

Cultural Heritage Impact Assessment Report

Royal Hamilton Yacht Club
555 Bay Street North

Date:
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Prepared for:
The Royal Hamilton
Yacht Club (RHYC)
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Land Acknowledgement

This report acknowledges that the subject lands addressed as 555 Bay Street N, City of Hamilton, are situated on the traditional territory of the Erie, Neutral, Huron-Wendat, Haudenosaunee and Mississaugas. This land is covered by Treaty 3 (the Between the Lakes Purchase), negotiated in 1784 and confirmed in 1792 between the Mississaugas and the British Crown. This Treaty cover almost 3 million acres between Lake Erie, Lake Ontario and Lake Huron.

Executive Summary

The subject property located at 555 Bay Street North, known locally as The Royal Hamilton Yacht Club (RHYC) has retained MHBC for cultural heritage services. The **property located at 555 Bay Street North (hereinafter noted as "the subject property")** is included in the City of Hamilton inventory and is not listed or designated under the *Ontario Heritage Act*. The property is currently owned by the City of Hamilton and is tenanted by the Royal Hamilton Yacht Club.

The subject property includes buildings and features to support the recreational use of the site. This includes a main clubhouse building constructed in the 1980s as well as an accessory building currently used to facilitate their sailing school which provide lockers and storage.

The building (**referred to in this report as the "sailing school facility"**) is in poor condition and is proposed for removal. The structural condition report provided in Appendix C identifies that the building has severe structural issues. The RHYC intends to construct a new facility to house the sailing school on the property. The purpose of this report is to determine whether or not the existing sailing school facility is of Cultural Heritage Value or Interest and is considered a heritage attribute of the property.

Summary of Cultural Heritage Evaluation

This report has completed an evaluation of the property at 555 Bay Street North and has concluded the following:

- The property is of Cultural Heritage Value or Interest primarily for its historical/associative values. The property has been used by the Royal Hamilton Yacht Club since 1937. The RHYC has been part of Hamilton's **recreational** activities since 1887, when the club was first formed and contributes to the recreational quality of life of the community;
- The property has contextual value given that buildings are physically and functionally located on the Hamilton waterfront in order to support the recreational use of the property;
- The property does not demonstrate design/physical value. The existing yacht club was constructed in the 1980s and is not identified as a heritage attribute the property. Other features of the property facilitate recreational use but are not considered heritage attributes.

- The existing sailing school facility which is proposed for demolition does not demonstrate design/physical value and is not considered a good candidate for conservation.

Summary of Impact Analysis & Mitigation Recommendations

- No adverse impacts are anticipated as a result of the removal of the sailing school. While the sailing school facility has historical/associative value and contextual value given its functional location adjacent to the bay, it is not considered a good candidate for conservation;
- The associative and contextual value of the lands will not change as a result of the building removal. The lands will continue to function as the yacht club and continue to support marine recreational culture along the waterfront;
- Documentation of the site (as provided in this report) is recommended in order to supplement the historic record;
- Two windows located at the south elevation of the sailing school facility may be salvaged and re-used as opposed to being deposited as landfill.

1.0 Introduction

1.1 Purpose of Report

MHBC has been retained by the Royal Hamilton Yacht Club ('RHYC') to undertake cultural heritage planning services for the property located at 555 Bay Street North. These lands are owned by the City of Hamilton and are leased to the RHYC. The RHYC operates a private yachting/sailing club with facilities for their members. There are two main structures located on the subject property:

- a) The main clubhouse building constructed in the 1980s; and
- b) An accessory building which houses the **club's private** sailing school, referred to in this report as the "sailing school facility".

The RHYC is proposing to remove the existing sailing school facility due to condition issues and construct a new sailing school facility.

The purpose of this report is to provide an evaluation of the subject property to determine whether or not it is of Cultural Heritage Value or Interest, and determine whether or not the proposed removal of the sailing school facility will result in adverse impacts.

1.2 Terms of Reference

This report has been scoped by City of Hamilton heritage planning staff to focus on the subject property and sailing school facility. The contents of this report are based on the Terms of Reference provided by the City of Hamilton (available on the City of Hamilton website at <https://www.hamilton.ca/build-invest-grow/planning-development/planning-policies-guidelines/cultural-heritage-impact>). This report is also consistent with guidelines for drafting impact assessments as per the Ontario Heritage Toolkit Info Sheet #5.

A site visit to the subject property occurred on June 9, 2023.

2.0 Property Description

2.1 Context

The subject property is located in the North End neighbourhood of Hamilton at the terminus of Bay Street North and Leander Drive. The subject property is bounded by Hamilton Harbour to the north and Leander Drive to the south. The property is located on the waterfront of Hamilton Harbour and is located within the vicinity of other boat clubs, City parks, and a public waterfront trail.

South of Leander Drive is a predominantly residential area with some commercial and retail uses. Further east of the subject property is the industrial sector of Hamilton. City parks are located to the west, including Pier 4 Park and Bayfront Park. South-west is the CN railyard. Refer to Appendix A for a context map.

The subject property consists of built and natural features, including the main clubhouse of the RHYC, the sailing school facility, an in-ground pool, landscaped open space, surface parking, a public trail, and storage areas. While not part of the subject property, the Yacht Club utilizes its waterfront access to launch boats and provide access to docks and wharfs which support the recreational use of the site.



Figure 1: Aerial photograph noting the location of the lands at 555 Bay Street North (Source: MHBC, 2023).

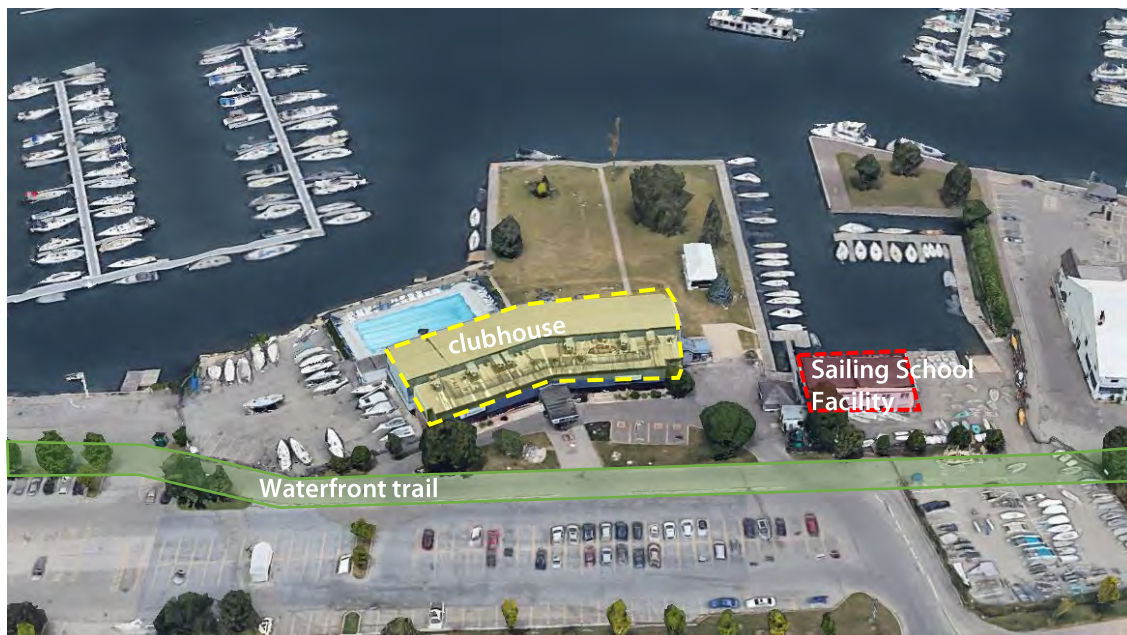


Figure 2: Aerial image of subject lands showing existing conditions. Looking north towards the marina. Sailing school facility outlined in red. (Google Maps, 2023)

2.2 Heritage Status

The City of Hamilton has a Municipal Heritage register consisting of listed and designated properties, as well as maintains an inventory of heritage properties. The City formerly had two heritage volumes/documents:

1. **'Volume 1: List of Designated Properties and Heritage Conservation easements under the Ontario Heritage Act' and,**
2. **'Volume 2: Inventory of Buildings of Architectural and/or Historical Interest'**

The volumes have been integrated into the *Heritage Property Mapping tool* as the City continues to update their inventory, either listing, designating or removing heritage properties.

The subject property is not designated under the *Ontario Heritage Act* and is not a **'listed' (non-designated)** property. The subject property is inventoried as a potential heritage resource. As such, the property remains on the *Heritage Property Mapping tool* as potentially having cultural heritage value.

