

**NOTICE OF PUBLIC HEARING**  
**Minor Variance**

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**You are receiving this notice because you are either:**

- Assessed owner of a property located within 60 metres of the subject property
  - Applicant/agent on file, or
  - Person likely to be interested in this application
- 

<b>APPLICATION NO.:</b>	<b>A-25:135</b>	<b>SUBJECT PROPERTY:</b>	392 Main Street West, Hamilton
<b>ZONE:</b>	C5, 298 (Mixed Use Medium Density)	<b>ZONING BY-LAW:</b>	Hamilton Zoning By-law 05-200

**APPLICANTS:**      Owner: Main Margaret Inc  
                             Agent: Bousfields Inc. c/o Ashley Paton

The following variances are requested:

1. Stacked parking shall be permitted within a parking lot for a Multiple Dwelling and without a parking attendant on site whereas stacked parking is only permitted in association with a Commercial Parking Facility or Hotel and only where a parking attendant is on site.
2. A minimum parking space size of 2.2m x 5.1m shall be provided for stacked parking spaces instead of the minimum required parking space size of 2.8m x 5.8m.

**PURPOSE & EFFECT:**      To facilitate the construction of a Six (6) storey multiple dwelling.

**Notes:**

- i) This variance application is to facilitate Site Plan application DA-22-167.
- ii) Please note there is a deficiency of accessible parking spaces for this development, applicant will address this through the Site Plan application DA-22-167 and has not included it as part of the variance application.

**This Notice must be posted by the owner of any land which contains seven or more residential units so that it is visible to all residents.**

This application will be heard by the Committee as shown below:

<b>DATE:</b>	<b>Thursday, August 14, 2025</b>
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## A-25:135

TIME:	2:05 p.m.
PLACE:	Via video link or call in (see attached sheet for details)
	City Hall Council Chambers (71 Main St. W., Hamilton)
	To be streamed (viewing only) at <a href="http://www.hamilton.ca/committeeofadjustment">www.hamilton.ca/committeeofadjustment</a>

For more information on this matter, including access to drawings illustrating this request and other information submitted:

- Visit [www.hamilton.ca/committeeofadjustment](http://www.hamilton.ca/committeeofadjustment)
- Visit Committee of Adjustment staff at 5<sup>th</sup> floor City Hall, 71 Main St. W., Hamilton

### PUBLIC INPUT

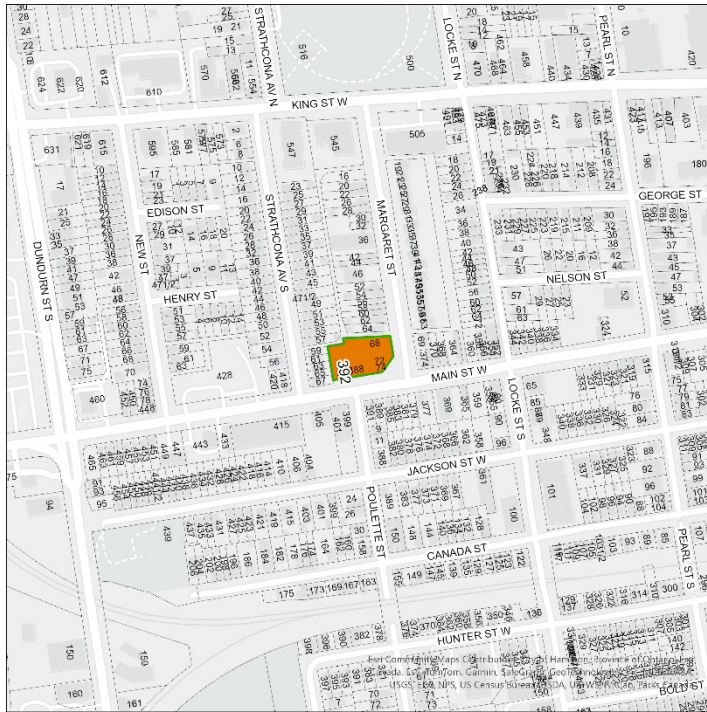
**Written:** If you would like to submit written comments to the Committee of Adjustment you may do so via email or hardcopy. Please see attached page for complete instructions, written comments must be received no later than noon August 12, 2025

**Orally:** If you would like to speak to this item at the hearing you may do so via video link, calling in, or attending in person. Please see attached page for complete instructions, registration to participate virtually must be received no later than noon August 13, 2025

### FURTHER NOTIFICATION

If you wish to be notified of future Public Hearings, if applicable, regarding A-25:135, you must submit a written request to [cofa@hamilton.ca](mailto:cofa@hamilton.ca) or by mailing the Committee of Adjustment, City of Hamilton, 71 Main Street West, 5th Floor, Hamilton, Ontario, L8P 4Y5.

If you wish to be provided a Notice of Decision, you must attend the Public Hearing and file a written request with the Secretary-Treasurer by emailing [cofa@hamilton.ca](mailto:cofa@hamilton.ca) or by mailing the Committee of Adjustment, City of Hamilton, 71 Main Street West, 5th Floor, Hamilton, Ontario, L8P 4Y5.



**Subject Lands**

DATED: July 28, 2025

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Justin Leung,  
Secretary-Treasurer  
Committee of Adjustment

Information respecting this application is being collected under the authority of the Planning Act, R.S.O., 1990, c. P. 13. All comments and opinions submitted to the City of Hamilton on this matter, including the name, address, and contact information of persons submitting comments and/or opinions, will become part of the public record and will be made available to the Applicant and the general public, and may include posting electronic versions.



Hamilton

## COMMITTEE OF ADJUSTMENT

City Hall, 5<sup>th</sup> floor, 71 Main Street West, Hamilton, ON L8P 4Y5

Telephone (905) 546-2424, ext. 4221

E-mail: [cofa@hamilton.ca](mailto:cofa@hamilton.ca)

## PARTICIPATION PROCEDURES

### Written Submission Ahead of the Meeting

Members of the public who wish to provide input without speaking at the Hearing may submit written comments in advance of the meeting. Comments must be received by 12:00 p.m. (noon) on the date listed on the Notice of Public Hearing.

### How to Submit Written Comments:

#### By Email:

Send to: [cofa@hamilton.ca](mailto:cofa@hamilton.ca)

#### By Mail:

Committee of Adjustment  
City of Hamilton  
71 Main Street West, 5th Floor  
Hamilton, Ontario  
L8P 4Y5

All written comments received will be made available to the Committee and the public by the Tuesday prior to the Hearing.

### Oral Submissions During the Hearing

Interested members of the public, agents, and owners may provide oral comments on Committee of Adjustment Hearing items either virtually via Webex (computer or phone) or by attending in person.

#### Speaking Time Limit:

All participants providing oral submissions, either in person or virtually are limited to a maximum of 5 minutes to speak. This is to ensure all parties have an equal opportunity to be heard and that the meeting runs efficiently.

### In-Person Oral Submissions

To participate in person, attend Council Chambers on the date and time listed in the Notice of Public Hearing. You will be required to provide your name and address for the record. It is recommended you arrive at least 10 minutes prior to the scheduled start time.



## **Virtual Oral Submissions**

To participate virtually, you must register by 12:00 p.m. (noon) on the date listed on the Notice of Public Hearing. To register, email [cofa@hamilton.ca](mailto:cofa@hamilton.ca) with the following information:

- Committee of Adjustment file number
- Hearing date
- Name and mailing address of each person wishing to speak
- Method of participation (phone or video), and, if applicable, the phone number to be used
- Each person must register separately

Registered participants will receive a Webex link one business day before the Hearing. Only those registered will be called upon to speak.

## **Presentations**

All presentations are permitted at the discretion of the Committee.

### **Virtual Presentations:**

Presenters participating virtually may be granted permission to share their screen during the Hearing. A copy of the presentation must be submitted to [cofa@hamilton.ca](mailto:cofa@hamilton.ca) no later than 12:00 p.m. (noon) on the business day prior to the Hearing. The submission must be one document in PDF format only.

### **In-Person Presentations:**

Presenters attending in person may be granted permission to use the presentation screen. Presentations must be brought on a USB device and opened by the owner/applicant. A copy of the presentation must also be sent to [cofa@hamilton.ca](mailto:cofa@hamilton.ca) by 12:00 p.m. (noon) on the business day prior to the Hearing in PDF format as a single document. Handouts are permitted only if the same content can be displayed on the presentation screen.

## **Additional Notes**

- Webex (video) participation requires a compatible computer or smartphone. The necessary application must be downloaded in advance.
- It is the interested party's responsibility to ensure their device is functional and compatible prior to the Hearing.

**For any questions, contact staff at [cofa@hamilton.ca](mailto:cofa@hamilton.ca) or call 905-546-2424 ext. 4221.**








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SITE PLAN APPROVAL   DA-22-167	2025-06-01
DESCRIPTION	DATE

**ISSUED RECORD**



MARENCO ARCHITECTS INC.  
94 LINCOLN RD, WINDSOR, ON N8W 2R7

SCALE	1:1	NORTH	
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DRAWING TITLE

CLIENT \_\_\_\_\_

MAIN MARGARET INC.  
[REDACTED]

DRAWN	IM	PAGE NO.  A000
CHECKED	WM	
DATE	2025-06-01	

A000	COVER
A001	STATISTICS
A003	GROSS FLOOR AREA DIAGRAMS
A005	CROSS SECTION PLAN
A006	SURVEY
A100	SITE PLAN
A201	GROUND FLOOR
A202	LEVEL 2-4
A203	LEVEL 5
A204	LEVEL 6
A205	ROOF PLAN
A400	SOUTH & EAST ELEVATION
A401	NORTH & WEST ELEVATION
A410	SOUTH & EAST ELEVATION - MATERIAL
A411	NORTH & WEST ELEVATION - MATERIAL
A500	SECTION
A501	PERSPECTIVE
A502	SECTION
A600	RENDERING
A601	RENDERING
A1000	SUN SHADOW STUDY MARCH 21
A1001	SUN SHADOW STUDY SEPTEMBER 21

