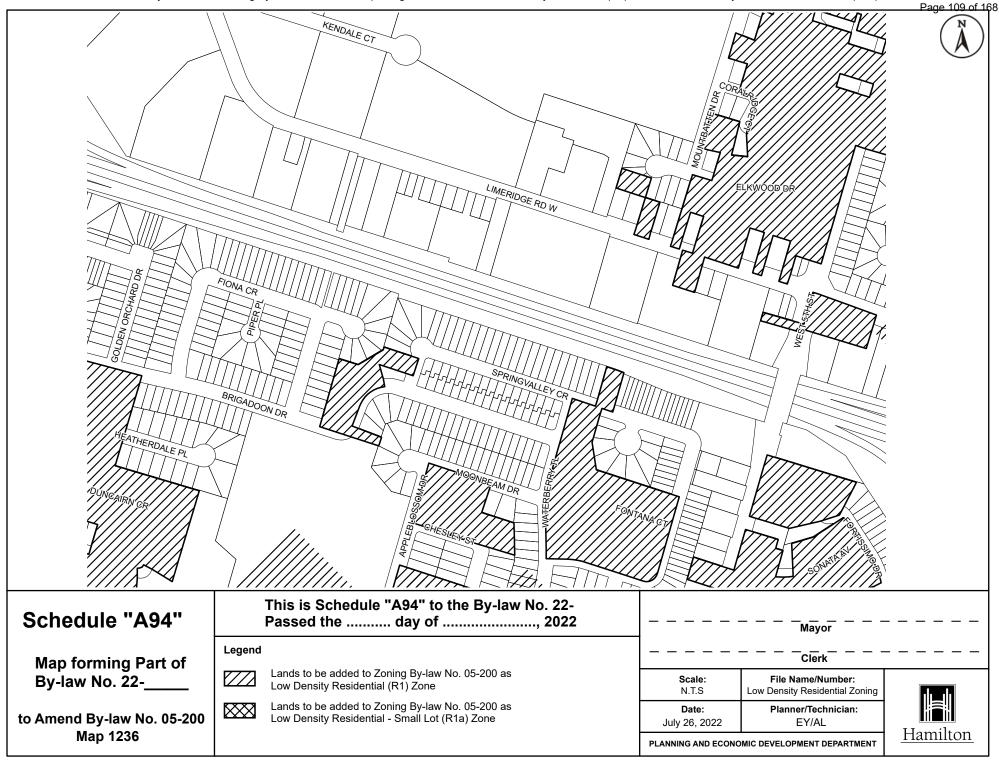


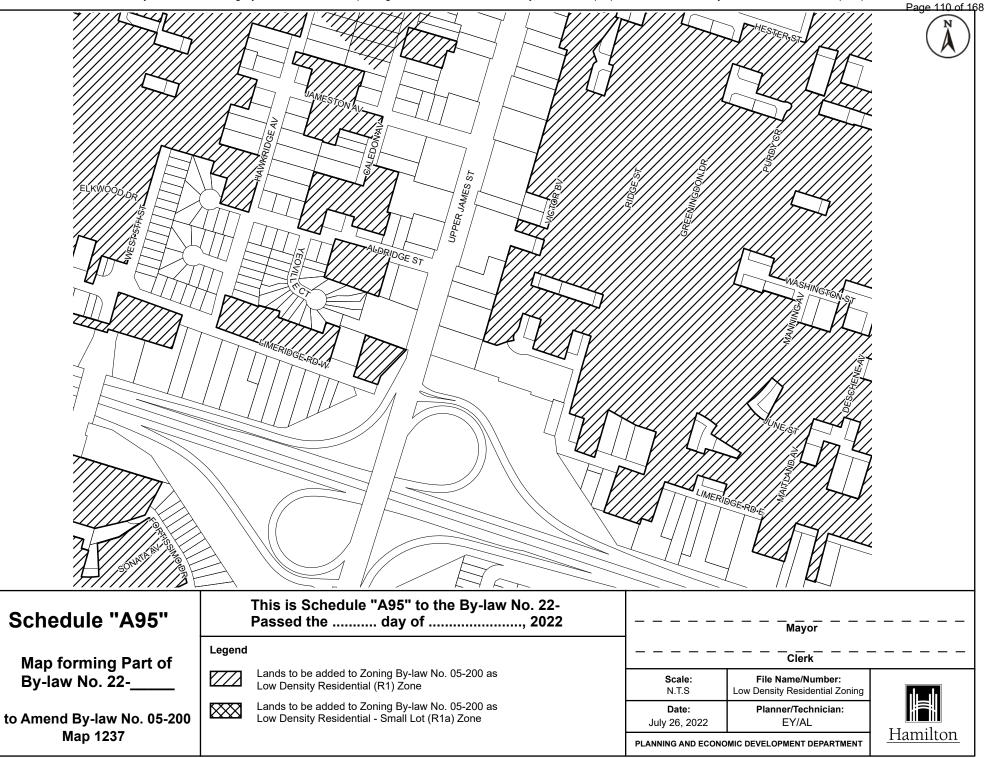


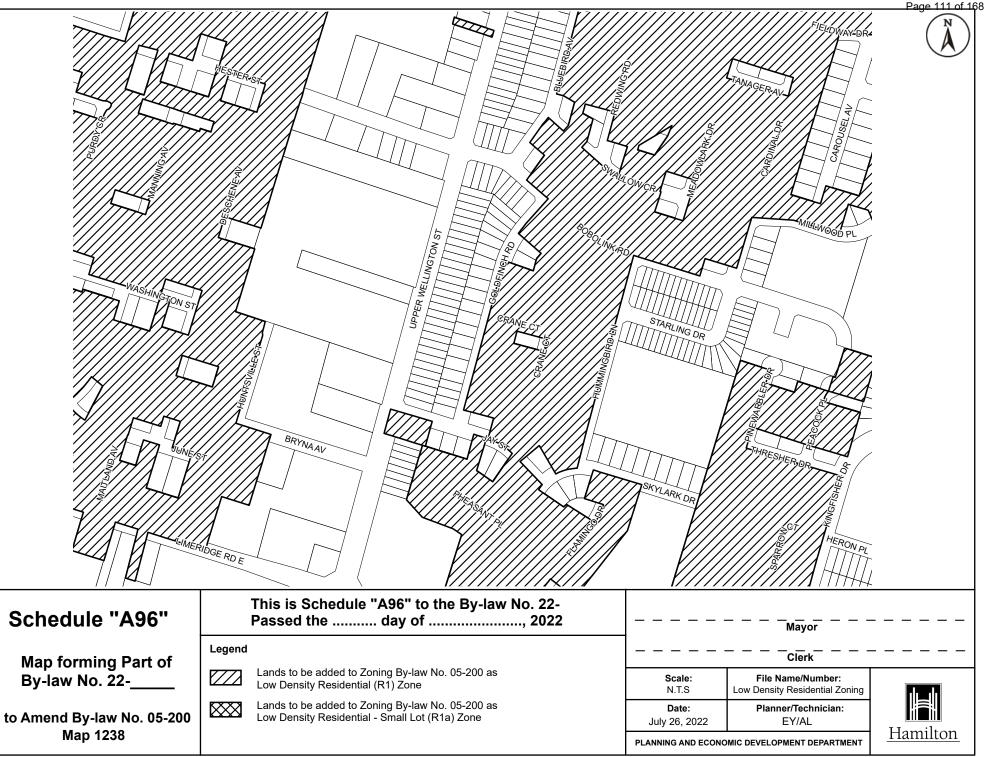
To Amend City of Hamilton Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Zone and Low Density Residential – Small Lot (R1a) Zone