The property was inventoried in 2013 as a result of a Class Environmental Assessment that was completed for the West Harbour pumping station. The Class EA identified the sailing school building at 555 Bay Street North as a potential cultural heritage resource. The Class EA completed in 2013 estimated that the existing sailing school facility was constructed in 1920.



Figure 3: Aerial map of the subject lands (outlined in red), included as an **"Inventoried Property"** on the City of Hamilton Interactive Map. Approximate location of the existing sailing school facility noted with red arrow. (Source: City of Hamilton Interactive Map, accessed 2023)

The inventory identifies the RHYC sailing school facility as the potential heritage resource, estimated to have been built in 1920 as noted in the Class EA report dated 2013.



Figure 4: View of the north elevation of the sailing school facility, looking south
(Source: MHBC, 2023)

3.0 Policy Context

3.1 Planning Act

The *Planning Act* contains a number of provisions respecting cultural heritage, either directly in Section 2 of the Act or Section 3 respecting policy statements and provincial plans. In Section 2, the *Planning Act* outlines 18 spheres of provincial interest. Regarding cultural heritage, Subsection 2(d) of the *Planning Act* provides that:

The Minister, the council of a municipality, a local board, a planning board and the Municipal Board, in carrying out their responsibilities under this Act, shall have regard to, among other matters, matters of provincial interest such as ...

(d) the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest;

The *Planning Act* provides the overall broad consideration of cultural heritage resources through the land use planning process.

In support of the provincial interest identified in Subsection 2 (d) of the *Planning Act*, and as provided for in Section 3, the Province has refined policy guidance for land use planning and development matters in the *Provincial Policy Statement, 2020* (PPS). Section 3 (5) identifies that all decisions of Council in respect of a planning matter shall be consistent with the PPS. When addressing cultural heritage planning, the PPS provides for the following:

2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

The PPS defines the following terms:

Adjacent (as it relates to cultural heritage resources: means for the purposes of policy 2.6.3, those lands contiguous to a protected heritage property or as otherwise defined in the municipal official plan.

Built Heritage Resource: means a building, structure, monument, installation or any **manufactured or constructed part or remnant that contributes to a property's cultural** heritage value or interest as identified by a community, including an Indigenous community. Built heritage resources are located on property that may be designated under Parts IV or V of the Ontario Heritage Act, or that may be included on local, provincial, federal and/or international registers.

Conserved: means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision maker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments.

Protected Heritage Property: means property designated under Parts IV, V or VI of the Ontario Heritage Act; property subject to a heritage conservation easement under Parts II or IV of the Ontario Heritage Act; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites.

Significant: e) in regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the Ontario Heritage Act.

3.2 Ontario Heritage Act

The *Ontario Heritage Act*, R.S.O., 1990, c.0.18 remains the guiding legislation for the conservation of significant cultural heritage resources in Ontario. The evaluation of resources contained in the HIA has been guided using the criteria provided in *Regulation 9/06* of the *Ontario Heritage Act* which outlines the mechanism for determining cultural heritage value or interest (CHVI). Bill 23 made amendments to the *Ontario Heritage Act*,

requiring that any property worthy of designation must meet a minimum of two criteria O.Reg 9/06.

3.3 City of Hamilton Urban Official Plan

The subject property is identified in Schedule E – Urban Structure as **“Non-Decision 117 (a)” and are subject to Non Decision 113 West Harbour Setting Sail.**

MMAH approved most of the UHOP in 2011, however, the revised Setting Sail Secondary Plan for West Harbour was appealed and is subject to Non-decision 117. These lands are therefore not in effect in the City of Hamilton Urban Official Plan, Volume 1.

Notwithstanding, the City of Hamilton Urban Official Plan, Volume 1 contains policies regarding the conservation of cultural heritage resources. These policies are provided in Chapter B – Communities, section 3.4.

Section 3.4.1 sets out the policy goals for heritage resources, applicable policies include the following:

3.4.1.3 Ensure that new development, site alterations, building alterations and additions are contextually appropriate and maintain the integrity of all on-site or adjacent cultural heritage resources.

3.4.1.4 Encourage the rehabilitation, renovation, and restoration of built heritage resources to maintain their active use.

General Cultural Heritage Policies are set out in section 3.4.2 and include the following:

- a) Protect and conserve the tangible cultural heritage resources of the City, including archaeological resources, built heritage resources, and cultural heritage landscapes for present and future generations*
- b) Identify cultural heritage resources through a continuing process of inventory, survey, and evaluation, as a basis for the wise management of these resources.*
- i) Use all relevant provincial legislation, particularly the provisions of the Ontario Heritage Act, the Planning Act, R.S.O., 1990 c. P.13, the Environmental Assessment Act, the Municipal Act, the Niagara Escarpment Planning and Development Act, the Cemeteries Act, the Greenbelt Act, the Places to Grow Act, and all related plans and strategies in order to appropriately manage, conserve and protect Hamilton’s cultural heritage resources.*

Cultural Heritage Evaluation Criteria are set out in section 3.4.2.9. The city will use and require the following criteria to assess and identify cultural heritage resources:

- a) *Prehistoric and historical associations with a theme of human history that is representative of cultural processes in the settlement, development, and use of land in the City*
- b) *prehistoric and historical associations with the life or activities of a person, group, institution, or organization that has made a significant contribution to the City;*
- c) *architectural, engineering, landscape design, physical, craft, or artistic value;*
- d) *scenic amenity with associated views and vistas that provide a recognizable sense of position or place;*
- e) *contextual value in defining the historical, visual, scenic, physical, and functional character of an area; and,*
- f) *landmark value*

Any property that meets one or more of the criteria will be considered a heritage resource.

3.4 Setting Sail Secondary Plan

The Setting Sail Secondary Plan was first issued under the previous Hamilton Official Plan and applies to the area of Hamilton referred as West Harbour.

The subject lands are identified as follows:

- Schedule M-1 Planning Area: **'Waterfront Recreation Area'**;
- Schedule M-2 General Land Use: **'Marine Recreational'**; and
- Schedule M-6 Fisheries Policy Areas: **'Area of Opportunity'**.

Key views are identified in Schedule M-5 Public Realm. These are key viewing opportunities of Hamilton Harbour and Lake Ontario. The subject lands are not identified as providing any key views, however, are surrounded by other key vistas and fall within identified view sheds.

Policy A.6.3.2.7 provides details on 'celebrating the City's Heritage'. This area of Hamilton is rooted in cultural and industrial heritage, with remnants of Hamilton's past. These remnants should be conserved and celebrated, and steps to conservation should include:

- i) *Conserving and strengthening the overall character of the West Harbour neighbourhoods and streetscapes*
- ii) *Conserving, restoring and reusing historic buildings and structures*

- iii) *Reflecting and interpreting the industrial, marine and cultural heritage in the design of new buildings and open spaces*
- iv) *Encourage development of cultural institutions to inform residents and visitors about the area, and*
- v) *Provide public open spaces for cultural festivals.*

Additional heritage policies are outlined in section A.6.3.3.3 of the secondary plan. These policies are as follows:

- 1) *In accordance with the Planning Act and the Ontario Heritage Act, West Harbour will promote the conservation of significant built heritage resources and landscapes*
- 2) *Land use regulation and incentive programs will be used in the retention, conservation, rehabilitation, restoration or adaptive re-use of properties that **have been designated under the Ontario Heritage Act or listed in the City's inventory***
- 3) *The City may introduce incentive programs such as loans, grants and density bonuses to encourage the appropriate retention of buildings with historic character or architectural value*
- 4) *A Heritage Impact Assessment may be required for any development that proposes to demolish, alter or construct new buildings on or adjacent to properties that are: designated, listed/inventoried, sites featuring open spaces or landscapes listed on the landscape resource inventory and that are within or adjacent to a heritage conservation district*
- 5) *The HIA is to be submitted with a development or building permit application and will be reviewed by the Heritage Committee*
- 6) *As part of development of land, the city may require heritage properties by retained on site and incorporated into the land use and easements may be required*
- 7) *New development adjacent to heritage buildings or in areas containing heritage buildings shall*
 - a. *Reflect the setbacks, heights and cornice lines of surrounding buildings*
 - b. *Support the creation of continuous street wall*
 - c. *Maintain consistent street orientation and building line*
 - d. *Reflect the character, massing and materials of surrounding buildings*
- 8) *City will pursue opportunities to conserve municipally owned heritage resources.*

3.5 West Harbour Waterfront Recreation Master Plan

The West Harbour Recreation Master Plan defines planning design guidelines to help inform development and landscapes of the West Harbour community. It establishes a waterfront vision, drawing on the Setting Sail secondary plan policies.