Bousfields Inc.  
3 Church Street, Suite 200  
Toronto ON L8N 1E7

BA GROUP  
95 St Clair Ave W #1000  
Toronto ON M4V 1N6

SITEPLANTECH INC.  
50 St Clements Avenue  
Toronto ON M4R 1G9

Marten-Nikzad  
1387 Bayview Avenue, Unit 105  
Toronto ON M4C 3A5

Reinbold Engineering Group  
145 Wellington St W #901,  
Toronto ON M5J 1H8

MCW Consultants Ltd.  
207 Queens Quay W #615  
Toronto ON M5J 2M6

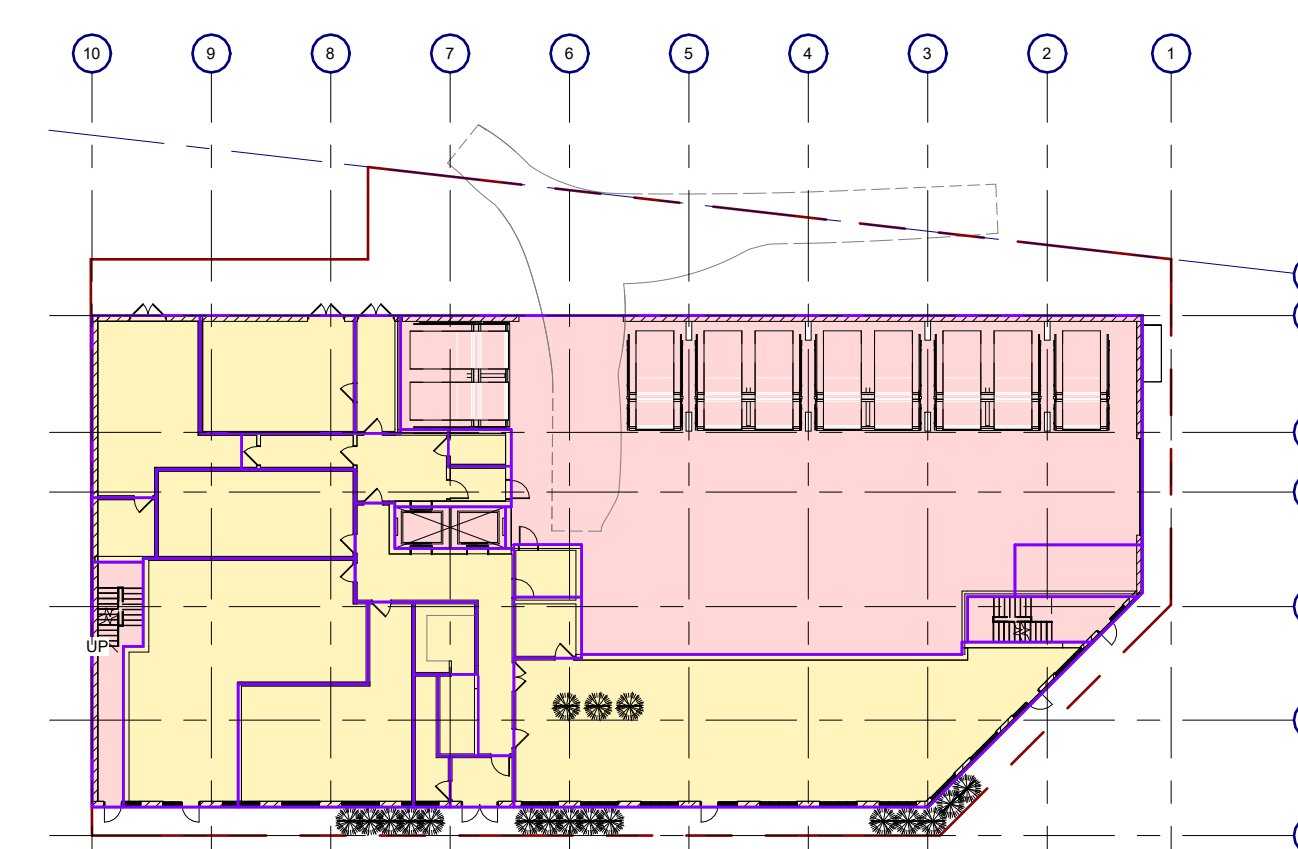
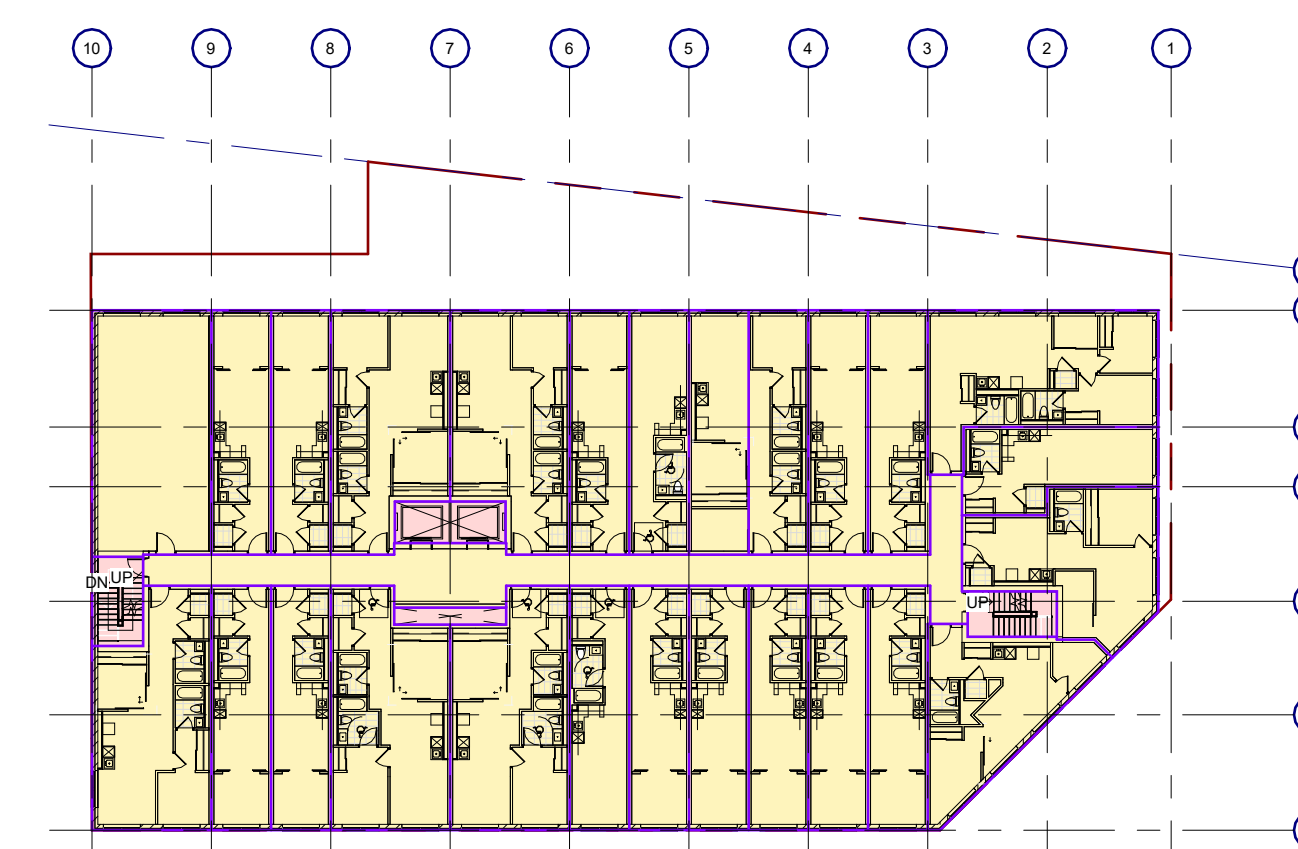
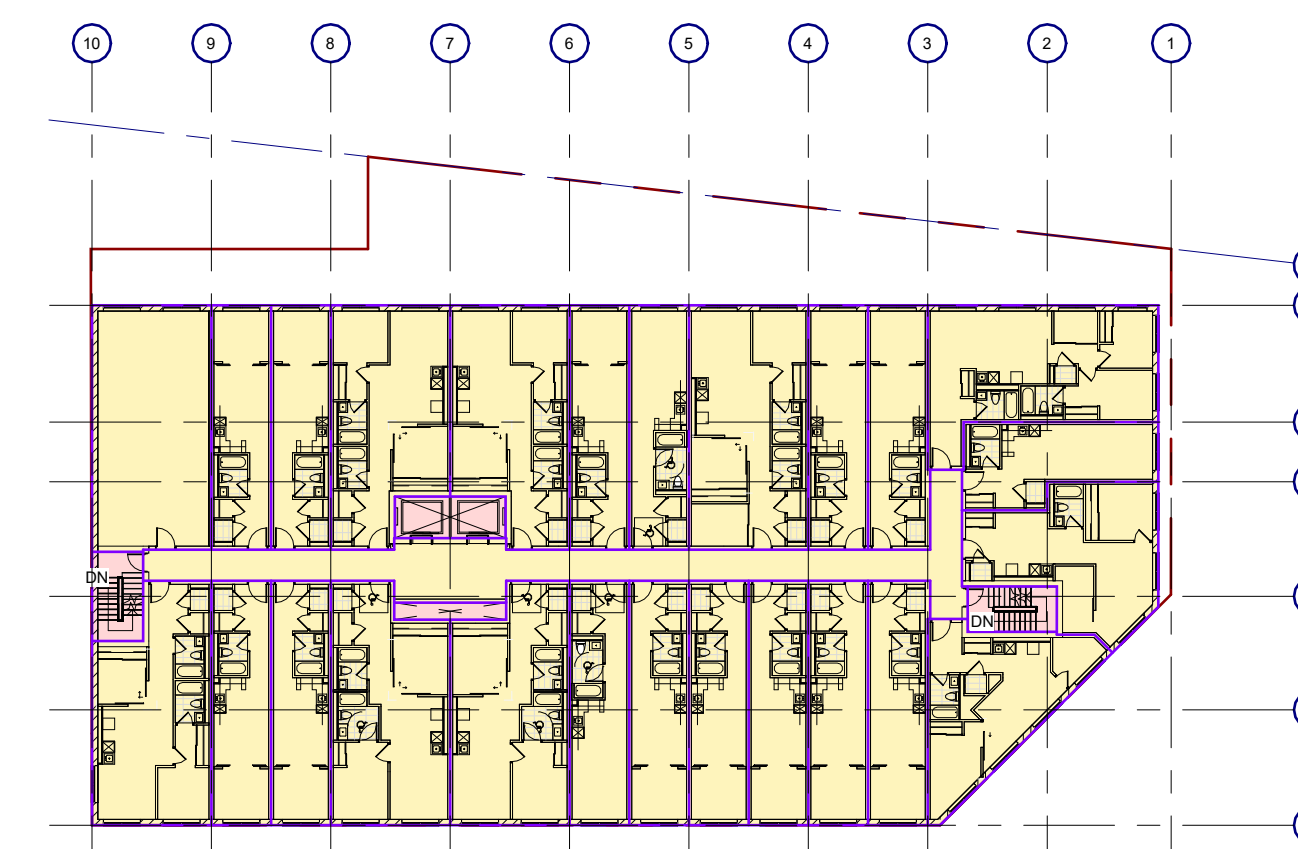
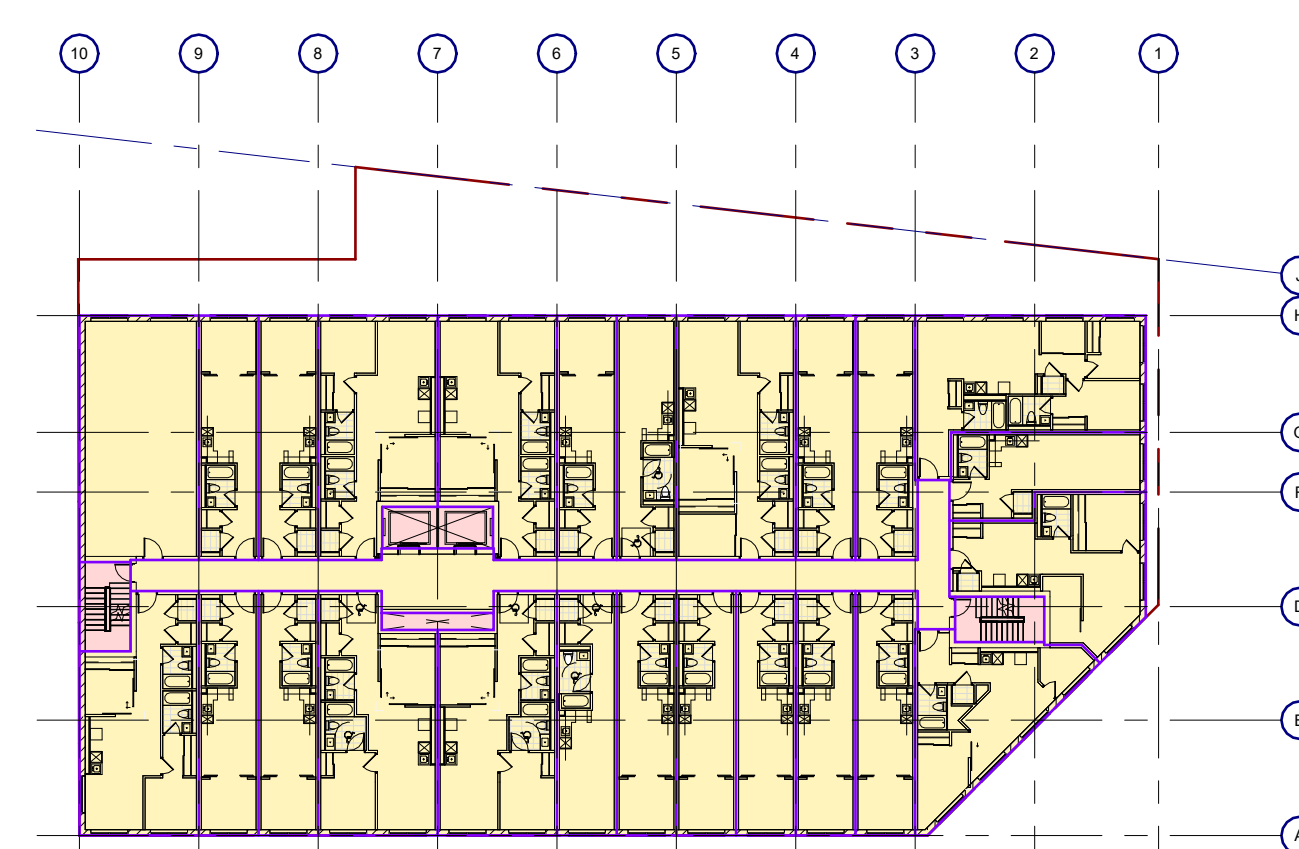
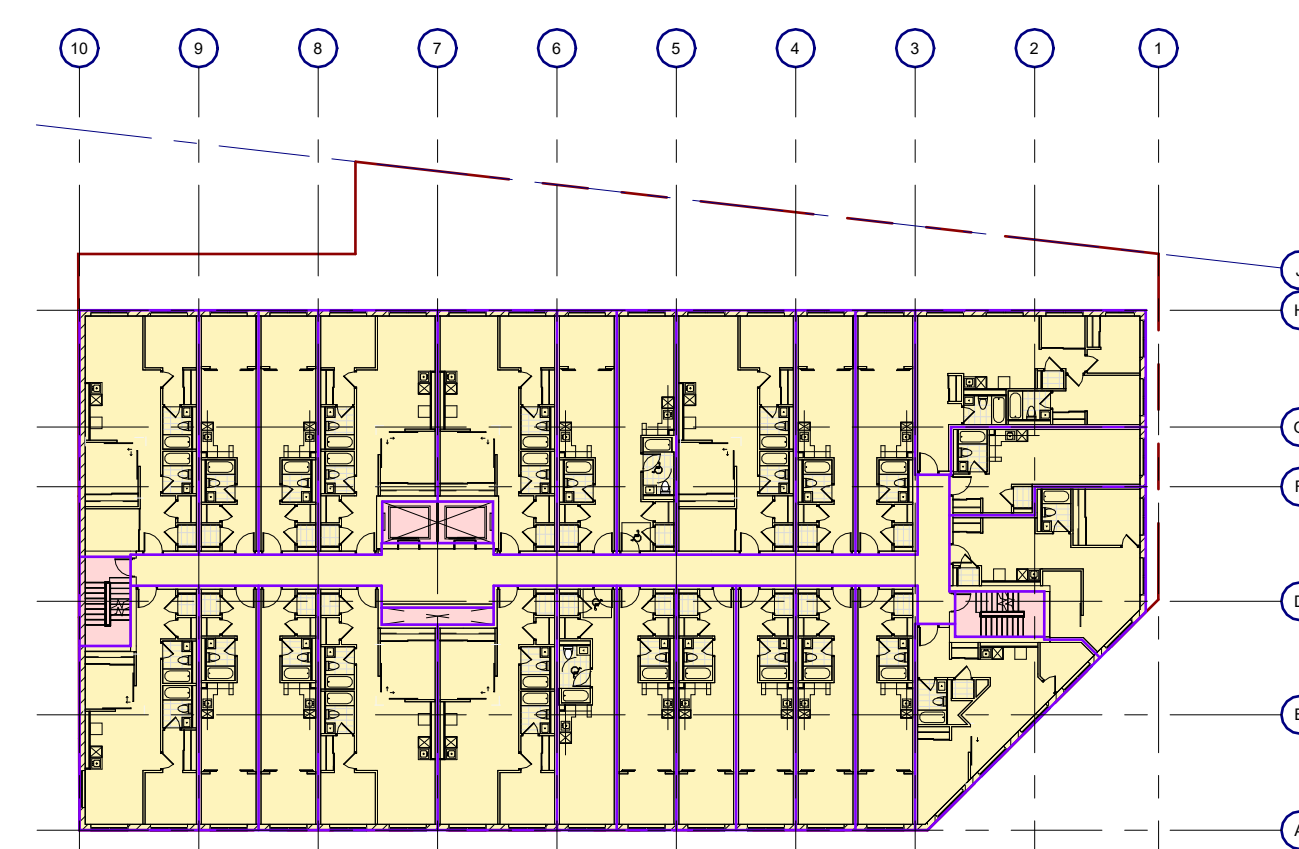
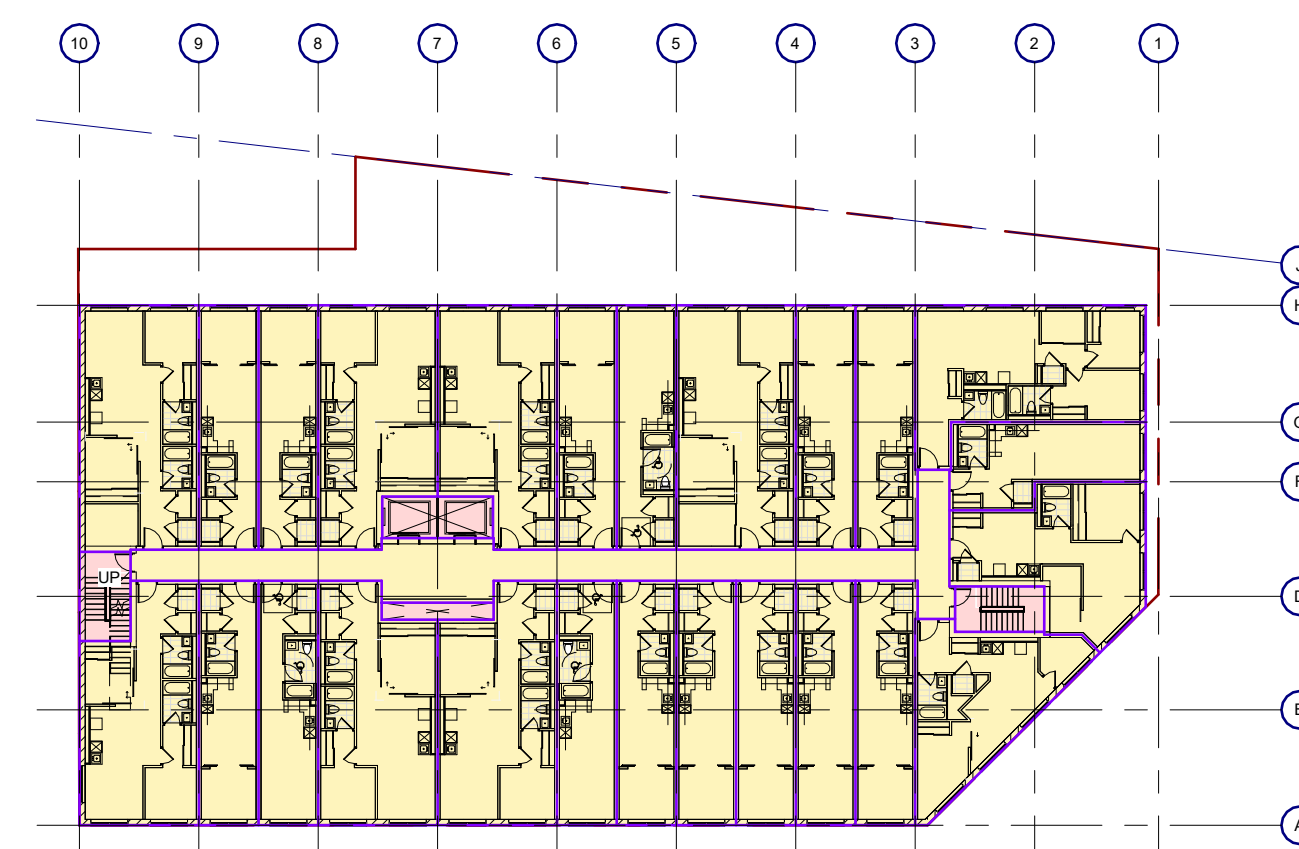
Nemetz  
214 King St West  
Toronto ON M5H 3S6