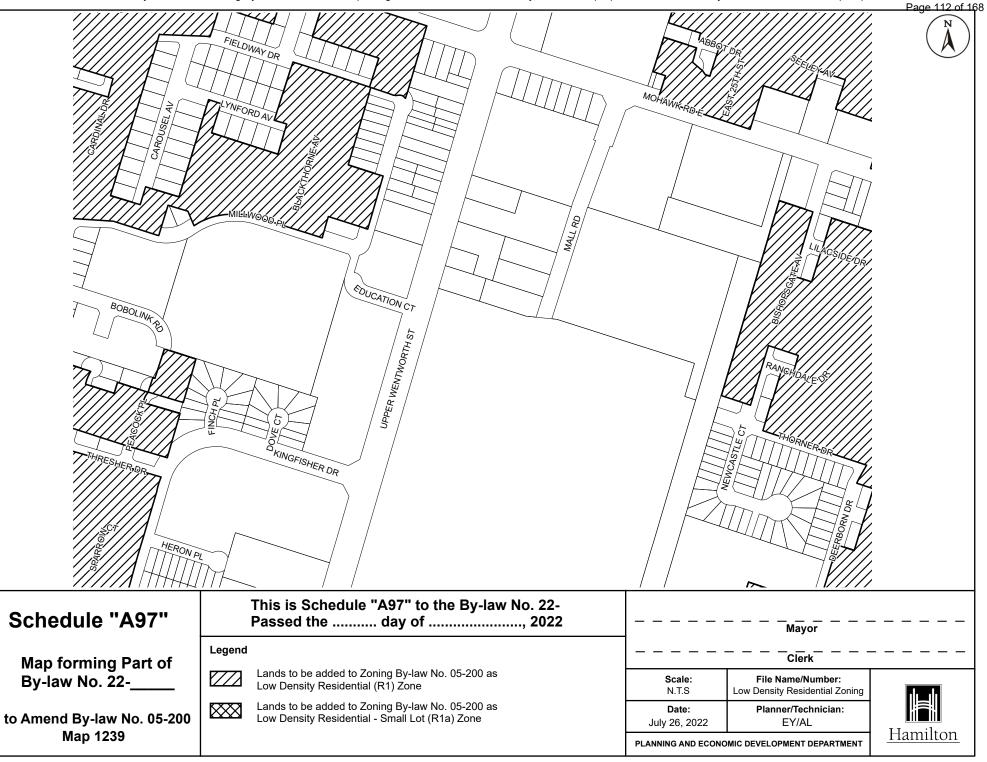


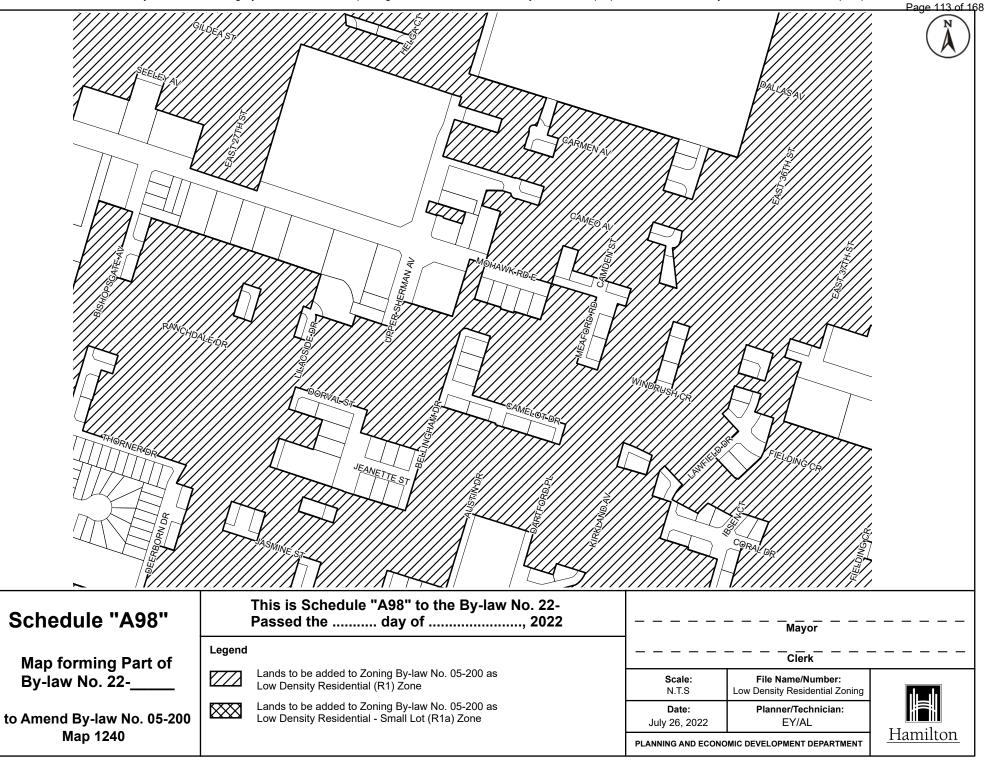


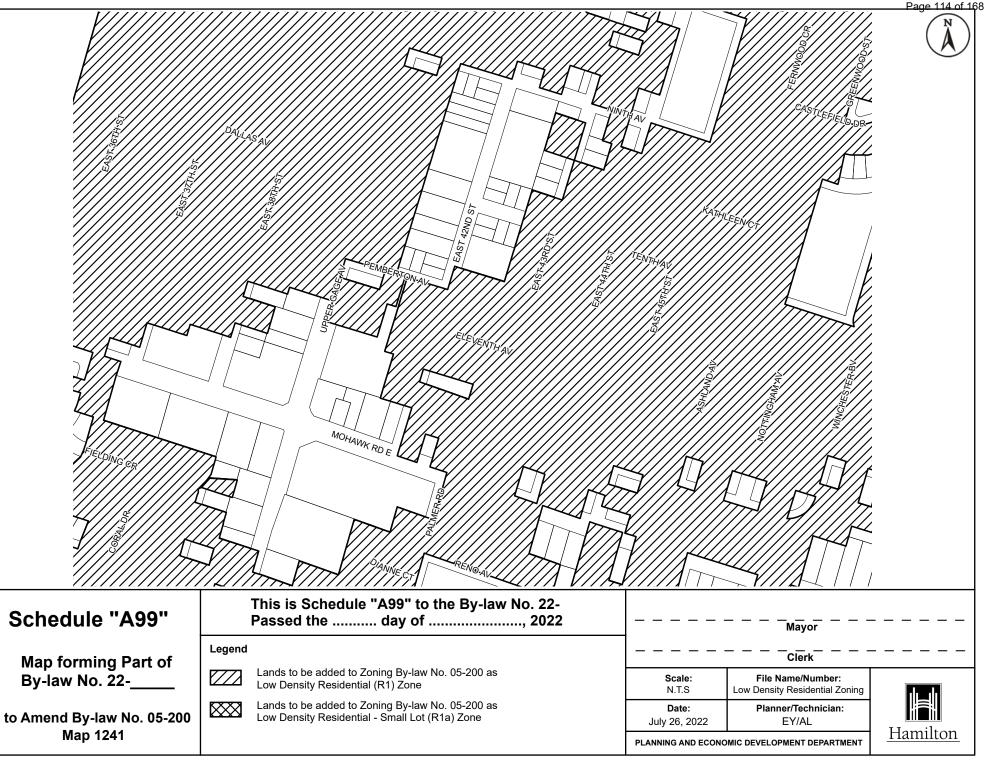


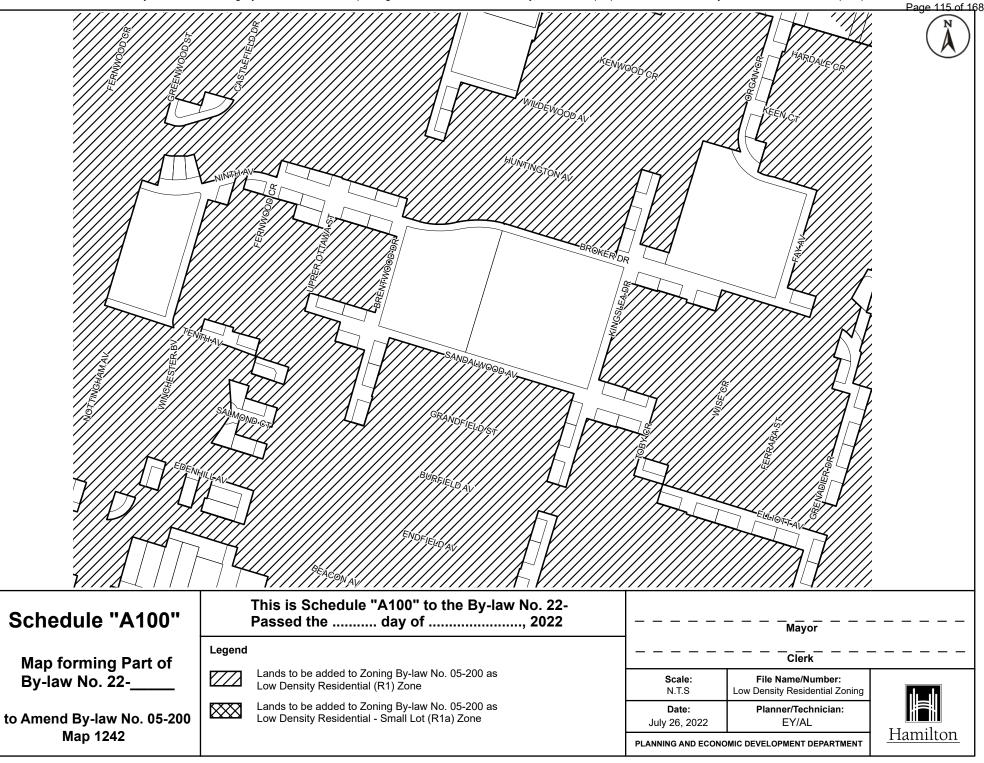


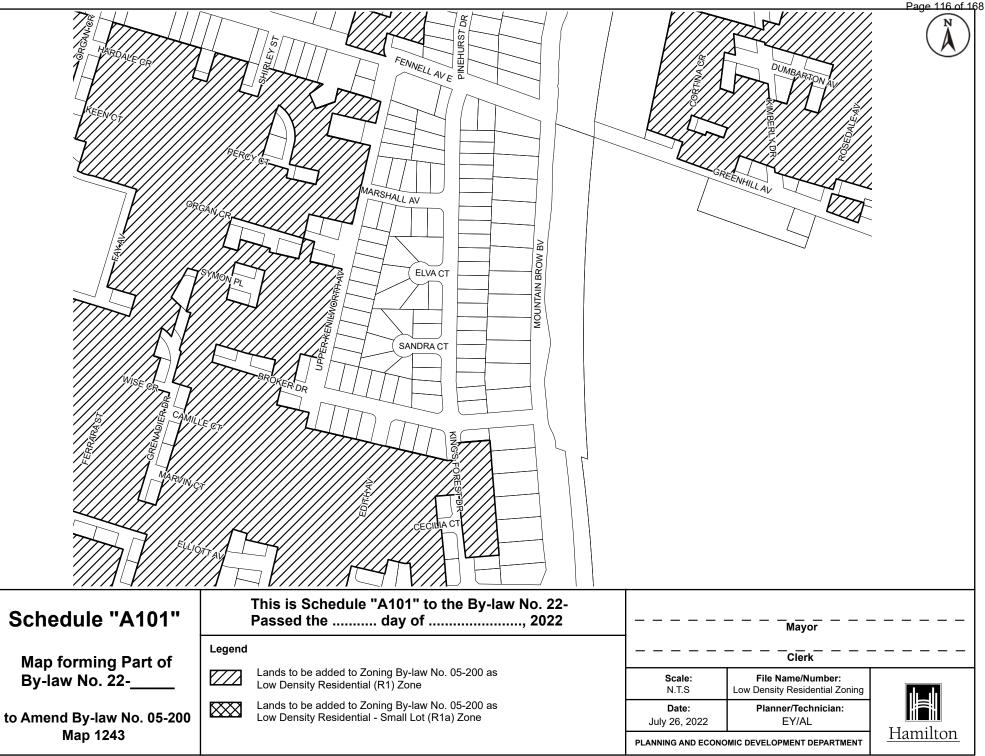


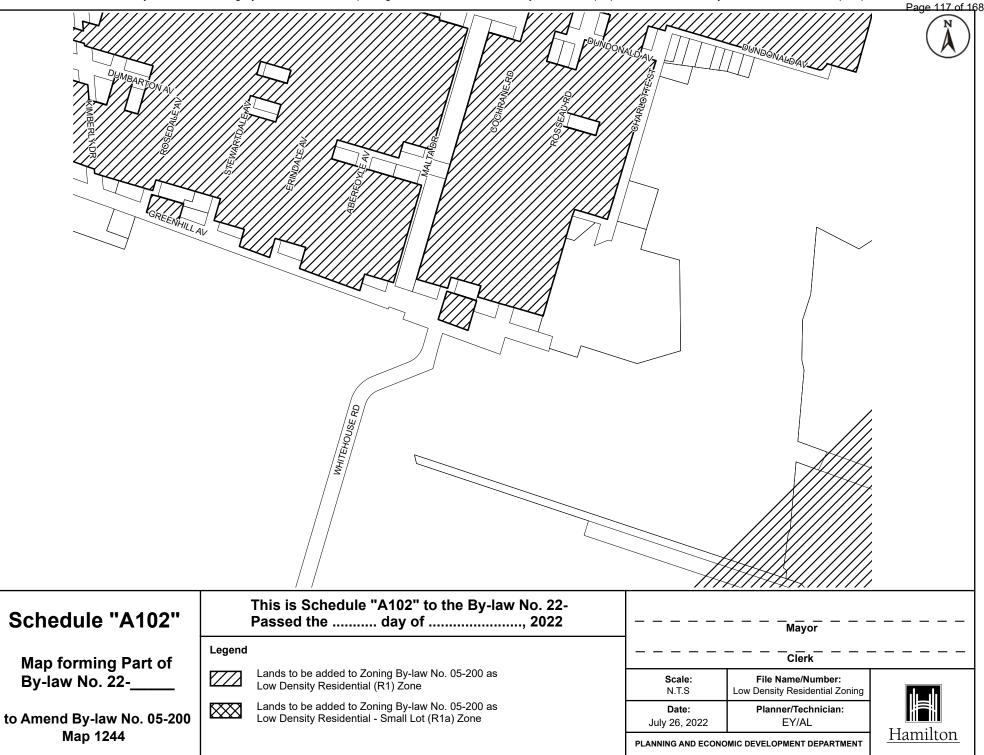




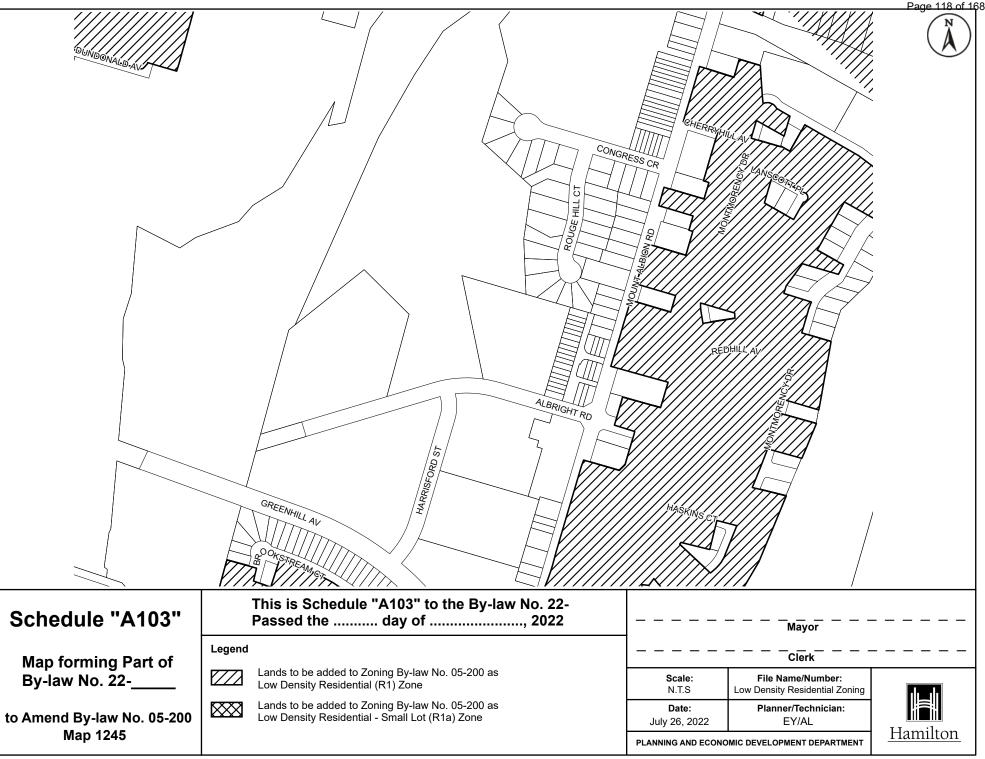


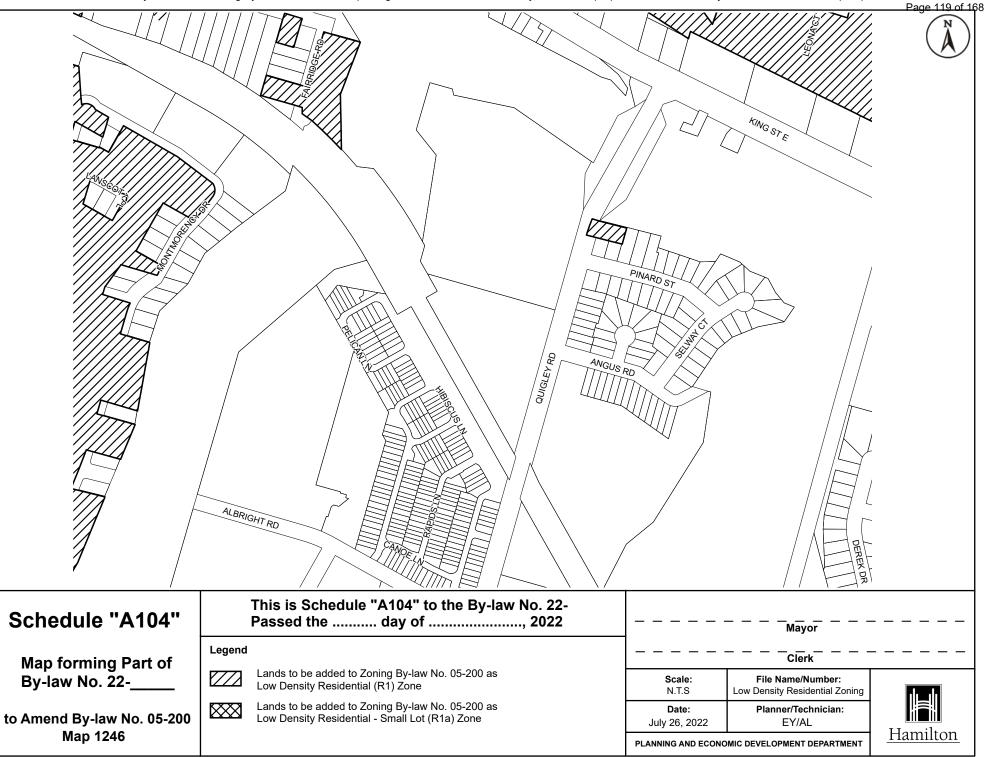


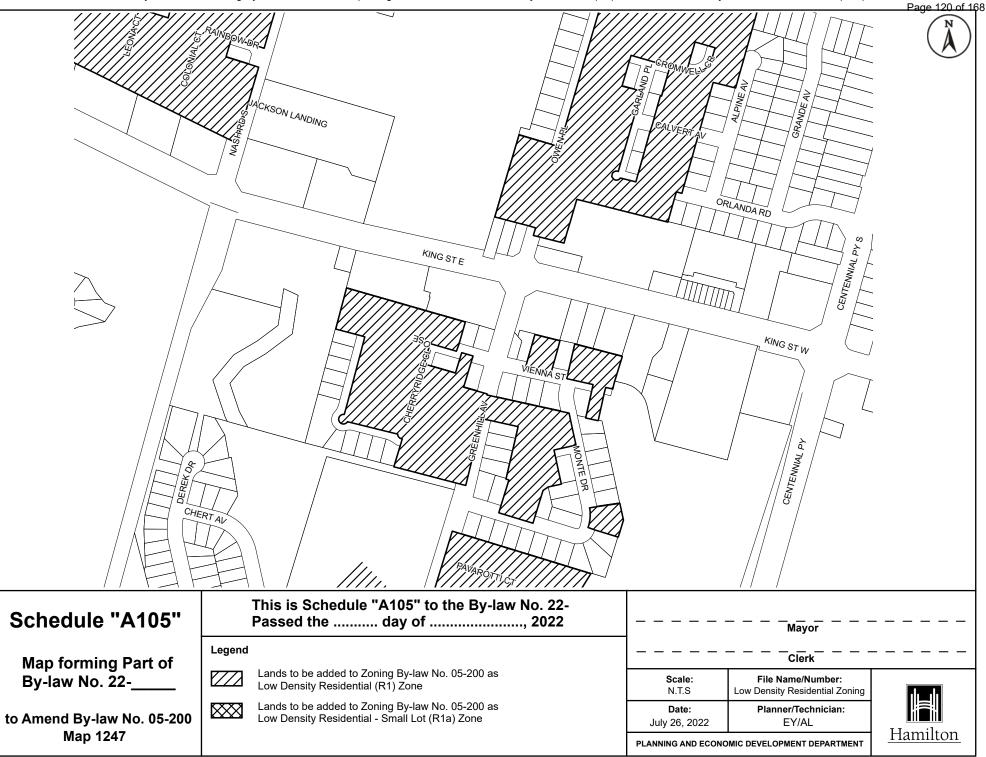




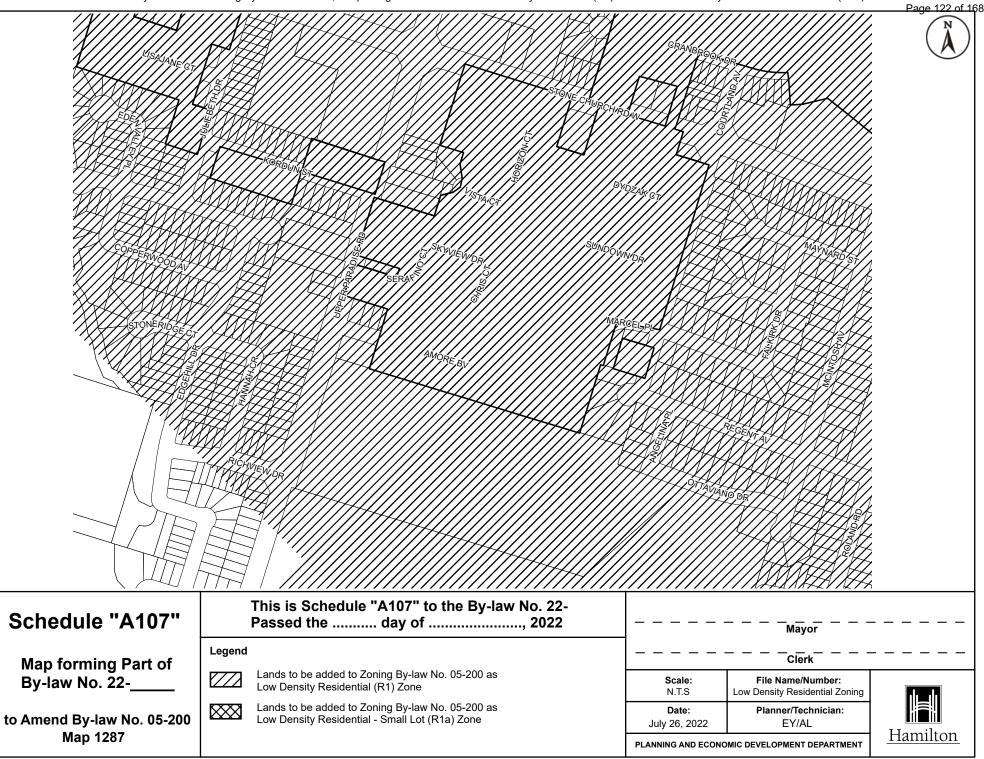
To Amend City of Hamilton Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Zone and Low Density Residential – Small Lot (R1a) Zone