The subject lands are in an area defined as part of **the 'Main Basin'**. **The Master Plan** discusses the RHYC and sailing club, stating that the main clubhouse will remain in its **current location, with some façade improvements to "freshen the aesthetics"**. **The sailing school facility is noted in the Master Plan as being "...beyond improvement and should be replaced"**. **The sailing school** building should be replaced with a contemporary buildings that is situated closer to the RHYC main clubhouse, making a stronger connection between the sailing school and RHYC, and open up views to the sailing school basin.

4.0 Historical Overview

4.1 Royal Hamilton Yacht Club

The Royal Hamilton Yacht Club was established in 1887. The first clubhouse was situated on the east side of James Street North, beside the Toronto and Hamilton Steamship Company (Unterman McPhail Cuming Associates, 1995; Houghton, 2007). The clubhouse was built in 1888 and consisted of a two storey wood frame building with **locker rooms on the first floor and a member's lounge on the second floor** (Unterman McPhail Cuming Associates, 1995; Houghton, 2007). In 1891 Queen Victoria permitted a name change from the Hamilton Yacht Club to the Royal Hamilton Yacht Club (RHYC Website, accessed online 2023).

In 1891, the building was relocated from James Street, across the frozen Burlington Bay to a new location near the foot of Bay Street (Unterman McPhail Cuming Associates, 1995). The building was renovated and enlarged at this time.



Figure 5: RHYC building, c.1894 (Courtesy of HPL)



Figure 6: Original RHYC building, c.1891 (Houghton, 2007)

The 1898 Fire Insurance Plan identifies the location of the **"Royal Hamilton Yacht Club"** at the terminus of Bay Street, north of Brock Street. The building is noted as a 2 storey wood frame building.

The original clubhouse burned down in 1915 and the RHYC subsequently relocated to the foot of McNab Street (Unterman McPhail Cuming Associates, 1995; Houghton, 2007; RHYC Website). However, the 1933 Fire Insurance Plan continues to illustrate a building in the same location at the terminus of Bay Street. It was not until 1938 that the RHYC began constructing a new clubhouse at what is now 555 Bay Street North.

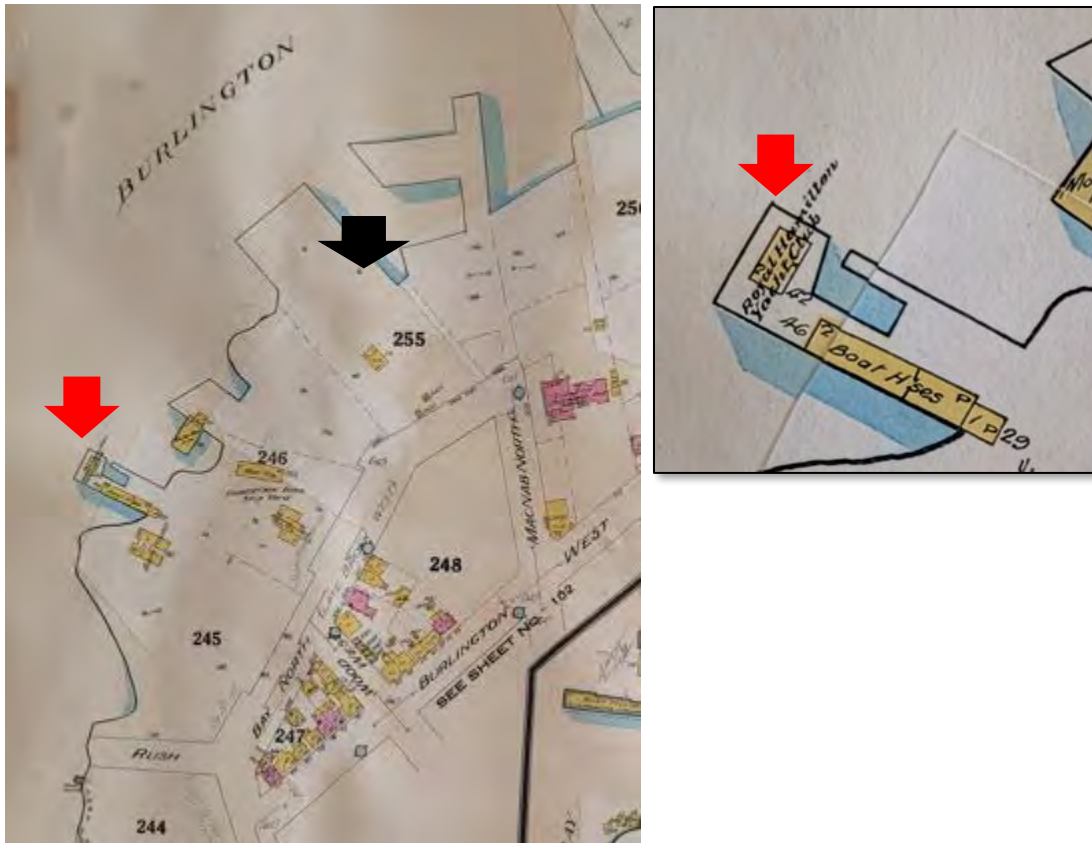


Figure 9: 1933 Fire Insurance Plan noting the former location of the Royal Hamilton Yacht Club. Existing location noted with black arrow. (Source: Courtesy of the Hamilton Public Library)



Figure 10: Original clubhouse, partially demolished prior to the re-location and construction of the 1938 building (RHYC)

In 1938 the RHYC relocated their operations to the foot of McNab Street North, on the subject property, where it currently resides today. The subject lands are owned by the City of Hamilton.

In 1911 the federal government established the Hamilton Harbour Commission for the management of the lands adjacent to the harbour (Unterman McPhail Cuming Associates, 1995). This was a joint agency with both federal and municipal members, with ownership of the lands being fully transferred to the City of Hamilton. In 1937, the RHYC negotiated the long-term lease of the current lands from the Commission, in addition to the rebuilding of the docking facilities. One year later in 1938, the RHYC built a clubhouse in the Art Deco architectural style on the subject property. This building was removed in the 1980s (See Figure 11).



Figure 11: Image of the 1938 Art Deco style Club House (since removed) (Robert Mazza c.1955)

The 1947 Fire Insurance Plan indicates the 1938 club house located at the terminus of McNab Street North (See Figure 12).



Figure 12: 1947 Fire Insurance Plan. Property at what is now 555 Bay Street outlined in red. (Source: Courtesy of the Hamilton Public Library)

In 1965 a pool was added to the subject property. In 1981 the Art Deco style clubhouse was demolished to make way for the existing club house facility. The main clubhouse that resides on the property today was built shortly after demolition.

4.2 Sailing School Facility

The sailing school facility was constructed in 1944 and can be described as a 2 storey wood frame structure. The original purpose of the structure was to provide lockers/storage areas ancillary to the main clubhouse (Penny, 1988). In 1946, the boat house was enlarged to add additional lockers and a kitchen area at the first storey. This resulted in the construction of a second storey. This structure appears on the 1947 Fire Insurance Plan and can be described as a wood frame building (See Figure 12).

The sailing school program began in 1941 to teach youth how to sail, with instruction by parents and volunteers. This program **continued through to the 1950's** (Penny, 1988). A

formal program was developed for junior members by the late 1950s, which resulted in the creation of a professional sailing school (Penny, 1988). In 1962 the top floor of the sailing school facility was expanded to accommodate a meeting area, office and washrooms for the junior sailors (See Figure 13).



Figure 13: Aerial photograph of the subject property. Location of the sailing school facility noted in red. Note that the third addition to the clubhouse had not yet been constructed (Robert Mazza c.1955)

5.0 Description of Built Features

5.1 Royal Hamilton Yacht Club (RHYC)

The subject property includes both natural and built features in order to support the recreational activities of the RHYC. This includes the main clubhouse, a pool, the sailing school facility, a patio, as well as access to the bay, marina and docks. The main club house can be described as a 2 storey metal clad building constructed in the 1980s. Open landscaped space is located north of the clubhouse, which includes the in-ground pool.

Immediately south of the property is the City of Hamilton Waterfront trail which runs along the waterfront of Hamilton Harbour (refer to figure 2). The context of the surrounding area varies, with a mix of recreational, residential, industrial and institutional uses. The immediate surrounding area for the lands located north of Leander Drive can be characterized as predominantly recreational, leveraging access to the waterfront.

5.2 Sailing School Facility

The sailing school facility can be described as a two storey wood frame building, located east of the main clubhouse. It is oriented towards the bay and marina, with storage lockers facing the docks. The RHYC operates as a private club. The sailing school facility is bounded by a fence and access to certain areas is restricted.

The sailing school building was constructed in 1944. Two additions were constructed in 1946 and again in 1962 (See Figures 15 & 16).