BLACKWELL  
134 Peter St Suite 1301,  
Toronto ON M5V 1X6

MARENCO ARCHITECTS  
68 Claremont St  
Toronto ON M6J 2M5







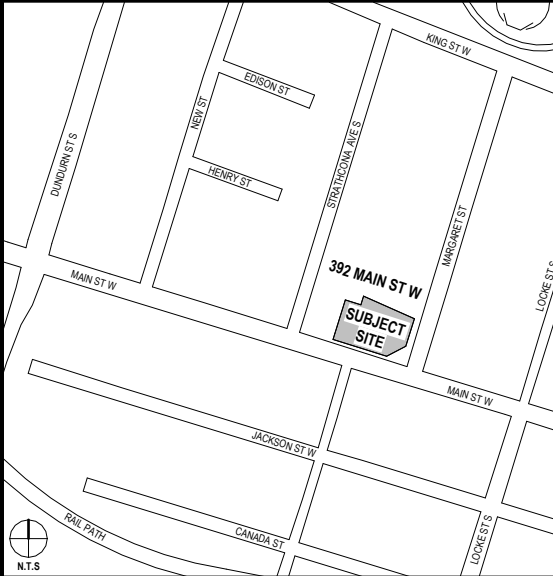
**GFA**

 INCLUDED

 EXCLUDED

**AMENITY AREA:**

INCLUDE THE EXTERIOR AND ENCLOSING PUBLIC WALLS ADJACENT TO THE AMENITY SPACE. IT EXCLUDES THE SHARED WALL BETWEEN AMENITY SPACE AND UNITS.



No.	DESCRIPTION	DATE
REVISION RECORD		
SITE PLAN APPROVAL   DA-22-167		2025-06-
DESCRIPTION		DATE



MARENCO ARCHITECTS

MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

SCALE	1:400	NORTH	
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DRAWING TITLE	
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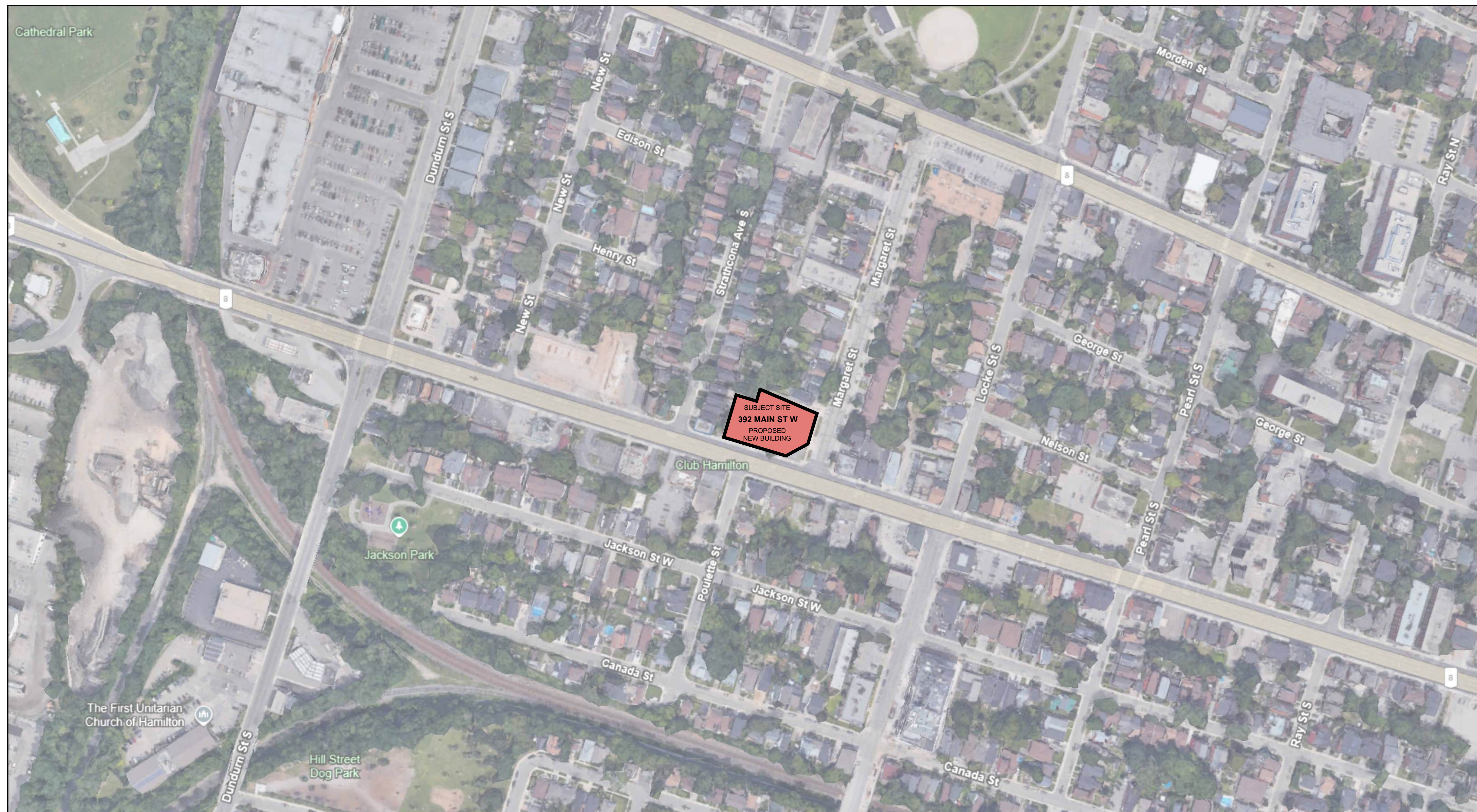
## GROSS FLOOR AREA DIAGRAMS

CLIENT  
MAIN MARGARET INC.  
[REDACTED]

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CHECKED	WM	
DATE	2025-06-01	

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MARENCO ARCHITECTS

MARENCO ARCHITECTS INC.  
494 LINCOLN RD, WINDSOR, ON N8W 2R7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

SCALE	N.T.S.	NORTH 
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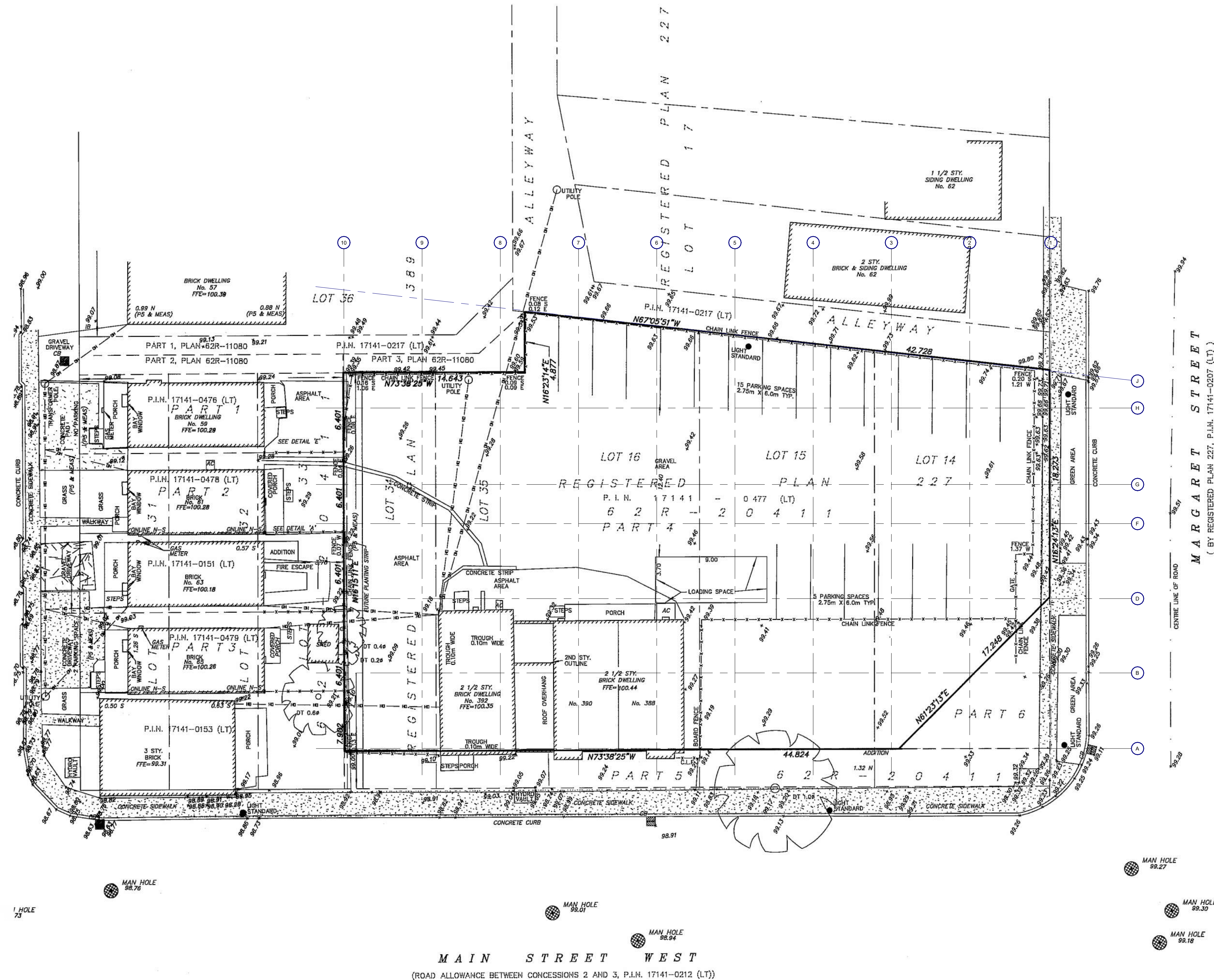
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CONTEXT PLAN

CLIENT  
MAIN MARGARET INC.  
[REDACTED]

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CHECKED	WM	
DATE	2025-06-01	





TOPOGRAPHIC PLAN OF  
LOT 14 TO 16 INCLUSIVE  
REGISTERED PLAN 227  
AND ALL OF  
LOT 34 AND 35 AND PART  
OF LOTS 31, 32 AND 33  
REGISTERED PLAN 389.  
IN THE  
CITY OF HAMILTON

SCALE & NOTES  
SCALE: 1:200  
0 5 10 20 Metres

GUIDO V. CONSOLI  
ONTARIO LAND SURVEYOR  
© COPYRIGHT 2018

METRIC  
DISTANCES AND CO-ORDINATES SHOWN ON THIS PLAN ARE IN METRES  
AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

ELEVATION NOTE  
ELEVATIONS ARE REFERRED TO THE CANADIAN GEODETIC VERTICAL DATUM  
(CGVD-1928:1978) AND ARE DERIVED FROM CITY OF HAMILTON BENCHMARK  
No. 07720020012 HAVING AN ELEVATION OF 84.543 m.

NOTE  
BOUNDARY SHOWN OF THIS TOPOGRAPHICAL PLAN ARE TAKEN FROM  
REFERENCE PLAN 62R-20411.