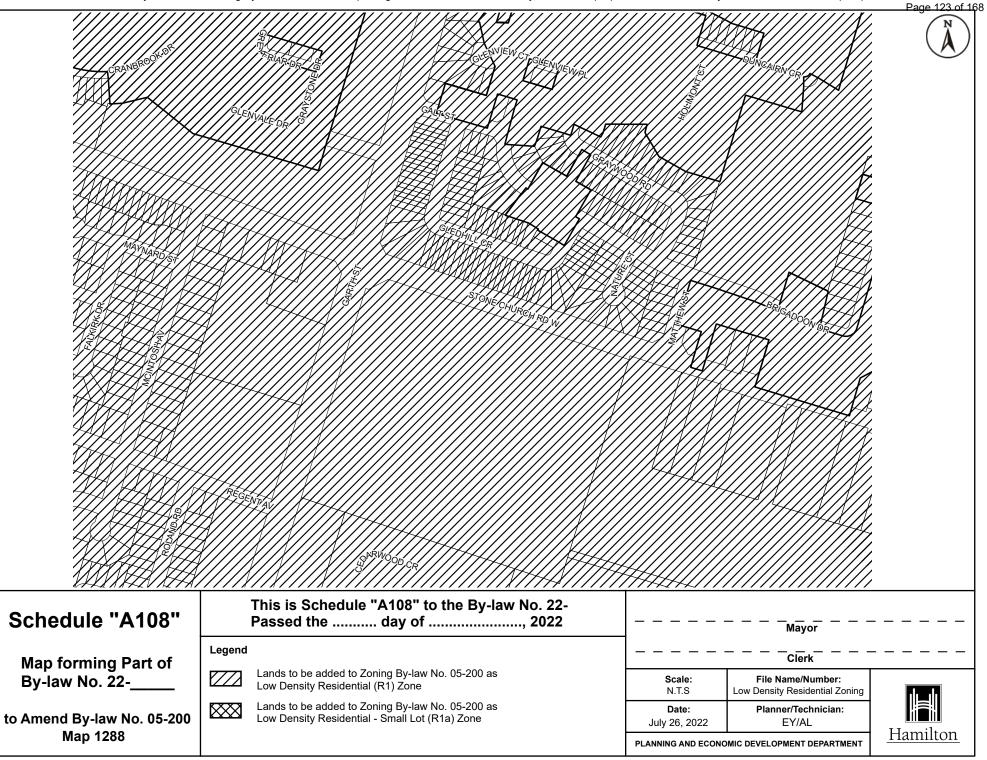


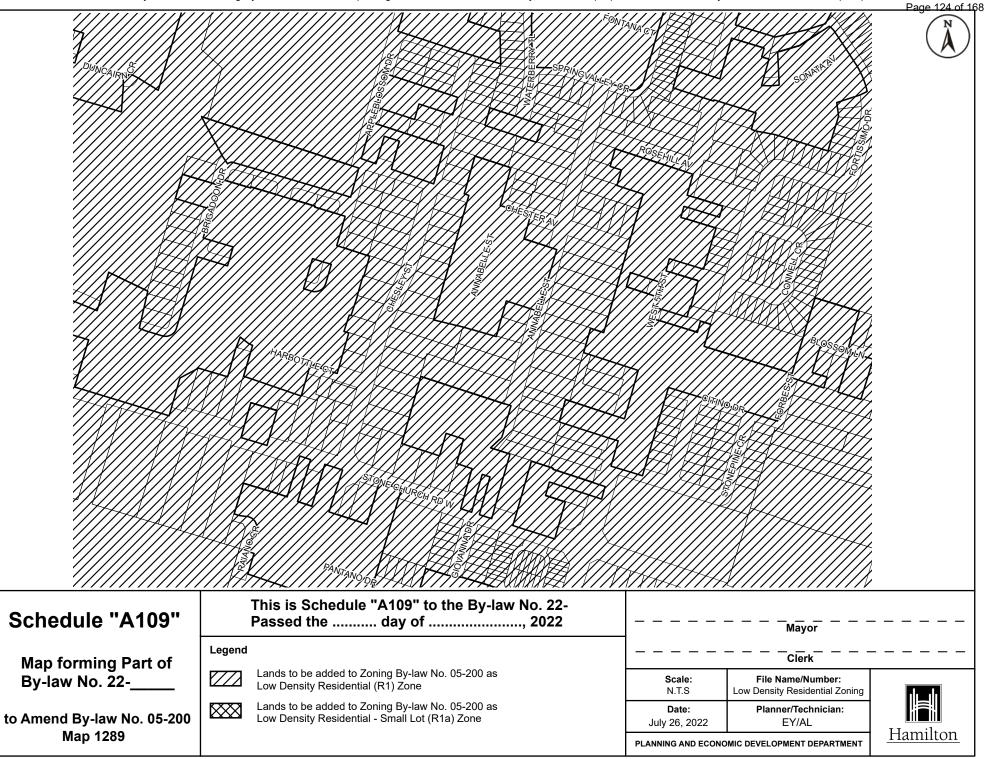


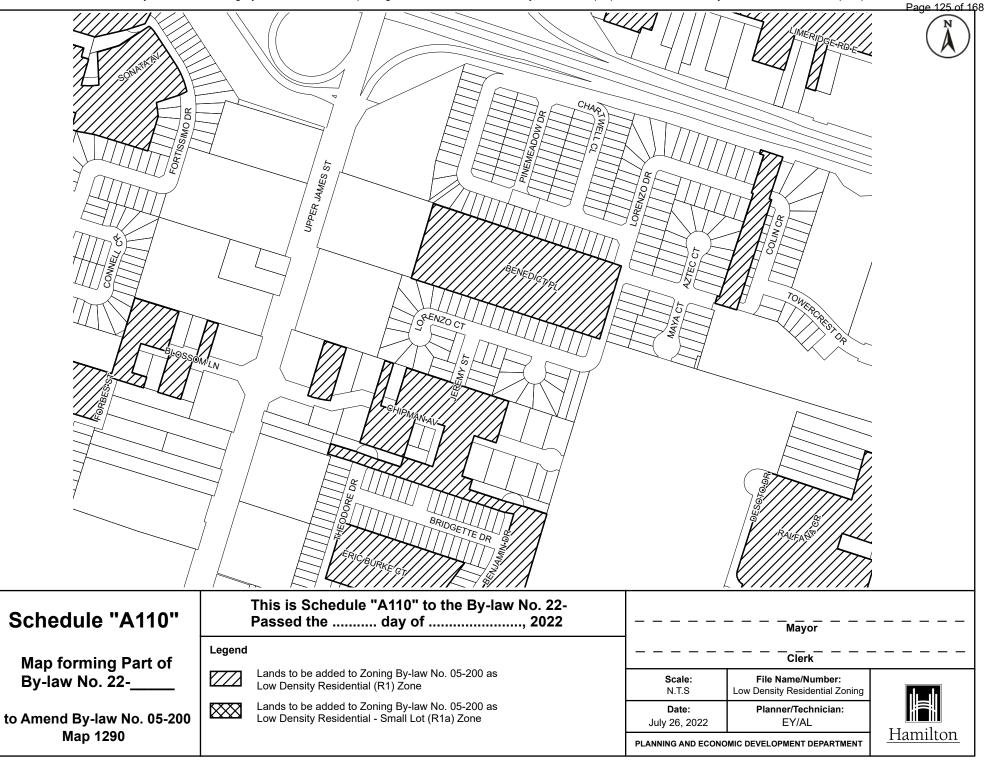


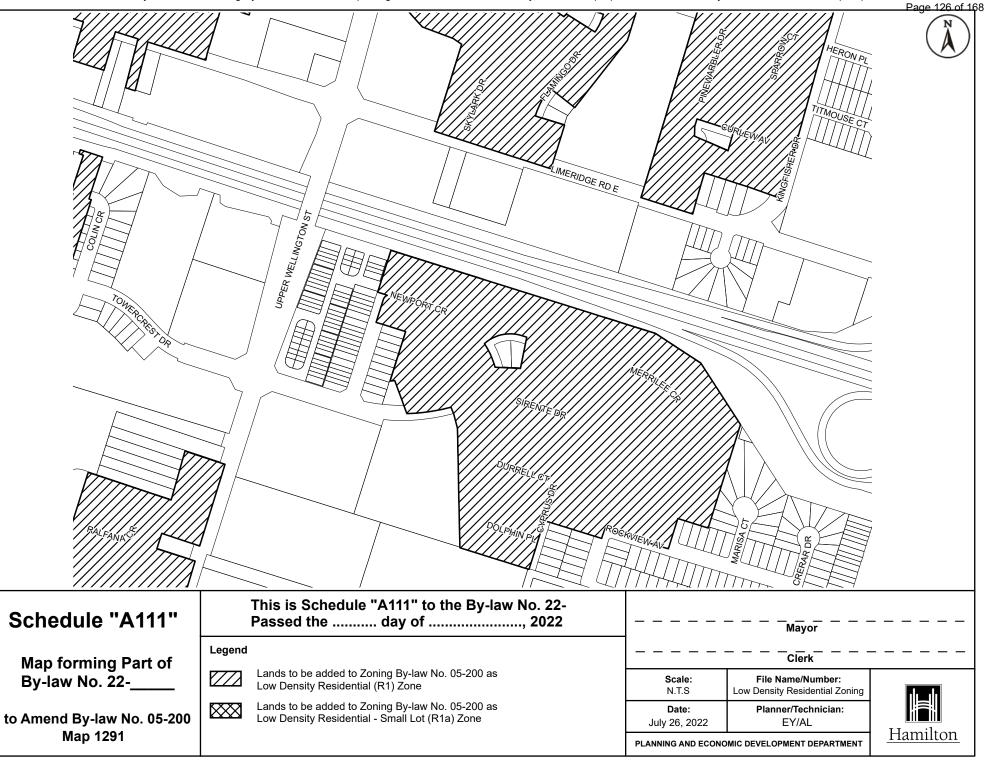
| To Amend City of Hamilto | n Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Z | one and Low Density Residential – Small Lot (R1a) Zone Page 121 of 16 |
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| | RAN RD HER PL B R R R R R R R R R R R R R R R R R R | EDERNE GT EDERNE TER EDERNE TER COPPERWOODAV |
| Schedule "A106" | This is Schedule "A106" to the By-law No. 22- Passed the day of | |
| Map forming Part of By-law No. 22 | Legend Lands to be added to Zoning By-law No. 05-200 as Low Density Residential (R1) Zone | Clerk Scale: File Name/Number: |
| to Amend By-law No. 05-200 Map 1286 | Lands to be added to Zoning By-law No. 05-200 as Low Density Residential - Small Lot (R1a) Zone | N.T.S Low Density Residential Zoning Date: Planner/Technician: July 26, 2022 EY/AL PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT Hamilton |