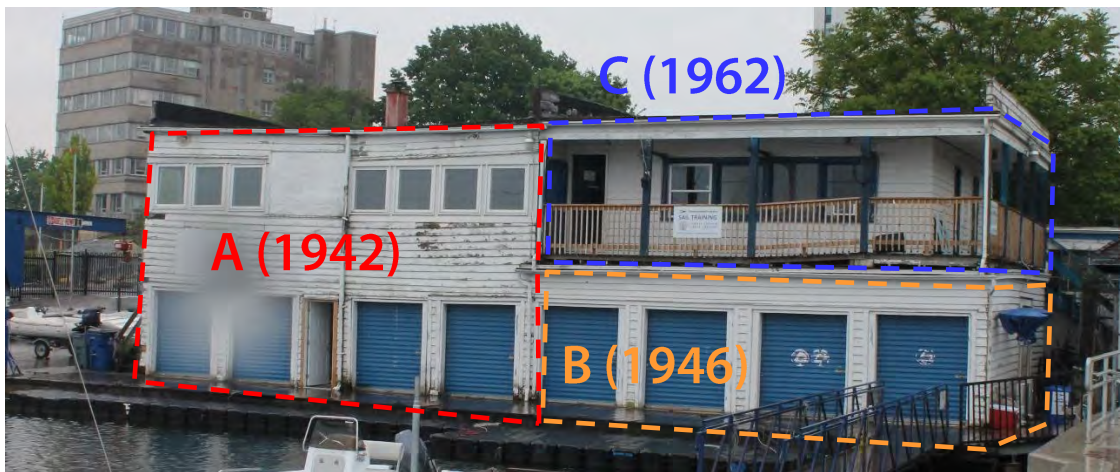


Figure 15: School facility as seen from the marina. Components identified as A, B and C (Source: MHBC, 2023).

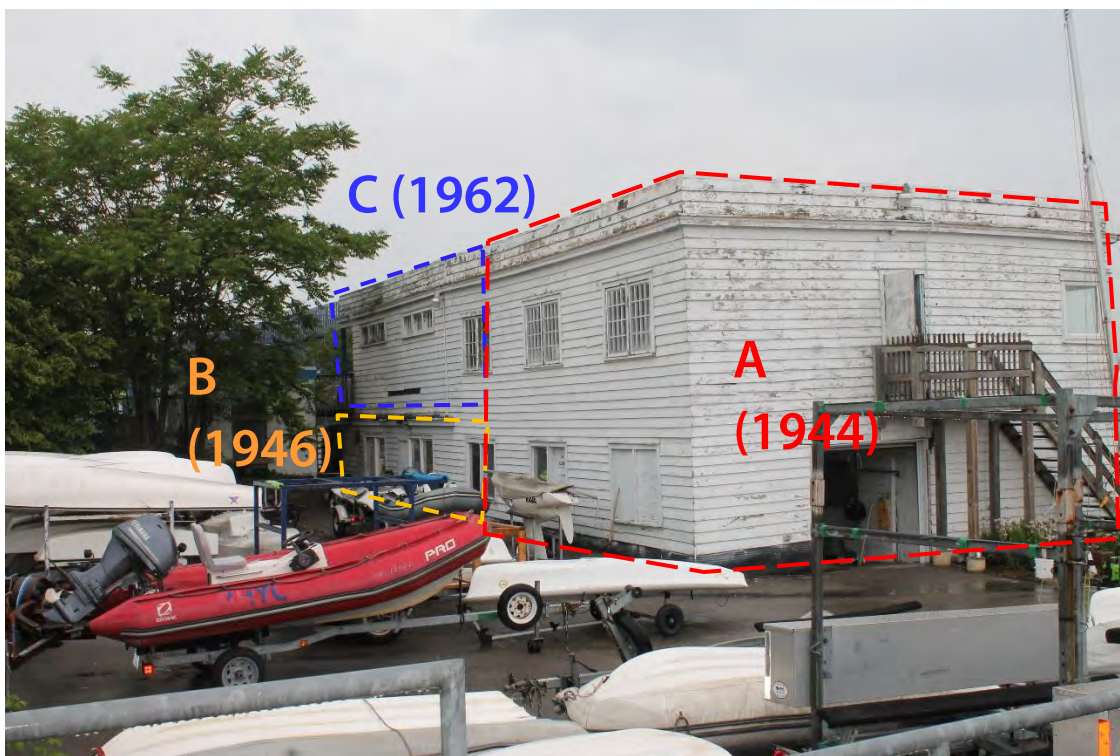


Figure 16: School facility as seen from the rear. Components identified as A, B and C (Source: MHBC, 2023).

The building morphology has been identified as sections A, B and C in the figures above. These components represent the alterations and additions to the building, as follows:

Component	Date of Construction	Notes
A	1944	Original Boat House & Locker Facility
B	1946	Extension to Locker Facility
C	1962	2 nd Storey Addition (washrooms, office)

5.3 Condition Review

The exterior of the building has been relatively un-altered since the last addition was constructed in 1962. The majority of the windows and window openings are original on the second floor. The building is in poor condition with visible signs of weathering and damage. This is most apparent on the north elevation which has signs of damage due to exposure to the elements. Damage is noted on the exterior wood siding and balcony posts. Additional areas of concern include warped flooring as well as heaving of the concrete floor/slab which has the potential to cause structural damage to the remainder of the building.

According to the condition report prepared by MTE dated September, 2023 (see Appendix C), the building has severe issues related to the following:

- Tilting of the building from the foundation material below the footing structure being washed out;
- Warping of the second floor;
- Cracked slabs; and
- Balcony post base condition issues.

The issues related to the footing structure of the building is an issue where, "If not corrected, the condition will worsen to the point where the building is no longer serviceable." In order to correct the issue, a deep foundation system would need to be implemented.



North elevation damage to exterior wood framing. Framing is rotting.

(MHBC, 2023)



North elevation roof gutter leaking.

(MHBC, 2023)



Compromised balcony post and support. Bottom of post is rotting.

(MHBC, 2023)



Three inch heave in concrete floor.

(MHBC, 2023)

6.0 Cultural Heritage Evaluation

6.1 Evaluation Criteria

The *Ontario Heritage Act* prescribes heritage evaluation criteria under *Ontario Regulation 9/06*, which is the legislated criteria for determining cultural heritage value or interest (CHVI). This criteria is related to design/physical, historical/associative and historical values as follows:

The property has design value or physical value because it,

- 1. is a rare, unique, representative or early example of a style, type, expression, material or construction method,*
- 2. displays a high degree of craftsmanship or artistic merit, or*
- 3. demonstrates a high degree of technical or scientific achievement.*

The property has historical value or associative value because it,

- 4. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,*
- 5. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or*
- 6. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.*

The property has contextual value because it,

- 7. is important in defining, maintaining or supporting the character of an area,*
- 8. is physically, functionally, visually or historically linked to its surroundings, or*
- 9. is a landmark.*

A property *may* be designated if it meets two or more of the criteria.

The City of Hamilton Official Plan also provides evaluation criteria. An evaluation using the City evaluation criteria is also provided, however, it should be noted that the municipal criteria is not recognized by the *Ontario Heritage Act* or the *Planning Act*.

6.2 Ontario Regulation 9/06 Evaluation

The following provides an evaluation of whether or not the property at 555 Bay Street North meets the legislated criteria for determining Cultural Heritage Value or Interest (CHVI). The summary of the evaluation is provided in table 1.0 below.

Table 1.0 – O.Reg 9/06 Evaluation

Criterion	Evaluation: 555 Bay Street North
Physical Value	
1. Is a rare, unique, representative or early example of a style, type, expression, material or construction method,	No. Neither the clubhouse nor the sailing school facility are considered rare, unique, or representative. The building was constructed as a utilitarian accessory building.
2. Displays a high degree of craftsmanship or artistic merit, or	No. The existing buildings located on the property do not demonstrate a high degree of craftsmanship or artistic merit.
3. Demonstrates a high degree of technical or scientific achievement.	No. The existing buildings do not demonstrate a high degree of technical or scientific achievement.
Historical Value	
4. Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,	Yes. The property is associated with the RHYC, which is a long-standing and significant organization to the community and has contributed to the recreational quality of life.
5. Yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or	No. The existing buildings are not likely to yield further information which would contribute to the understanding of the community.
6. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.	No. The designer/builders are not known, but could be added to the historic record should this information become available in the future.
Contextual Value	
7. Is important in defining, maintaining or supporting the character of an area,	Yes. The character of the area includes a range of recreational and community uses. The property is not important in defining the character of the area for the existing buildings. However, the use of the property and the presence of the docks and recreational facilities is important in maintaining the character of the area.
8. Is physically, functionally, visually or historically linked to its surroundings, or	Yes. The property is functionally and physically linked to its surroundings. The property requires access to the bay in order to facilitate its recreational purposes.
9. Is a landmark.	No. The property does not include any buildings which are considered landmarks of the community.