- LEGEND
- DENOTES SURVEY MONUMENT FOUND
  - DENOTES SURVEY MONUMENT PLANTED
  - SIB DENOTES STANDARD IRON BAR
  - IB DENOTES IRON BAR
  - CC DENOTES CUT CROSS
  - OU DENOTES ORIGIN UNKNOWN
  - WIT DENOTES WITNESS
  - N-E-W-S DENOTES NORTH - EAST - WEST - SOUTH
  - MEAS DENOTES MEASURED
  - D DENOTES INST. 33105CD
  - D1 DENOTES INST. 399129AB
  - P DENOTES PLAN 62R-16147
  - P1 DENOTES REGISTERED PLAN 389
  - P2 DENOTES REGISTERED PLAN 227
  - P3 DENOTES PLAN BY MARSHALL WACKLIN MONAGHAN
  - P4 DENOTES PLAN No. P-906A
  - P5 DENOTES PLAN BY GUIDO V. CONSOLI, O.L.S.
  - (824) DENOTES 62R-20411
  - (912) DENOTES A. T. MCCLAREN, O.L.S.
  - (JOB) DENOTES A. J. CLARKE, O.L.S.
  - (1135) DENOTES J. D. BARNES LIMITED, O.L.S.
  - MH DENOTES LARRY WOODS, O.L.S.
  - CB DENOTES MANHOLE
  - LS DENOTES CATCHBASIN
  - LS DENOTES LIGHT STANDARD
  - TC DENOTES TOP OF CURB ELEVATION
  - BC DENOTES BOTTOM OF CURB ELEVATION
  - OH DENOTES OVERHEAD UTILITY CABLES
  - DT DENOTES DECIDUOUS TREE
  - CT DENOTES CONIFEROUS TREE
  - LS DENOTES LIGHT STANDARD
  - FF DENOTES FINISHED FLOOR ELEVATION
  - GF DENOTES GARAGE FLOOR ELEVATION
  - INV DENOTES INVERT ELEVATION
  - HYD DENOTES FIRE HYDRANT
  - IN DENOTES SUBJECT LANDS BOUNDARY
  - HYD DENOTES DEED LINE
  - IN DENOTES LOT LINE
  - IN DENOTES LIMIT OF STREET
  - IN DENOTES FENCE LINE
  - IN DENOTES EAVES

SURVEYOR'S CERTIFICATE  
I CERTIFY THAT  
1. THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE  
SURVEYS ACT, THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM.  
2. THE SURVEY WAS COMPLETED ON JANUARY 03, 2018.

JANUARY 18, 2018

*G. Consoli*  
GUIDO V. CONSOLI  
ONTARIO LAND SURVEYOR

Barich Grenkie  
Surveying Ltd.  
297 HWY No. 9 UNIT 101 - STONEY  
1809-662-6767

A DIVISION OF GEOMAPLE

DWN BY: MM

JOB No. 14-1554



REVISION RECORD

SITE PLAN APPROVAL | DA-22-167 | 2025-06-01  
DESCRIPTION DATE

ISSUED RECORD



MARENCO ARCHITECTS

MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N9B 1P7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

SCALE 1:200 NORTH

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SURVEY

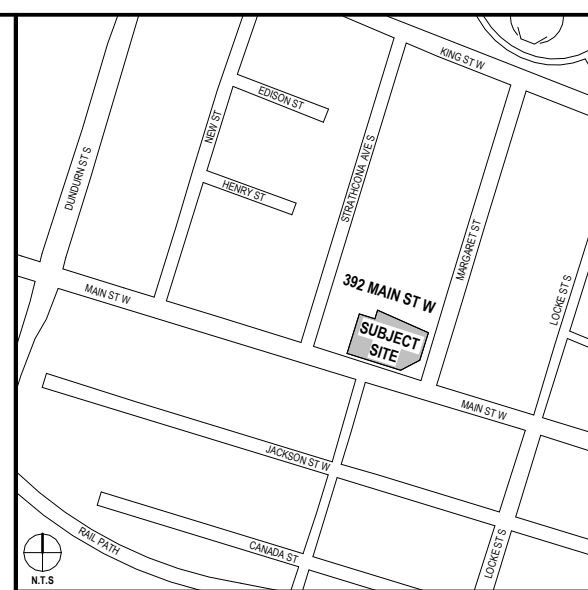
CLIENT  
MAIN MARGARET INC.

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CHECKED WM  
DATE 2025-06-01 A006









SITE PLAN APPROVAL   DA-22-167	2025-06-01
DESCRIPTION	DATE

MARENCO ARCHITECTS

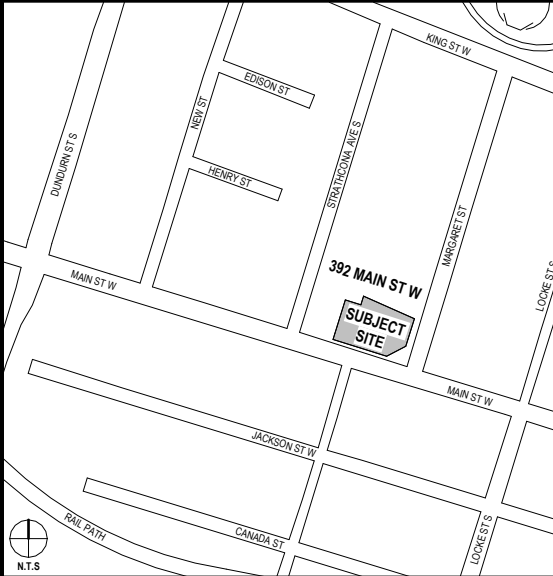
MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

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LEVEL 2-4


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DATE	2025-06-01	



ISSUED RECORD
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392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

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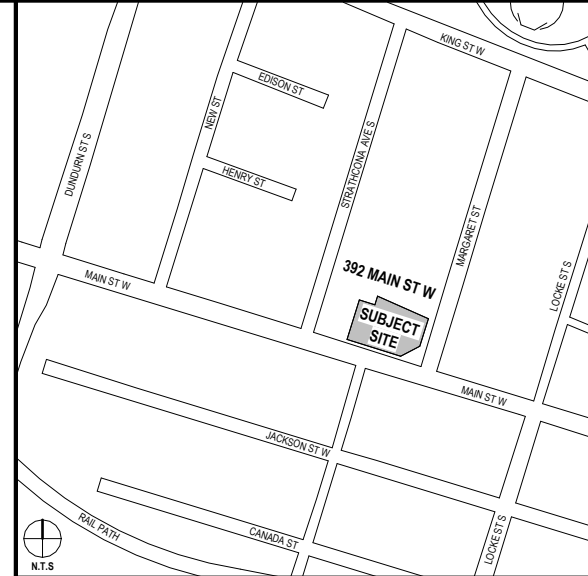
LEVEL 5

CLIENT  
MAIN MARGARET INC.  
[REDACTED]

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DATE	2025-06-01	

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SITE PLAN APPROVAL   DA-22-167	2025-06-01
DESCRIPTION	DATE

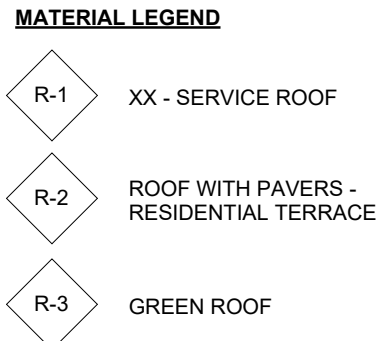
MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

SCALE	1:100	NORTH	
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LEVEL 6

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DATE	2025-06-01	

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CHECKED	WM	
DATE	2025-06-01	

[illegible]

ONTARIO ASSOCIATION  
OF  
ARCHITECTS  
WILFRED M. MARENCO  
LICENCE  
10529

MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

SCALE  
As indicated

NORTH 

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DATE	2025-06-01	



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MARENCO ARCHITECTS

MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

SCALE	1:100	NORTH	
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DRAWING TITLE
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SOUTH & EAST  
ELEVATION

CLIENT  
MAIN MARGARET INC.

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DATE	2025-06-01	

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MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

SCALE	1:100	NORTH	
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CLIENT  
MAIN MARGARET INC.  
[REDACTED]

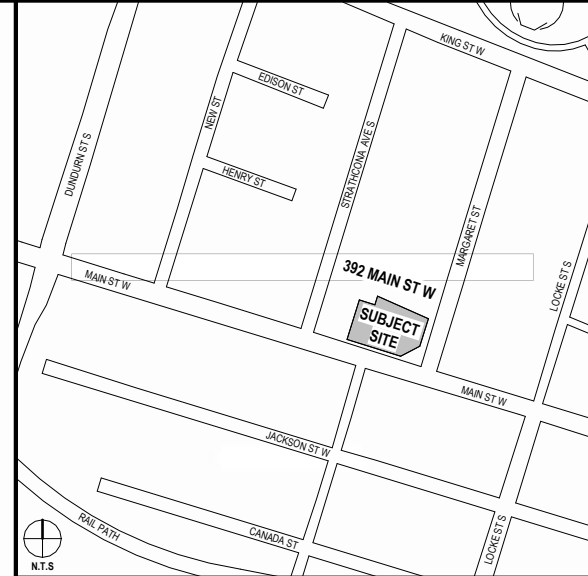
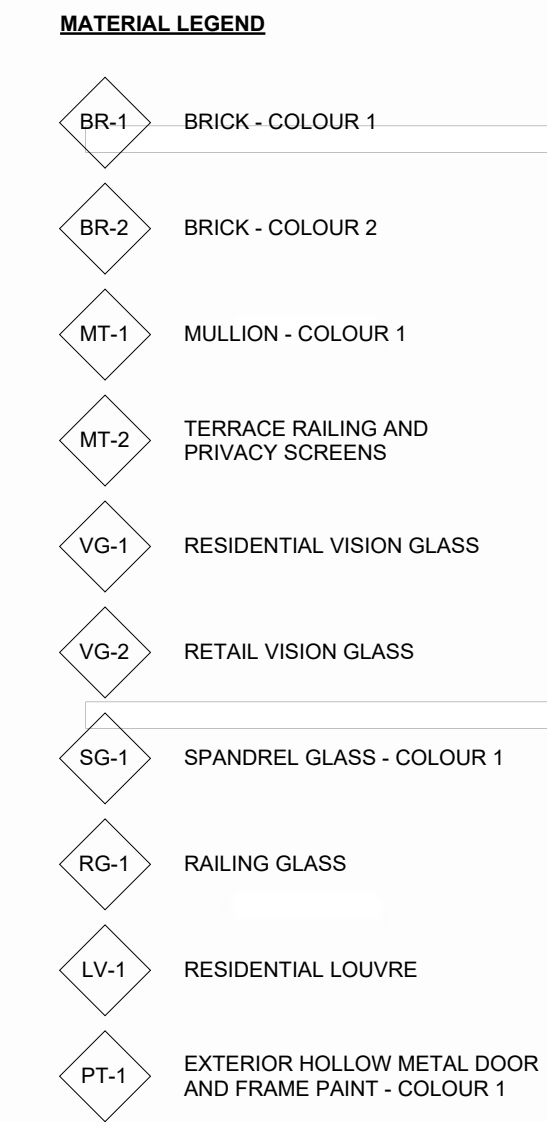
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DATE	2025-06-01	



### MATERIAL LEGEND

- |      |  |
|------|--|
| BR-1 | BRICK - COLOUR 1   |
| BR-2 | BRICK - COLOUR 2   |
| MT-1 | MULLION - COLOUR 1                                       |
| MT-2 | TERRACE RAILING AND<br>PRIVACY SCREENS                   |
| VG-1 | RESIDENTIAL VISION GLASS                                 |
| VG-2 | RETAIL VISION GLASS                                      |
| SG-1 | SPANDREL GLASS - COLOUR 1                                |
| RG-1 | RAILING GLASS  |
| LV-1 | RESIDENTIAL LOUVRE                                       |
| PT-1 | EXTERIOR HOLLOW METAL DOOR<br>AND FRAME PAINT - COLOUR 1 |



[illegible]