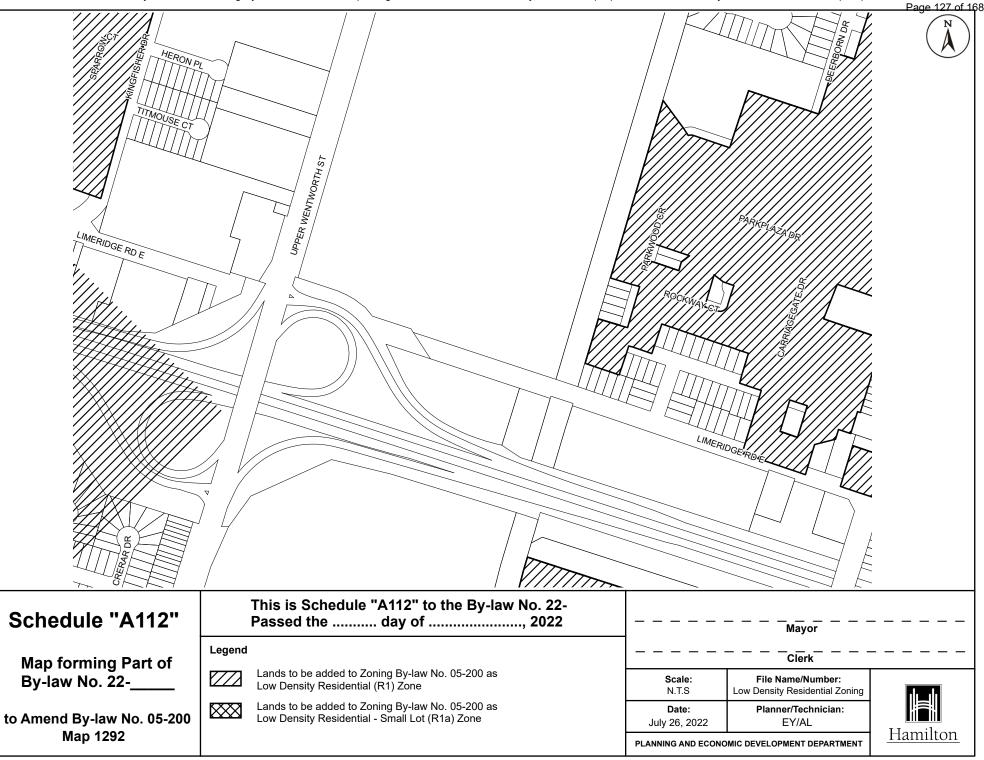


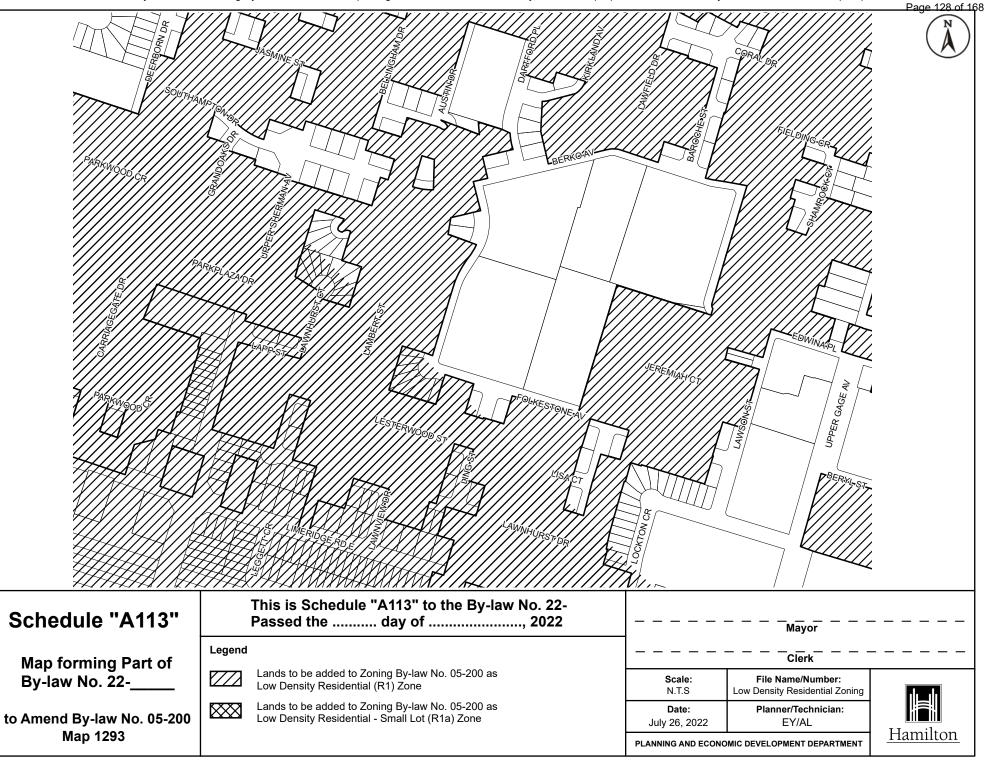


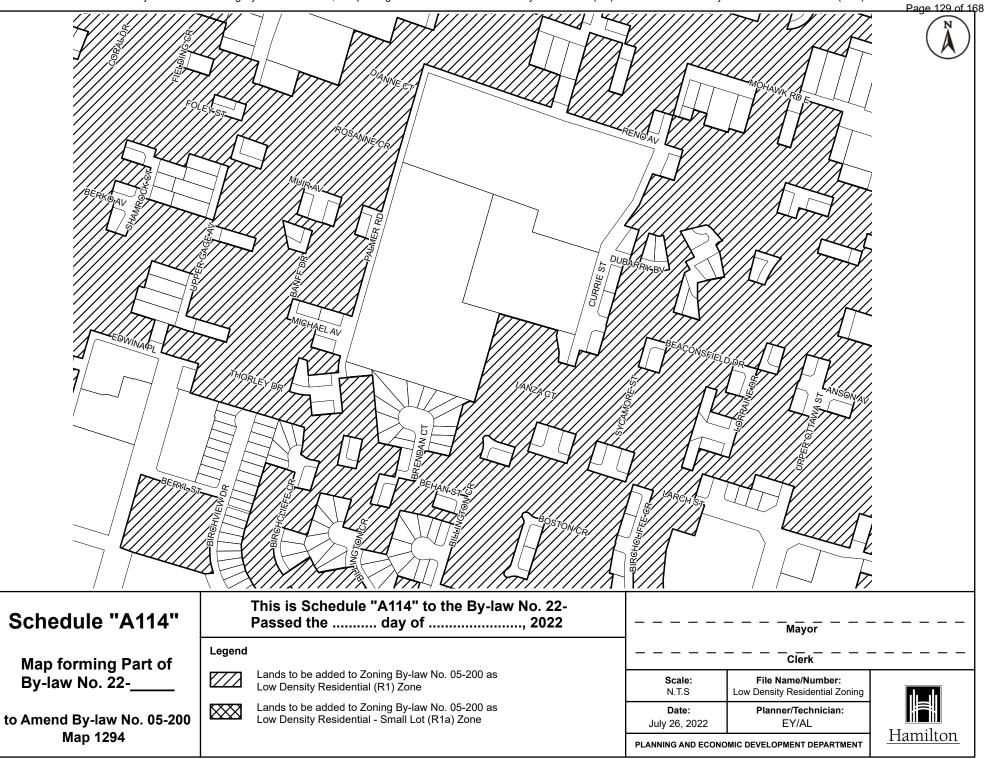


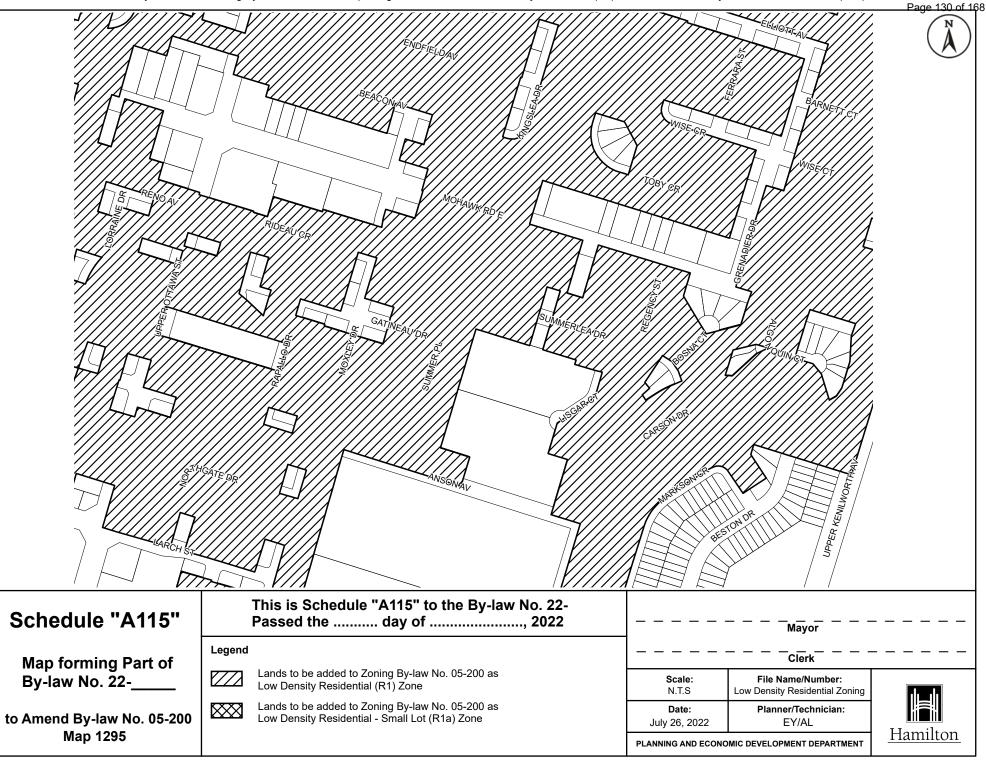


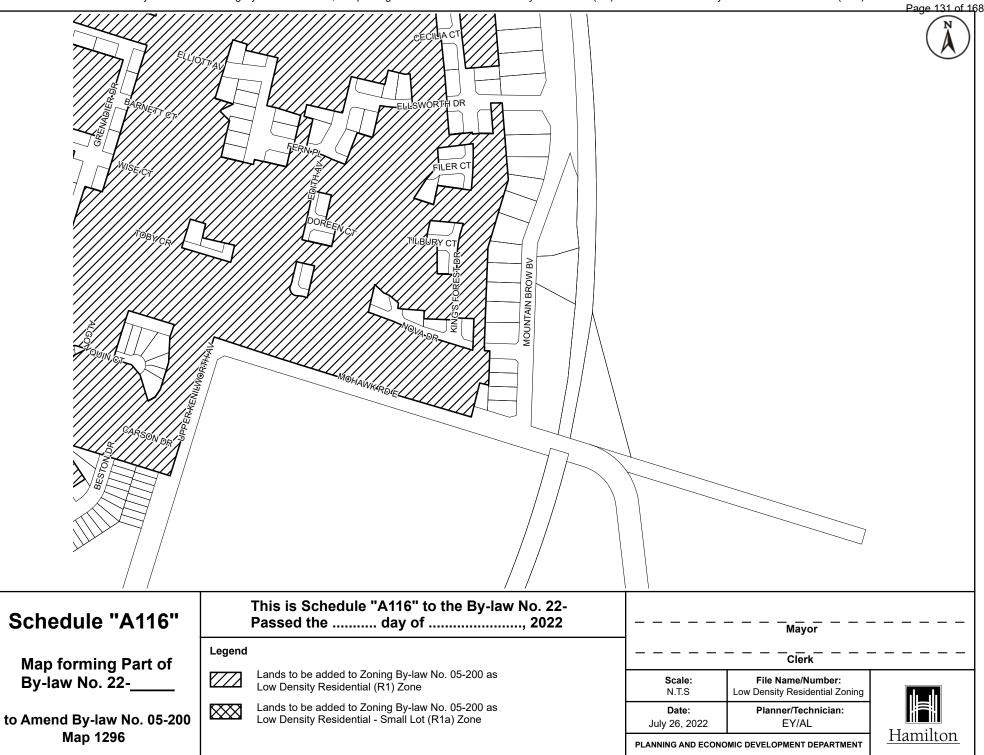


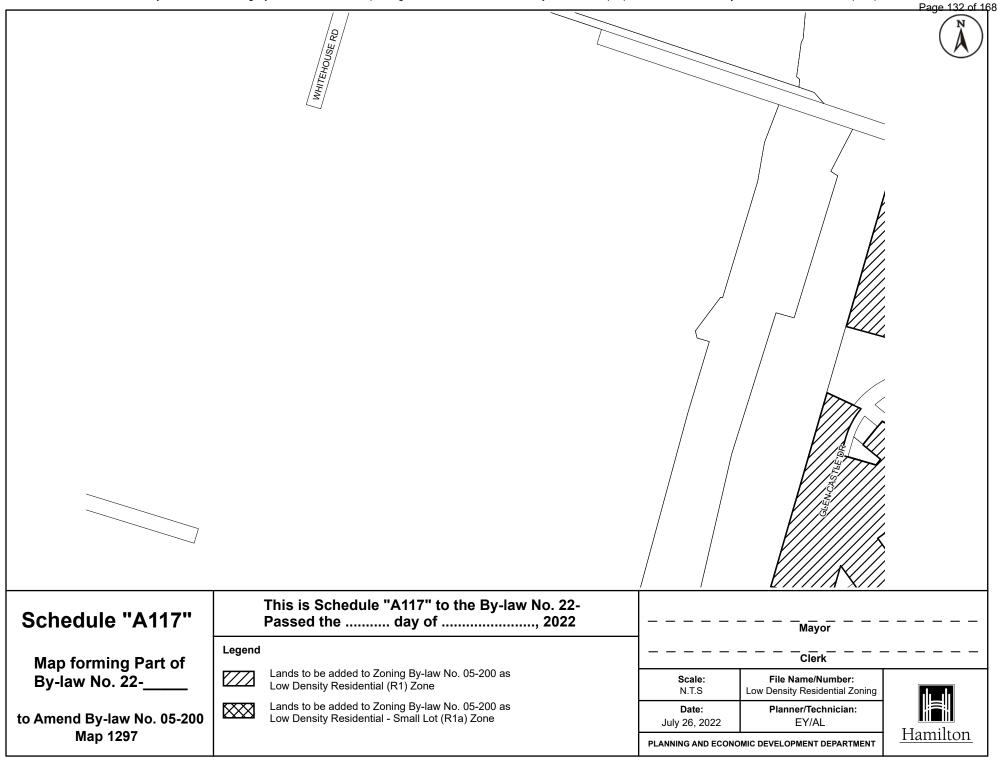




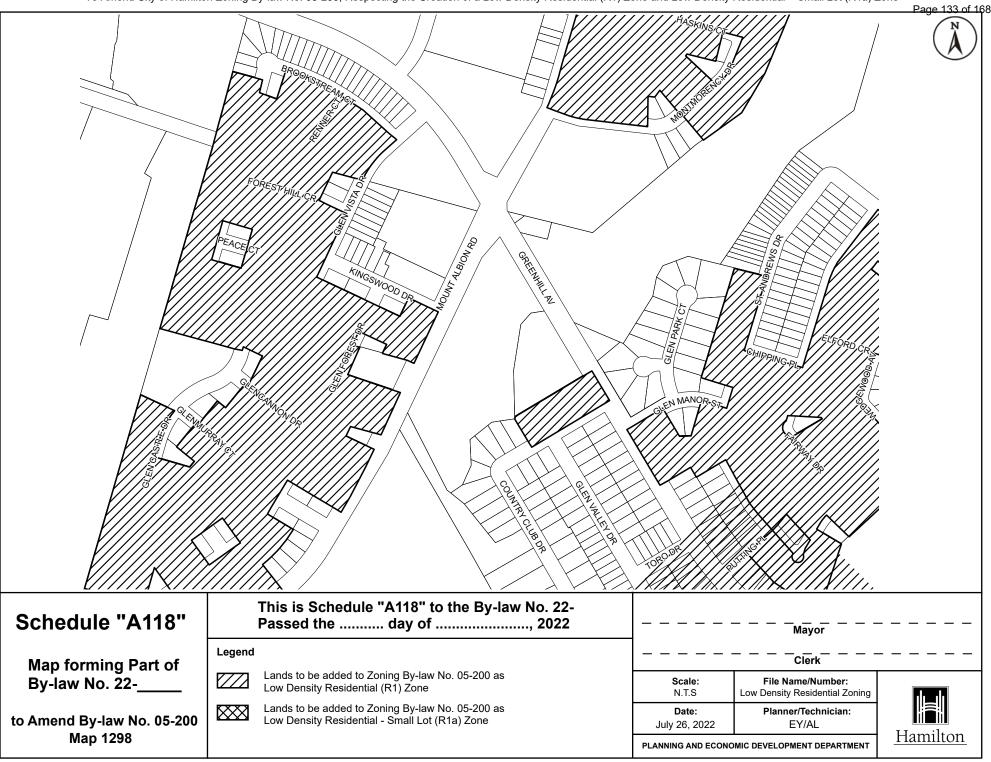


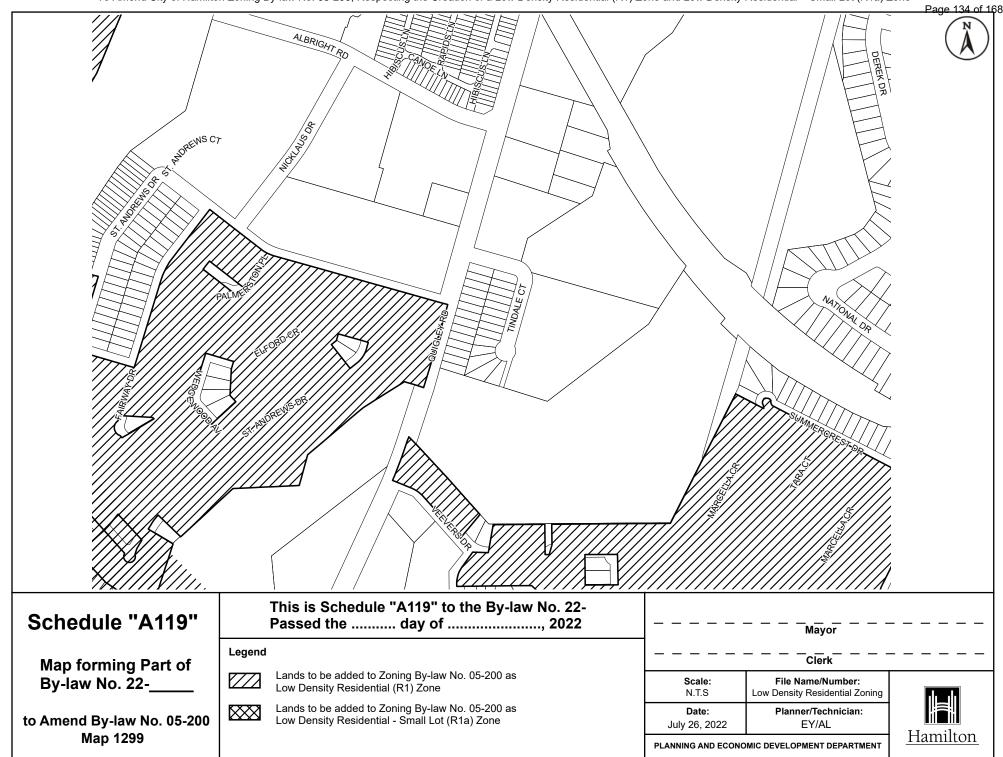