6.2.1 *Design/physical value*

The subject property does not demonstrate design/physical value. None of the buildings located on the subject property are rare, early, unique, or representative of an architectural style. The main club house is contemporary and is not of CHVI. The Sailing School facility was constructed as a utilitarian structure in 1944 and is not representative of any architectural style. The subsequent additions to the building do not add any Cultural Heritage Value or Interest to the structure. The building is not representative of any style, era or significant construction method, nor does it display a high degree of craftsmanship or scientific achievement. The building is simple in its design and construction.

6.2.2 *Historical/associative value*

The subject property demonstrates historical/associative value for its use by the RHYC. The RHYC itself has been an organization within the City of Hamilton since it was formed in 1887. Over its 135 year history, it has accumulated recognition and status within the City. The subject property was selected as a site for the RHYC facilities in 1938.

The RHYC was the first sailing club on Hamilton Harbour and is a long-standing organization which is integrated with the local community. The Sailing School building facilitates the utilitarian needs of the RHYC. While it is part of the property, the building in itself does not demonstrate significant historical/associative value.

6.2.3 *Contextual value*

The subject property demonstrates contextual value given that it is physically and functionally linked to its surroundings. The context of the area is important for the use of the property for marine recreational purposes, given its access to the harbour. The buildings and structures located on the subject property are functionally linked to each other, providing areas for various activities and ancillary uses to one another. Provided that the use of the property and orientation of buildings continue to have access to the waterfront, they would maintain this **contextual value. Therefore, the buildings' existing** location in-situ is not considered significant. The contextual value of the property can be maintained by new structures. The property does not include buildings or features which are considered landmarks of the local community.

6.3 City of Hamilton Evaluation Criteria

Criterion	Yes/ No
a) prehistoric and historical associations with the life or activities of a person, group, institution, or organization that has made a significant contribution to the City;	Yes. The RHYC has continued to provide and support for recreational activities that leverage the Hamilton waterfront.
b) Prehistoric and historical associations with a theme of human history that is representative of cultural processes in the settlement, development, and use of land in the City	Yes. The building is associated with RHYC, an early recreational establishment that contributed to the recreational culture of Hamilton and use of the waterfront for sailing and boating activities.
c) prehistoric and historical associations with the life or activities of a person, group, institution, or organization that has made a significant contribution to the City;	Yes. RHYC has contributed to the recreational quality of life within the City.
d) architectural, engineering, landscape design, physical, craft, or artistic value;	No. The existing buildings do not have physical value.
e) scenic amenity with associated views and vistas that provide a recognizable sense of position or place;	No. The Secondary Plan does not identify significant views from the lands. Notwithstanding, views of Lake Ontario are available from the subject lands, as well as the public pathway. These views are available at many locations along the shoreline.
f) contextual value in defining the historical, visual, scenic, physical, and functional character of an area; and,	No. The property maintains the character of the area due to the presence of the harbour and docks. The existing buildings do not contribute to any historical, visual, or scenic value. The buildings are over important in terms of facilitating and supporting the existing recreational uses.
g) landmark value	No. The buildings on the property are not considered landmarks.

6.3.1 *Summary of Cultural Heritage Value or Interest*

While the property at 555 Bay Street North exhibits historical/associative and contextual values, none of the existing buildings demonstrate design/physical value. The sailing school facility is associated with the operations of the RHYC and maintains its physical and functional relationship with the Hamilton waterfront.

7.0 Heritage Impact Assessment

The proposal includes the demolition of the sailing school facility. The removal of the building is required in order to construct a new sailing school facility. The existing sailing school facility is in poor condition, which is recognized by the West Harbour Waterfront Master Plan.

The following impact assessment is guided by the Heritage Toolkit of the Ministry of Heritage, Sport, Tourism and Culture Industries as follows:

- Destruction: of any, or part of any *significant heritage attributes* or features;
- Alteration: that is not sympathetic, or is incompatible, with the historic fabric and appearance;
- Shadows: created that alter the appearance of a *heritage attribute* or change the viability of a natural feature or plantings, such as a garden;
- Isolation: of a *heritage attribute* from its surrounding environment, context or a significant relationship;
- Direct or Indirect Obstruction: of significant views or vistas within, from, or of built and natural features;
- A change in land use: such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces;
- Land disturbances: such as a change in grade that alters soils, and drainage patterns that adversely affect an archaeological resource.

The below table provides an assessment of the proposed demolition of the sailing school facility.

Table 2.0 – Impact Assessment

Impact	Comment
Destruction / alteration of heritage attributes	No impact. Given that the Sailing School facility is not considered a heritage attribute of the property, no adverse impacts are anticipated as a result of its removal. The historical/associative and contextual values of the property will not be impact by the removal of the building provided that the property continues to function in its current capacity.
Shadows	The removal of the building will not impact shadows. No development concept is proposed at this time, therefore potential shadowing impacts are unknown.
Isolation	No impact. No physical heritage attributes have been identified for the building or property. The removal of the building will not isolate any heritage attributes.
Direct or Indirect Obstruction of Views	No Impact. No views to or from the building are considered heritage attributes. For a short period of time, a portion of the Bay will be visible from the street along Bay Street North until a new building is constructed.
A Change in Land Use	No Impact. The subject property will continue recreational use. A future building will be constructed which will continue the function of the lands.
Land Disturbance	No Impact. There are no heritage attributes on the property. Regardless, removal of the building will not cause land disturbances.

7.1.1 *Summary of Impact Analysis*

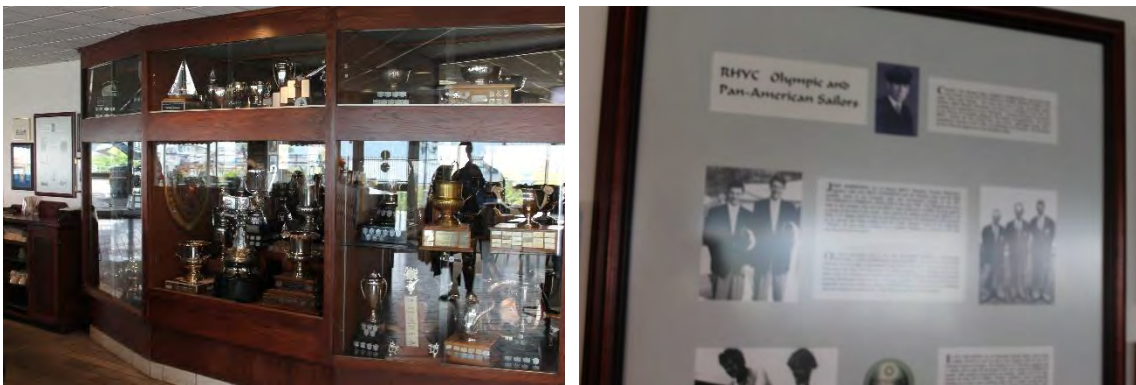
The assessment concludes that no adverse impacts are expected as a result of the removal of the sailing school facility. The heritage value of the property is primarily related to its historical/associative and contextual **values given the property's** recreational use and history. None of the built or natural features located on the subject property demonstrate design/physical value. The historical/associative and

contextual relationship between the sailing school facility, clubhouse and waterfront can be maintained with a new building which will allow for the continued use of the site as part of the RHYC.

8.0 Recommendations

8.1 Commemoration and Salvage

Given that the existing sailing school facility has historical/associative and contextual value as part of the overall history and evolution of the RHYC, it is recommended that the existing sailing school facility be documented in order to supplement the historic record. The documentation provided in this report is considered sufficient documentation. It is important to note that commemoration of the site is provided at the interior of the building. This includes several display cases, artifacts, memorabilia, commemorative photos and plaques throughout the facility. These are considered sufficient in the ongoing commemoration and interpretation of the activities of the RHYC.



Figures 17 & 18: (left) View of artifact and trophy case located within the clubhouse, (right) Example of clubhouse memorabilia located throughout the facility (Source: MHBC, 2023).

This report also considers whether or not the sailing school facility includes any materials which may be salvaged as opposed to being deposited as landfill. The purpose of salvage is to a) identify features which may be of high cultural heritage value and require conservation/commemoration on-site, or a similar setting, such as a museum, b) to identify features which are not of cultural heritage value, but are could be salvaged and adaptively re-used as opposed to being deposited as landfill.