MARENCO ARCHITECTS

MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

SCALE	1:100	NORTH	
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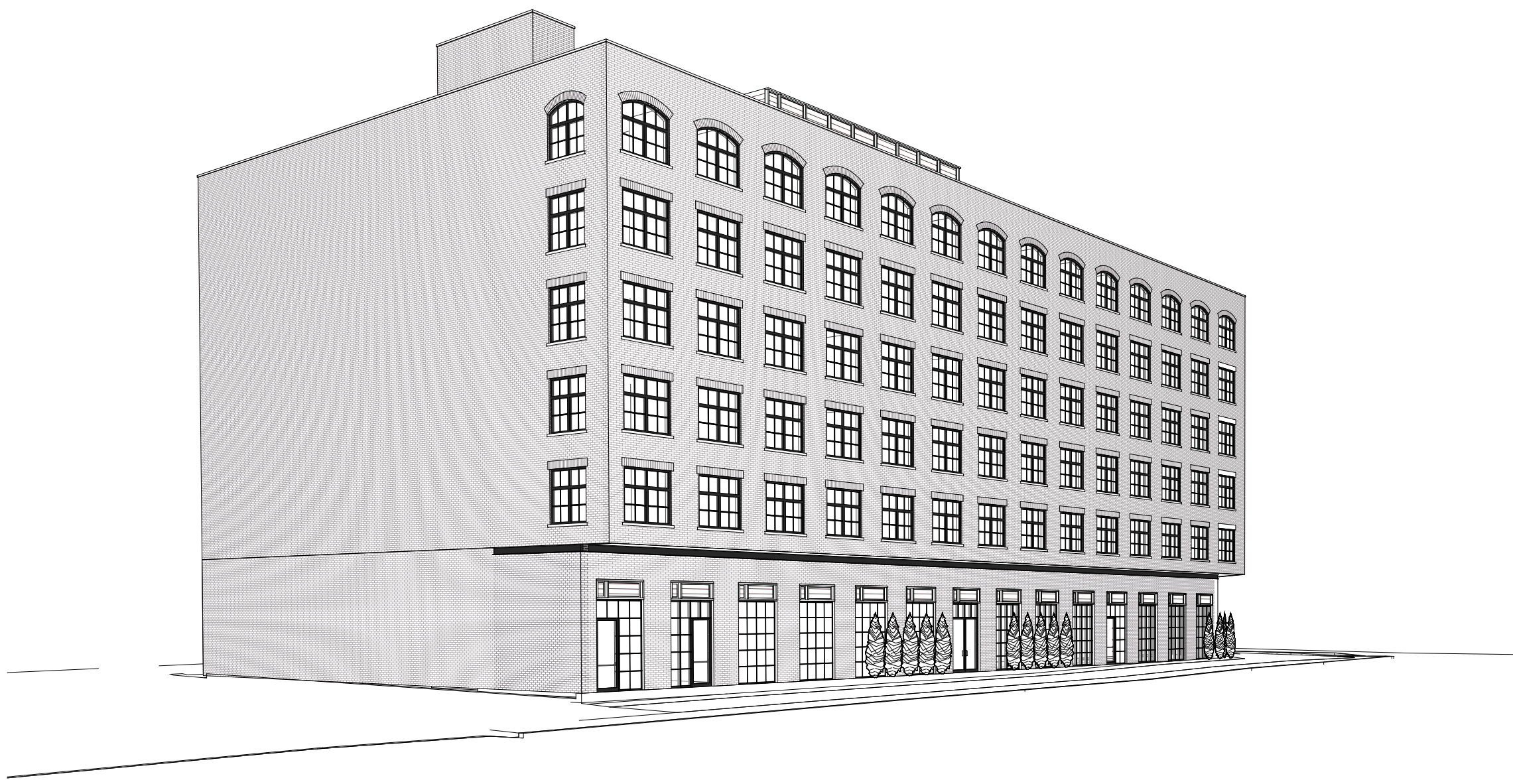
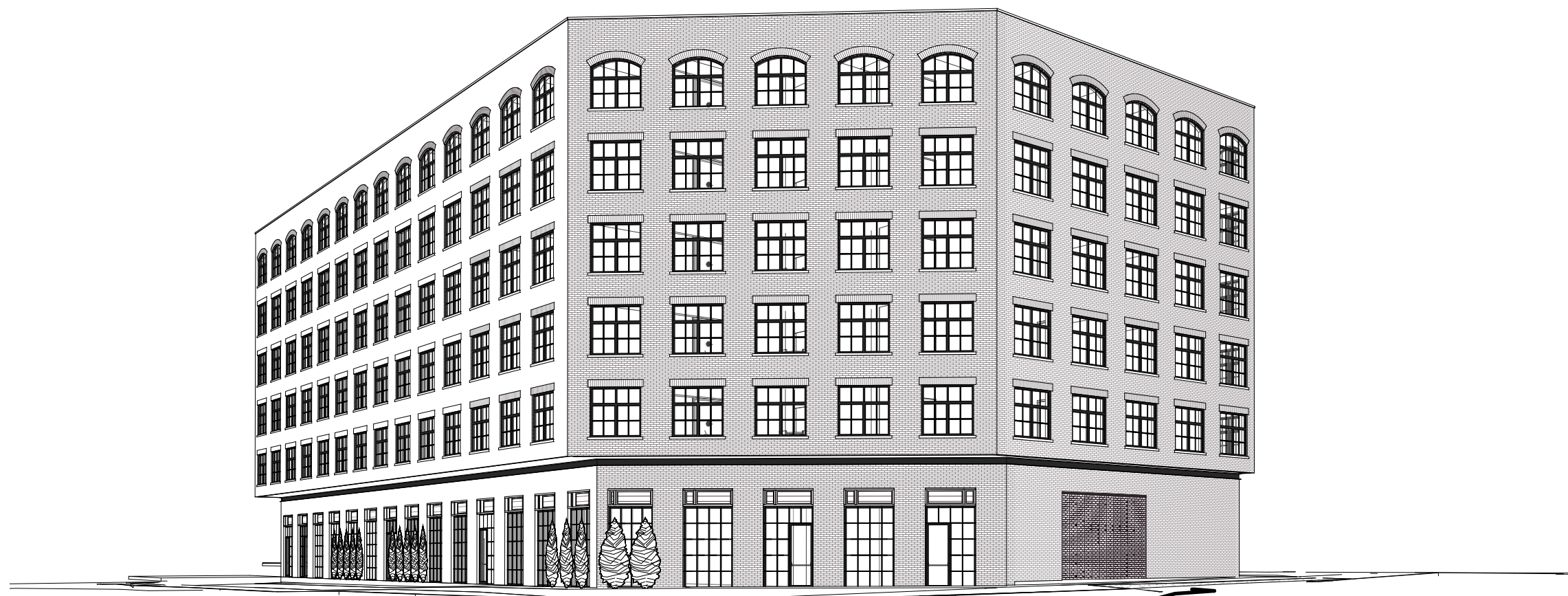
NORTH & WEST  
ELEVATION - MATERIAL

CLIENT  
MAIN MARGARET INC.  
[REDACTED]

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DATE	2025-06-01	

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MARENCO ARCHITECTS

MARENCO ARCHITECTS INC.  
494 LINCOLN RD, WINDSOR, ON N8W 2R7

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

SCALE	NORTH
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DRAWING TITLE

## PERSPECTIVES

CLIENT  
MAIN MARGARET INC.  
[REDACTED]

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DATE	2025-06-01	

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SITE PLAN APPROVAL   DA-22-167	2025-06-01
DESCRIPTION	DATE

ONTARIO ASSOCIATION  
 OF  
 ARCHITECTS  
 WILFRED M. MARENCO  
 LICENCE  
 10529

MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R6

SCALE  
1:100

SECTION

MAIN MARGARET INC.

DRAWN	
CHECKED	
DATE	2025-0



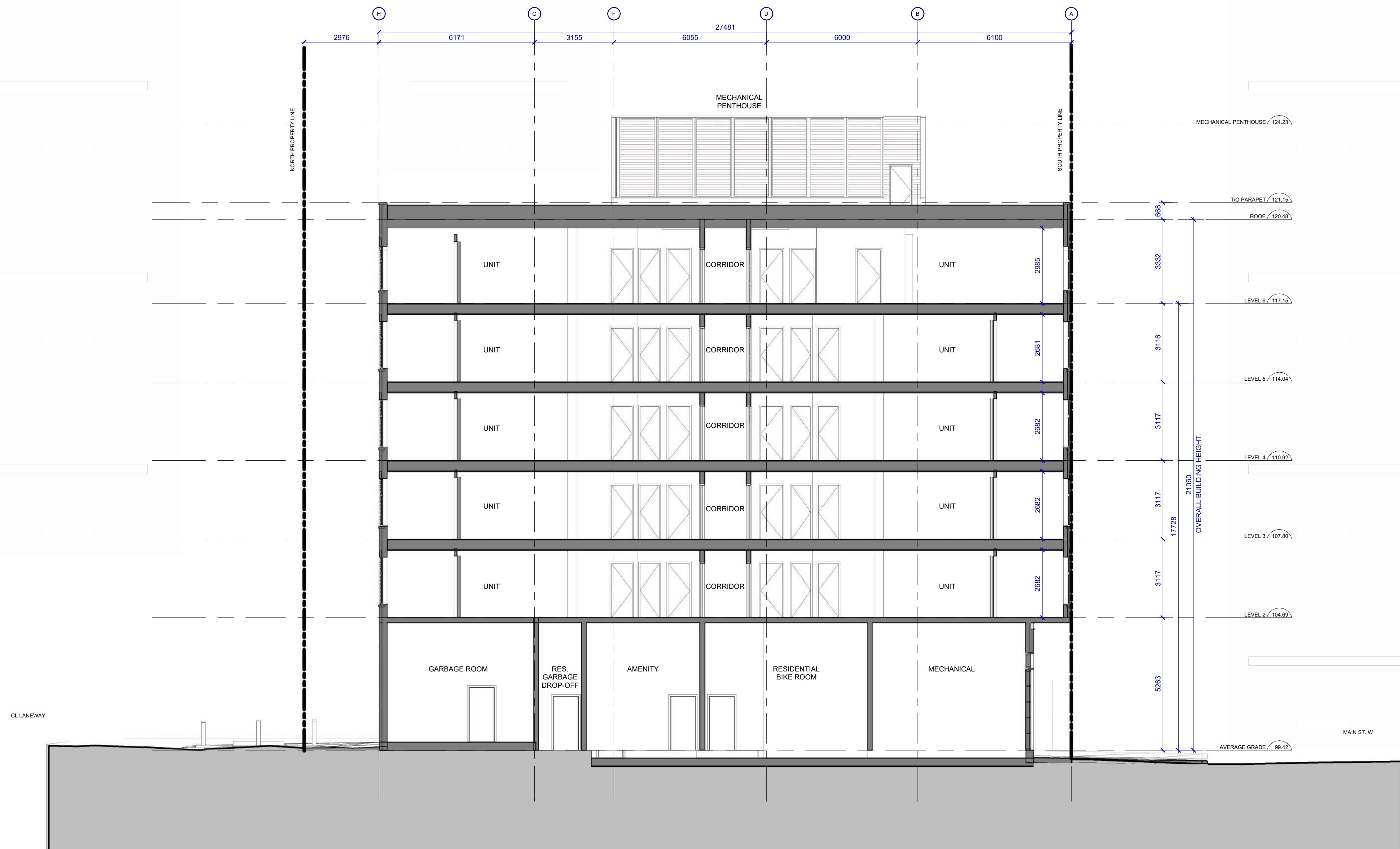
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MARENCO ARCHITECTS INC.  
2494 LINCOLN RD, WINDSOR, ON N8W 2R7

SCALE	1:100	NORTH	
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CLIENT  
MAIN MARGARET INC.  
[REDACTED]

DRAWN	IM	PAGE NO.  A501
CHECKED	WM	
DATE	2025-06-01	



1 SECTION THROUGH ELEVATOR  
A501 SCALE: 1 : 100



[illegible]**SUED RECORD**

MARENCO ARCHITECTS INC.  
94 LINCOLN RD, WINDSOR, ON N8W 2R7

592 MAIN ST. W.  
HAMILTON ON, L8P 1K4

DRAWING TITLE

## ENDING

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DATE	2025-06-01	

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SCALE: 1 : 1

[illegible]

392 MAIN ST. W.  
HAMILTON ON, L8P 1K4

A large rectangular area with a black border, representing a drawing. The text 'DRAWING TITLE' is located in the top-left corner. The word 'RENDERING' is centered within the rectangle.

DRAWN	IM	PAGE NO.  <b>A901</b>
CHECKED	WM	
DATE	2025-06-01	





June 27, 2025

Justin Leung,  
Secretary Treasurer, Committee of Adjustment  
City of Hamilton  
71 Main Street West, 5<sup>th</sup> Floor  
Hamilton, Ontario L8P 4Y5

Dear Mr. Leung:

**Re:    *Minor Variance Application***  
***Site Plan Approval Application DA-22-167***  
***392 Main Street West, Hamilton***

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## **1.0 INTRODUCTION**

We are the planning consultants for Main Margaret Inc., the owners of the lands municipally known as 392 Main Street West in the City of Hamilton, (hereinafter referred to as the “site” or the “subject site”). On behalf of our client, we are pleased to submit the enclosed Minor Variance Application to permit the use of parking stackers for a residential use, and for reduced-size parking spaces, which are associated with the conditionally approved Site Plan Control application (DA-22-167). This letter will provide an overview of the application history, an overview of the subject site and surroundings, describe the requested minor variances, and will provide our opinion as to how the requested variances satisfy the four (4) tests prescribed by Section 45(1) of the *Planning Act*.

## **2.0 BACKGROUND**

A Site Plan Control Application was submitted to the City of Hamilton (the “City”) on October 14, 2022 (City file number DA-22-167) for a 9-storey (plus rooftop amenity/mechanical) mixed-use building with ground floor non-residential uses and residential above. Initial comments were received from the City on January 6, 2023, and a presentation to the Design Review Panel was made on March 9, 2023.

The site and building design were modified to respond to the comments received from the City, and revised materials were submitted to the City on August 9, 2023. A Minor Variance application was also submitted on August 9, 2023. The Minor Variance application was heard and approved by the City of Hamilton Committee of Adjustment on October 19, 2023 (City file number HM/A-23:218).

The approved variances included: permission for restaurant and retail uses in accordance with the site-specific Secondary Plan policy; modifications to building setbacks at the ground level along the western and northerly yards and for setbacks to upper storeys from the northern lot line; permission for balcony projections into the northerly required setback; increase to the maximum building height; increase to the maximum size of a rooftop amenity area and associated architectural features; reduction to the minimum required vehicular parking spaces; removal of the planting strip requirement; and modifications to the locational requirements for an access driveway based on the former 6593 Zoning By-law. No appeals were filed to the Ontario Land Tribunal and a Final and Binding Notice was issued by the City on November 9, 2023.

The City subsequently issued a conditional approval for the Site Plan Control Application on December 8, 2023. The conditions of approval generally relate to the finalization, submission, and approval of detailed design drawings and studies, including architectural plans, a grading and drainage control plan, landscape plan, erosion and siltation control plan, etc.

On June 5<sup>th</sup>, 2025, several plans and technical reports were submitted to clear the conditions of Site Plan Approval. These materials reflected modifications made to the building and site design which, based on our understanding, were to respond to the evolving market conditions and to improve building constructability. These modifications represent an overall decrease in building mass, height and density (as described in more detail in Section 4.0) but two (2) variances are required to facilitate the proposed modifications.

### **3.0 SITE AND SURROUNDINGS**

#### ***Subject Site***

The subject site is irregular in shape and has a total area of approximately 1,767 square metres (0.17 hectares) with a frontage of approximately 56 metres along Main Street West and 24.5 metres along Margaret Street. The subject site is currently occupied by a 2-storey brick building that is currently vacant. The subject site also has a large surface parking lot at the interior of the site that is accessed from Margaret Street. The subject site is generally flat, with limited landscaping that includes street trees along both street frontages and a small unpaved portion of the site which is covered in grass and gravel.

### ***Surrounding Area***

The subject site is located within the Strathcona neighbourhood, which is characterized as an area that is in active transition from a predominantly low-rise character to one of varied building heights and styles.

- **North:** A City-owned, unassumed alleyway abuts the site to the north, with low-rise residential uses to the north and northeast. Further north, along King Street West, is a mix of low- and mid-rise residential, commercial, retail, and institutional uses.
- **East:** Directly east of the subject site is Margaret Street, a local road that runs north-south between Main Street West and King Street West. Further east is a mix of low-rise office, commercial, restaurant, surface parking, and mid-rise residential uses along the north and south sides of Main Street West.
- **South:** Directly south of the subject site is Main Street West, a major arterial road that runs east-west across the City of Hamilton. A mix of commercial, retail, office and residential uses are located along Main Street West, with a seven (7) storey residential apartment building being recently completed (415 Main Street West) and a seven (7) storey affordable residential apartment building (405 Main Street West) currently under construction just southwest of the subject site.
- **West:** Low-rise residential dwellings abut the subject site to the west with a mix of commercial and office uses in converted dwellings, surface parking areas, and residential uses further west along Main Street West. A 9-storey residential apartment is proposed on the west side of Strathcona Avenue South, located within approximately 100 metres to the west of the subject site (428 Main Street West).

### **4.0 REQUESTED MINOR VARIANCES**

As mentioned above, revised plans and technical reports were submitted on June 5, 2025, to support the clearance of conditions of Site Plan Approval.