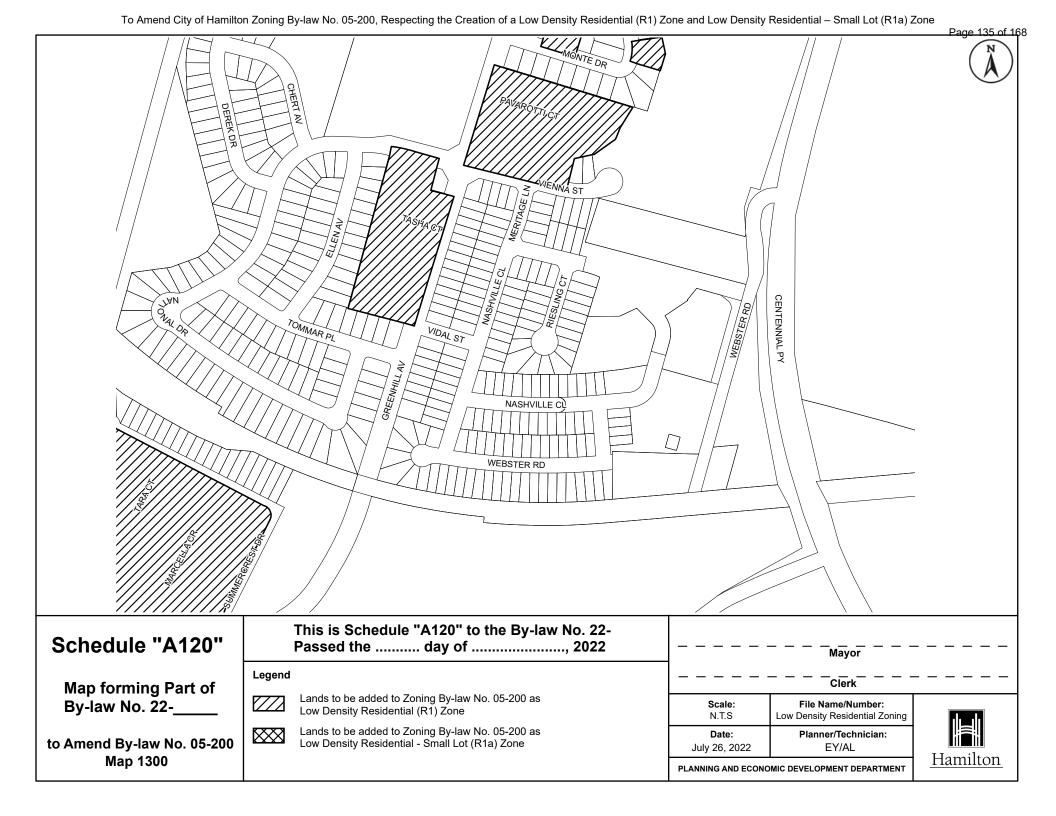


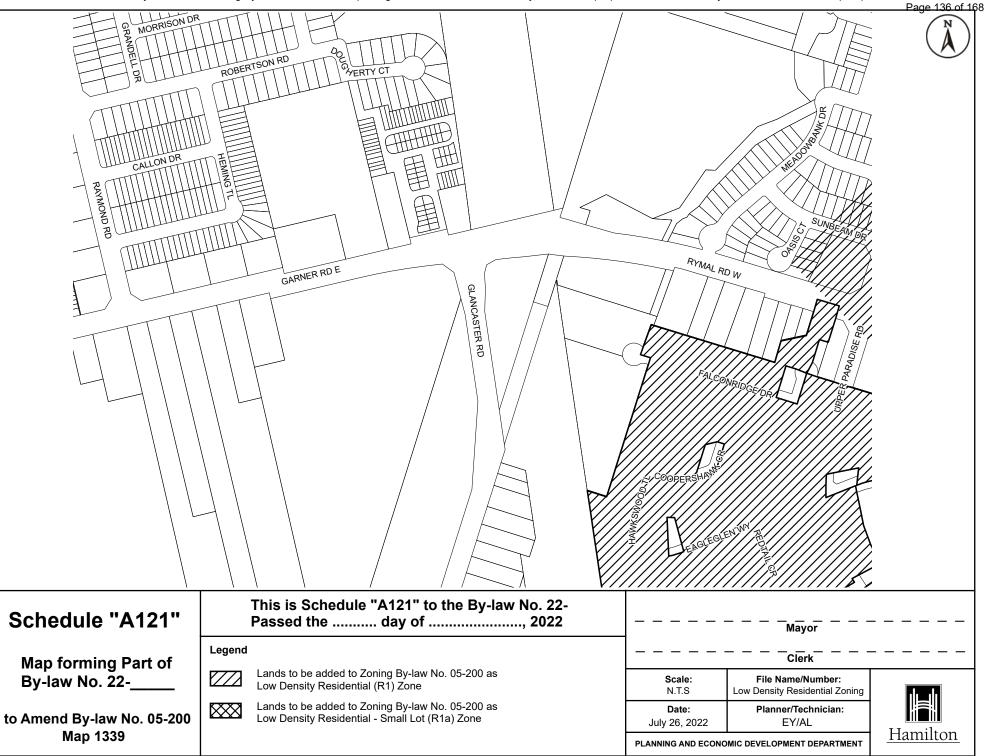


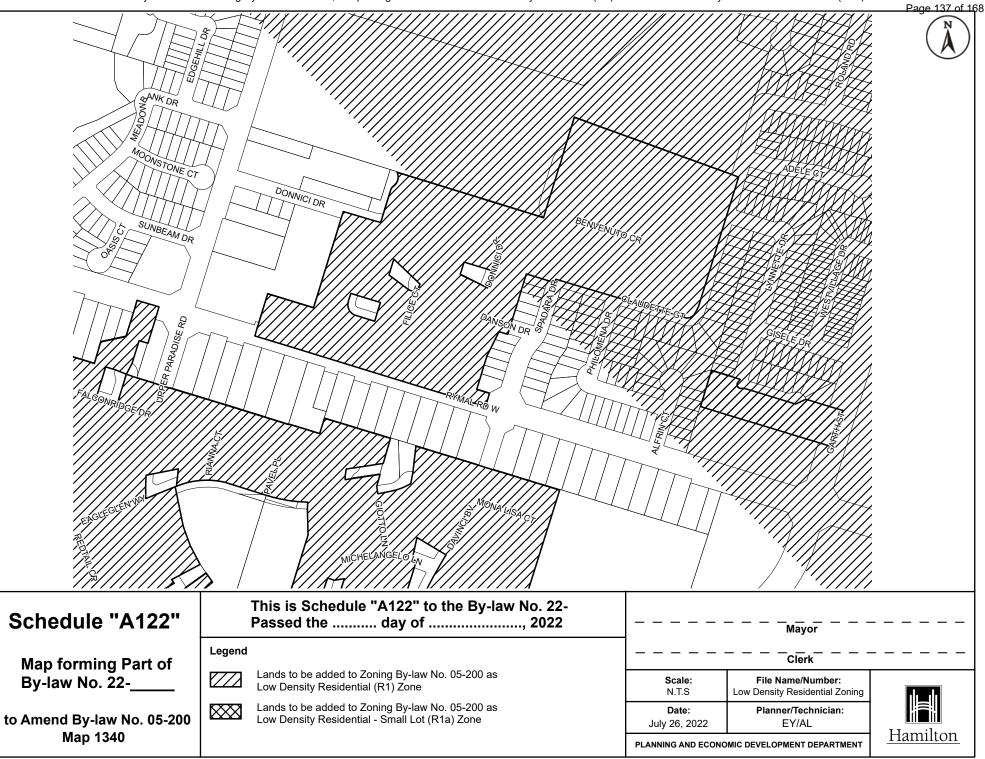


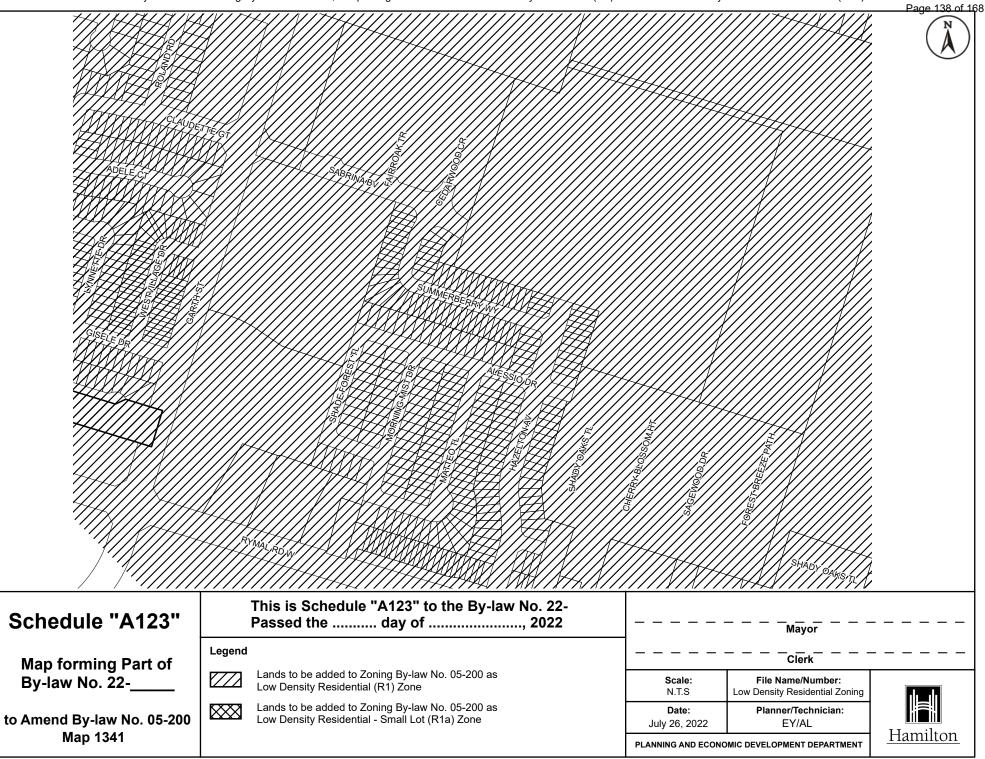


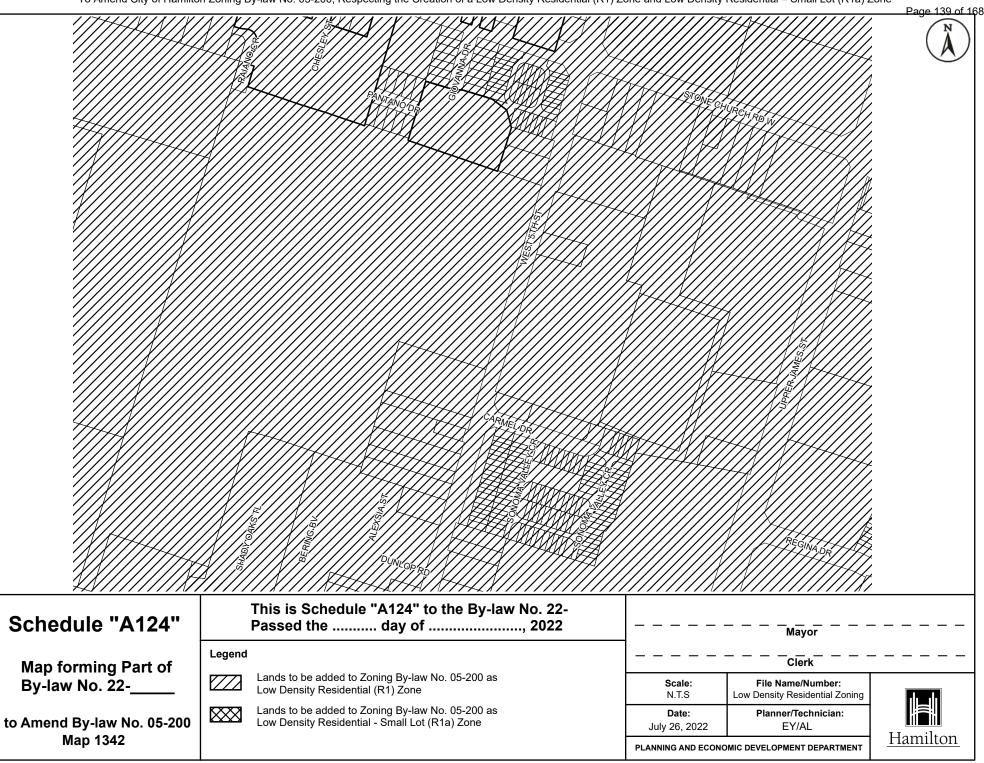
To Amend City of Hamilton Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Zone and Low Density Residential – Small Lot (R1a) Zone