Given the existing condition of the building and alterations which have taken place over time, there are few features which may be good candidates for salvage. The only

features which may be considered for salvage are mid. 20th century windows located on the south façade of the building (See Figure 17). These windows could be set aside from landfill materials during the demolition process and adaptively re-used for other projects within the City.



Figure 19: View of south façade of the Sailing School Facility noting the location of mid. 19th century wood windows which may be candidates for salvage provided that they are in good condition (Source: MHBC, 2023).

8.2 Future Development

The RHYC does not have a current concept plan or development proposal for a new sailing school facility. It is their intention to replace the sailing school facility once the existing facility has been removed. The intent is to remove the sailing school facility and determine all options for maximizing space in order to support the activities of the RHYC.

Generally, development along the waterfront is limited to two storeys. The new building should be consistent with existing heights and massing found along the waterfront. Any new building would require access to the waterfront and should continue to provide the

Heritage Impact Assessment, 555 Bay Street North

same opportunities and reinforce the use of the lands for marine recreational activities associated with the RHYC and the sailing school.

9.0 Conclusions

The RHYC is proposing to demolish the mid. 19th century sailing school facility which is in poor condition. The evaluation concludes that the sailing school facility demonstrates historical/associative value and contextual value but does not demonstrate design/physical value and is not considered a physical heritage attribute of the property.

This report concludes that no adverse impacts to cultural heritage resources are anticipated as a result of the removal of the existing sailing school facility. The associative and contextual value of the lands will not change as a result of proposed development. The lands will continue to function with marine recreational activities of the RHYC. A new building is necessary and will continue to support the contextual and associative value by sustaining the long-term use of the lands as recreational.

It is recommended that the property and building be documented with photographs, as provided in this report. Two windows at the south elevation of the sailing school facility may be salvaged, provided that they are in good condition and can either be used on-site or be donated to the public. Provided that any new facility continues to support the existing uses of the RHYC, no additional Heritage Impact Assessment is recommended any new building.

10.0 Bibliography

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MHBC. Site visit completed June 9, 2023.

Penny, Harry L. "One Hundred Years and Still Sailing! A history of Hamilton yachts, yachtsmen and yachting." D.G Seldon Printing Company Ltd; Hamilton, ON; 1988.

Unterman McPhail Cuming Associates. "West Harbourfront Heritage Study". Prepared for the City of Hamilton. February, 1995.

Appendix A

Property Map



Property Map

LEGEND



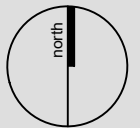
Subject Lands

DATE: June 2023

SCALE: 1:4,000

FILE: 23112A

DRAWN: LC



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555 Bay St N
City of Hamilton



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

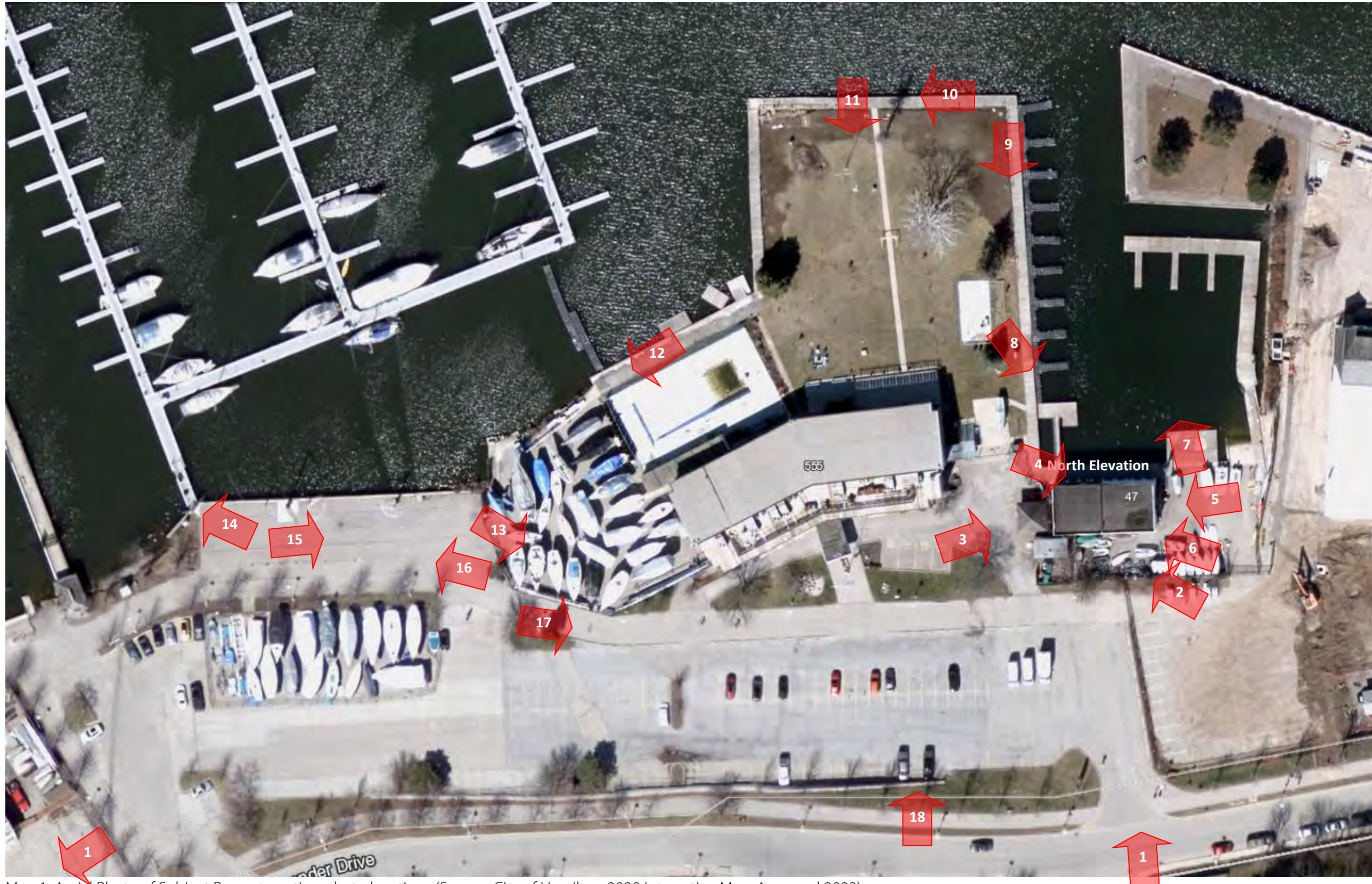
200-540 BINGEMANS CENTRE DR. KITCHENER, ON, N2B 3X9
P: 519.576.3650 F: 519.576.0121 | WWW.MHBCPLAN.COM

Appendix B

Photo Documentation

Appendix B (i)

Photo Map – Setting and Context



Map 1: Aerial Photo of Subject Property noting photo locations (Source: City of Hamilton 2020 Interactive Map, Accessed 2023)