Since the December 2023 conditional Site Plan Approval, the proposal has been revised from a 9-storey mixed-use building to a 6-storey residential building. The building setbacks are generally consistent with the original proposal, but the upper storeys of the building have been removed, and the massing has been modified to eliminate stepping at the rear of the building. The single level of underground parking proposed in the December 2023 conditionally-approved plans has been removed and the ground floor has been reconfigured to accommodate the required resident, visitor, and barrier-free parking.

Based on discussions with City Planning Staff, it was agreed that the modifications are minor and that the application could proceed through the submission of materials to clear conditions.

The proposal now contains 122 dwelling units, whereas 169 were previously proposed, and has a Floor Space Index of 4.74, whereas 5.93 was previously proposed. 29 parking spaces are proposed, of which 8 are for visitors (including 1 barrier-free visitor space), and 21 residential parking spaces are provided (20 spaces within 10 stacked parking units and 1 as a barrier-free). Visitor and barrier-free spaces are not proposed within a parking stacker system. 132 long-term bicycle parking spaces and 16 short-term/visitor bicycle parking spaces are proposed.

The use of parking stackers for residential use is not permitted in the Zoning By-law. As such, the following variances are required to facilitate the proposal:

**1. Use of Parking Stacker System for Provided Residential Parking (5.2.4.c)ii), By-law 05-200)**

Residential parking may be provided within a parking stacker system.

**2. Minimum Parking Space Dimensions (5.2.1.a), By-law 05-200)**

Parking space sizes for spaces provided within a parking stacker system shall be a minimum of 2.3 metres in width and 5.2 metres in length.

## **5.0 PLANNING ANALYSIS**

As set out below, it is our opinion the requested variances both individually and cumulatively satisfy the four tests for a minor variance set out in Section 45(1) of the *Planning Act*, as follows:

**(1) Maintains the General Intent and Purpose of the Official Plan**

The subject site is designated *Mixed Use – Medium Density* as illustrated on Schedule E-1 of the Urban Hamilton Official Plan (“UHOP”) and is located along a Primary Corridor as per Schedule E – Urban Structure. The *Mixed Use – Medium Density* designation permits a range of retail, service commercial, entertainment, and residential uses. Policy 4.6.4 provides that it is the function of areas designated *Mixed Use – Medium Density* to serve as vibrant people places with increased day and night

activity through the introduction of residential development. The intent of the UHOP is also to ensure that new development provides for sufficient parking and manoeuvring and that the placement of parking and loading area is done in such a way to minimize the impact of parking on urban environments and pedestrian activity (Policy C.4.5.18).

The subject site falls within the Strathcona Secondary Plan area where it is designated *Mixed Use – Medium Density* and is subject to Area Specific Policy 'B'. The general intent and purpose of the Official Plan, specifically Area Specific Policy B within the Strathcona Secondary Plan is to provide for the intensification of the Main Street corridor with a continuous, animated street edge at transit-supportive densities while also providing for appropriate transitions to existing residential uses, minimizing built form impacts, promoting human-scaled design and improving the pedestrian experience.

With respect to the requested variances, the general intent and purpose of the Strathcona Secondary Plan, as described in Policy B.6.6.6.1.f) is to provide adequate internal traffic circulation, parking, loading and manoeuvring facilities. Policy 6.6.6.1.m) also specifies that private and public parking areas should be buffered from the street through the use of building placement or enhanced landscaping and should be located in a way that they do not negatively affect the pedestrian environment or access to buildings. Policy 6.6.6.2 k) encourages required parking to be located underground or accommodated in parking structures, where possible.

The requested variances are not seeking relief to the minimum number of required parking spaces, rather are seeking permission for the proposed residential parking spaces to be permitted within a parking stacker system. The proposed parking stackers will be located on the ground floor, behind the main walls of the building and will therefore be screened from view from the public realm.

The proposed parking stackers also maintain the general intent and purpose of the UHOP, specifically the Strathcona Secondary Plan, by providing for adequate internal traffic circulation and manoeuvring space which is demonstrated by the enclosed "Parking Stacker Review" memo prepared by BA Group, specifically "Attachment A: Passenger Vehicle Manoeuvring and Queuing Diagrams". The reduction in parking space size is required to enable the use of the parking stackers and is therefore technical in nature.

In our opinion, the variances maintain the general intent and purpose of the UHOP, including the Strathcona Secondary Plan, as the use of a stacked parking system will enable the proposed parking supply to be provided in a more efficient manner while providing adequate internal traffic circulation and adequate manoeuvring space as well as promoting an animated, pedestrian-focused public realm.

## (2) Maintains the General Intent and Purpose of the Zoning By-law

The subject site is zoned C5 Exception 298 within Zoning By-law 05-200. The Mixed Use Medium Density (C5) zone permits a range of residential types, including multiple dwellings. Special Exception 298 refines the list of permitted uses, and continues to include permissions for multiple dwellings, as well as offices, medical clinics, studios, and hotels, among others.

The general intent and purpose of the Zoning By-law, specifically Section 5.2.4 c) with respect to access requirements, is to ensure that access to a parking space is not obstructed, and that access (i.e., ingress and egress) to parking spaces is available at all times without the necessity of moving another motor vehicle. The purpose of this regulation is to ensure a user of a motor vehicle can move their vehicle freely and without relying on someone not in their household to move their vehicle before being allowed to enter or exit the parking space.

As described in more detail in the enclosed Parking Stacker Review memo prepared by BA Group and shown on **Figure 1**, pit stackers are proposed which will allow for 2 cars to be parked per stacker. Each stacker will have 3 vertical levels and two vehicular platforms that move to and between the levels. In the lowered position, the bottom platform and vehicle is located below grade, and the top platform and vehicle are located at-grade. It is in the lowered position that a vehicle on the top platform can be accessed. In the raised position, the bottom platform and vehicle are located at-grade, and the top platform and vehicle are located above grade. It is in the raised position that the bottom platform and vehicle can be accessed.

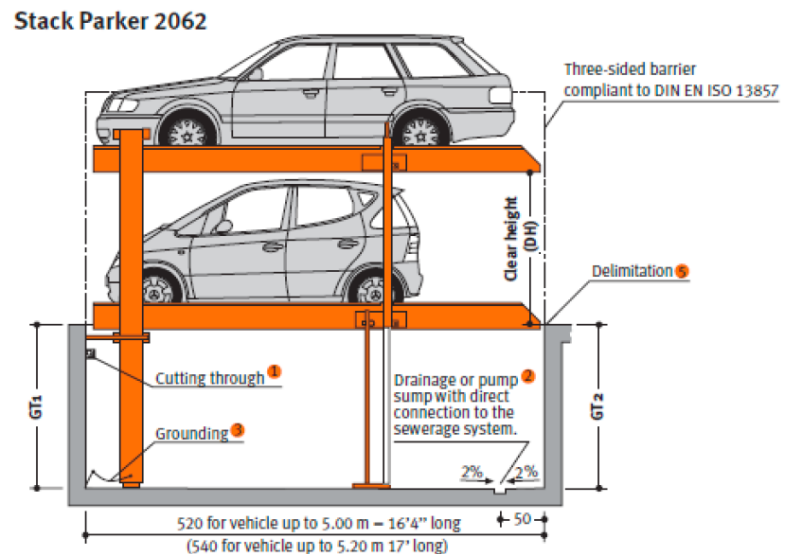


Figure 1: Sample Parking Stacker (Klaus Single Vario Stack Parker 2062)

The parking stacker system will be used exclusively by residents of the building and operated by remote controls that each parking space user will own. This means that

each owner can control the movement of the stacker to access their vehicle independently of the other vehicle on the stacker, thereby meeting the general and intent and purpose of the Zoning By-law.

As it relates to parking space size, the intent of a minimum parking space size is to ensure there is adequate space to accommodate: the parked motor vehicle itself; vehicle door swings and pedestrian movement into and out of the vehicle; and manoeuvring space for the vehicle to safely access the parking space without damaging other vehicles.

The requested variance to reduce the required parking space size is technical in nature as the stacked parking is designed to only accommodate the parked vehicle itself, but given vehicles will be accessed when the vehicle platform is in the “at-grade position”, there will be adequate space on either side for each vehicle to be accessed.

The design of the parking stacker creates a gap of approximately 0.7 metres between each parking space which can be walked on and accessed, which provides the necessary space for opening vehicle doors and for pedestrians to access the doors of the parked vehicles.

Further, to ensure that residents do not park or attempt to park vehicles that are larger than the dimensions of the stacked parking spaces, there will be clauses included in future agreements to inform residents of the maximum parking space size. As specified in BA Group’s memo, a wide range of commonly used vehicles (i.e., cars and SUVs) can be accommodated such as Honda Civics, Accords, and HR-Vs, BMW 5-Series, Ford Escapes, Hyundai Konas, Jeep Wranglers, Nissan Rogue, and Toyota Rav4s and Camrys, among others.

Accordingly, it is our opinion that the requested variances maintain the intent and purpose of the Zoning By-law.

### **(3) Minor in Nature**

The test of “minor” is not intended as a numerical assessment, but rather an assessment of impact. In our opinion, the impacts associated with the requested variances are minor in nature and will not result in any unacceptable or undue impacts on the subject site or surrounding neighbourhood.

As described above, the proposed use of the parking stacker system for residential parking spaces will allow residents to access their parking space independently and without relying on other vehicle owners or residents. The proposed parking space size



will allow for adequate space to enter, exit and access all doors of the vehicle as well as adequate space to safely manoeuvre in and out of the parking space.

As outlined in the attached Parking Stacker Review memo provided by BA Group, the stacked parking system can process vehicles at a much faster rate than the anticipated arrival rate and therefore minimal queueing is anticipated to occur. If queueing does occur, there is adequate space provided on-site to ensure the queues do not impact the public realm or the operations of the adjacent public streets.

As mentioned above, the proposed maximum vehicle sizes will be included in future agreements with residents along with the proposed parking space size for spaces within the parking stacker system.

Accordingly, it is our opinion that the proposed variances are minor in nature.

#### **(4) Desirable for the Appropriate Development and Use of the Land**

In our opinion, the proposal and associated minor variances are desirable and appropriate for the proposed use of the land given the site's location along a Primary Corridor and within walking distance to the proposed Hamilton Light Rail Transit (LRT). The parking stackers will promote the efficient use of space and will help facilitate the construction of the conditionally-approved residential building which is supportive of numerous provincial and city policies.

Accordingly, in our opinion, the variances are desirable and appropriate for the proposed use of the land.

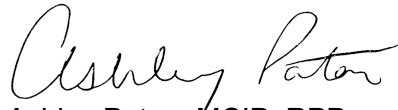
### **7.0 Conclusion**

In support of the requested Minor Variance application, we submit the following:

- A Completed and Signed 2025 Committee of Adjustment Application Form;
- Architectural Plans (including Site Plan, Floor Plans, Building Elevations, Building Sections) prepared by Marengo Architects, dated June 1, 2025;
- Parking Stacker Review memo prepared by BA Group, dated June 24, 2025;
- A copy of this Planning Rationale Letter.

We trust that the foregoing is satisfactory. However, if you have any questions or require additional information, please do not hesitate to contact the undersigned or Dan Watchorn of our office at [dwatchorn@bousfields.ca](mailto:dwatchorn@bousfields.ca).

Yours very truly,  
**Bousfields Inc.**

  
Ashley Paton, MCIP, RPP

cc. *Main Margaret Inc.*



June 24, 2025

Main Margaret Inc.  
[REDACTED]  
[REDACTED]  
[REDACTED]

**Re: 392 Main Street West, Hamilton – Site Plan Application # DA-22-167**  
**Parking Stacker Review**

Dear Kasra,

BA Group has been retained to provide transportation advisory services in relation to the development of 392 Main Street West in the City of Hamilton. The site is generally located at the northwest corner of the Main Street West and Margaret Street intersection. A lane generally extends along the north side of the property.

The proposal involves the construction of a 6-storey mid-rise building with 122 dwelling units. A supporting parking garage is provided on the ground floor and accessed through a driveway onto a City owned lane which connects to Margaret Street. Supporting visitor parking spaces are also proposed along the north side of the property. A total of 29 parking spaces are proposed (8 visitor and 21 resident). Of the resident spaces, 20 are proposed to be in a stacker configuration, whereby vehicle parking spaces are stacked on top of one another and vehicles accessed through a semi-automatic system.

The stacker arrangement is the subject of a Minor Variance Application.

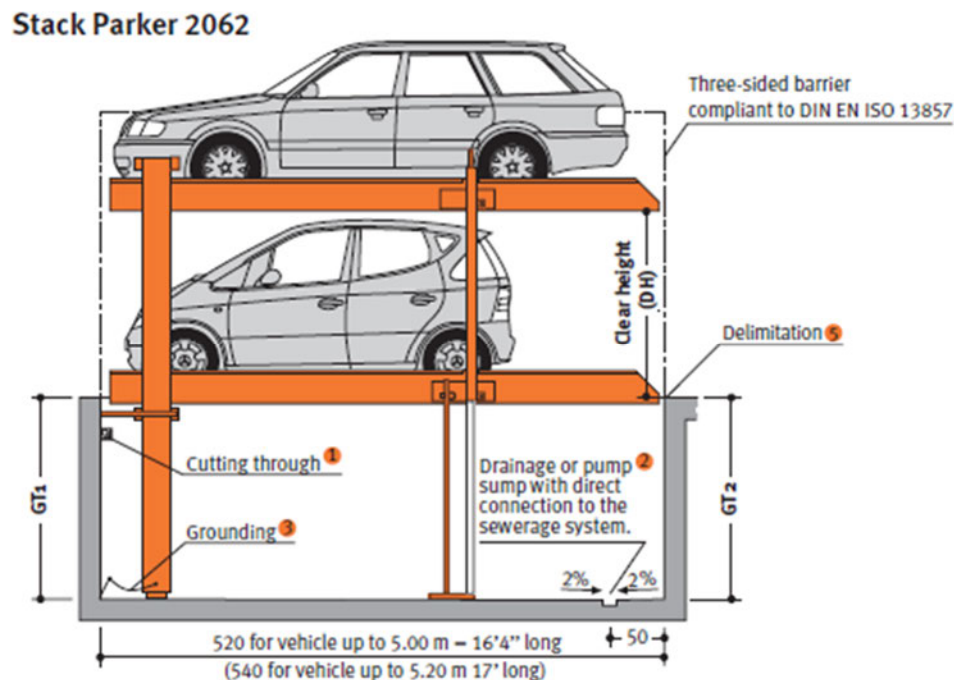
This letter has been prepared to summarize our review of the proposed stacked parking arrangements on site. Key elements reviewed include:

- General parking stacker operations
- Stacker Dimensions and accommodated vehicle types
- Potential queuing associated with the proposed parking arrangements.