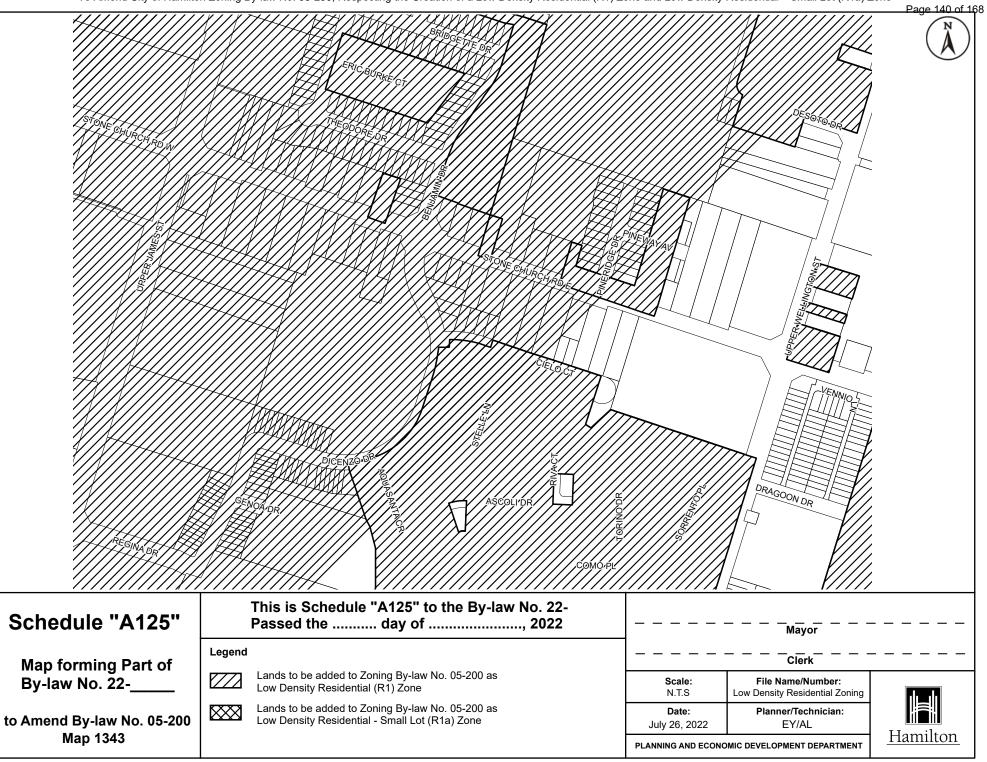


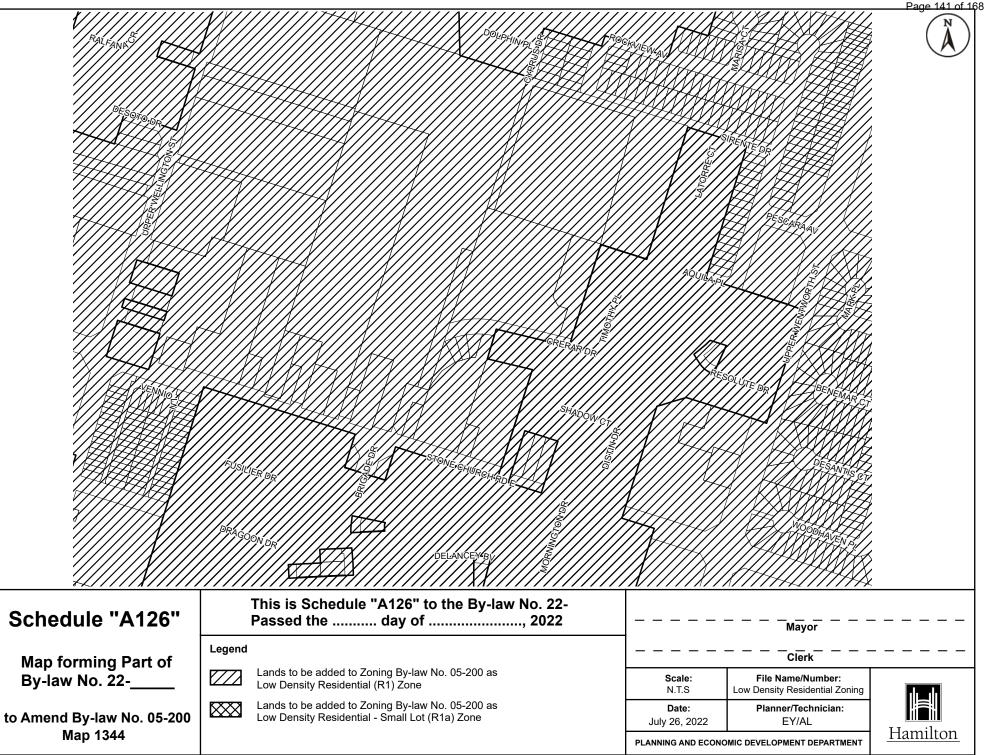


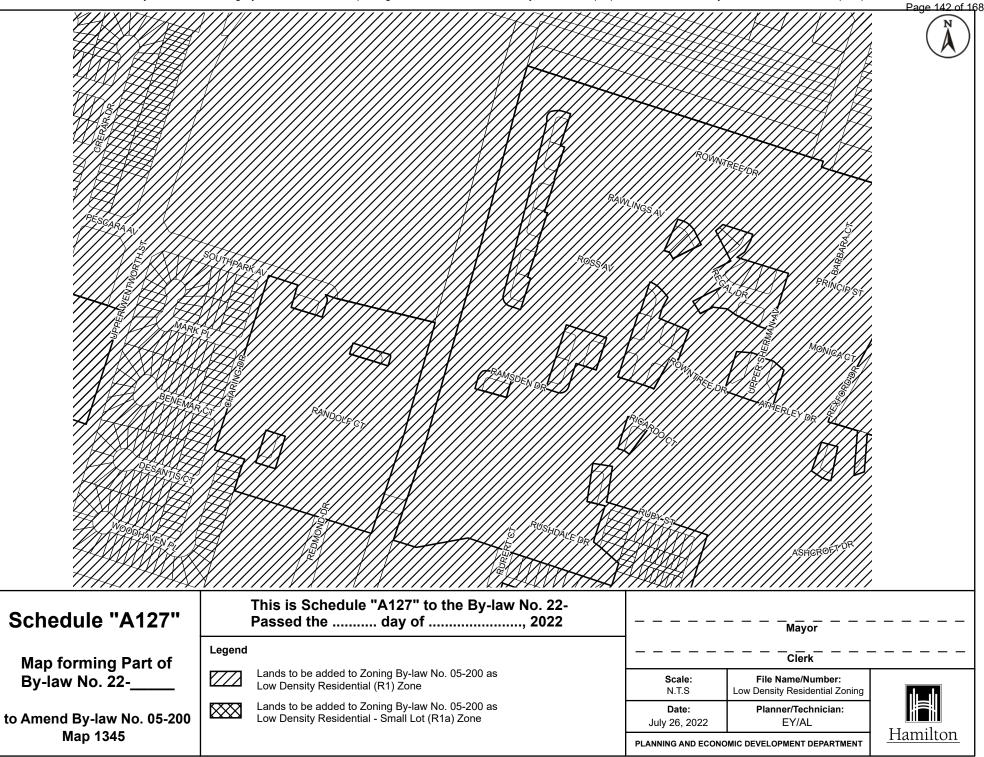


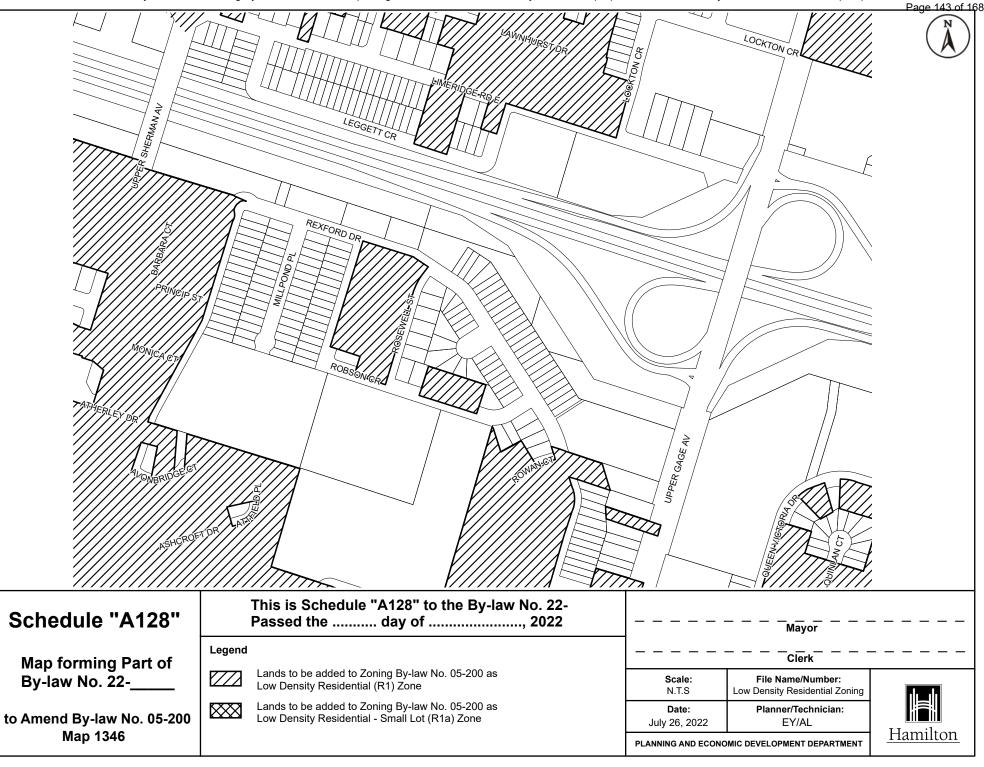


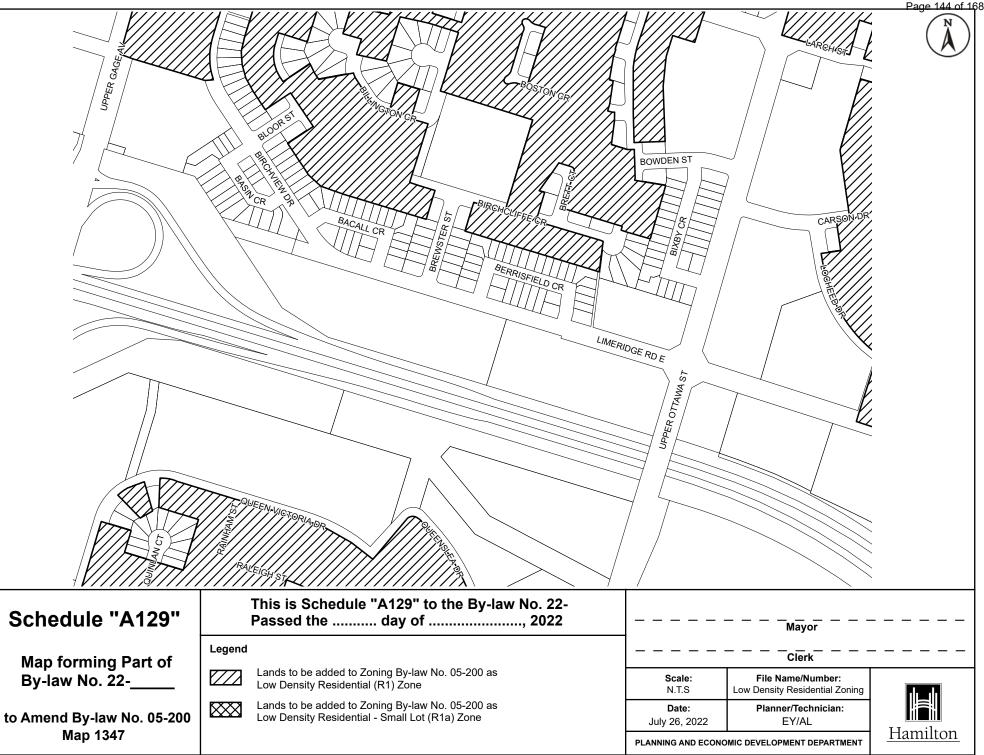


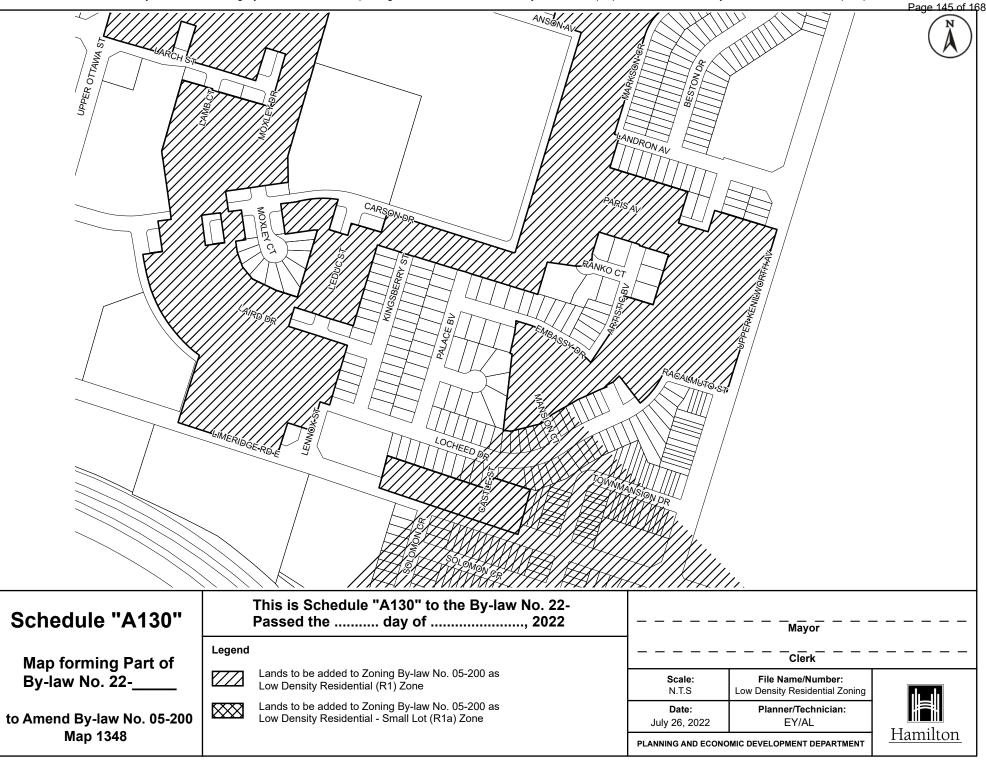


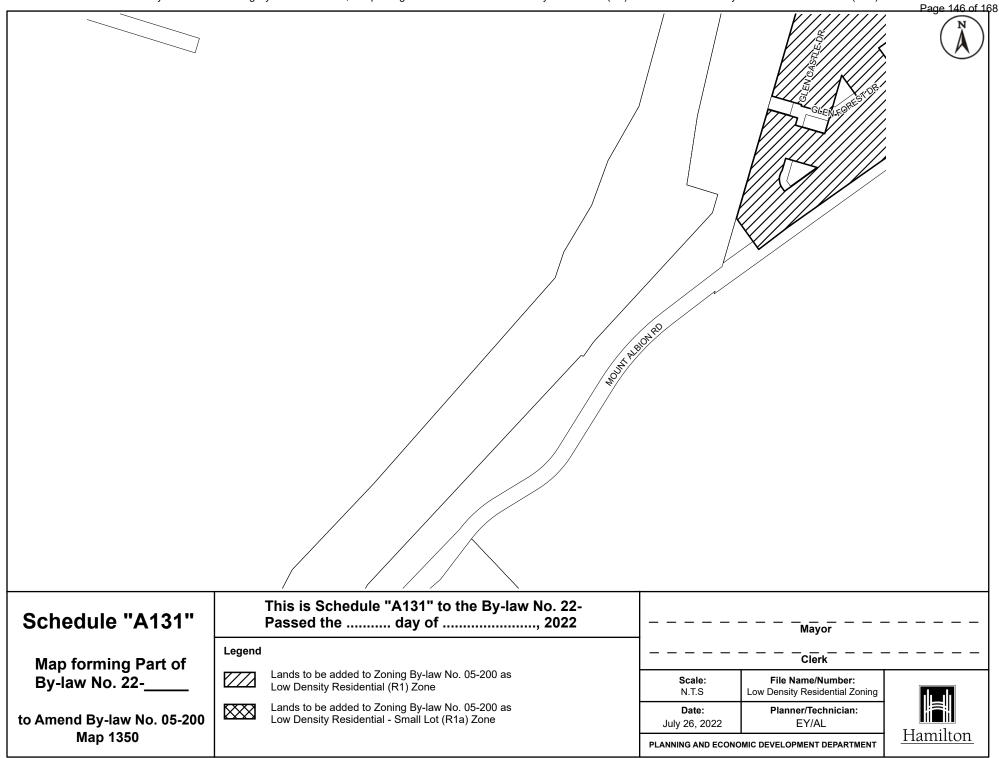


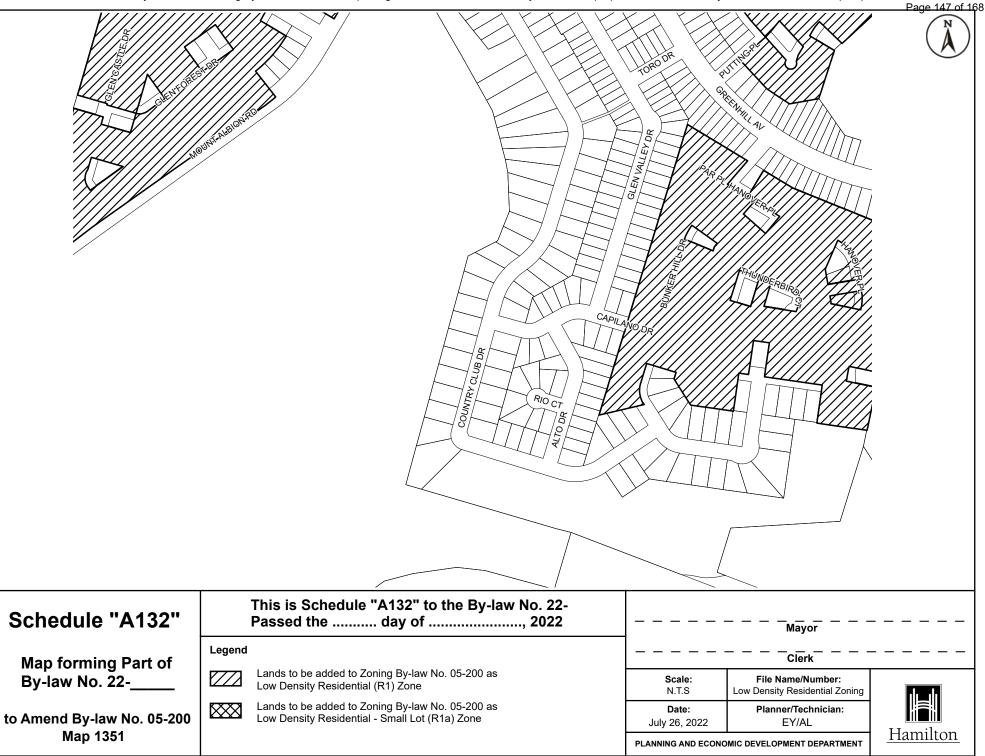


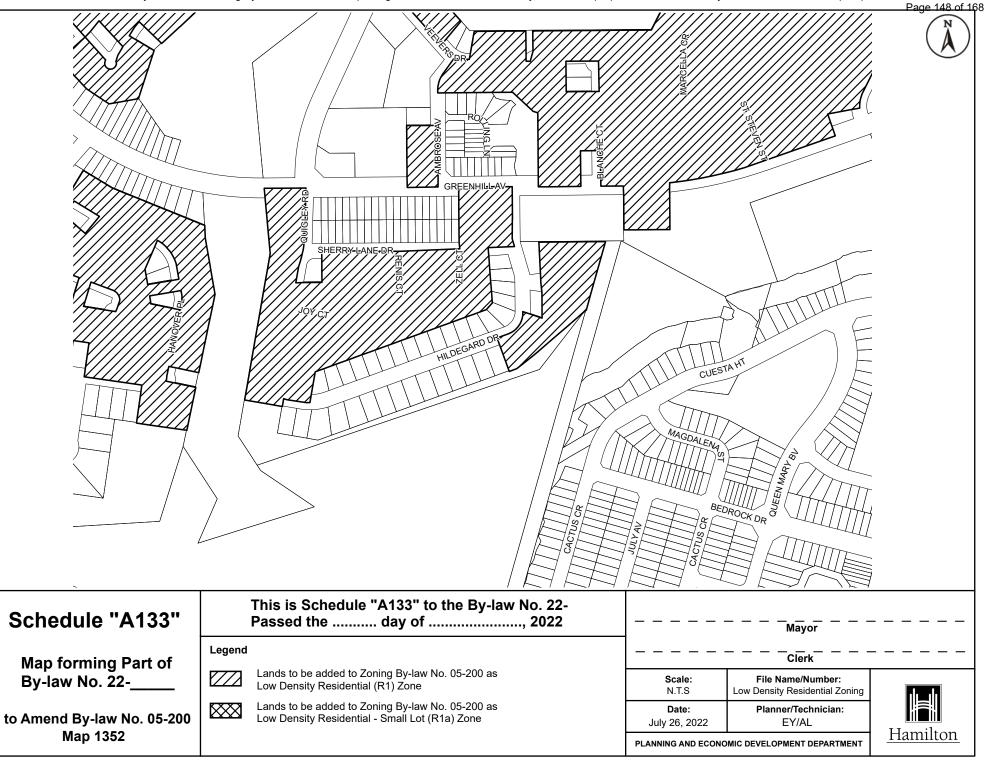


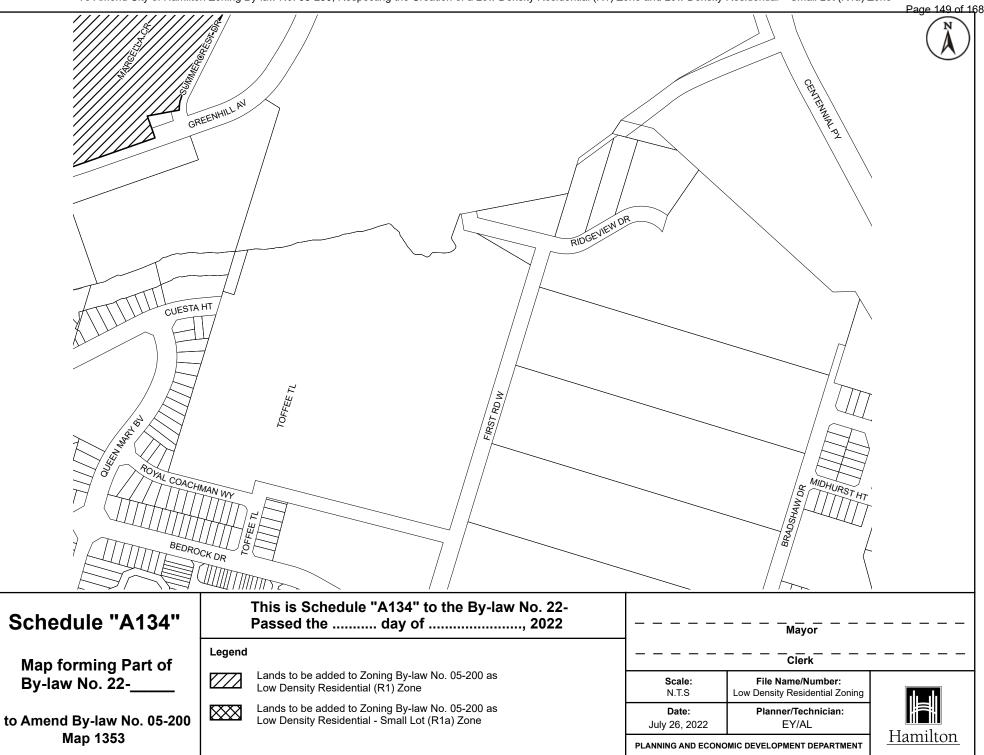