Appendix B

Sailing School/Lockers – First Floor

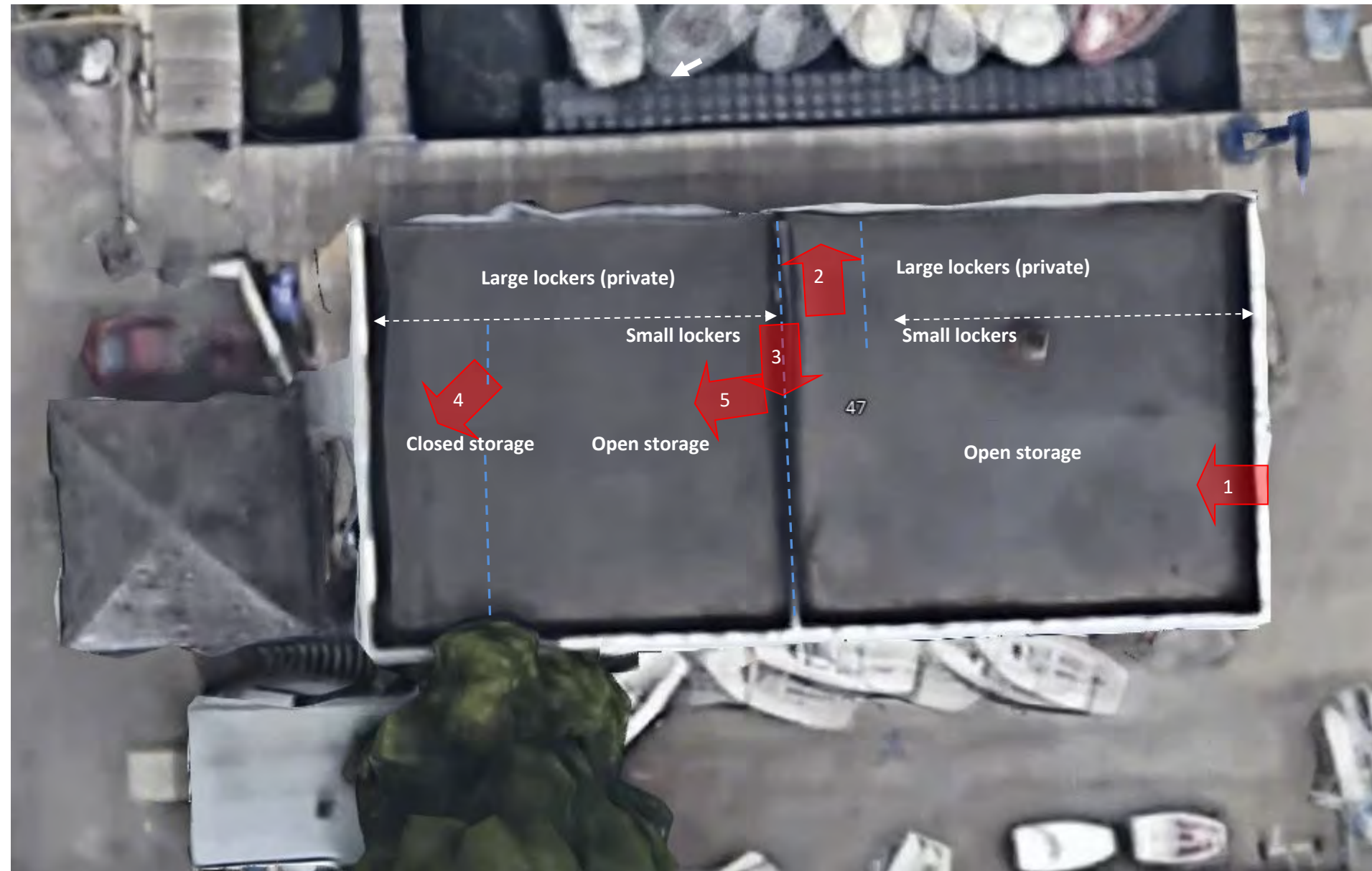


Figure 1: Measured drawing of building footprint of 474 Queen Street South noting footprint of attic in red and location of photographs (Source: MHBC, 2017).



Photos 1 & 2: (left) View of attic looking east towards window providing view of Queen Street South (right) View of attic looking west (Source: MHBC, 2017)



Photos 3 & 4: (left) Detail view of attic and roof, looking south-east (right) Detail view of attic flooring material (Source: MHBC, 2017)



Photo 5: (left) View of storage area, looking west (Source: MHBC, 2017)



Photos 1 & 2: (left) View of rear (south) elevation of the sailing school/locker facility, looking north, (right) Detail view of the south elevation of the sailing school/locker facility (Source: MHBC, 2023)



Photos 3 & 4: (left) View of the west elevation of the sailing school/locker facility looking east (note small outbuilding in the foreground) (right) View of north elevation of the sailing school/locker facility, looking east along lockers (Source: MHBC, 2023)



Photos 5 & 6: (left) View of east elevation of the sailing school/locker facility, (right) View of south elevation of sailing school/locker facility (Source: MHBC, 2023)



Photos 7 & 8: (left) View of bay, looking north towards McCassa Bay from sailing school launch area, (right) View of sailing school/locker facility looking south towards front (north) elevation (Source: MHBC, 2023)



Photos 9 & 10: (left) View of front (north) elevation of sailing school, looking south from the boardwalk (right) View of the boardwalk looking west towards the RHYC Marina, (Source: MHBC, 2023)



Photos 11 & 12: (left) View of the north elevation of the RHYC clubhouse (rear) View of the boardwalk, looking south-west towards parking lot (Source: MHBC, 2023)



Photos 13 & 14: (left) View of storage yard, looking east towards the east elevation of the clubhouse, (right) View of controlled access to the RHYC marina, (Source: MHBC, 2023)



Photos 15 & 16: (left) View of adjacent parking lots and clubhouse, looking east (right) View of adjacent parking lot and marina access, looking west, (Source: MHBC, 2023)



Photos 17 & 18: (left) View of public path and front (south) elevation of the clubhouse, (right) View of the front (south) elevation of the RHYC clubhouse, (Source: MHBC, 2023)

Appendix B (iii)

Sailing School/Lockers – Second Floor

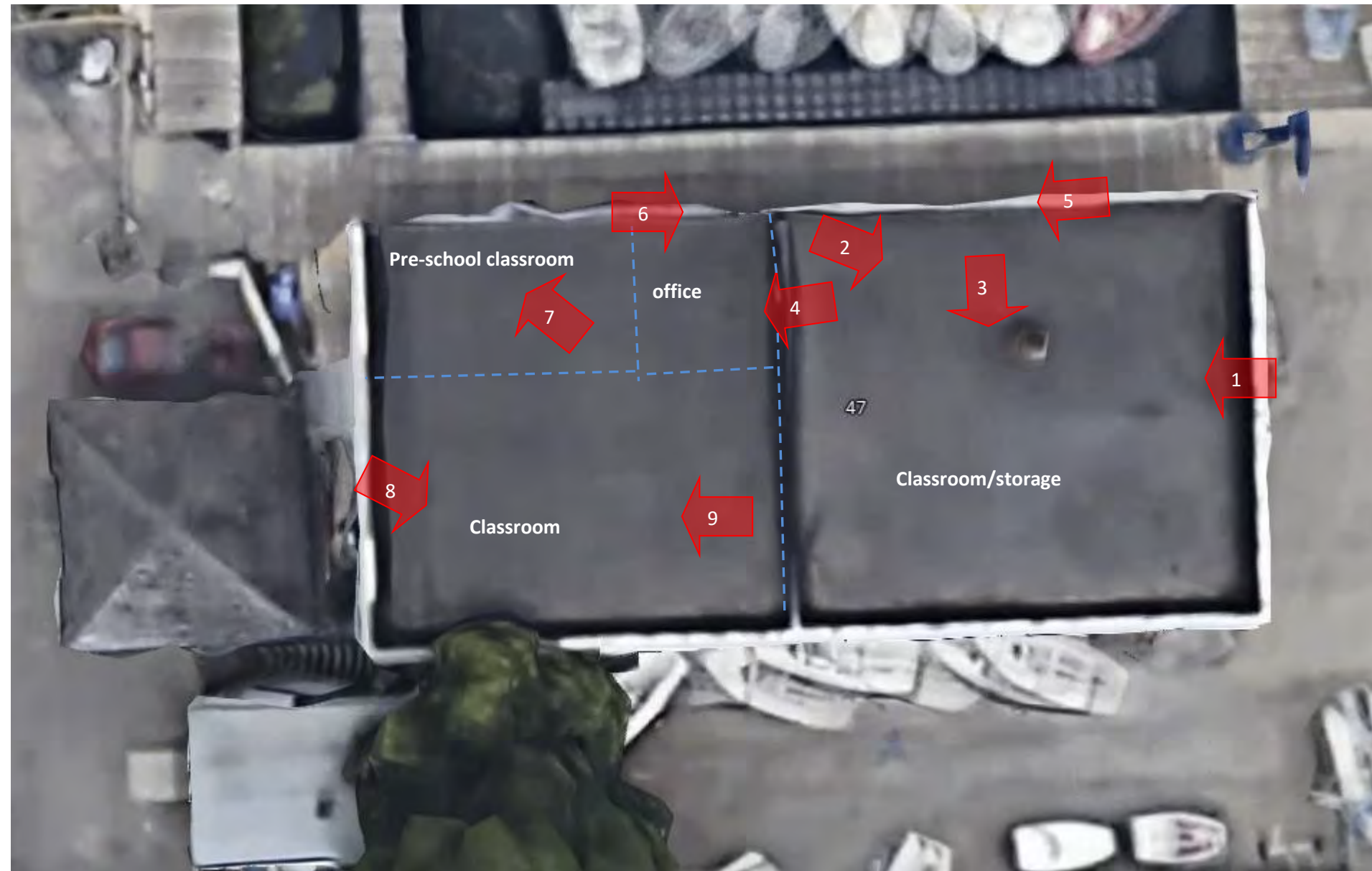


Figure 1: Measured drawing of building footprint of 474 Queen Street South noting footprint of attic in red and location of photographs (Source: MHBC, 2023).