## 1.0 GENERAL PARKING STACKER OPERATIONS

Pit stackers would be used within the parking garage allowing for more efficient deployment of parking spaces. The pit stacker system has three levels as shown in **Figure 1**. Each stacker allows for two cars to be parked.

The grade level is for entering and exiting the stacker. The upper and lower levels are where cars are ultimately parked. The lower level is below grade (i.e. in a “pit”).



**Figure 1 - Sample Parking Stacker (Klaus Single Vario Stack Parker 2062)**

The stacker system operations for parking and retrieval are generally as follows:

### Parking the Vehicle

- The driver manoeuvres the car into the grade level of the stacker and parks
- The driver and passengers exit the car
- The driver presses a button to move the car into a free parking position on the upper or lower level of the stacker.

### Retrieving the Vehicle

- The driver presses a button which brings the car from the upper or lower level of the stacker to grade level
- The driver and passengers enter the car
- The driver manoeuvres out of the space and exits the garage.

Supporting vehicle manoeuvring diagrams illustrating passenger car entering and exiting the stacker is shown in **Attachment A**.

## 2.0 STACKER DIMENSIONS AND ACCEPTED VEHICLES

While parking stackers allow for efficiently use of a parking garage, particularly for resident spaces, there can be limitations on the vehicle sizes that can use these systems. The maximum permitted vehicle size dimensions and representative vehicles accommodated within those dimensions are shown in **Table 1**. In addition, parking spaces located within the parking stackers are not considered obstructed and an additional width of 0.30m is not required as per the by-law for door openings.

**Table 1     Parking Stacker Dimensions- Parking Platform**

	Height	Width	Length	Vehicle Examples Within Size Restrictions
Parking Stacker	1.95m	2.3m	5.2m	Dodge Caravan Chevy Equinox Ford Explorer GMC Terrain Honda HR-V Hyundai Santa Fe Jeep Wrangler Nissan Rogue Subaru Forester Tesla Model X Toyota Rav4

Notes:  
1.        Vehicle dimension size based on 2023 Transportation Canada Vehicle Database.

The condominium and/or rental agreements for the building will require that residents be informed of the vehicle size limitations for stacked parking spaces. Further, to accommodate this system the zoning by-law parking requirements would need to be updated to permit parking spaces that are a minimum of 1.95m tall, 2.3m wide and 5.2m long.

### 3.0 QUEUING ASSESSMENT

To review the potential queuing from the garage onto the lane, the weekday afternoon peak hour was analyzed. This timeframe is when the number of trips into the garage is expected to be the greatest, and the largest queues on the lane would occur.

Prior traffic analysis work prepared by BA Group indicated that the Site is anticipated to generate 20 inbound trips during the weekday afternoon peak hour. The 20 inbound trips translate to 1 vehicle every 3 minutes on average (0.33 vehicles per minute). Considering random arrival patterns, there may be a timeframe in the peak hour, where a vehicle arrives every 2 minutes (0.50 vehicles per minute).

A review of performance times for various stacked parking systems indicates that cars can be parked at a rate of 40 to 70s per vehicle (0.85 to 1.5 vehicles per minute). A processing time of 0.85 vehicles per minute has been conservatively adopted for this analysis.

The stacked parking system can process vehicles at a much faster rate (0.85 vehicles per minute) than the arrival rate (0.50 vehicles per minute) and therefore minimal queuing activity is anticipated to occur on the lane. However, there may be occasions where queueing can occur on the lane. Based on our review, a queue of no more than 2 vehicles is forecast at any given time. These results indicate that queues will not extend back to Margaret Street, which is considered to be acceptable. A diagram illustrating maximum queueing is provided in **Attachment A**. Queueing analysis results are also summarized in **Table 2**.

**Table 2 Weekday Afternoon Peak Hour – Queuing Summary**

Site	Inbound Demand		System Speed	Queues	
	Average	95 <sup>th</sup> Percentile		95 <sup>th</sup> Percentile	Maximum
122 units (20 inbound trips)	0.33 vehicles per minute	0.50 vehicle per minute	0.85 vehicles per minute	1 vehicle	2 vehicles

## 4.0 SUMMARY

Based upon our review, the proposed developments use of parking standards is considered to be an effective and appropriate method of accommodating parked resident vehicles. Should you have any questions, please do not hesitate to contact us.

**BA Consulting Group Ltd.**

Stephen J. Bahadoor

A handwritten signature in black ink, reading "Stephen Bahadoor". The signature is fluid and cursive, with the first name "Stephen" and last name "Bahadoor" clearly legible.

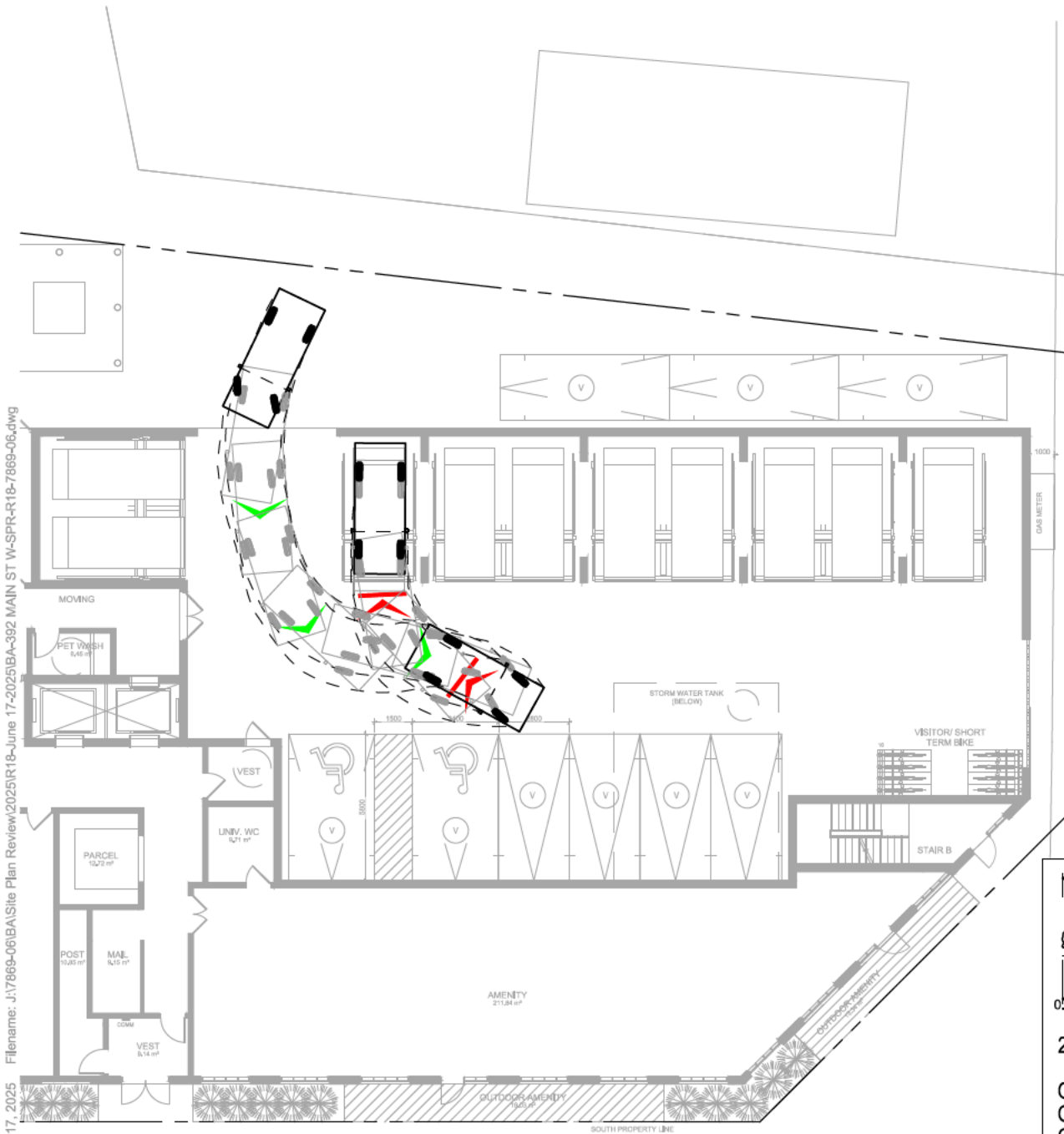
**cc:** Mohammad Bari



## **Attachment A: Passenger Vehicle Manoeuvring and Queuing Diagrams**

INBOUND

OUTBOUND (ONE CORRECTION)



2020 Dodge Grand Caravan GT

Overall Length	5.17m
Overall Width (without Mirrors)	2.00m
Overall Width (with Mirrors)	2.25m
Overall Body Height	1.75m
Outside Wall to Wall Turning Radius	6.50m
Inside Wall to Wall Turning Radius	3.35m

Filename: J:\7869-06\BA\Site Plan Review\2025\18-June 17-2025\BA-392 MAIN ST W-SPR-18-7869-06.dwg  
Date Plotted: June 17, 2025



392 MAIN STREET WEST  
VEHICLE MANOEUVRING DIAGRAM  
PARKING STACKERS  
95TH PERCENTILE PASSENGER VEHICLE (DODGE CARAVAN)

Project: 392 MAIN ST. W.  
Project No. 7869-06  
Date: JUNE 17, 2025  
Revised: -

Scale

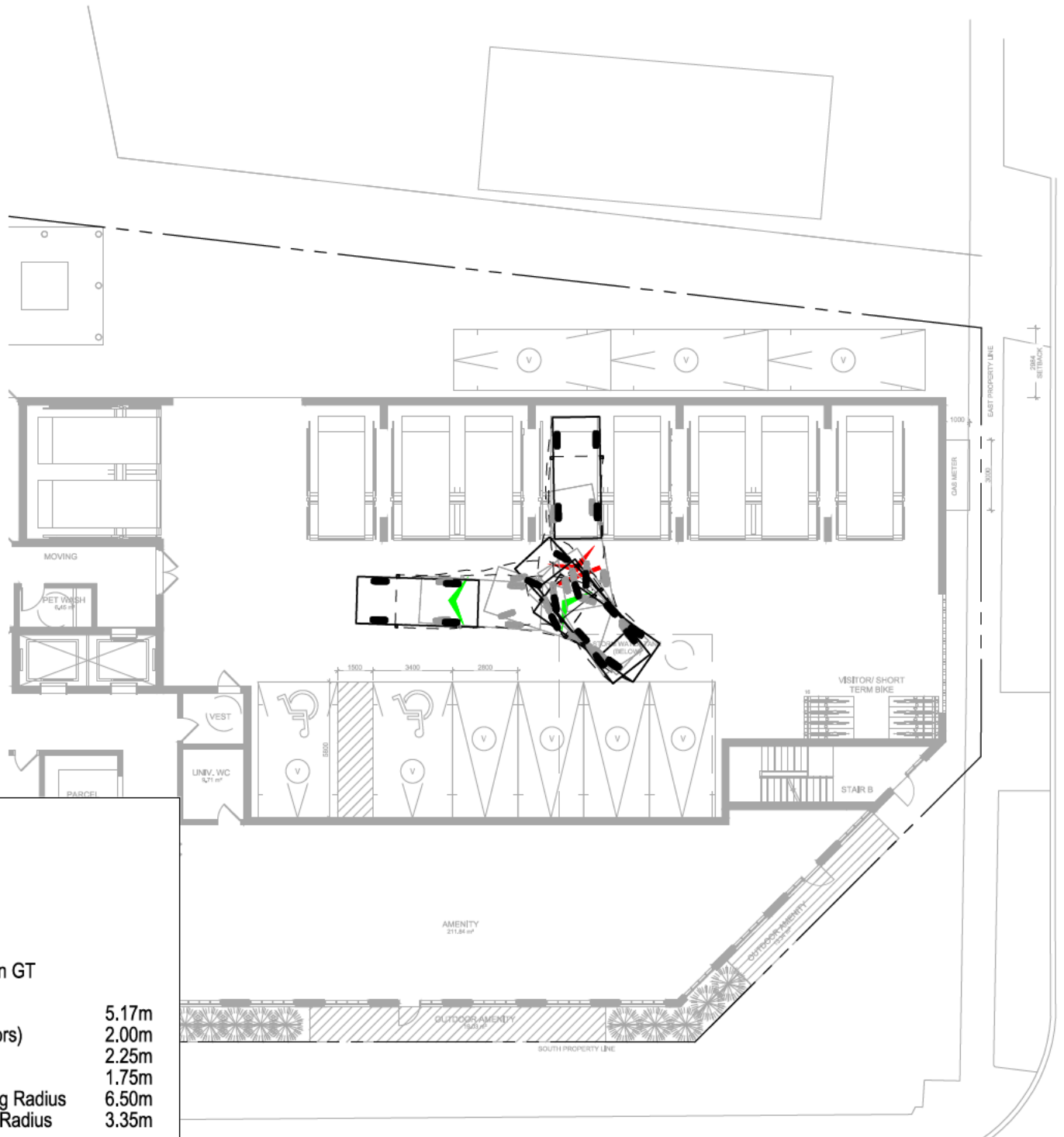
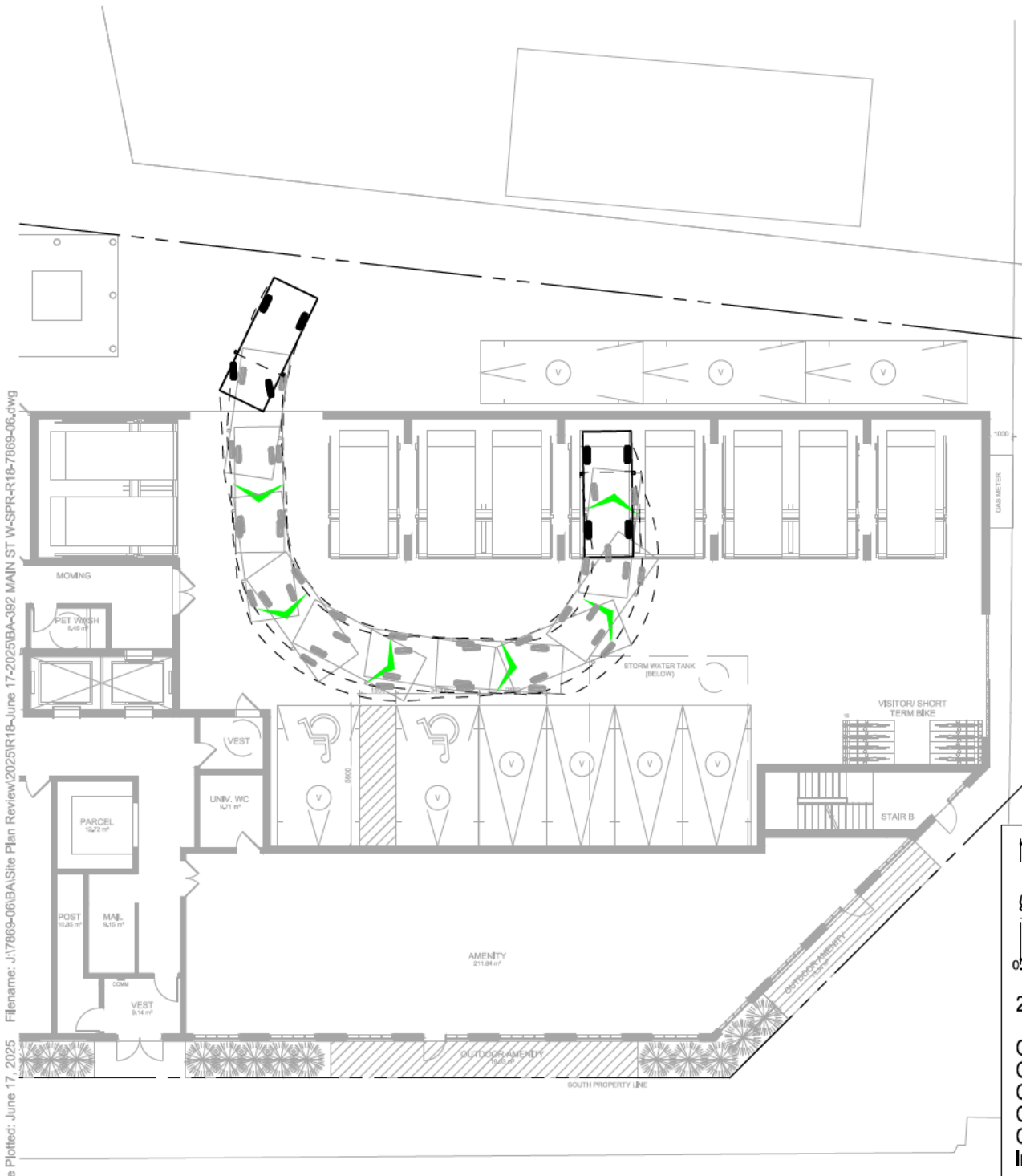
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Drawing No.

VMD-03

INBOUND

OUTBOUND (ONE CORRECTION)



2020 Dodge Grand Caravan GT

Overall Length	5.17m
Overall Width (without Mirrors)	2.00m
Overall Width (with Mirrors)	2.25m
Overall Body Height	1.75m
Outside Wall to Wall Turning Radius	6.50m
Inside Wall to Wall Turning Radius	3.35m

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Date Plotted: June 17, 2025



392 MAIN STREET WEST  
VEHICLE MANOEUVRING DIAGRAM  
PARKING STACKERS  
95TH PERCENTILE PASSENGER VEHICLE (DODGE CARAVAN)

Project: 392 MAIN ST. W.  
Project No. 7869-06  
Date: JUNE 17, 2025  
Revised: -

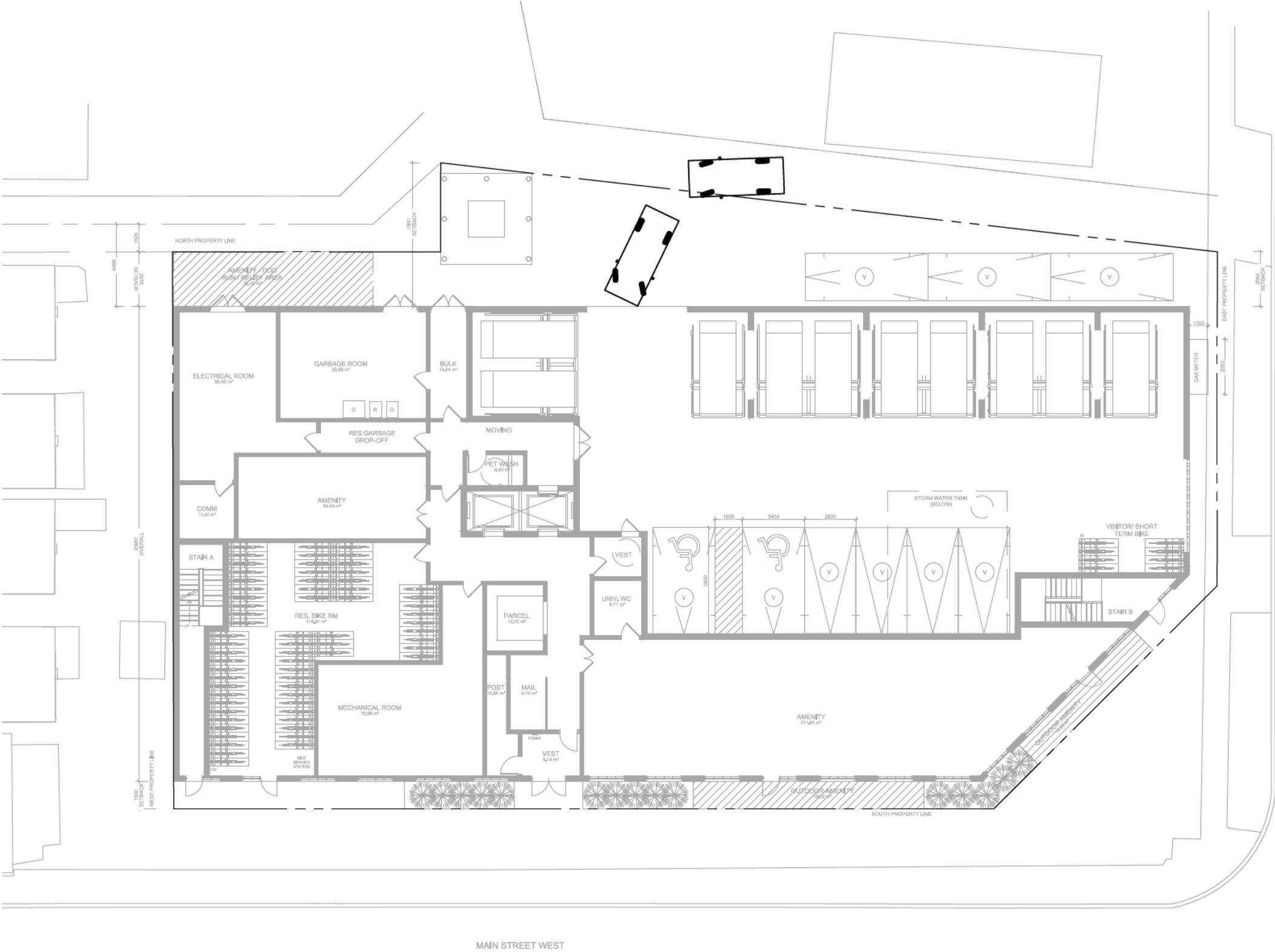
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392 MAIN STREET WEST  
PARKING STACKERS VEHICLE QUEUING

Project: 392 MAIN ST. W.  
Project No. 7869-06  
Date: JUNE 17, 2025  
Revised: -

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0 5 10m  
Drawing No. VH-01

**APPLICATION FOR A MINOR VARIANCE/PERMISSION**  
UNDER SECTION 45 OF THE *PLANNING ACT*

**1. APPLICANT INFORMATION**

	NAME	MAILING ADDRESS
<b>Registered Owners(s)</b>	Main Margaret Inc.	
<b>Applicant(s)</b>	Main Margaret Inc.	
<b>Agent or Solicitor</b>	Bousfields Inc. c/o Ashley Paton	

1.2 Primary contact ☐ Applicant ☐ Owner ☒ Agent/Solicitor

1.3 Sign should be sent to ☐ Applicant ☐ Owner ☒ Agent/Solicitor

1.4 Request for digital copy of sign ☒ Yes\* ☐ No

If YES, provide email address where sign is to be sent

1.5 All correspondence may be sent by email ☒ Yes\* ☐ No

If Yes, a valid email must be included for the registered owner(s) AND the Applicant/Agent (if applicable). Only one email address submitted will result in the voiding of this service. This request does not guarantee all correspondence will sent by email.

1.6 Payment type ☐ In person ☒ Credit over phone\*  
☐ Cheque

\*Must provide number above

## 2. LOCATION OF SUBJECT LAND

2.1 Complete the applicable sections:

Municipal Address	388-392 Main Street West, 68, 72, & 74 Margaret Street		
Assessment Roll Number			
Former Municipality	Hamilton		
Lot		Concession	
Registered Plan Number	*see below	Lot(s)	*see below
Reference Plan Number (s)	62R-20411	Part(s)	

\*Lot 14 to 16, Registered Plan 227, and Lot 34 and 35 and Part of Lots 31, 32, and 33, Registered Plan 389

2.2 Are there any easements or restrictive covenants affecting the subject land?

☐ Yes ☒ No

If YES, describe the easement or covenant and its effect:

## 3. PURPOSE OF THE APPLICATION

**Additional sheets can be submitted if there is not sufficient room to answer the following questions. Additional sheets must be clearly labelled**

All dimensions in the application form are to be provided in metric units (millimetres, metres, hectares, etc.)

3.1 Nature and extent of relief applied for:

Permission for parking stackers for a residential use  
Reduction of the minimum required parking space size

☐ Second Dwelling Unit ☐ Reconstruction of Existing Dwelling

3.2 Why it is not possible to comply with the provisions of the By-law?

Refer to attached planning rationale letter

3.3 Is this an application 45(2) of the Planning Act.

☐ Yes ☒ No

If yes, please provide an explanation:

## 4. DESCRIPTION OF SUBJECT LAND AND SERVICING INFORMATION

4.1 Dimensions of Subject Lands:

Lot Frontage	Lot Depth	Lot Area	Width of Street
44.8m (Main), 18.3m (Margaret)	Approx. 57.1m (from Margaret)	1,767 sq.m.	~23m (Main), ~20m (Margaret)



4.2 Location of all buildings and structures on or proposed for the subject lands:  
(Specify distance from side, rear and front lot lines)

Existing:

Type of Structure	Front Yard Setback	Rear Yard Setback	Side Yard Setbacks	Date of Construction
Dwelling	~29m (Margaret)	~6.5m (westerly side yard)	0.0m (Main)	Pre-1898

Proposed:

Type of Structure	Front Yard Setback	Rear Yard Setback	Side Yard Setbacks	Date of Construction
Apartment Building	1.5m (Margaret)	0m (westerly side yard)	1.5m (Main), ~3.0m (north)	

4.3. Particulars of all buildings and structures on or proposed for the subject lands (attach additional sheets if necessary):

Existing:

Type of Structure	Ground Floor Area	Gross Floor Area	Number of Storeys	Height
Dwelling	unknown	unknown	2.5	~9m

Proposed:

Type of Structure	Ground Floor Area	Gross Floor Area	Number of Storeys	Height
Apartment Building	1,378.06 sq.m.	8,372 sq.m.	6	21.0m

- 4.4 Type of water supply: (check appropriate box)
- ☒ publicly owned and operated piped water system
- ☐ privately owned and operated individual well

- ☐ lake or other water body
- ☐ other means (specify)
- \_\_\_\_\_

- 4.5 Type of storm drainage: (check appropriate boxes)
- ☒ publicly owned and operated storm sewers
- ☐ swales

- ☐ ditches
- ☐ other means (specify)
- \_\_\_\_\_

4.6 Type of sewage disposal proposed: (check appropriate box)

☒ publicly owned and operated sanitary sewage

☐ system privately owned and operated individual

☐ septic system other means (specify) \_\_\_\_\_

4.7 Type of access: (check appropriate box)

☐ provincial highway

☐ right of way

☐ municipal road, seasonally maintained

☐ other public road

☒ municipal road, maintained all year \_\_\_\_\_

4.8 Proposed use(s) of the subject property (single detached dwelling duplex, retail, factory etc.):

Residential apartment building

4.9 Existing uses of abutting properties (single detached dwelling duplex, retail, factory etc.):

Low-rise residential dwellings (north and west), low-rise commercial and residential uses (east), low- and mid-rise residential and commercial (south)

## 7 HISTORY OF THE SUBJECT LAND

7.1 Date of acquisition of subject lands:

2015

7.2 Previous use(s) of the subject property: (single detached dwelling duplex, retail, factory etc)

Low-rise residential uses and commercial-office uses

7.3 Existing use(s) of the subject property: (single detached dwelling duplex, retail, factory etc)

Commercial-office uses

7.4 Length of time the existing uses of the subject property have continued:

50+ years

7.5 What is the existing official plan designation of the subject land?

Rural Hamilton Official Plan designation (if applicable): N/A

Rural Settlement Area: N/A

Urban Hamilton Official Plan designation (if applicable) Mixed Use - Medium Density

Please provide an explanation of how the application conforms with the Official Plan.

See attached Planning Rationale Letter

7.6 What is the existing zoning of the subject land? C5 (Mixed Use Medium Density), 298

7.8 Has the owner previously applied for relief in respect of the subject property?  
(Zoning By-law Amendment or Minor Variance)

☒ Yes

☐ No

If yes, please provide the file number: HM/A-23:218



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7.9 Is the subject property the subject of a current application for consent under Section 53 of the *Planning Act*?

☐ Yes

☒ No

If yes, please provide the file number: \_\_\_\_\_

## 8 ADDITIONAL INFORMATION

8.1 Number of Dwelling Units Existing: 0

8.2 Number of Dwelling Units Proposed: 122

8.3 Additional Information (please include separate sheet if needed):

In conjunction with Site Plan Control application City File No. DA-22-167

## 11 COMPLETE APPLICATION REQUIREMENTS

### 11.1 All Applications

- ☒ Application Fee
- ☒ Site Sketch
- ☒ Complete Application form
- ☒ Signatures Sheet

### 11.4 Other Information Deemed Necessary

- ☒ Cover Letter/Planning Justification Report
  - ☐ Authorization from Council or Director of Planning and Chief Planner to submit application for Minor Variance
  - ☐ Minimum Distance Separation Formulae (data sheet available upon request)
  - ☐ Hydrogeological Assessment
  - ☐ Septic Assessment
  - ☐ Archeological Assessment
  - ☐ Noise Study
  - ☒ Parking Study
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