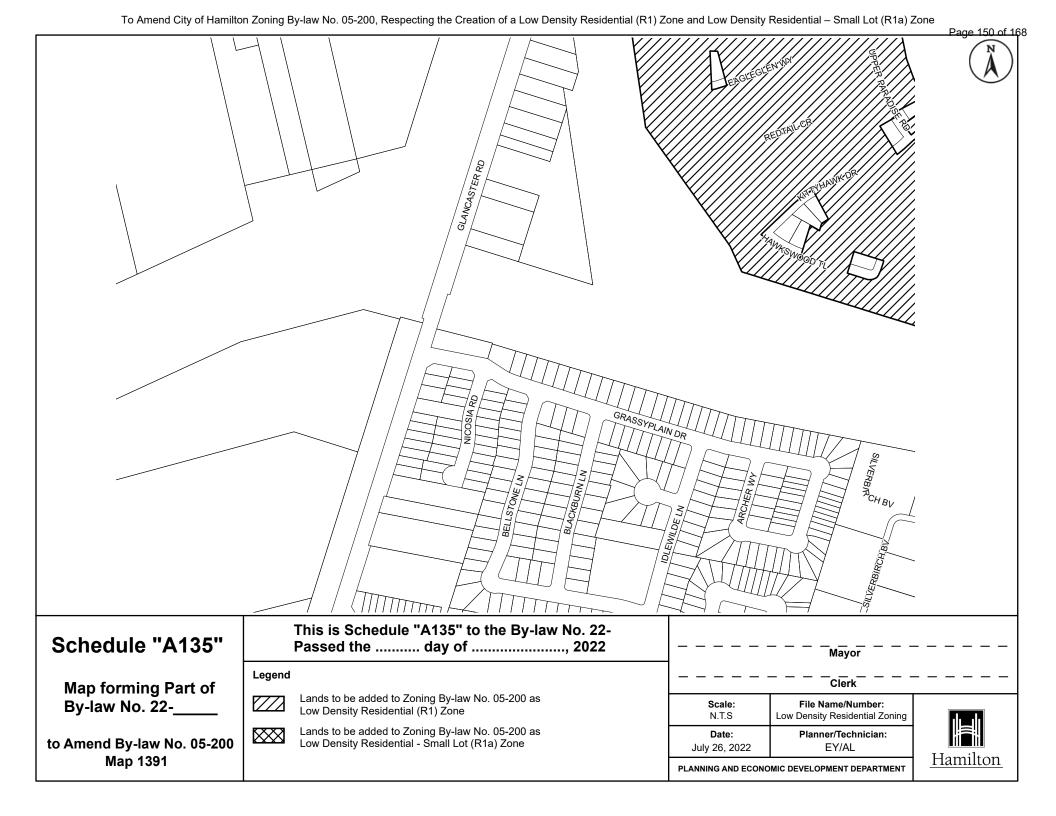


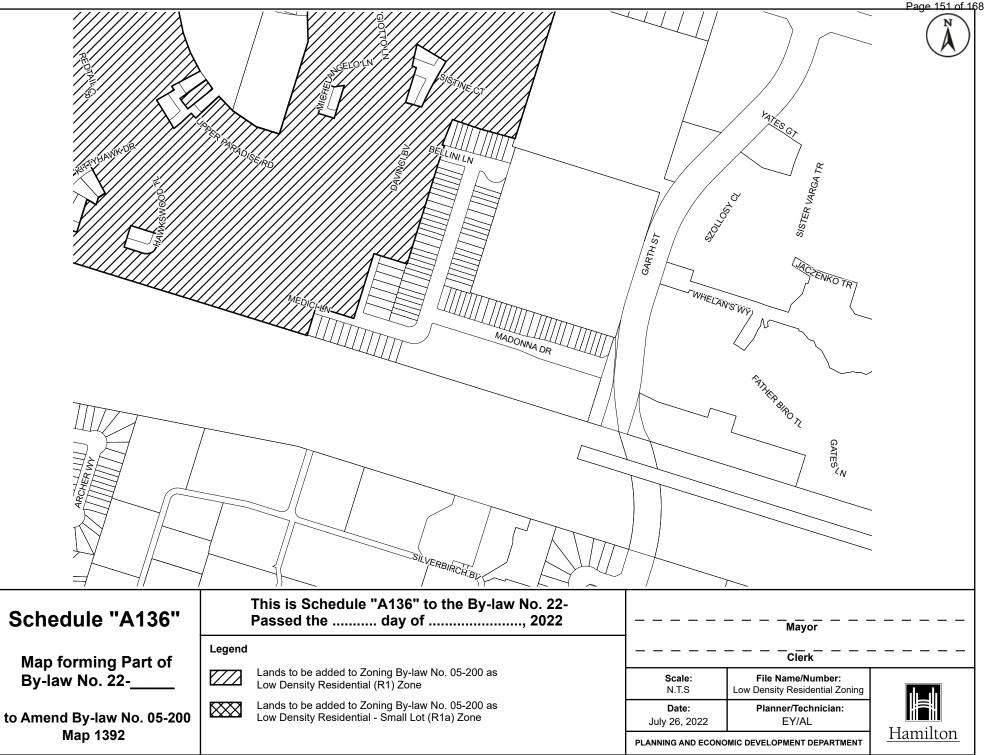




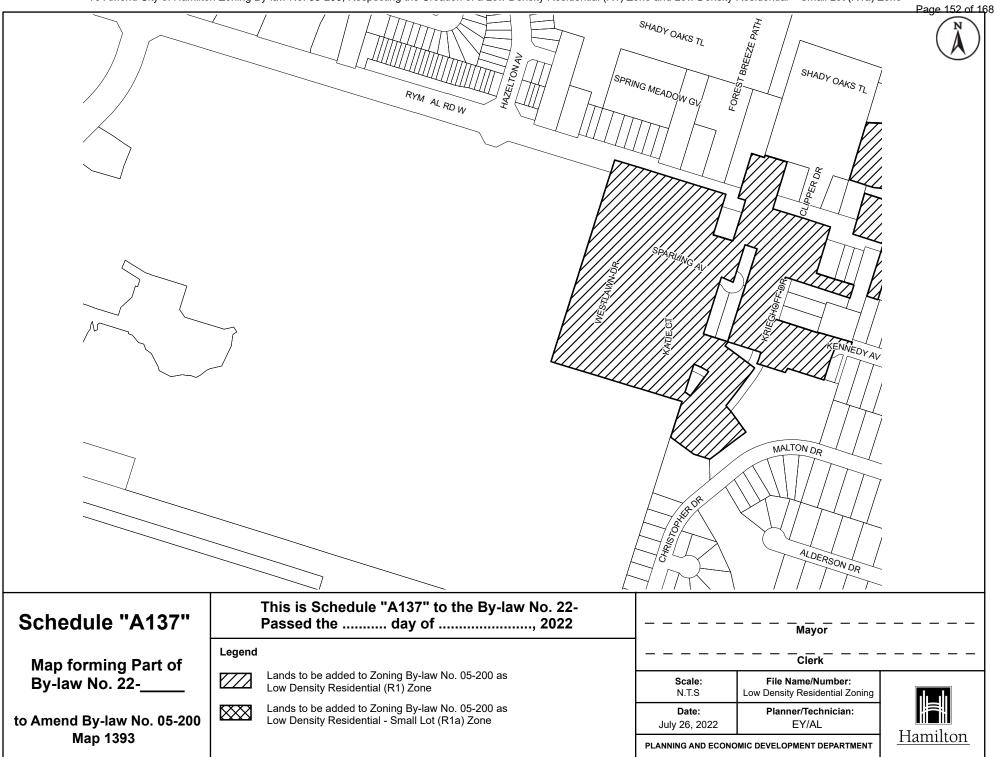




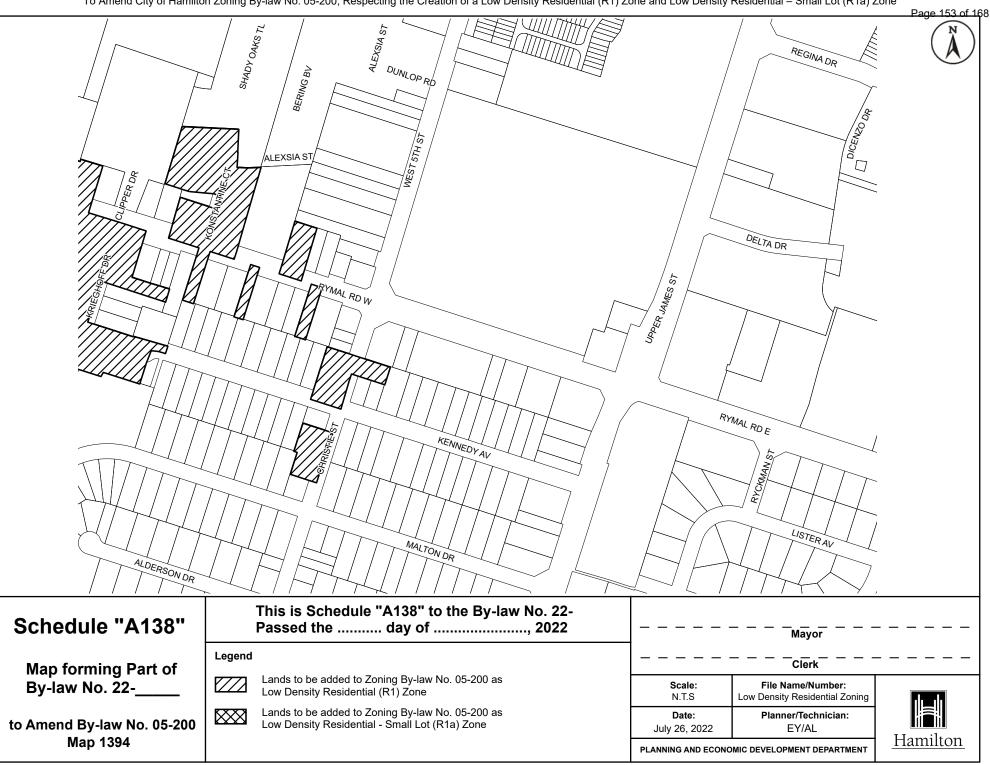


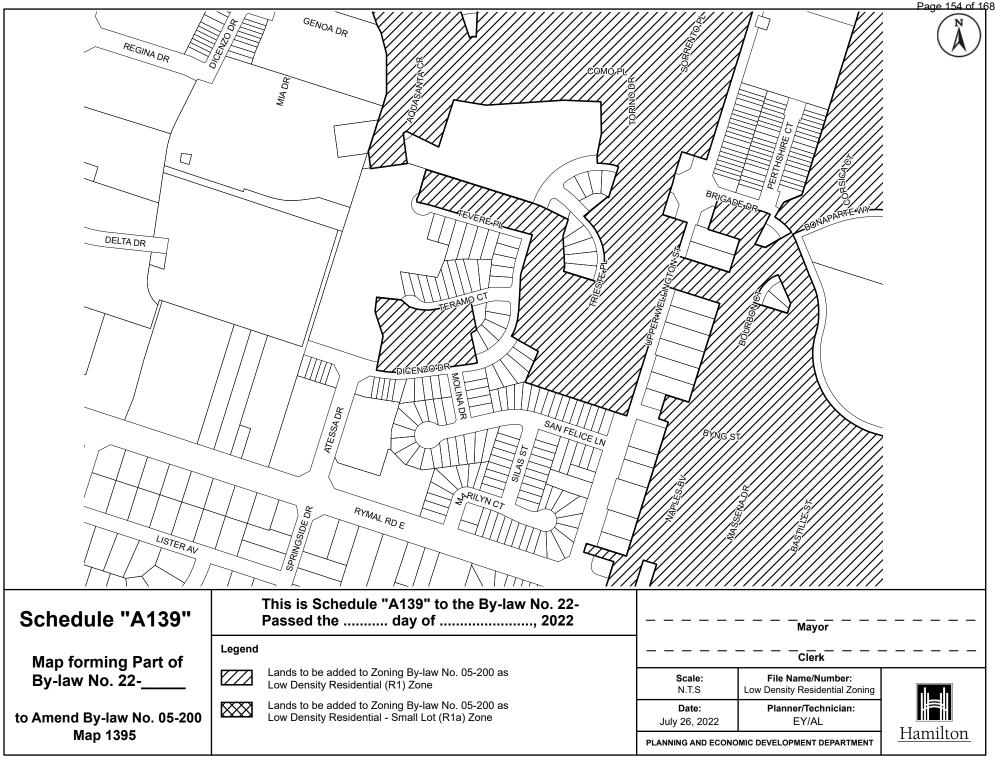


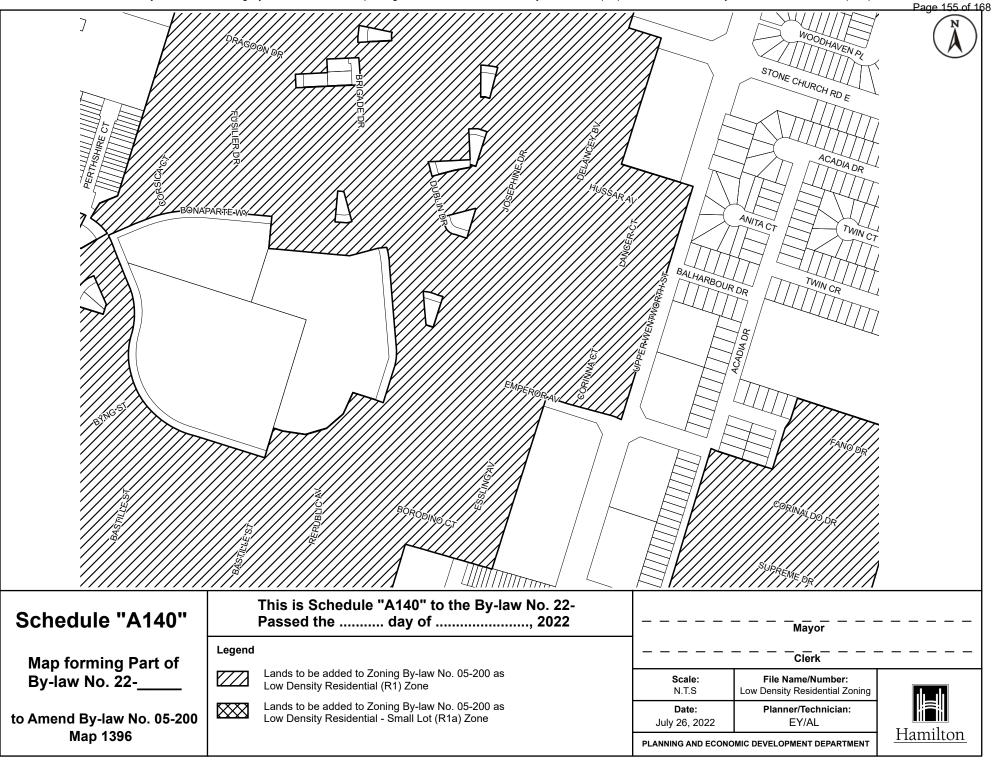
To Amend City of Hamilton Zoning By-law No. 05-200, Respecting the Creation of a Low Density Residential (R1) Zone and Low Density Residential – Small Lot (R1a) Zone