Photos 1 & 2: (left) View of second storey sailing school facility area, looking west, (right) View sailing school facility (second floor) looking east *note warped flooring (Source: MHBC, 2023)



Photos 3 & 4: (left) View of south elevation, looking south from inside sailing school facility, (right) View of office area, looking south-west, (Source: MHBC, 2023)



Photos 5 & 6: (left) View of exterior access (second storey) looking west along north elevation, (right) View exterior access (second storey), looking east along north elevation, (Source: MHBC: 2023)



Photos 7 & 8: (left) View of pre-school learning area/room, looking north towards the bay, (right) View of classroom, looking west, (Source: MHBC: 2023)



Photo 9: (left) View of classroom, looking west(Source: MHBC: 2023)

Appendix C

Structural Condition Report (MTE, 2023)



RHYC Jr. Sail Building Structural Assessment (DRAFT)

Project Location:

555 Bay Street North, Hamilton ON

Prepared for:

AMRA J Architect
63 Snowbridge Crescent Ancaster ON L9G 5E1

Prepared by:

MTE Consultants
1016 Sutton Drive
Burlington ON L7L 6B8

September 17, 2023

MTE File No.: 54051-101





MTE Consultants
1016 Sutton Dr., Unit A, Burlington, Ontario L7L 6B8

September 28, 2023

MTE File No.: 54051-101

Ms. Paul Vayda
555 Bay Street North,
Hamilton, ON

Dear Mr. Vayda:

**RE: RHYC Junior Sail Building Structural Assessment
555 Bay Street North, Hamilton, Ontario**

MTE Consultants Inc. has been retained to review the existing structure of the RHYC Junior Sail Building.

Information Received

- Architectural drawings of the as built by Amra J Architects on August 8th, 2023.
- No existing structural drawings were provided.

As-built Conditions

A site visit was conducted on August 22, 2023, to review the existing structure of the RHYC Junior Sail building. MTE was notified that the building was leaning towards the lake. As shown in Figure 1 the clouded walls (lake side) and its foundations was the focus of this assessment.

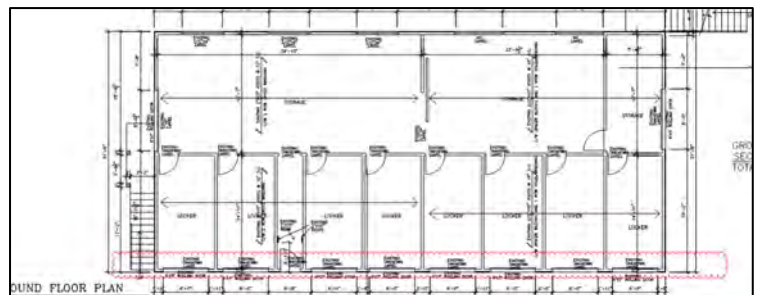


Figure 1. Ground Floor Architectural Plan

The 2-storey building is of wood construction with load bearing stud walls at the exterior and along the center with 2x10 wood joists framing the second floor as highlighted in Figure 2. The framing appears to follow the same convention for the roof but much of the framing was not accessible to verify its exact construction.

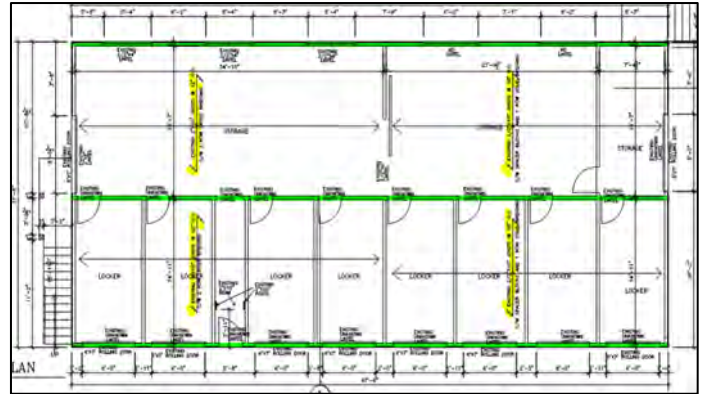


Figure 2. 2nd Floor Framing

The joist framing as shown in Photo 1 is in relatively good condition structurally considering its age. No visible signs of rot from what could be seen, joist checking was minimal and no major signs of damage to the joists were visible. The floor however had an extreme tilt towards the lake. The wood planks were significantly warped which hindered the serviceability of the structure.

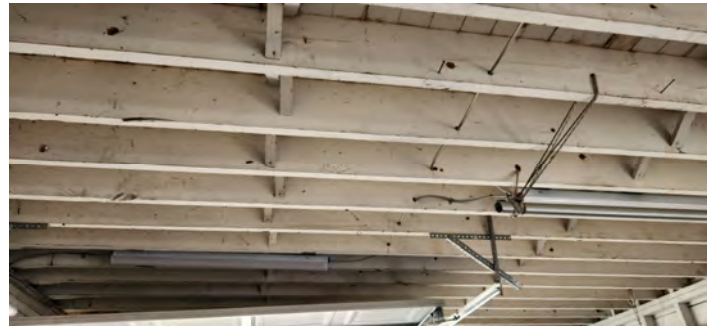


Photo 1: Second Floor Joist Framing at Rear Storage

In photograph 2, the roof line is visibly sagging down at the lakeside (left of the photo). The balcony post bases are resting on wood shims that appear to be rotting and will need some remediation.



Photo 2: Exterior View of the Second Floor Balcony

The slab supporting the structure cracked and separated completely as shown in Photo 3. This photograph was taken in the open storage locker shown in Photo 2. This condition is consistent all along the lake side of the building.



Photo 3: Close Up View of the Cracked Slab Inside the Lockers

MTE was able to view the foundation below the lockers from the underside of the deck. As can be seen from the right side of the photograph, large footings that were placed below the existing slab. This was a temporary remediation for the same issue done years prior.



Photo 4: View Below the Deck Which Show the Exposed Foundation

Photograph 5 is a close-up of the foundation. There is a large void below the footing and bearing material. It is believed that the founding soil is being washed out over time causing the building to tilt towards the lake.



Photo 5: Close-up View of the Building Foundation

Findings

As the building currently stands, it does have severe serviceability issues with the warping of the second floor, cracked slabs, balcony post bases as outlined above will need to be remediated. The major issues and cause of the tilting of the RHYC Junior Sail Building appears to be from the founding material below the footing structure being washed out. If not corrected, the condition will worsen to the point where the building is no longer serviceable. It is in our opinion to correct this issue; deep foundation system will need to be implemented. The cost of the installing such a system can be high due to its location, access and keeping the existing building intact. It is recommended to reach out to a geotechnical engineer to explore possible design solutions and feasibility of undertaking the repair.

We trust this provides the information you require at this time. Should you have any questions or comments, please do not hesitate to contact the undersigned.

Yours Truly,

MTE Consultants Inc.

Michael Corrado, P.Eng.

Design Manager, Building Structures Division

905-639-2552 ext. 2407

mcorrado@mte85.com

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Limitations

This report has been prepared by **MTE Consultants Inc.** (MTE). The material in it reflects the best judgment of MTE in light of the information available at the time of preparation. Any use which a third party makes of this report, or any reliance on or decisions made based on it, are the responsibility of such third parties. MTE accepts no responsibility for damages, if any, suffered by any third party as a result of decisions made or actions based on this letter.

This assessment does not wholly eliminate uncertainty regarding the potential for existing or future costs, hazards or losses in connection with a property. No physical or destructive testing and no design calculations have been performed unless specifically recorded. Conditions existing but not recorded were not apparent given the level of study undertaken. We can perform further investigation on items of concern if so required. Only the specific information identified has been reviewed. The consultant is not obligated to identify mistakes or insufficiencies in the information obtained from the various sources or to verify the accuracy of the information. The Consultant may use such specific information obtained in performing its services and is entitled to rely upon the accuracy and completeness thereof.

Responsibility for detection of or advice about pollutants, contaminants or hazardous materials is not included in our mandate. In the event the Consultant or any other party encounters any hazardous or toxic materials, or should it become known to the Consultant that such materials may be present on or about the jobsite or any adjacent areas that may affect the performance of the Consultant's services, the Consultant may, at its option and without liability for consequential or any other damages, suspend performance of its services under this Agreement until the Client retains appropriate consultants to identify and abate or remove the hazardous or toxic materials and warrants that the jobsite is in full compliance with all applicable laws and regulations.

Any time frame given for undertaking work represents an educated guess based on apparent conditions existing at the time of our letter. Failure of the item, or the optimum repair/replacement process, may vary from our estimate. We accept no responsibility for any decisions made or actions taken as a result of this letter unless we are specifically advised of and participate in such action, in which case our responsibility will be as agreed to at that time. Any user of this letter specifically denies any right to claims against the Consultant, Sub-Consultants, their Officers, Agents and Employees in excess of the fee paid for professional services.



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& L A N D S C A P E
A R C H I T E C T U R E