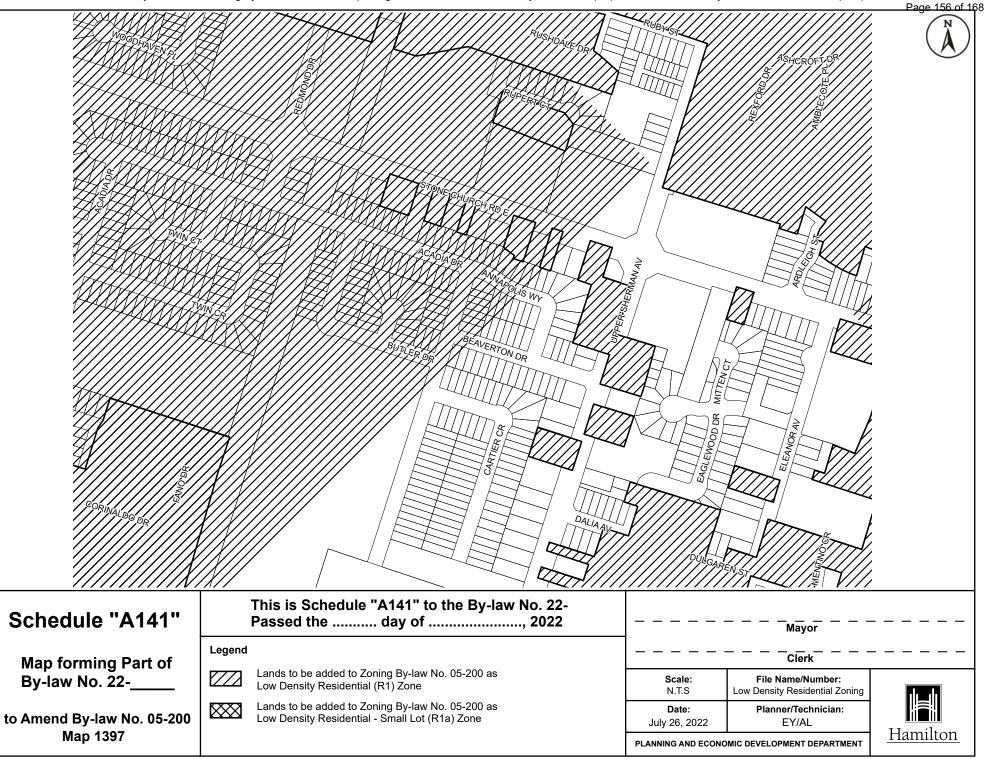


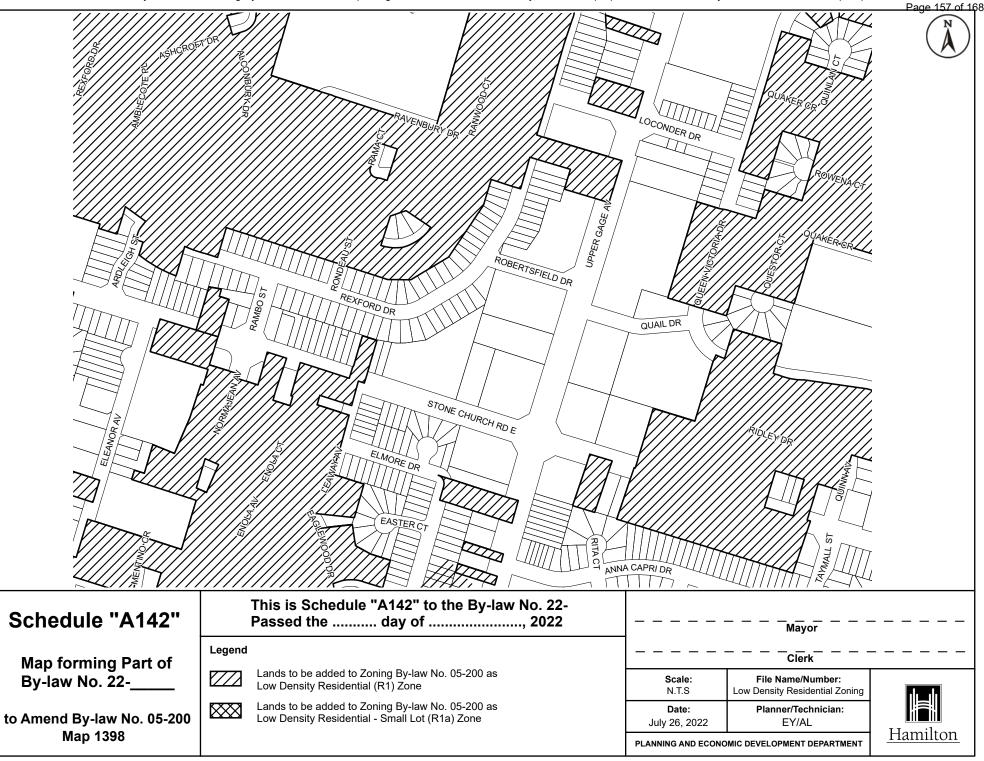


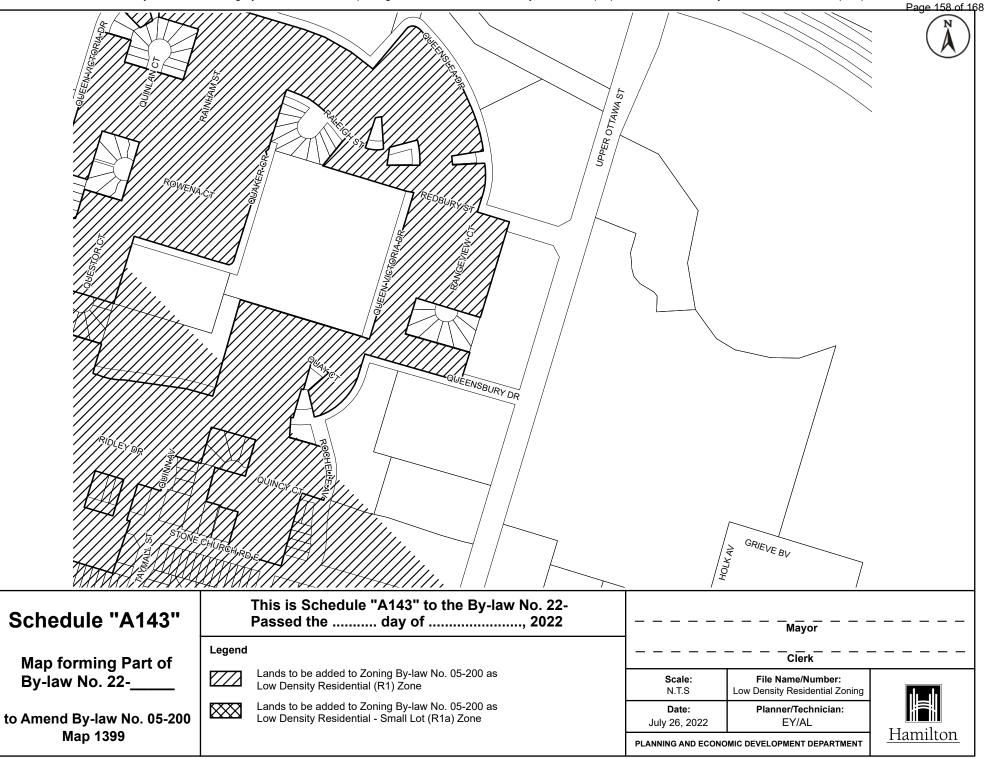


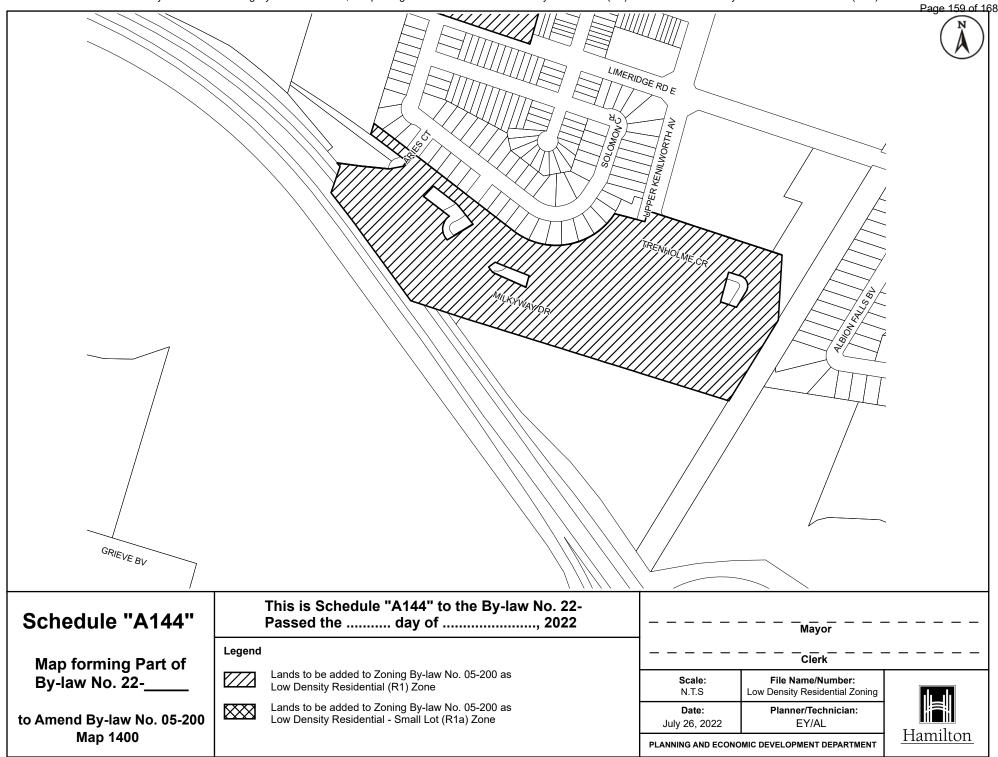


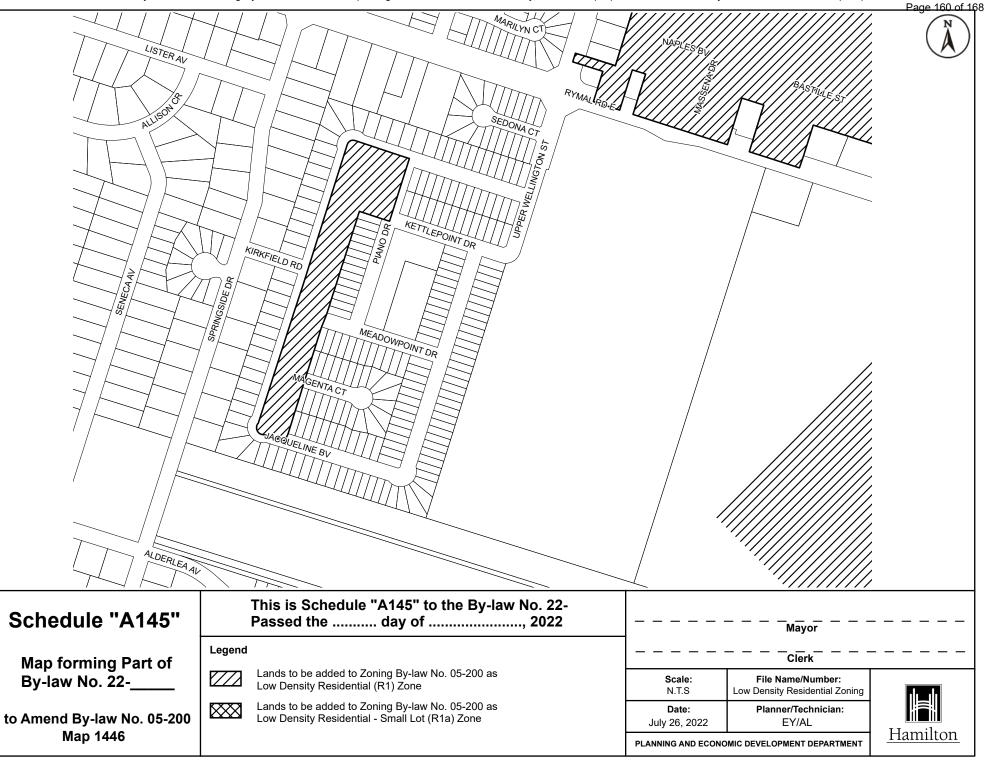


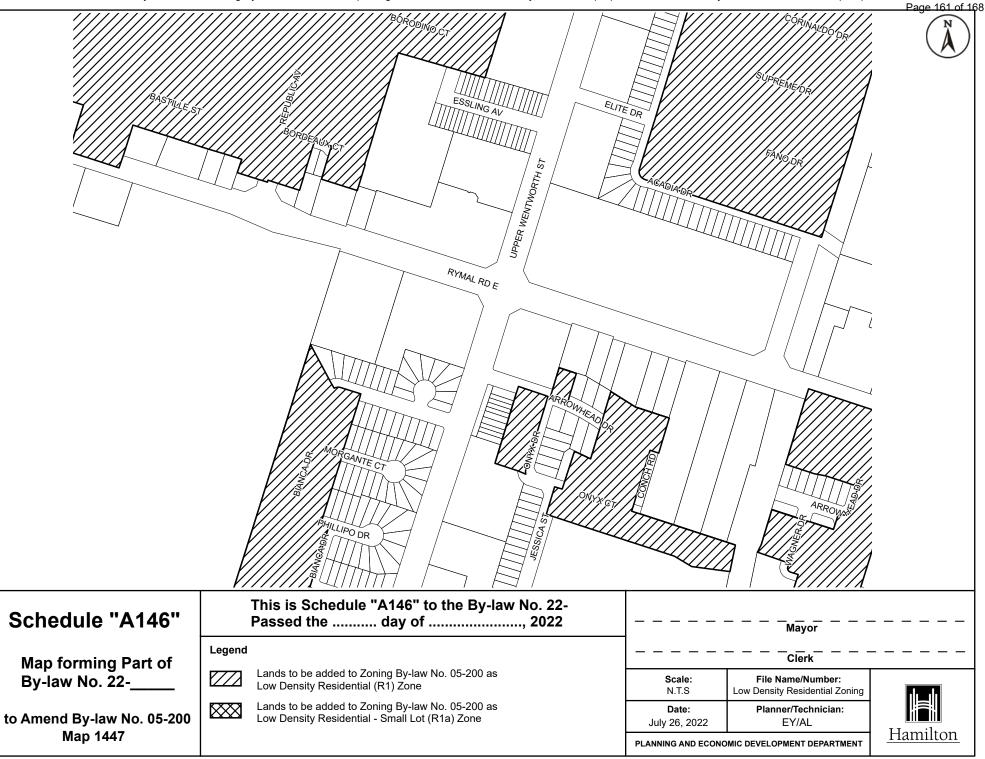


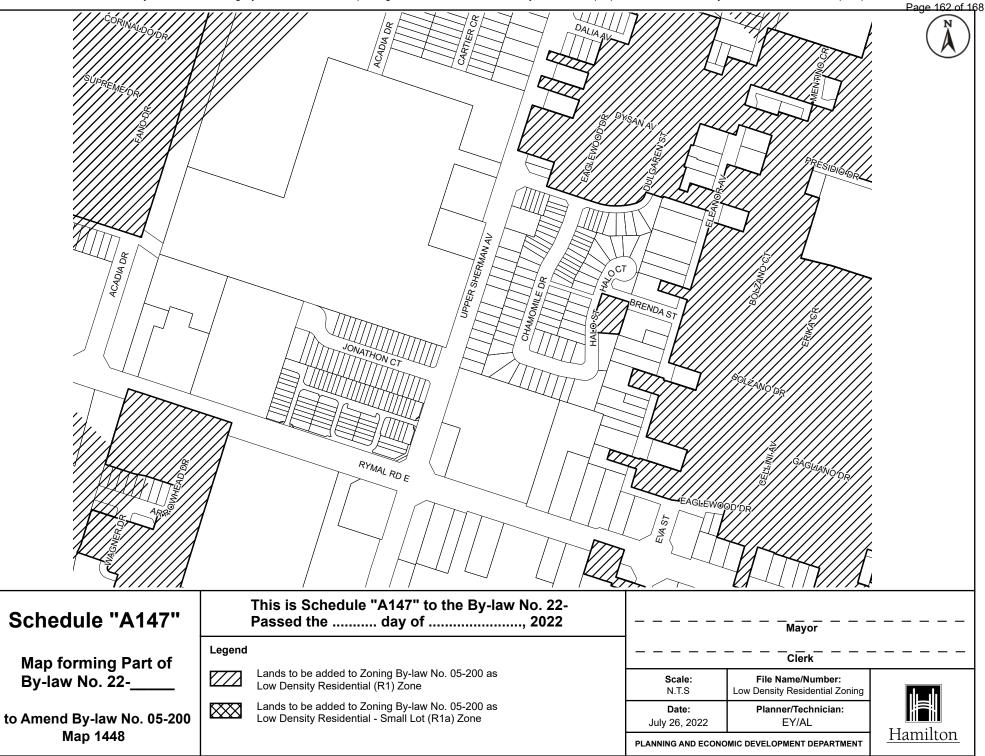


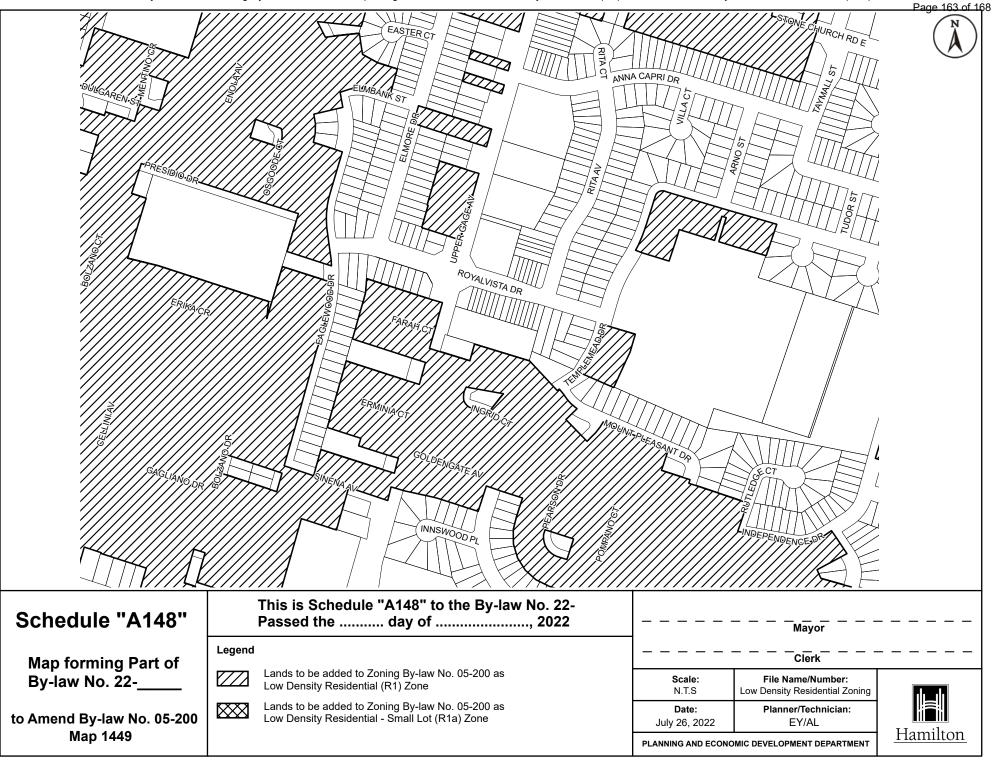


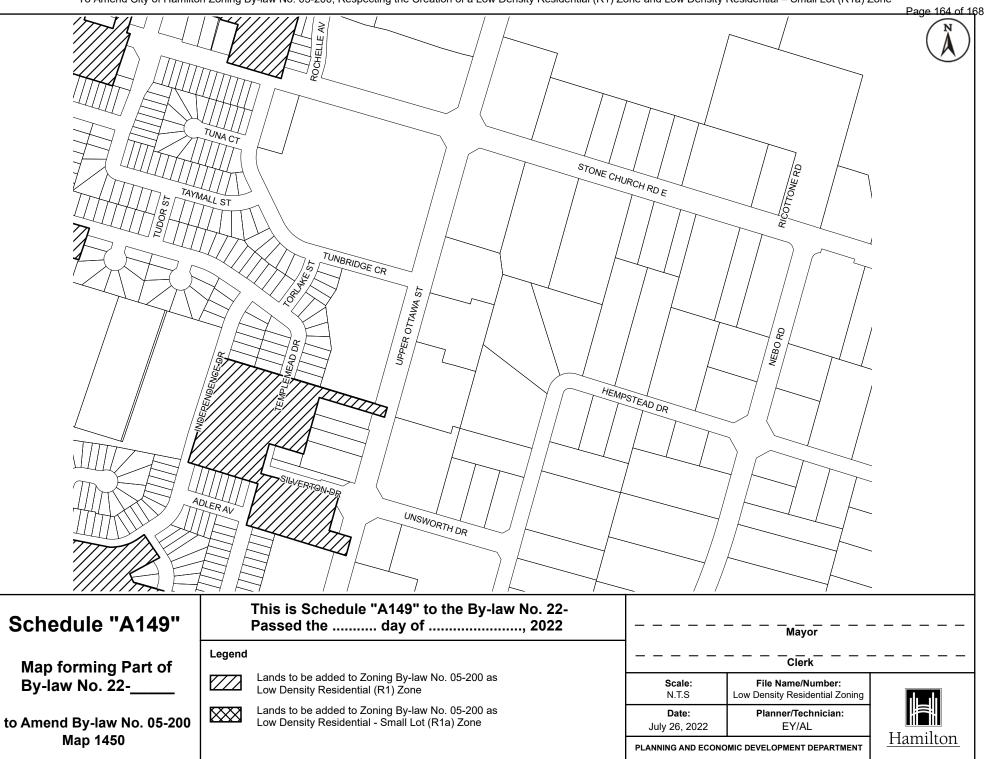












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| | | |
| Schedule "A150" | This is Schedule "A150" to the By-law No. 22- Passed the day of | |
| Map forming Part of By-law No. 22 | Legend | |
| | Lands to be added to Zoning By-law No. 05-200 as Low Density Residential (R1) Zone | Scale: File Name/Number: |
| to Amend By-law No. 05-200 Map 1496 | Lands to be added to Zoning By-law No. 05-200 as Low Density Residential - Small Lot (R1a) Zone | N.1.5 Low Density Residential Zoning Date: Planner/Technician: July 26, 2022 EY/AL Hamilton |
| | | PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT |

